

D.O.T. 66

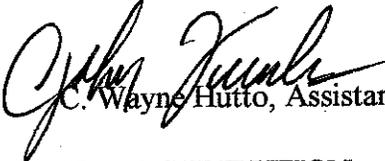
DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA

INTERDEPARTMENT CORRESPONDENCE

**FILE** HPP-9347(1) DeKalb County  
P. I. No. 753230

**OFFICE** Preconstruction

**DATE** June 4, 2002

**FROM**  C. Wayne Hutto, Assistant Director of Preconstruction

**TO** SEE DISTRIBUTION

**SUBJECT** PROJECT CONCEPT REPORT APPROVAL

Attached for your files is the approval for subject project.

CWH/cj

Attachment

DISTRIBUTION:

David Mulling  
Harvey Keepler  
Jerry Hobbs  
Herman Griffin  
Michael Henry  
Phillip Allen  
Marta Rosen  
Paul Liles  
Ben Buchan  
Joe Palladi  
Steve Henry  
BOARD MEMBER

**DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA**

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**INTERDEPARTMENT CORRESPONDENCE**

**FILE** HPP-9347(1) DeKalb County **OFFICE** Preconstruction  
P.I. No. 753230  
**DATE** May 16, 2002

**FROM**  C. Wayne Hutto, P.E., Assistant Director of Preconstruction

**TO** Frank L. Danchetz, P.E., Chief Engineer

**SUBJECT PROJECT CONCEPT REPORT**

This project is Phase I of the Lithonia Industrial Boulevard extension from Stone Mountain Lithonia Road to Rogers Lake Road for a total of 1.26 miles. (Phase II will extend the roadway to SR 124.) The purpose of the Lithonia Industrial Boulevard extension is to provide an industrial bypass around the west side of the city of Lithonia for commercial truck traffic traveling to and from Gwinnett and east DeKalb counties to I-20. The extension will also provide local access and circulation to and from properties in the western side of the Lithonia Industrial District. Currently, there is a retail mall (Stonecrest) at the southwest corner of the Turner Hill Road (SR 124) and I-20 interchange. Turner Hill Road is also an industrial/commercial truck traffic route from Gwinnett County and from the northeast portion of the Lithonia Industrial District in DeKalb County. By the year 2025, the mix of this industrial, commercial and residential traffic will begin to reduce traffic safety conditions at the SR 124/I-20 interchange. Lithonia Industrial Boulevard Extension is projected to carry 26,500 VPD and SR 124 is projected to carry 66,300 VPD by the year 2025. State Route 124 at its interchange with I-20 is projected to operate at Level of Service (LOS) "F" by the year 2025 if the extension is not constructed. With the proposed extension, the reduced traffic on SR 124 at its interchange with I-20 will improve the LOS.

The proposed construction will extend Lithonia Industrial Boulevard from Stone Mountain Lithonia Road on new location to Rogers Lake Road. The proposed typical section will consist of a four lane facility with a 20' median, curb and gutter, and 5' sidewalks. Left and right turn lanes will be provided at all intersections. Also included in this project is a proposed CSX Railroad bridge with the roadway going under the railroad tracks. This crossing will replace the at-grade crossing at Coffee Road, which will be closed upon completion of the project.

Environmental concerns include requiring an Environmental Assessment will be prepared; a public hearing will be held; time saving procedures are not appropriate.

Frank L. Danchetz

Page 2

HPP-9347(1) DeKalb

May 16, 2002

The estimated costs for this project are:

	<u>PROPOSED</u>	<u>APPROVED</u>	<u>PROG DATE</u>	<u>LET DATE</u>
Construction (includes E&C and inflation)	\$5,192,000	\$901,000	2003	FY-04)
Right-of-Way & Utilities*	Local	Local		

\*DeKalb County signed PMA for PE, right-of-way, and utilities on 11-16-01.

This project is in the STIP. I recommend this project concept be approved.

CWH:JDQ/cj

Attachment

CONCUR



Thomas L. Turner, P.E., Director of Preconstruction

APPROVE



Frank L. Danchetz, P.E., Chief Engineer

**DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA**

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**INTERDEPARTMENT CORRESPONDENCE**

**FILE** HPP-9347(1), DeKalb County  
PI 753230

Lithonia Industrial Blvd.  
Stone Mtn.-Lithonia Rd. to Rogers Lake Rd.  
& CSX R/R Bridge

**OFFICE** Urban Design

**DATE** April 24, 2002

*Joseph P. Palladi*  
**FROM** Joseph P. Palladi, P.E., State Urban Design Engineer

**TO** Wayne Hutto, P.E., Assistant Director of Preconstruction

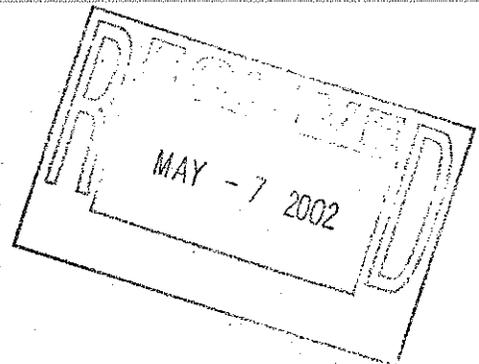
**SUBJECT** Project Concept Report

Attached is the original copy of the Concept Report for your further handling for approval in accordance with the Plan Development Process (PDP).

If you have any questions concerning this matter, please call Darryl VanMeter or Scott Kordys at (404) 656-5447.

<sup>DVM</sup>  
JPP:DVM:vcps  
Attachment

cc: David Mulling, Project Review Engineer, w/attachment  
Harvey Keepler, State Environmental/Location Engineer, w/attachment  
Phillip Allen, State Traffic Safety and Design Engineer, w/attachment  
Marta Rosen, State Transportation Planning Administrator, w/attachment  
Herman Griffin, Office of Financial Management Administrator, w/attachment  
Paul Liles, State Bridge & Structural Engineer, w/attachment  
Steve Henry, District 7 Engineer, w/attachment



**DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA**

*Urban Design*

Lithonia Industrial Boulevard – Phase I

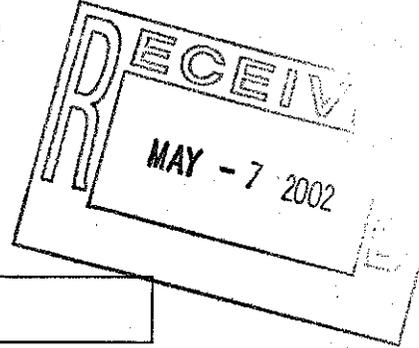
Project Number: HPP-9347(1)

County: DeKalb

P. I. Number: 753230

Federal Route Number: N/A

State Route Number: N/A



*See Page 2 for Location Sketch*

Recommendation for approval:

DATE 4/25/02

*Carmel C. Van Meter*  
Project Manager

DATE 5/4/02

*Joseph P. Ricci*  
State Urban Design Engineer

The concept as presented herein and submitted for approval is consistent with that which is included in the Regional Transportation Improvement Program (RTP) and/or the State Transportation Improvement Program (STIP).

DATE \_\_\_\_\_

\_\_\_\_\_  
State Transportation Planning Administrator

DATE \_\_\_\_\_

\_\_\_\_\_  
Office of Financial Management Administrator

DATE \_\_\_\_\_

\_\_\_\_\_  
State Environmental/Location Engineer

DATE \_\_\_\_\_

\_\_\_\_\_  
State Traffic Safety and Design Engineer

DATE \_\_\_\_\_

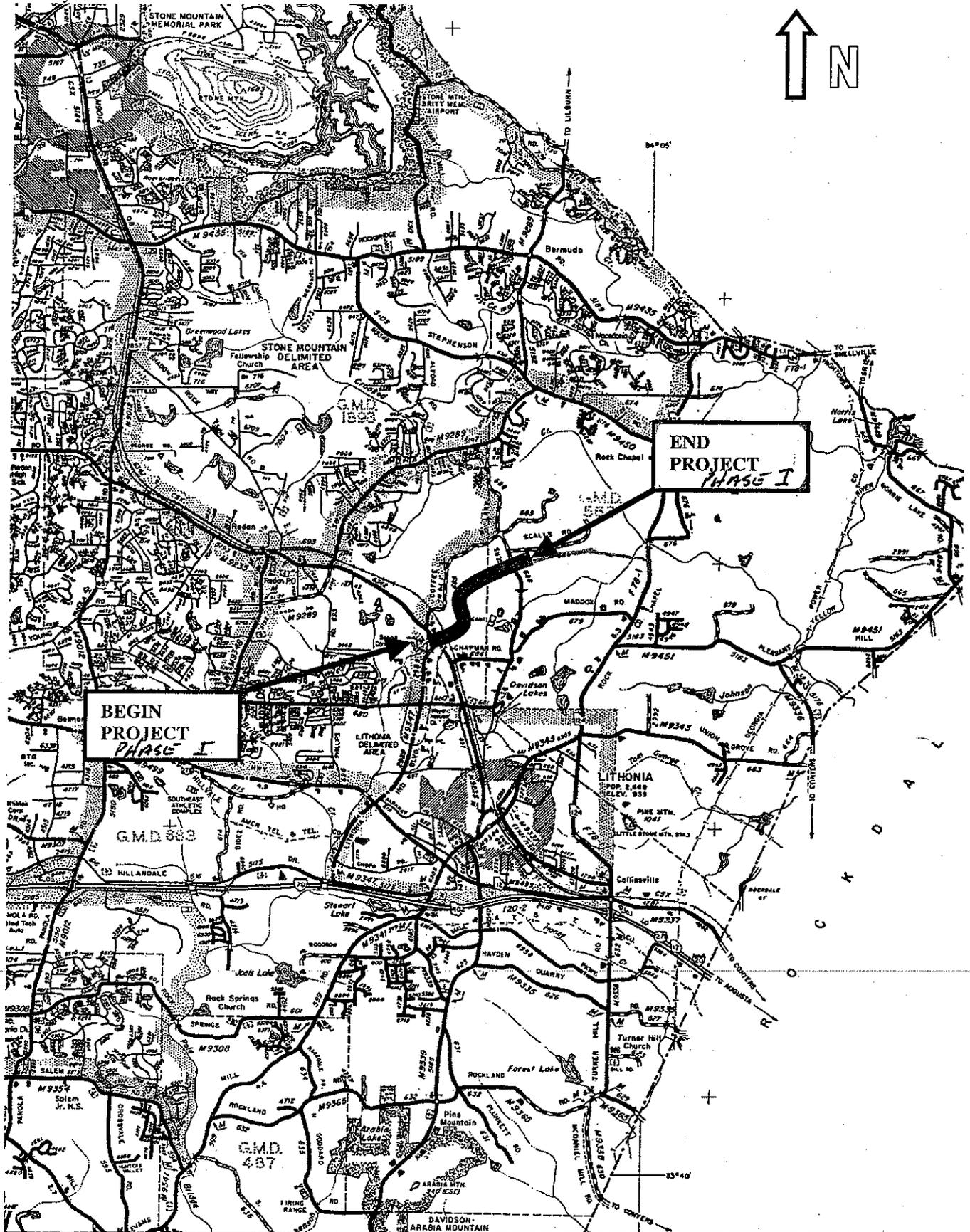
\_\_\_\_\_  
District Engineer

DATE \_\_\_\_\_

\_\_\_\_\_  
Project Review Engineer

DATE \_\_\_\_\_

\_\_\_\_\_  
State Bridge and Structural Engineer



**Need and Purpose:** *See attached Need & Purpose Statement*

**Description of the proposed project:**

*This project consists of an extension of the existing Lithonia Industrial Boulevard in eastern DeKalb County. The first phase of this project begins at the intersection of Lithonia Industrial Boulevard and South Stone Mountain Lithonia Road (mile log 0238) and terminates at a proposed intersection with Rogers Lake Road in Lithonia. Phase II will extend the roadway to State Route 124, Rock Chapel Road. The proposed Lithonia Industrial Boulevard will be a 4-lane facility with a 20-foot median, curb and gutter, and 5-foot sidewalks. South Stone Mountain Lithonia Road and Rogers Lake Road will be 2-lane roads with curb and gutter. Left and right turn lanes will be provided at all intersections. This concept report covers only Phase I of this project, which is 1.26 miles long.*

Is the project located in a Non-attainment area?  X  Yes   No

PARAMETER	PLAN MODEL	PROPOSED PROJECT
Project Limits	South Stone Mountain Lithonia Road to Rogers Lake Road	South Stone Mountain Lithonia Road to Rogers Lake Road
Number of Through Lanes	4	4
Open to Traffic Year	2005	2005
Length	1 mile	1.26 miles

**PDP Classification:** Major Project / New Location  
 FEDERAL OVERSIGHT: Full Oversight ( ), Exempt(**X**), State Funded( ), or Other ( )

**Functional Classification (Lithonia Industrial Boulevard):** Urban Collector

**U. S. Route Number(s):** N/A **State Route Number(s):** N/A

**Traffic (AADT):**

ROAD	CURRENT YEAR (2005)	DESIGN YEAR (2025)
Lithonia Industrial Boulevard	15,700	22,600
South Stone Mountain Lithonia Road	14,800	21,200
Rogers Lake Road	4,000	5,700

**Accident Data:**

<b>Lithonia Industrial Boulevard</b>			
<b>YEAR</b>	<b>ACCIDENTS</b>	<b>FATALITIES</b>	<b>INJURIES</b>
1995	5	0	3
1996	7	0	8
1997	4	0	6

<b>Stone Mountain Lithonia Road</b>			
<b>YEAR</b>	<b>ACCIDENTS</b>	<b>FATALITIES</b>	<b>INJURIES</b>
1995	4	0	2
1996	5	0	4
1997	3	0	0

*Note: Statewide average accident data not available*

**Existing design features:**

Lithonia Industrial Boulevard

- Typical Section - West of Stone-Mtn./Lithonia Rd.:
  - Four 12' lanes
  - 20' raised median
  - Urban shoulders with curb & gutter
- There is a one-half mile section of an abandoned private paved road west of Rogers Lake Road, which follows the proposed alignment of the Lithonia Ind. Blvd. Extension, with two 12 foot lanes and graded shoulders. The pavement is in poor condition.
- Posted speed: 45 mph
- Maximum degree of curvature: 8° 22'
- Maximum grade: 4.72%
- Width of right of way: 100'
- Major structures: None
- Major interchanges or intersections along the project: South Stone Mountain Lithonia Rd.
- Existing length of roadway segment and the beginning mile logs for each county segment: Beginning mile log 0228; Current end mile log 0238; Total length 0.1 mile

South Stone Mountain Lithonia Road

- Typical Section:
  - Two 12' lanes
  - Graded shoulder
- Posted speed: 35 mph
- Maximum degree of curvature: 5° 43'
- Maximum grade: 2.76%
- Width of right of way: 70'
- Major structures: None
- Major interchanges or intersections along the project: Lithonia Industrial Boulevard
- Existing length of roadway segment and the beginning mile logs for each county segment: Beginning mile log 0343; Ending mile log 0379; Total length 0.4 mile

Rogers Lake Road

- Typical Section:
  - Two 12' lanes
  - Graded shoulder
- Posted speed: 35 mph
- Maximum degree of curvature: 9° 38'
- Maximum grade: 4.60%
- Width of right of way: 40'
- Major structures: None
- Major interchanges or intersections along the project: None
- Existing length of roadway segment and the beginning mile logs for each county segment: Total length 0.4 mile

**Proposed Design Features:**

Lithonia Industrial Boulevard

- Proposed typical section:
  - Four 12' lanes
  - 20' raised median
  - 12' urban shoulders with 30" curb & gutter and 5' sidewalks
  - Left and right turn lanes provided at both intersections
- Functional Classification: Urban Collector
- Proposed Design Speed: 45 mph
- Proposed Maximum grade: 5.0% Maximum grade allowable: 9.0%
- Proposed Maximum grade driveway: 11.0% (trucks)
- Proposed Maximum degree of curve: 8° 00' Maximum degree allowable: 8° 00'
- Right of way width: 100' (Minimum)

South Stone Mountain Lithonia Road

- Proposed typical section:
  - Two 12' lanes
  - 10' urban shoulders with 30" curb & gutter
  - Left and right turn lanes provided at intersection with Lithonia Industrial Blvd.
- Functional Classification: Minor Arterial
- Proposed Design Speed: 35 mph
- Proposed Maximum grade: 5.0% Maximum grade allowable: 8.0%
- Proposed Maximum grade driveway: 11.0% (trucks)
- Proposed Maximum degree of curve: 6° 00' Maximum degree allowable: 11° 30'
- Right of way width: 80' (Minimum)

Rogers Lake Road

- Proposed typical section:
  - Two 12' lanes
  - 10' urban shoulders with 30" curb & gutter
  - Left and right turn lanes provided at intersection with Lithonia Industrial Blvd.
- Functional Classification: Local Road
- Proposed Design Speed Mainline: 35 mph

Rogers Lake Road (continued)

- Proposed Maximum grade: 5.0% Maximum grade allowable: 10.0%
- Proposed Maximum grade driveway: 11.0% (trucks)
- Proposed Maximum degree of curve: 6° 00' Maximum degree allowable: 11° 30'
- Right of way width: 80' (Minimum)

All Roadways

- Right of way
  - Easements: Temporary (X), Permanent (X), Utility ( ), Other ( ).
  - Type of access control: Full ( ), Partial ( ), By Permit (X), Other ( ).
  - Number of parcels: 12 Number of displacements:
    - Business: 2
    - Residences: 0
    - Mobile homes: 0
    - Other: 0
- Structures:
  - Bridges: *170'x50' three-track railroad bridge over Lithonia Industrial Boulevard*
  - Retaining walls: *GASE walls will be used to minimize the fill slope impacts to business owners.*
- Major intersections and interchanges:
  - Lithonia Industrial Boulevard at South Stone Mountain Lithonia Road
  - Rogers Lake Road
- Traffic control during construction:
 

*Temporary paving is required to provide a detour on the west side of both South Stone Mountain Lithonia Road and Rogers Lake Road*
- Design Exceptions to controlling criteria anticipated:

	<u>UNDETERMINED</u>	<u>YES</u>	<u>NO</u>
HORIZONTAL ALIGNMENT:	( )	( )	(X)
ROADWAY WIDTH:	( )	( )	(X)
SHOULDER WIDTH:	( )	( )	(X)
VERTICAL GRADES:	( )	( )	(X)
CROSS SLOPES:	( )	( )	(X)
STOPPING SIGHT DISTANCE:	( )	( )	(X)
SUPERELEVATION RATES:	( )	( )	(X)
HORIZONTAL CLEARANCE:	( )	( )	(X)
SPEED DESIGN:	( )	( )	(X)
VERTICAL CLEARANCE:	( )	( )	(X)
BRIDGE WIDTH:	( )	( )	(X)
BRIDGE STRUCTURAL CAPACITY:	( )	( )	(X)

- Design Variances:
 

*The existing intersection at South Stone Mountain Lithonia Road must be lowered to provide adequate sight distance across the east and west approaches of Lithonia Industrial Boulevard. It appears that this can be accomplished without interfering with the railroad spur south of this intersection. However, if it is determined in the formal design process that this is not possible, a design variance will be required.*
- Environmental concerns: None anticipated SEVERAL LANDFILLS IN THE PROJECT AREA

- Level of environmental analysis:
  - Are Time Savings Procedures appropriate? Yes ( ), No (X),
  - Categorical exclusion ( ),
  - Environmental Assessment/Finding of No Significant Impact (FONSI) (X), or
  - Environmental Impact Statement (EIS) ( ).
- Utility involvements:
  - *Georgia Power Company*
  - *CSX Railroad*
  - *Atlanta Gas Light*
  - *Bellsouth Telecommunications*
  - *DeKalb County Department of Water and Sewer*

**Project responsibilities:**

- Design: *DeKalb County – Jordan, Jones & Goulding, Inc*
- Right of Way Acquisition: *DeKalb County*
- Relocation of Utilities: *DeKalb County*
- Letting to contract: *Georgia DOT*
- Supervision of construction: *Georgia DOT*
- Providing material pits: *N/A*
- Providing detours: *N/A*
- On-site construction staging: *Construction Contractor*

**Coordination**

- Initial Concept Meeting date and brief summary. Attach minutes.
- Concept meeting date and brief summary. Attach minutes.
- P. A. R. meetings, dates and results: *N/A*
- FEMA, USCG, and/or TVA
- Public involvement:
  - *A public information meeting was held on September 21, 1999*
  - *A public hearing will be required*
- Local government comments: *DeKalb County is responsible for P&E, reimbursable utility relocation and R/W acquisition*
- Other projects in the area: *Lithonia Industrial Boulevard - Phase II*
- Other coordination to date:
  - *A meeting was held on July 14, 2000, at the Office of Urban Design to discuss the concept with GA DOT personnel and to explain DeKalb County's plans to contract the design of the project*
  - *A coordination meeting was held on April 5, 2001, at the Office of Urban Design to discuss the concept status and project execution with GA DOT personnel*
  - *A meeting was held on May 1, 2001, at the Office of Environment Location to introduce the concept to GA DOT and FHWA personnel*

**Scheduling – Responsible Parties' Estimate**

- Time to complete the environmental process: ~~6~~ <sup>1 year - JDEP</sup> Months
- Time to complete preliminary construction plans: 6 Months
- Time to complete right of way plans: 2 Months
- Time to complete the Section 404 Permit: N/A Months
- Time to complete final construction plans: 8 Months
- Time to complete to purchase right of way: 10 Months
- Time to complete CSX railroad agreement: 12 Months

**Other alternates considered:**

- **Bridge Lithonia Industrial Boulevard over CSX railroad:** *This alternate would require raising the existing LIB intersection at South Stone Mountain Lithonia Road (SSMLR) approximately 28 feet in order to provide adequate clearance over the railroad, which is located 485 feet from the intersection. In order to raise this intersection in accordance with current AASHTO guidelines, the existing profile of SSMLR must be raised gradually beginning 813 feet south of the intersection and ending 800 feet north of the intersection. As a result, the construction of fill slopes would require the acquisition of 4 businesses in close proximity to the intersection. Raising the profile of SSMLR would require raising the existing lead industrial track that crosses SSMLR 450 feet south of the intersection by a distance of 9 1/2 feet. The distance along the lead industrial track between SSMLR and the main track is too small to allow for the design of a satisfactory lead industrial track profile if the track is raised the required distance at the roadway crossing. If the main track were raised in an effort to mitigate this problem, the distance that the SSMLR/LIB intersection must be raised would increase proportionately. As a result, the lead industrial track would also be raised proportionately. In conclusion, this alternate cannot be constructed without eliminating the lead industrial track. This is not acceptable to CSX. This fatal flaw eliminates this alternative from consideration.*
- **No Build:** *This alternate does not meet the operational safety and capacity requirements of this area.*

**Comments:** *This report covers Phase I of a two-phase project.*

**Attachments:**

1. Need and Purpose Statement (prepared by Moreland Altobelli and Associates, Inc.)
2. Cost Estimates:
  - a. Construction including E&C
  - b. Right of Way
  - c. Utilities
3. Typical sections
4. Capacity analysis
5. Minutes of Concept Team Meeting
6. Public Informational Meeting hand-out
7. LGPA
8. Conforming plan's network schematics showing thru lanes

## NEED & PURPOSE STATEMENT

The Atlanta Regional Commission (ARC) adopted the 2025 Transportation Plan for the 13-county Atlanta Metropolitan area in April 2000. The Plan addresses travel needs through the year 2025. The Regional Transportation Plan (RTP) is the direct result of a comprehensive, cooperative and continuous planning process conducted by ARC, local governments and the Georgia Department of Transportation in cooperation with the Federal Highway and Federal Transit Administrations. The RTP recommends extending Lithonia Industrial Boulevard from South Stone Mountain-Lithonia Road to Rogers Lake Road in DeKalb County.

The purpose of the Lithonia Industrial Boulevard Extension is twofold: First, the extension provides an industrial bypass around the west side of the City of Lithonia for commercial truck traffic traveling to and from Gwinnett and east Dekalb counties and I-20. Second, the extension provides local access and circulation to and from properties in the western side of the Lithonia Industrial District.

Currently, there is a retail mall being constructed at the southwest corner of the Turner Hill Road (S.R. 124) and I-20 Interchange. The Stonecrest mall will be open for business November 2001. Turner Hill Road (S.R. 124) is also an industrial/commercial truck traffic route from Gwinnett County and from the northeast portion of the Lithonia Industrial District in DeKalb County. Future traffic volumes associated with trips to and from the mall combined with industrial truck traffic will create capacity conditions at the interchange at I-20 and Turner Hill Road by the design year 2025. Twenty-four hour truck traffic in this area is projected to be ten percent of daily traffic with 6% being heavy industrial trucks. By the year 2025, the mix of this industrial, commercial and residential traffic will begin to reduce traffic safety conditions at the S.R. 124/I-20 Interchange. The extension of Lithonia Industrial Boulevard to S.R. 124 provides a bypass for the industrial traffic to access I-20 via the future I-20/Lithonia Industrial Boulevard Interchange and separate the industrial traffic from the vehicular passenger trips to the mall, I-20 or the City of Lithonia.

Lithonia Industrial Boulevard/Extension is projected to carry approximately 26,500 vehicles per day (vpd) and S.R. 124 is projected to carry 66,300 vpd by the year 2025. S.R. 124 at its interchange with I-20 is projected to operate at level of service (LOS) F by the year 2025 if no action is taken to construct a bypass route for industrial traffic. This bypass route established by the extension of Lithonia Industrial Boulevard will provide improved access to I-20 to and from the industrial areas along Lithonia Industrial Boulevard. With the proposed extension, the intersection of Lithonia Industrial Boulevard/Extension and S.R. 124 is projected to operate at LOS D by the year 2025 and likewise the reduced traffic on S.R. 124 at its interchange with I-20 will improve the levels of service of the interchange.

A secondary purpose of the Lithonia Industrial Boulevard Extension would be to provide the much needed access and circulation for industrial traffic on the west side of the Lithonia Industrial District. Presently, the Lithonia Industrial District is divided in the middle by S.R. 124. There is no roadway continuity to access the southwest portion of the Lithonia Industrial District from S.R. 124. Industrial traffic must use residential collector streets or travel through the City of Lithonia.

The Lithonia Industrial Boulevard Extension has a logical beginning at the end of the improved Lithonia Industrial Boulevard at South Stone Mountain Road and a logical ending at Rock Chapel Road (S.R. 124) which is a four-lane divided highway. The logical termini at S.R. 124, an urban principal arterial, was chosen due to the fact that it provides the completion of an industrial/commercial route to and from Gwinnett and east Dekalb counties and I-20. The proposed project consist of constructing a four-lane divided highway from South Stone Mountain Road along the existing alignment of Lithonia Industrial Boulevard (2.2 miles in length) including a railroad overpass and extending to S.R. 124 along new location (1.13 miles in length).

The proposed project will coordinate with other planned projects concurrent with the Atlanta Regional Commissions's Transportation Improvement Plan (TIP) and Regional Transportation Plan (RTP) and the DeKalb County Transportation Plan to upgrade the existing transportation infrastructure of DeKalb County.

**PRELIMINARY COST ESTIMATE**

PROJECT NUMBER: HPP-9347(1)

COUNTY: DeKalb

DATE: April 2002

ESTIMATED LETTING DATE: June 2005

PREPARED BY: PGR

PROJECT LENGTH: 1.3 Miles

( ) PROGRAMMING PROCESS (X) CONCEPT DEVELOPMENT ( ) DURING PROJECT DEV.

PROJECT COST				
<b>A. RIGHT-OF-WAY:</b>				
1. PROPERTY (LAND & EASEMENT)				\$ 886,171
2. DISPLACEMENTS; RES: 0, BUS: 0, M.H.: 0				\$ -
3. OTHER COST (ADM./COST, INFLATION)				\$ 2,190,614
			<b>SUBTOTAL: A</b>	\$ 3,076,785
<b>B. REIMBURSABLE UTILITIES:</b>				
1. RAILROAD				\$ 200,000
2. TRANSMISSION LINES				\$ 200,000
3. SERVICES				\$ 200,000
			<b>SUBTOTAL: B</b>	\$ 600,000
<b>C. CONSTRUCTION:</b>				
1. MAJOR STRUCTURES				
a. BRIDGES				
Grade Separations ( Railroad )				\$ 1,785,000
Stream Crossings ( 0 )				\$ -
			<b>SUBTOTAL: C-1.a</b>	\$ 1,785,000
b. OTHER				
Walls	GASE walls	20x150=3000 ft.2		\$ 135,000
Box Culverts	size	length (ft.)	volume (yd3)	\$
	5x5	485	554.8	\$ 291,270
	6x5	189	228	\$ 119,700
	5x4	151	186.1	\$ 97,703
	6x5	91	119.8	\$ 62,895
			<b>TOTAL</b>	\$ 571,568
Bridge Culverts ( 0 )				\$ -
			<b>SUBTOTAL: C-1.b</b>	\$ 135,000
			<b>SUBTOTAL: C-1</b>	\$ 1,920,000
2. GRADING AND DRAINAGE:				
a. EARTHWORK				
In Place Embankment		24,000	CY @ \$3.99	\$ 95,760
Rock Excavation		5,027	CY @ \$10.00	\$ 50,270

**PRELIMINARY COST ESTIMATE**

PROJECT NUMBER: HPP-9347(1)

COUNTY: DeKalb

DATE: April 2002

ESTIMATED LETTING DATE: June 2005

PREPARED BY: PGR

PROJECT LENGTH: 1.3 Miles

( ) PROGRAMMING PROCESS (X) CONCEPT DEVELOPMENT ( ) DURING PROJECT DEV.

PROJECT COST			
<b>b. DRAINAGE</b>			
1) Cross Drain Pipe			\$ 216,096
2) Curb and Gutter			\$ 299,599
3) Longitudinal System (incl. catch basins)			\$ 398,933
SUBTOTAL: C-2.b			\$ 914,629
SUBTOTAL: C-2			\$ 1,060,659
<b>3. BASE AND PAVING:</b>			
a. AGGREGATE BASE	18,981	Tons @ \$17.03	\$ 323,247
<b>b. ASPHALT PAVING (Mainline &amp; Cross-Roads):</b>			
Drainage - Type D	-	Tons @ \$50.8	\$ -
Surface - SMA	-	Tons @ \$54.93	\$ -
Surface - Superpave	3,408	Tons @ \$42.56	\$ 145,061
Binder - SMA	-	Tons @ \$56.9	\$ -
Binder - Superpave	2,489	Tons @ \$38.43	\$ 95,640
Base - Superpave	4,976	Tons @ \$34.63	\$ 172,313
Pavement Reinf. Fabric Strips	13,304	Lane Ft @ \$2.84	\$ 37,783
			\$ 450,797
c. CONCRETE PAVING	-	SY @ \$33.57	\$ -
d. OTHER (Leveling, Tack Coat, Milling)			\$ 143,041
SUBTOTAL: C-3			\$ 917,085
<b>4. LUMP ITEMS</b>			
a. GRASSING			\$ 38,874
b. CLEARING AND GRUBBING			\$ 40,100
c. LANDSCAPING			\$ -
d. EROSION CONTROL			\$ 134,208
e. TRAFFIC CONTROL			\$ 54,851
SUBTOTAL: C-4			\$ 268,032
<b>5. MISCELLANEOUS:</b>			
a. LIGHTING			\$ -
b. SIGNING - MARKING - SIGNALIZATION			\$ 128,946
<b>c. GUARDRAIL</b>			
Single-Faced			\$ 2,436
Double-Faced			\$ -
Anchors			\$ 7,245
SUBTOTAL: C-5.c			\$ 9,681

**PRELIMINARY COST ESTIMATE**

PROJECT NUMBER: HPP-9347(1)

COUNTY: DeKalb

DATE: April 2002

ESTIMATED LETTING DATE: June 2005

PREPARED BY: PGR

PROJECT LENGTH: 1.3 Miles

( ) PROGRAMMING PROCESS (X) CONCEPT DEVELOPMENT ( ) DURING PROJECT DEV.

PROJECT COST		
d. SIDEWALK	SF @ \$2.58	\$ 171,619
e. MEDIAN / SIDE BARRIER	- LF @ \$32.03	\$ -
f. MOVABLE BARRIER SECTION		\$ -
g. ACCESS FENCE		\$ -
h. BRIDGE JACKING		\$ -
i. APPROACH SLABS		\$ 15,000
j. REMOVAL		
Concrete Paving		\$ -
Bridges		\$ -
	SUBTOTAL: C-5.j	\$ -
k. ATMS Conduit	- LF @ \$37.78	\$ -
l. OTHER		\$ 24,936
	SUBTOTAL: C-5	\$ 350,181
6. RAILROAD DETOUR		
	SUBTOTAL: C-6	\$ 400,000
<b>SUMMARY</b>		
A. RIGHT-OF-WAY		\$ 3,076,785
B. REIMBURSABLE UTILITIES		\$ 600,000
C. CONSTRUCTION		
1. MAJOR STRUCTURES		\$ 1,920,000
2. GRADING AND DRAINAGE		\$ 1,060,659
3. BASE AND PAVING		\$ 917,085
4. LUMP ITEMS		\$ 268,032
5. MISCELLANEOUS		\$ 350,181
6. RAILROAD DETOUR		\$ 400,000
SUBTOTAL CONSTRUCTION COST		\$ 4,915,957
E. & C. (10%)		\$ 491,596
INFLATION (5% PER YEAR)		\$ 503,886
NUMBER OF YEARS	2	
TOTAL CONSTRUCTION COST		\$ 5,911,438
<b>GRAND TOTAL PROJECT COST</b>		<b>\$ 9,588,223</b>



## MEMORANDUM

**DATE:** November 28, 2001  
**TO:** Distribution  
**FROM:** Greg Ramsey  
**SUBJECT:** Lithonia Industrial Boulevard, Phase I  
Concept Meeting  
Project HPP-9347(1), DeKalb County, P.I. No. 753230

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A concept meeting for the subject project was held at the Georgia Department of Transportation, Office of Urban Design on November 28, 2001. The meeting began at approximately 10:00 a.m. The following individuals were in attendance:

Michael Anderson DeKalb R&D  
Windy Bickers, GDOT  
Jerry Brooks, MAAI  
Michelle Brouillette, GDOT OEL  
Robert Crawford, GDOT Dist. 7 Precon.  
Dan Hall, DeKalb Public Works  
Arlene Jackson, BellSouth  
Scott Kordys, GDOT Urban  
John Little, DeKalb Water & Sewer  
Mike Malcom, GDOT Dist. 7 Precon.  
Tom Maloch, CSX Transp.  
Chris Marsengill, JIG  
James Mashburn, CSX Transp.  
David Mulling, GDOT

Joe Palladi, GDOT Urban  
Thomas Parker, GDOT  
Vinesha Pegram, GDOT Urban  
David Pelton, DeKalb Transp.  
Karla Poshedly, MAAI  
Greg Ramsey, JIG  
Richard Randall, BellSouth  
Ed Schureck, PBS&J  
Eleanor Smith, GDOT Dist. 7  
Darryl VanMeter, GDOT Urban  
Wade Woodard, GDOT  
Taylor Wright, PBS&J  
Susan Wynn, JIG  
Scott Zehngraff, GDOT TS&D

The meeting began with attendee introductions, and an explanation of the two phases by Scott Kordys. Phase I of the project is a state aid project, and the design is being performed by JIG. Phase II of the project will be designed by MAAI, and is not currently a state aid project. Darryl explained that Phase II, while not on GDOT's CWP, should be considered with Phase I, which is in the CWP. Following introductions, Chris Marsengill gave a project overview of Phase I.

Scott Kordys asked for comments from the group:

### Miscellaneous Discussion

Darryl commented that the 24,000 traffic volume is in the range requiring a 5 lane flush median section, and does not warrant a raised median per the median design guidelines directive from

the Chief Engineer, Joe Palladi asked that DeKalb County submit a letter to him requesting a 4 lane raised median section.

- *DeKalb County committed to the 20' raised median section in a letter to GDOT on December 3, 2001.*

Joe Palladi requests that the shoulder width be reviewed. A width in the range of 14'-16' may be required for future utilities.

- *According to DeKalb County and the utility companies, the 12' shoulder will suffice.*

Joe Palladi asked about the traffic accident trends, and suggested that the project should be designed in order to correct any major accident trend by increasing/adding turn lanes, improving sight distance, etc.

- *JJG has reviewed the accident data, and the design for both intersections will be adequate enough to solve the turn lane storage and sight distance problems.*

Joe Palladi asked about any future projects on South Stone Mountain Lithonia Road

- *No projects are currently planned*

Joe Palladi asked about any future track or access roads for the CSX railroad

- *As requested by CSX, the bridge will be designed to accommodate a future second mainline track.*

Joe Palladi stated that the wheelchair ramp standard is being revised to include a 5' pad behind the ramp. The revised standard will be available by the first of the year.

- *JJG has been using 5' pads on recent projects.*

In response to Joe Palladi's question, Tom Maloch of CSX requested that standard railroad shoulders be provided on the bridge over Lithonia Industrial Boulevard.

- *JJG has received design criteria from CSX, which will be followed for both the typical section and track layout design.*

Michelle Brouillette has spoken with Todd Hill of MAAI concerning potential archaeological impacts of this project. The CSX railroad is a historical resource, and Todd indicated to Michelle that one or more structures in the area may have potential historic value. Michelle will investigate this further with Todd as the project progresses.

Michelle Brouillette indicated that there are 6 landfills in the area of both Phase I and Phase II of this project.

- *Phase I alignment follows an abandoned roadbed which does not impact any landfill.*

One EA will be completed encompassing both phases of this project. The EA is being prepared by MAAI. Following discussions with Todd Hill, Michelle Brouillette indicated that a nationwide permit is anticipated for the project.

DeKalb County and JJG will coordinate median opening design with the property owners. GDOT wants maximum median opening spacing, and all driveways should be coordinated

before the design is completed. The county and JIG will pay special attention to possible truck turnouts which would require extra right of way.

Darryl VanMeter added that the traffic numbers in the report did not agree with the traffic numbers shown on the diagram.

- *JIG will correct this error.*

Georgia Power has a major transmission line crossing LIB. No transmission poles will be affected by the new construction. Also, right of way will be required for any special utility needs, and the owners will be apprised of these needs as the design progresses.

### Engineering Services

David Mulling asked about the detour required while lowering the intersection of Lithonia Industrial Boulevard (LIB) and South Stone Mountain Lithonia Road (SSMLR). Chris Marsengill replied that temporary construction easement will be required as traffic is detoured along the southwest side of SSMLR.

David Mulling also asked about the rejected alternative of bridging over the railroad. He requested the cost estimate for this alternative, including unit costs and quantities for the roadway and bridge.

- *JIG will include discussion on the other justifications for choosing the preferred alternative. The existing LIB approaches SSMLR at a steep grade. If the intersection was raised, the profile tie-in would shift the project limits SE along LIB a significant distance. More importantly, impacts to the CSX Lead Industrial Track crossing SSMLR are avoided using the preferred alternative. Impacts to this crossing can not be avoided if the intersection is raised to the necessary elevation to insure a safe intersection. Also, the selected alternative is less intrusive to the surrounding industrial area, and the construction limits would most likely require displacement of property owners at this intersection. A detailed summary of these issues will be included in the report.*

David Mulling suggested that potential rocky soil be considered in the report and the analysis of the alternatives.

- *JIG will consider this in comparing the cost and impacts.*

Darryl VanMeter requested that the typical section contain a more accurate pavement design, including any required superpave.

- *JIG will revise the sections to show 12.5mm, 19mm and 25mm Superpave.*

Joe Palladi requested that the concept cost estimate include sizes of culverts and walls and that major quantities be shown.

- *JIG will revise the concept cost estimate.*

The project is scheduled for a 2004 letting.

Taylor Wright will check into the status of the LGPA.

Traffic Operations-No comment

Environmental-

Michelle Brouillette added that OEL is treating both Phase I and Phase II as one project.

Planning-No one present from this office

District 7 Office-

A representative for Sharon Witherspoon asked about water and sewer being a contract item. The county replied that this had been done in the past, and that the county will assure that this is accounted for in the contract. Joe Palladi stated that the county will be required to pre-pay for any DeKalb County utility items included in the construction plans.

DeKalb County is responsible for both R/W and utilities.

Right of Way-

No one present from this office

CSX Transportation-

The bridge will be designed to accommodate three tracks.

The railroad detour will be designed for 50 mph.

Tom Maloch requested a project number to which his department can charge time required on this project. Joe Palladi will check with Mike Malloy of the GDOT Railroad Department to see what can be done for CSX.

Track centers should be 15' on center, and 10' from the headwalls.

- *JJG has the design criteria from CSX and will follow these guidelines in all facets of the design.*

Tom Maloch added that CSX would like to close both the Coffee Road and Chapman Road at-grade crossings. The Coffee Road crossing will be closed following the opening of LIB. The county will investigate the closing of the Chapman Road crossing, but they are doubtful of this being possible due to the heavy truck and industrial traffic using Chapman Road to access the properties and landfills in the area.

A concept plot was presented to CSX for their internal use.

BellSouth-

Existing utilities at the intersection of LIB and SSMLR will require relocation, but no major conflict is expected.

Memorandum  
November 28, 2001  
Page 5

DeKalb County Water & Sewer-

DeKalb has a 20" water main along the southeast side of SSMLR. West side of SSMLR and Rogers Lake Road have an 8" main, and there is a 12" main along LIB. Joe Palladi suggested that the water mains should be coordinated with property owners during right of way negotiations.

Joe Palladi suggested that the county maintain open discussions with the property owners concerning utility work along the roadways.

DeKalb has no sanitary sewer in the Phase I project limits.

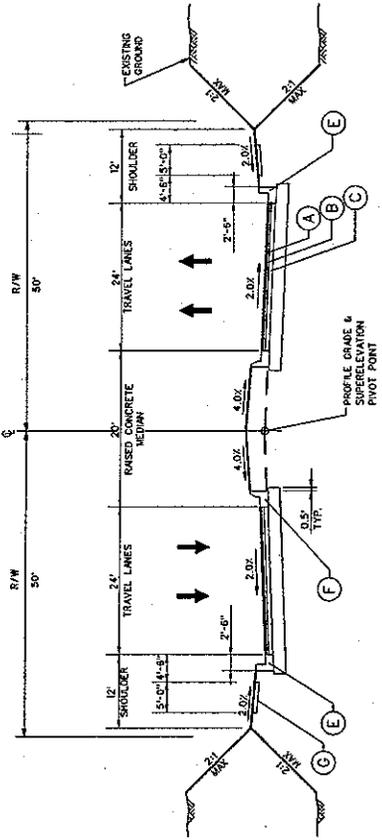
The meeting adjourned at approximately 11:15 a.m.

STATE	PROJECT NUMBER	SHEET NO.	TOTAL
GA.	HPP 9347(1)		

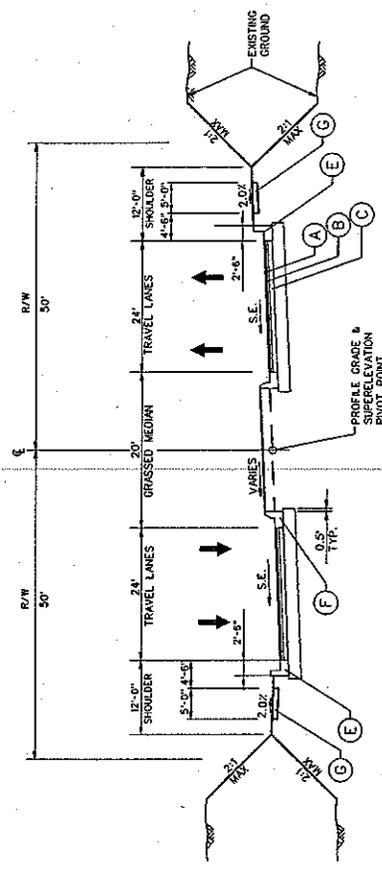
**PAVEMENT DESIGN**

- (A) ASPHALTIC CONCRETE 12.5mm SUPERPAVE
- (B) ASPHALTIC CONCRETE 19mm SUPERPAVE
- (C) ASPHALTIC CONCRETE 25mm SUPERPAVE
- (D) 12" GRADED AGGREGATE BASE
- (E) GA DOT CONSTRUCTION DETAIL, CONCRETE CURB & GUTTER, CONCRETE CURBS, CONCRETE MEDIANS
- (F) GA DOT CONSTRUCTION DETAIL, CONCRETE CURB & GUTTER, CONCRETE CURBS, CONCRETE MEDIANS
- (G) GA DOT CONSTRUCTION DETAIL, CONCRETE SIDEWALK DETAILS, CURB CUT (WHEELCHAIR) RAMPS

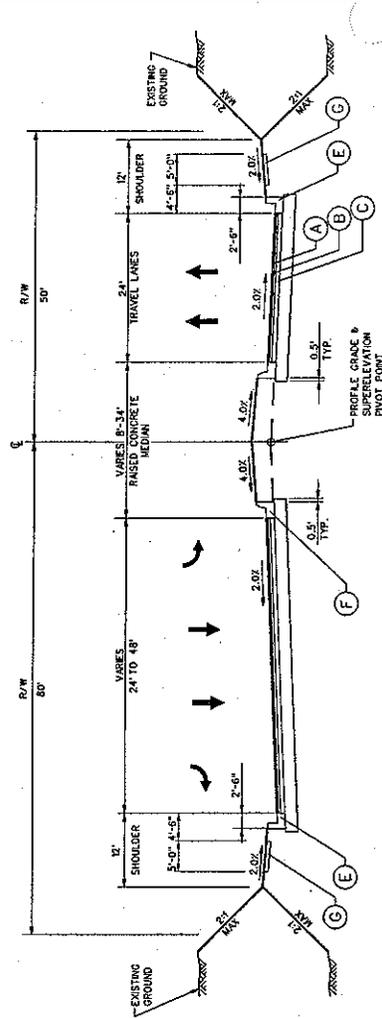
TYPICAL SECTIONS SHOWING SUPERELEVATION AND TURN LANES ALSO APPLY TO OPPOSITE HAND SECTIONS - SEE CONSTRUCTION PLAN SHEETS FOR LOCATIONS AND DIRECTION OF SUPERELEVATION AND LOCATIONS AND DIMENSIONS OF TURN LANES.



**TYPICAL SECTION 1**  
NORMAL CROWN  
LITHONIA INDUSTRIAL BOULEVARD



**TYPICAL SECTION 2**  
SUPERELEVATION  
LITHONIA INDUSTRIAL BOULEVARD



**TYPICAL SECTION 3**  
NORMAL CROWN  
LITHONIA INDUSTRIAL BOULEVARD  
MIRROR FOR RIGHT SIDE  
STA 116+50 TO STA 125+00

**Jordan Jones & Boulding**

ROADS AND DRAINAGE  
DEKALB COUNTY, GEORGIA

LITHONIA INDUSTRIAL BOULEVARD

TYPICAL SECTIONS

NO. DATE DESCRIPTION OF REVISION

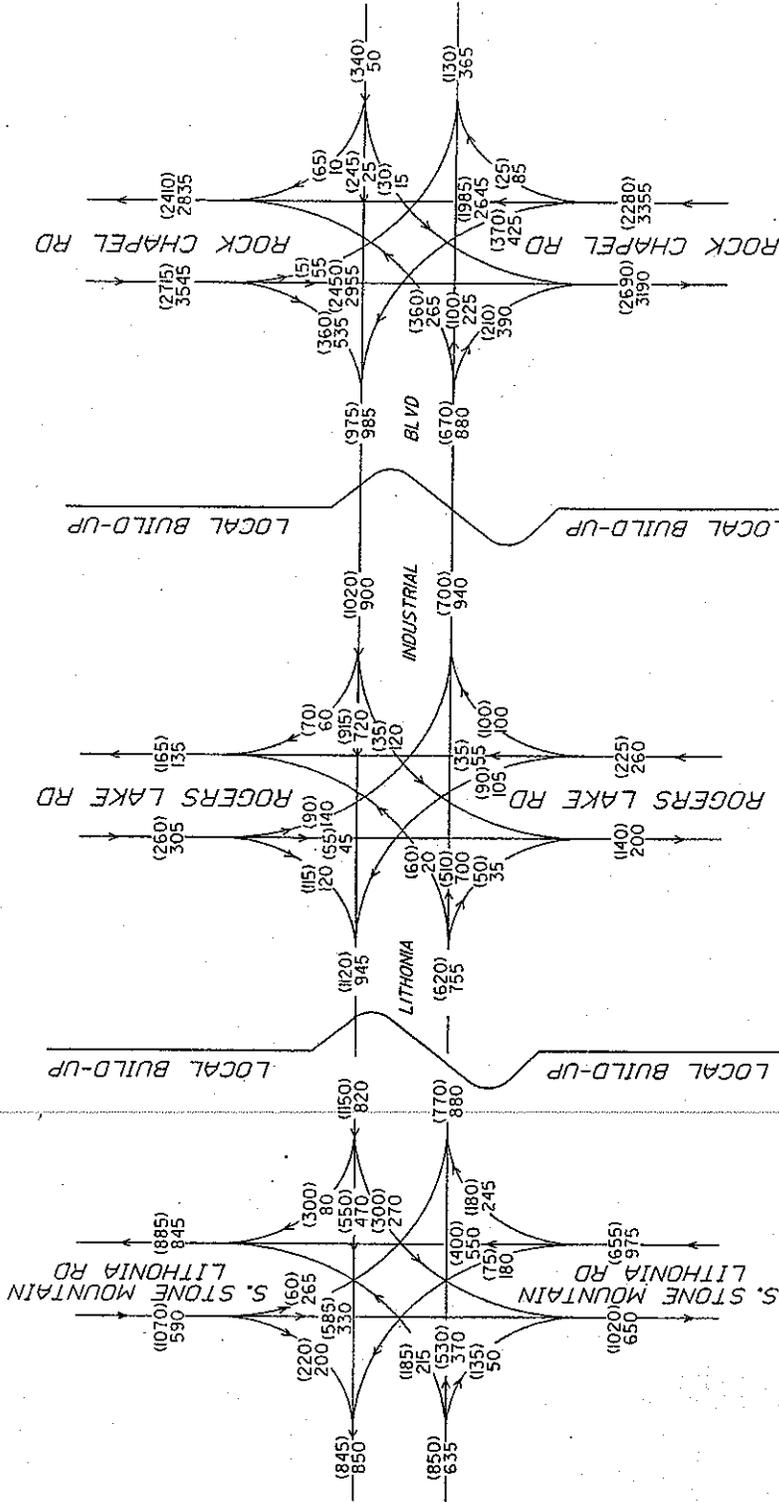
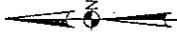
DESIGNED: PBR CHECKED: JSS FOR NO. 3033-08

DATE: SCALE AS SHOWN

4.01 SHEET

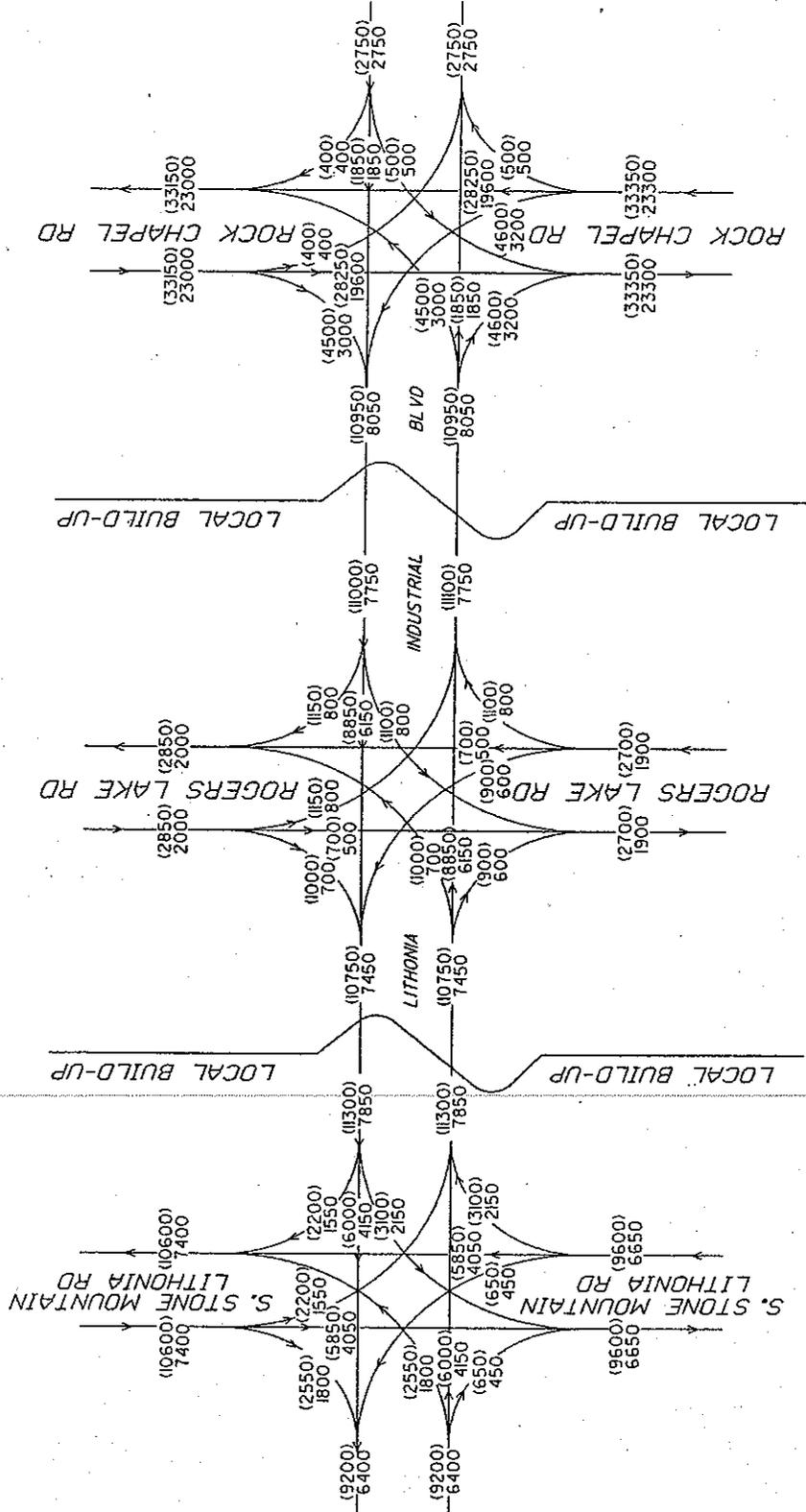
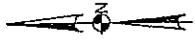
NOT RELEASED FOR CONSTRUCTION





YEAR 2025  
(00) PM PEAK HOUR  
00 AM PEAK HOUR

LITHONIA INDUSTRIAL BLVD  
YEAR 2025 PEAK HOUR DESIGN TRAFFIC  
TRAFFIC FLOW DIAGRAM



24 HR T = 10%  
 SU = 4%  
 COMB = 6%

(00) YEAR 2025 ADT  
 00 YEAR 2005 ADT

LITHONIA INDUSTRIAL BLVD  
 AVERAGE DAILY TRAFFIC  
 TRAFFIC FLOW DIAGRAM

HCS2000: Signalized Intersections Release 4.1

Analyst: MAAI  
 Agency: GDOT  
 Date: 02/11/2002  
 Period: AM Peak Hour  
 Project ID: Lithonia Ind, Blvd.  
 E/W St: Lithonia Ind. Blvd.

Inter.: Lithonia Ind @ S. Stone Mtn-Li  
 Area Type: All other areas  
 Jurisd: DeKalb County  
 Year : Year 2025

N/S St: S. Stone Mtn.- Lithonia Rd.

SIGNALIZED INTERSECTION SUMMARY

	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
No. Lanes	1	2	1	1	2	1	1	1	1	1	1	1
LGConfig	L	T	R	L	T	R	L	T	R	L	T	R
Volume	215	370	50	270	470	80	180	550	245	265	330	200
Lane Width	12.0	12.0	12.0	12.0	12.0	12.0	12.0	12.0	12.0	12.0	12.0	12.0
RTOR Vol			0			0			0			0

Duration 0.25 Area Type: All other areas

Signal Operations

Phase Combination	1	2	3	4	5	6	7	8
EB Left		P			NB Left	P		
Thru			P		Thru		P	
Right				P	Right			P
Peds					Peds			
WB Left		P			SB Left	P		
Thru			P		Thru		P	
Right				P	Right			P
Peds					Peds			
NB Right		P			EB Right	P		
SB Right		P			WB Right	P		
Green	15.0	40.0			20.0	45.0		
Yellow	4.0	4.0			4.0	4.0		
All Red	1.0	1.0			1.0	1.0		

Cycle Length: 140.0 secs

Intersection Performance Summary

Appr/ Lane Grp	Lane Group Capacity	Adj Sat Flow Rate (s)	Ratios		Lane Group		Approach	
			v/c	g/C	Delay	LOS	Delay	LOS
<b>Eastbound</b>								
L	379	1805	0.60	0.43	34.3	C		
T	1031	3610	0.38	0.29	41.1	D	37.2	D
R	750	1615	0.07	0.46	21.0	C		
<b>Westbound</b>								
L	427	1805	0.67	0.43	35.9	D		
T	1031	3610	0.48	0.29	43.0	D	38.6	D
R	750	1615	0.11	0.46	21.5	C		
<b>Northbound</b>								
L	468	1805	0.40	0.50	24.4	C		
T	611	1900	0.95	0.32	71.9	E	51.4	D
R	750	1615	0.34	0.46	25.2	C		
<b>Southbound</b>								
L	325	1805	0.86	0.50	66.4	E		
T	611	1900	0.57	0.32	43.2	D	46.1	D
R	750	1615	0.28	0.46	24.0	C		
Intersection Delay = 44.0 (sec/veh)					Intersection LOS = D			

HCS2000: Signalized Intersections Release 4.1

Phone:  
E-Mail:

Fax:

OPERATIONAL ANALYSIS

Analyst: MAAI  
 Agency/Co.: GDOT  
 Date Performed: 02/11/2002  
 Analysis Time Period: AM Peak Hour  
 Intersection: Lithonia Ind @ S. Stone Mtn-Li  
 Area Type: All other areas  
 Jurisdiction: DeKalb County  
 Analysis Year: Year 2025  
 Project ID: Lithonia Ind, Blvd.

East/West Street  
 Lithonia Ind. Blvd.

North/South Street  
S. Stone Mtn.- Lithonia Rd.

HCS2000: Signalized Intersections Release 4.1

Analyst: MAAI  
 Agency: GDOT  
 Date: 4/11/2002  
 Period: PM Peak Hour  
 Project ID: Lithonia Ind, Blvd.  
 E/W St: Lithonia Ind. Blvd.

Inter.: Lithonia Ind @ S. Stone Mtn-Li  
 Area Type: All other areas  
 Jurisd: DeKalb County  
 Year : Year 2025  
 N/S St: S. Stone Mtn.- Lithonia Rd.

SIGNALIZED INTERSECTION SUMMARY

	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
No. Lanes	1	2	1	1	2	1	1	1	1	1	1	1
LGConfig	L	T	R	L	T	R	L	T	R	L	T	R
Volume	185	530	135	300	550	300	75	400	180	60	585	220
Lane Width	12.0	12.0	12.0	12.0	12.0	12.0	12.0	12.0	12.0	12.0	12.0	12.0
RTOR Vol			0			0			0			0

Duration 0.25 Area Type: All other areas

Signal Operations

Phase Combination	1	2	3	4	5	6	7	8
EB Left		P			NB Left	P		
Thru			P		Thru		P	
Right			P		Right		P	
Peds					Peds			
WB Left		P		P	SB Left	P		P
Thru				P	Thru			P
Right				P	Right			P
Peds					Peds			
NB Right		P			EB Right	P		
SB Right		P			WB Right	P		
Green	15.0	30.0	0.0	0.0	10.0	40.0	0.0	0.0
Yellow	4.0	4.0	0.0		4.0	4.0	0.0	
All Red	1.0	1.0	0.0		1.0	1.0	0.0	

Cycle Length: 115.0 secs

Intersection Performance Summary

Appr/ Lane Grp	Lane Group Capacity	Adj Sat Flow Rate (s)	Ratios		Lane Group		Approach	
			v/c	g/C	Delay	LOS	Delay	LOS
Eastbound								
L	235	1805	0.83	0.13	76.2	E		
T	942	3610	0.59	0.26	39.9	D	45.3	D
R	632	1615	0.22	0.39	24.2	C		
Westbound								
L	378	1805	0.84	0.43	43.4	D		
T	942	3610	0.61	0.26	40.4	D	38.3	D
R	632	1615	0.50	0.39	29.3	C		
Northbound								
L	157	1805	0.50	0.09	61.2	E		
T	661	1900	0.64	0.35	36.1	D	33.3	C
R	843	1615	0.22	0.52	15.5	B		
Southbound								
L	353	1805	0.18	0.48	19.8	B		
T	661	1900	0.93	0.35	58.0	E	44.7	D
R	843	1615	0.28	0.52	16.2	B		
Intersection Delay = 40.6 (sec/veh)					Intersection LOS = D			

HCS2000: Signalized Intersections Release 4.1

Phone:  
E-Mail:

Fax:

OPERATIONAL ANALYSIS

Analyst: MAAI  
 Agency/Co.: GDOT  
 Date Performed: 4/11/2002  
 Analysis Time Period: PM Peak Hour  
 Intersection: Lithonia Ind @ S. Stone Mtn-Li  
 Area Type: All other areas  
 Jurisdiction: DeKalb County  
 Analysis Year: Year 2025  
 Project ID: Lithonia Ind, Blvd.

East/West Street  
 Lithonia Ind. Blvd.

North/South Street  
S. Stone Mtn.- Lithonia Rd.

HCS2000: Signalized Intersections Release 4.1

Analyst: MAAI Inter.: Lithonia Ind.@ Rogers Lake Rd.  
 Agency: MAAI Area Type: All other areas  
 Date: 7/17/2001 Jurisd: Dekalb County  
 Period: AM Peak Hour Year : Build Condition  
 Project ID: Lithonia Industrial Boulevard Extension  
 E/W St: Lithonia Industrial Blvd. N/S St: Rogers Lake Road

SIGNALIZED INTERSECTION SUMMARY

	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
No. Lanes	1	2	1	1	2	1	1	1	1	1	1	1
LGConfig	L	T	R	L	T	R	L	T	R	L	T	R
Volume	20	700	35	120	720	60	105	55	100	140	45	120
Lane Width	12.0	12.0	12.0	12.0	12.0	12.0	12.0	12.0	12.0	12.0	12.0	12.0
RTOR Vol			0			0			0			0

Duration 0.25 Area Type: All other areas

Signal Operations

Phase Combination	1	2	3	4	5	6	7	8
EB Left		P			NB Left	P		
Thru			P		Thru	P		
Right			P		Right	P		
Peds					Peds			
WB Left		P			SB Left	P		
Thru			P		Thru	P		
Right			P		Right	P		
Peds					Peds			
NB Right		P			EB Right			
SB Right		P			WB Right			
Green	15.0	30.0			30.0			
Yellow	4.0	4.0			4.0			
All Red	1.0	1.0			1.0			

Cycle Length: 90.0 secs

Intersection Performance Summary

Appr/ Lane Grp	Lane Group Capacity	Adj Sat Flow Rate (s)	Ratios		Lane Group		Approach	
			v/c	g/C	Delay	LOS	Delay	LOS
Eastbound								
L	269	1615	0.08	0.17	32.2	C		
T	1203	3610	0.61	0.33	27.5	C	27.3	C
R	510	1530	0.07	0.33	20.8	C		
Westbound								
L	269	1615	0.47	0.17	39.7	D		
T	1203	3610	0.63	0.33	27.8	C	29.0	C
R	510	1530	0.12	0.33	21.4	C		
Northbound								
L	412	1235	0.27	0.33	23.6	C		
T	633	1900	0.09	0.33	20.9	C	17.8	B
R	850	1530	0.12	0.56	9.8	A		
Southbound								
L	408	1223	0.36	0.33	25.2	C		
T	633	1900	0.07	0.33	20.7	C	18.6	B
R	850	1530	0.15	0.56	10.1	B		
Intersection Delay = 25.7 (sec/veh)					Intersection LOS = C			

HCS2000: Signalized Intersections Release 4.1

MA  
MA

Phone:  
E-Mail:

Fax:

OPERATIONAL ANALYSIS

Analyst: MAAI  
 Agency/Co.: MAAI  
 Date Performed: 7/17/2001  
 Analysis Time Period: AM Peak Hour  
 Intersection: Lithonia Ind.@ Rogers Lake Rd.  
 Area Type: All other areas  
 Jurisdiction: Dekalb County  
 Analysis Year: Build Condition  
 Project ID: Lithonia Industrial Boulevard Extension

East/West Street	North/South Street
Lithonia Industrial Blvd.	<u>Rogers Lake Road</u>

HCS2000: Signalized Intersections Release 4.1

Analyst: MAAI Inter.: Lithonia Ind,@ Rogers Lake Rd.  
 Agency: GDOT Area Type: All other areas  
 Date: 7/17/2001 Jurisd: Dekalb County  
 Period: PM Peak Hour Year : Year 2025  
 Project ID: Lithonia Industrial Boulevard Extension  
 E/W St: Lithonia Industrial Blvd. N/S St: Rogers Lake Road

SIGNALIZED INTERSECTION SUMMARY

	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
No. Lanes	1	2	1	1	2	1	1	1	1	1	1	1
LGConfig	L	T	R	L	T	R	L	T	R	L	T	R
Volume	60	510	50	35	915	70	90	35	100	90	55	115
Lane Width	12.0	12.0	12.0	12.0	12.0	12.0	12.0	12.0	12.0	12.0	12.0	12.0
RTOR Vol			0			0			0			0

Duration 0.25 Area Type: All other areas

Signal Operations

Phase Combination	1	2	3	4	5	6	7	8
EB Left		P			NB Left	P		
Thru			P		Thru	P		
Right			P		Right	P		
Peds					Peds			
WB Left		P			SB Left	P		
Thru			P		Thru	P		
Right			P		Right	P		
Peds					Peds			
NB Right		P			EB Right			
SB Right		P			WB Right			
Green	15.0	65.0			30.0			
Yellow	4.0	4.0			4.0			
All Red	1.0	1.0			1.0			

Cycle Length: 125.0 secs

Intersection Performance Summary

Appr/ Lane Grp	Lane Group Capacity	Adj Sat Flow Rate (s)	Ratios		Lane Group		Approach	
			v/c	g/C	Delay	LOS	Delay	LOS
Eastbound								
L	194	1615	0.32	0.12	54.8	D		
T	1877	3610	0.29	0.52	17.3	B	20.7	C
R	796	1530	0.07	0.52	15.1	B		
Westbound								
L	194	1615	0.19	0.12	51.7	D		
T	1877	3610	0.51	0.52	20.6	C	21.4	C
R	796	1530	0.09	0.52	15.4	B		
Northbound								
L	294	1223	0.32	0.24	42.0	D		
T	456	1900	0.08	0.24	37.2	D	33.6	C
R	612	1530	0.17	0.40	24.8	C		
Southbound								
L	299	1246	0.32	0.24	41.9	D		
T	456	1900	0.13	0.24	37.8	D	33.6	C
R	612	1530	0.20	0.40	25.2	C		
Intersection Delay = 24.0 (sec/veh)					Intersection LOS = C			

HCS2000: Signalized Intersections Release 4.1

MA  
MA

Phone:  
E-Mail:

Fax:

OPERATIONAL ANALYSIS

Analyst: MAAI  
 Agency/Co.: GDOT  
 Date Performed: 7/17/2001  
 Analysis Time Period: PM Peak Hour  
 Intersection: Lithonia Ind, @ Rogers Lake Rd.  
 Area Type: All other areas  
 Jurisdiction: Dekalb County  
 Analysis Year: Year 2025  
 Project ID: Lithonia Industrial Boulevard Extension

	East/West Street	North/South Street
Lithonia Industrial Blvd.		<u>Rogers Lake Road</u>

**DeKALB COUNTY ROADS AND DRAINAGE**  
**PUBLIC INFORMATIONAL MEETING**  
**SEPTEMBER 21, 1999**  
**LITHONIA INDUSTRIAL BOULEVARD**

Welcome to tonight's Public Informational Meeting. We value your input into the planning process for this project. The displays located inside our meeting room represent three alternative alignments. PBS&J and DeKalb County staff are available at each display to answer your questions and take your comments and ideas.

PBS&J has been retained by DeKalb County as Program Managers and to evaluate possible alignments for this project. Tonight's meeting represents a presentation of these Preliminary Alignments.

**PROJECT DESCRIPTION**

The project consists of extending existing Lithonia Industrial Boulevard from South Stone Mountain Lithonia Road approximately 2.3 miles to Rock Chapel Road. The alignment from South Stone Mountain Lithonia Road to Rogers Lake Road will utilize an existing graded roadway and existing right-of-way corridor. Also included in this section is a proposed CSX Railroad bridge with the roadway going under the railroad tracks. This crossing will replace the at-grade railroad crossing at Coffee Road, which will be closed upon completion of the project. The alignment from Rogers Lake Road will be new location and tie to the Georgia DOT's widening project of Rock Chapel Road. This new alignment involves wetland and other environmental issues.

The proposed roadway includes four 12-foot lanes with curb and gutter at the outside edges of pavement. Alternate three includes a 20' raised median through the entire length of the project.

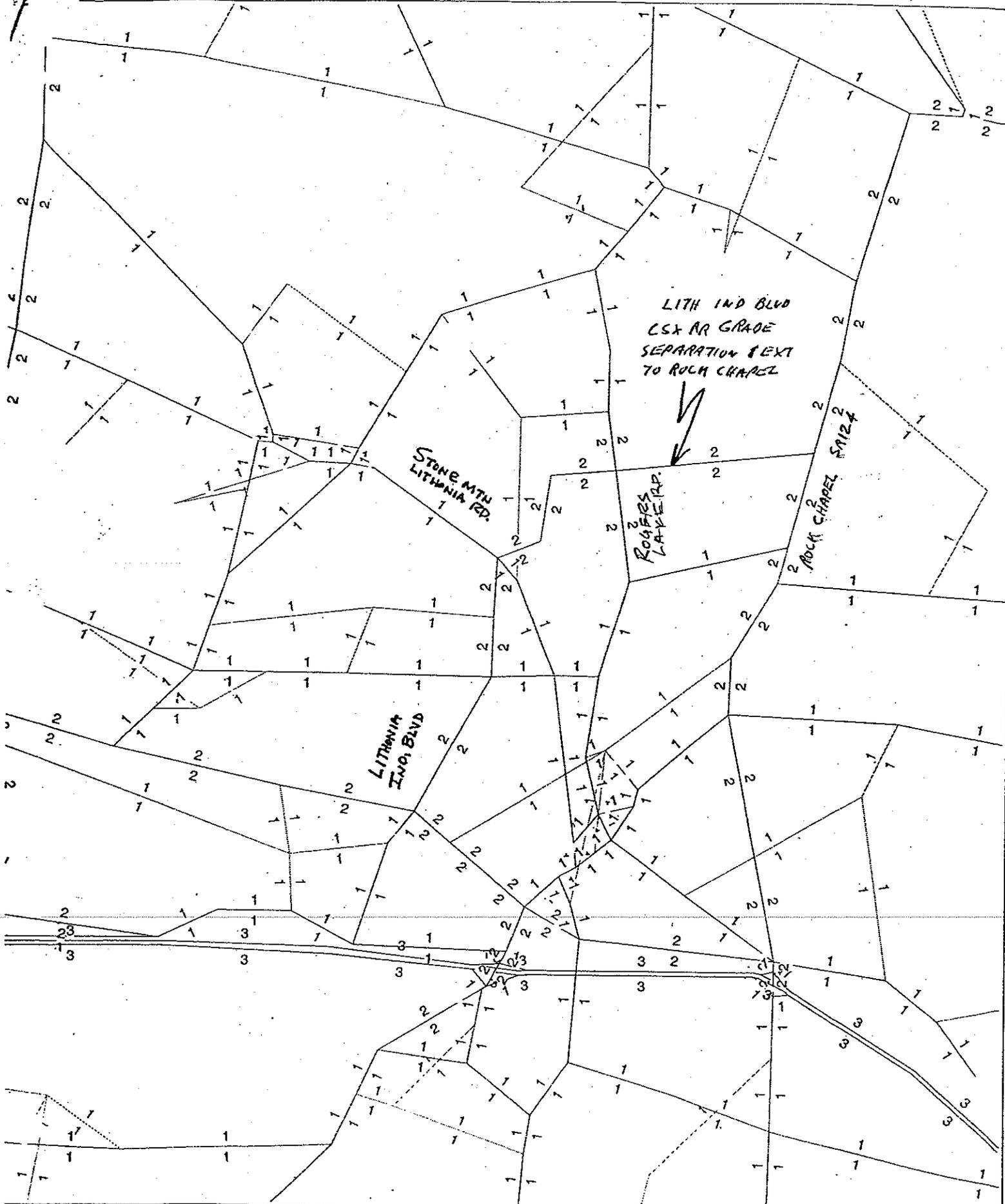
The project will provide a more direct route to Rock Chapel Road for the many trucks in this highly industrialized area. The roadway will increase safety and facilitate traffic movements with the grade-separated crossing at the CSX Railroad and with the addition of turn lanes and traffic signals at the major intersections.

Pending county, state, and federal approval, the proposed 100' right-of-way corridor will be acquired by DeKalb County, and the construction cost will be paid for with state and federal funds. The approximate construction cost of Alternates 1 and 2 is \$6.9 million and \$7.6 million for Alternate 3.

**COMMENTS WELCOMED**

Public comments are welcomed regarding the design of this project. Please use the comment sheet provided. You may leave your comments with us tonight or mail them to us within two weeks.

Lithonia Industrial Blvd @ CSX RR DK 270 2005





**AGREEMENT**

**BETWEEN**

**DEPARTMENT OF TRANSPORTATION**

**STATE OF GEORGIA**

**AND**

**DEKALB COUNTY, GEORGIA**

**for**

**LITHONIA INDUSTRIAL BLVD. AT CSX RR GRADE SEPARATION**

This AGREEMENT is made and entered into this 16<sup>th</sup> day of November 2001, by and between the DEPARTMENT OF TRANSPORTATION, an agency of the State of Georgia, hereinafter called the "**DEPARTMENT**", and DEKALB COUNTY, GEORGIA, acting by and through its Board of Commissioners, hereinafter called the "**COUNTY**".

WHEREAS, the COUNTY has represented to the DEPARTMENT a desire to reconstruct Lithonia Industrial Blvd. at CSX RR grade separation in DeKalb County, Georgia, currently described as Georgia Department of Transportation Project Number HPP-9347 (1), P.I. Number 753230, hereinafter referred to as the "**PROJECT**"; and

WHEREAS, the COUNTY represented to the DEPARTMENT a desire to participate in providing the preconstruction engineering activities needed for the improvements, relocating the utilities, and other costs as specified in the AGREEMENT, and the DEPARTMENT has relied upon such representations; and

WHEREAS, the DEPARTMENT has expressed a willingness to participate in the funding of the construction of the PROJECT with funds of the DEPARTMENT, funds apportioned to the DEPARTMENT by the Federal Highway Administration, hereinafter referred to as the "FHWA", under Title 23, United States Code, Section 104, or a combination of funds from any of the above sources; subject to those certain conditions set forth in the AGREEMENT.

THEREFORE, in consideration of the mutual promises made and of the benefits to flow from one to the other, the DEPARTMENT and the COUNTY hereby agree each with the other as follows:

1. All Primary Consultant firms hired by the COUNTY to provide services on the PROJECT shall be prequalified with the DEPARTMENT in the appropriate area-classes. The DEPARTMENT shall, on request, furnish the COUNTY with a list of prequalified consultant firms in the appropriate area-classes.

2. The PROJECT construction plans shall be prepared in English units and right of way plans shall be prepared in dual units-English and metric.

3. Both the COUNTY and the DEPARTMENT hereby acknowledge that time is of the essence and both parties shall adhere to the priorities established in the approved State Transportation Improvement Program (STIP) or earlier. Furthermore, all parties shall adhere to the detailed project schedule, as approved by the DEPARTMENT. In the completion of respective commitments contained herein, if a change in the schedule is needed, the DEPARTMENT shall have final authority. If, for any reason, the COUNTY does not produce acceptable deliverables at the milestone dates defined in the STIP, or in the approved schedule, the DEPARTMENT reserves the right to delay the project's implementation until funds can be re-identified for construction or right of way, as applicable.

4. All drafting and design work performed on the project shall be done utilizing Microstation and CAiCE software respectively, and shall be organized as per the Department's guidelines on electronic file management.

5. The COUNTY shall contribute towards the PROJECT by funding all cost for the preconstruction engineering (design). The preconstruction engineering activities shall be accomplished in accordance with the DEPARTMENT's Plan Development Process, the applicable guidelines of the American Association of State Highway and Transportation Officials, hereinafter referred to as "AASHTO", the DEPARTMENT's Standard Specifications for the Construction of Transportation Systems, PROJECT schedules, and applicable guidelines of the DEPARTMENT. The COUNTY's responsibility for design shall include, but is not limited to the following items:

a. Prepare the PROJECT concept report in accordance with the format used by the DEPARTMENT. The concept for the PROJECT shall be developed to accommodate the future traffic volumes as generated by the COUNTY as provided for in paragraph 5b and approved by the DEPARTMENT. The concept report shall be approved by the DEPARTMENT prior to the COUNTY beginning further development of the PROJECT plans. It is recognized by the parties that the approved concept may be modified by the COUNTY as required by the DEPARTMENT and reapproved by the DEPARTMENT during the course of design due to public input, environmental requirements, or right of way considerations.

b. Develop the PROJECT's base year (year facility is expected to be open to traffic) and design year (base year plus 20 years) traffic volumes. This shall include average daily traffic (ADT) and morning (am) and evening (pm) peak hour volumes. The traffic shall show all through and turning movement volumes at intersections for the ADT and peak hour volumes and shall indicate the percentage of trucks expected on the facility.

c. Validate (check and update) the approved PROJECT concept and prepare a PROJECT Design Book for approval by the DEPARTMENT prior to the beginning of preliminary plans.

d. Prepare environmental studies, documentation, and reports for the PROJECT that show the PROJECT is in compliance with the provisions of the National Environmental Protection Act and Georgia Environmental Protection Act, as appropriate to the PROJECT funding. This shall include any and all archaeological, historical, ecological, air, noise, underground storage tanks (UST), and hazardous waste site studies required. The COUNTY shall submit to the DEPARTMENT all environmental documents and reports for review and approval by the DEPARTMENT and the FHWA.

e. Prepare all public hearing and public information displays and conduct all required public hearings and public information meetings in accordance with DEPARTMENT practice.

f. Perform all surveys, mapping, soil investigation, existing pavement analysis, and final pavement design needed for design of the PROJECT.

g. Perform all work required to obtain project permits, including, but not limited to, US Army Corps of Engineers 404 and Federal Emergency Management Agency (FEMA) approvals. These efforts shall be coordinated with the DEPARTMENT.

h. Prepare the PROJECT's drainage design including erosion control plans and the development of the hydraulic studies for the Federal Emergency Management Agency Floodways and acquisition of all necessary permits associated with the drainage design.

i. Prepare traffic studies, preliminary construction plans, preliminary and final utility plans, preliminary and final right of way plans, staking of the required right of way, and final construction plans including erosion control, traffic handling, and construction sequence plans and specifications including special provisions for the PROJECT.

j. Provide certification, by a Georgia Registered Professional Engineer, that the construction plans have been prepared under the guidance of the professional engineer and are in accordance with acceptable industry standards.

6. The DEPARTMENT shall review and has approval authority for all aspects of the PROJECT. The DEPARTMENT will work with the FHWA to obtain all needed approvals with information furnished by the COUNTY.

7. Upon the COUNTY's determination of the rights of way required for the PROJECT and the approval of the right of way plans by the DEPARTMENT, the COUNTY shall fund the acquisition and acquire the necessary rights of way for the PROJECT. Right of way acquisition shall be in accordance with the law and the rules and regulations of the FHWA including but not limited to, Title 23, United States Code; 23 CFR 710, et. seq., and 49 CFR Part 24, and the rules and regulations of the DEPARTMENT. Failure to follow these requirements will result in loss of Federal funding for the PROJECT and it will be the responsibility of the COUNTY to make up the loss of that funding. All required right of way shall be obtained and cleared of obstructions, including underground storage tanks, prior to the DEPARTMENT's advertising the PROJECT for bids. The COUNTY shall further be responsible for making all changes to the approved right of way plans, as deemed necessary by the DEPARTMENT, for whatever reason, as needed to purchase the right of way or to match actual conditions encountered.

8. The COUNTY shall be responsible for the design of the bridge crossing CSX RR, which lies within the limits of this PROJECT. The COUNTY shall be responsible for providing all necessary survey information required for the completion of the hydraulic study report. The COUNTY shall perform all necessary survey efforts regarding the design of the bridge and shall incorporate these plans into this PROJECT as a part of this Agreement.

9. The COUNTY shall be responsible for 100% of the utility relocation costs necessary for the construction of the PROJECT.

10. The COUNTY shall be responsible for all costs for providing energy, maintenance, and operational costs of any roadway and interchange lighting within the PROJECT limits.

11. The COUNTY shall follow the DEPARTMENT's procedures for identification of existing and proposed utility facilities on the PROJECT. These procedures, in part, require all requests for existing, proposed, or relocated facilities to flow through the DEPARTMENT's Project Liaison and the District Utilities Engineer.

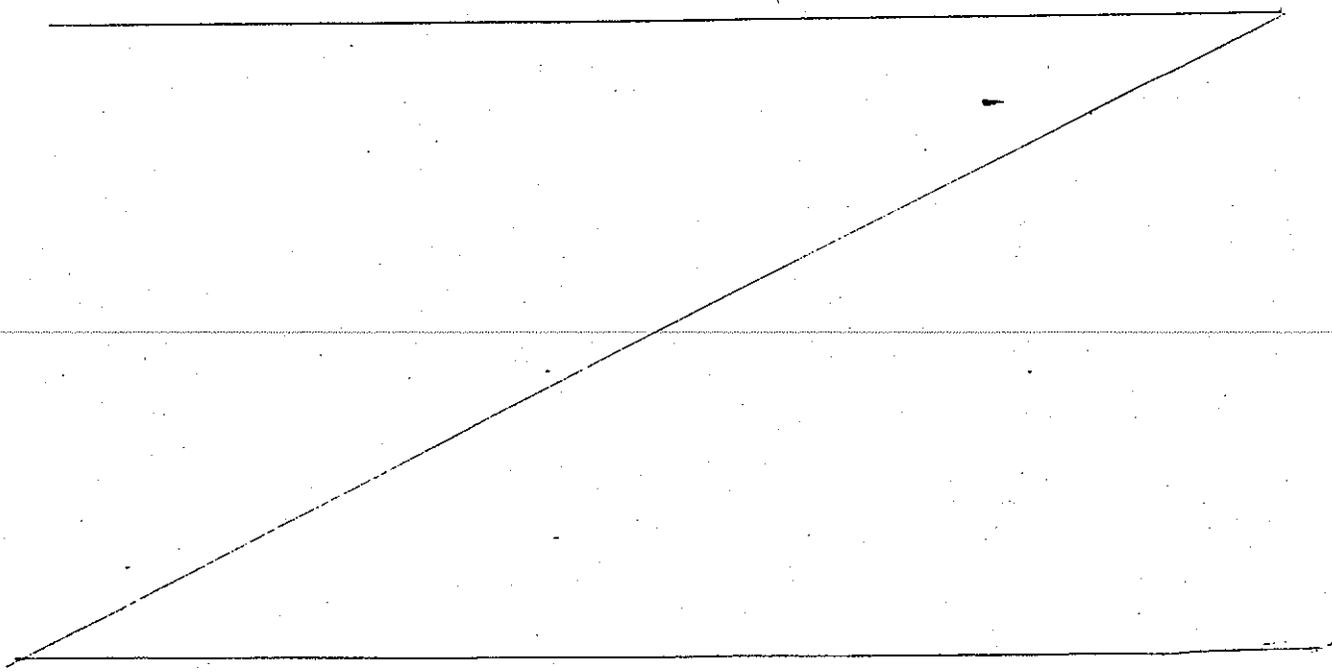
12. Upon completion and approval of the PROJECT plans, certification that all needed rights of way have been obtained and cleared of obstructions, and that certification that all needed permits for the PROJECT have been obtained by the COUNTY, the DEPARTMENT shall let the PROJECT for construction. Except as provided herein and upon receipt of an acceptable bid, the DEPARTMENT shall bear all costs for construction, including all costs associated with inspection and materials testing during construction. The DEPARTMENT shall be solely responsible for securing and awarding the construction contract for the PROJECT.

13. The COUNTY agrees that all reports, plans, drawings, studies, specifications, estimates, maps, computations, computer diskettes and printouts, and any other data prepared under the terms of this agreement shall become the property of the DEPARTMENT. This data shall be organized, indexed, bound, and delivered to the DEPARTMENT no later than the advertisement of the PROJECT for letting. The DEPARTMENT shall have the right to use this material without restriction or limitation and without compensation to the COUNTY.

14. The COUNTY shall be responsible for the professional quality, technical accuracy, and the coordination of all designs, drawings, specifications, and other services furnished by or on behalf of the COUNTY pursuant to this AGREEMENT. The COUNTY shall correct or revise, or cause to be corrected or revised, any errors or deficiencies in the designs, drawings, specifications, and other services furnished for this PROJECT. All revisions shall be coordinated with the DEPARTMENT prior to issuance. The COUNTY shall also be responsible for any claim, damage, loss or expense that is attributable to negligent acts, errors, or omissions related to the designs, drawings, specifications, and other services furnished by or on behalf of the COUNTY pursuant to this AGREEMENT.

15. The COUNTY shall prepare all shop drawings for approval by the DEPARTMENT.

16. This AGREEMENT is made and entered into in Fulton County, Georgia, and shall be governed and construed under the laws of the State of Georgia. The covenants herein contained shall, except as otherwise provided, accrue to the benefit of and be binding upon the successors and assigns of the parties hereto.



IN WITNESS WHEREOF, the DEPARTMENT and DEKALB COUNTY have caused these presents to be executed under seal by their duly authorized representatives.

RECOMMENDED:

DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA

DEKALB COUNTY, GEORGIA

Joseph P. Palladi  
Joseph P. Palladi, P.E.  
State Urban Design Engineer

Vernon Jones (by Dir.)(SEAL)  
Vernon Jones  
Chief Executive Officer  
DeKalb County, Georgia

Thomas L. Turner  
Thomas L. Turner, P.E.  
Director of Preconstruction

Signed, sealed and delivered this  
16<sup>th</sup> day of November, 2001, in  
The presence of:

Dois A. Blackmon  
Witness

Frank L. Danchetz  
Frank L. Danchetz  
Chief Engineer

Barbara H. Sanders  
Notary Public  
Barbara H. Sanders  
Notary Public, Gwinnett County, Georgia  
My Commission Expires April 20, 2004

BY: Tom Coleman  
Tom Coleman  
Commissioner

This Agreement approved by the DeKalb  
County Commission at a meeting held at  
Decatur, Georgia on  
This 13<sup>th</sup> day of November, 2001,

BFS  
ATTEST:

ATTEST:  
Michael J. Bell  
Michael J. Bell, Ex Officio Clerk of the  
Chief Executive Officer and  
Board of Commissioners of  
DeKalb County, Georgia

Billy F. Sharp  
Billy F. Sharp  
Treasurer

Approved as to form:

Reviewed as to Legal Forms:  
[Signature]  
Office of legal Services  
Date: 1/9/02

Terry A. Phillips  
Asst. County Attorney

**DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA**

*Urban Design*

Lithonia Industrial Boulevard – Phase I

Project Number: HPP-9347(1)

County: DeKalb

P. I. Number: 753230

Federal Route Number: N/A

State Route Number: N/A

*See Page 2 for Location Sketch*

Recommendation for approval:

DATE 4/25/02

*Caryn O. Van Meter*  
Project Manager

DATE 5/4/02

*Joseph P. Ricci*  
State Urban Design Engineer

The concept as presented herein and submitted for approval is consistent with that which is included in the Regional Transportation Improvement Program (RTP) and/or the State Transportation Improvement Program (STIP).

DATE \_\_\_\_\_

\_\_\_\_\_  
State Transportation Planning Administrator

DATE \_\_\_\_\_

\_\_\_\_\_  
Office of Financial Management/Administrator

DATE 5/10/02

*Thomas D. Kasper*  
State Environmental/Location Engineer

DATE \_\_\_\_\_

\_\_\_\_\_  
State Traffic Safety and Design Engineer

DATE \_\_\_\_\_

\_\_\_\_\_  
District Engineer

DATE \_\_\_\_\_

\_\_\_\_\_  
Project Review Engineer

DATE \_\_\_\_\_

\_\_\_\_\_  
State Bridge and Structural Engineer

**DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA**

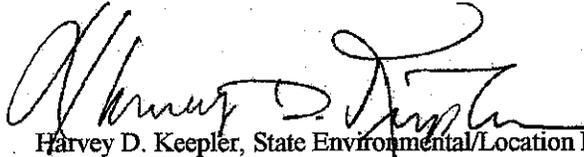
**INTERDEPARTMENT CORRESPONDENCE**

**FILE:** P.I. No. 753230

**OFFICE:** Environment/Location

**DATE:** May 13, 2002

**FROM:**

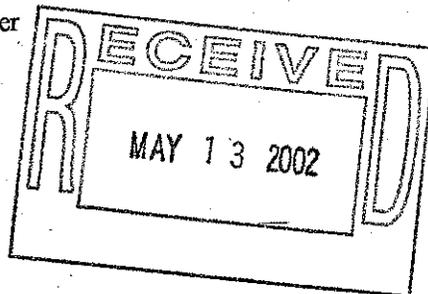
  
Harvey D. Keepler, State Environmental/Location Engineer

**TO:**

Wayne Hutto, P.E., Assistant Director of Preconstruction

**SUBJECT:**

**PROJECT CONCEPT REPORT**  
HPP-9347(1), DEKALB COUNTY



The above subject concept report has been reviewed. Page 8 - It will take close to one year to complete the Environmental Process. Page 6 - There are several landfills in the project area. Hazardous waste should be listed as an environmental concern.

If you have any questions, please contact me at (404) 699-4401.

HDK/rtt

Attachment

cc: David Mulling  
Joseph Palladi, P.E.

DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA

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Project Manager

DATE 5/4/02

Joseph P. Ricci  
State Urban Design Engineer

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DATE \_\_\_\_\_

State Transportation Planning Administrator

DATE 5/7/02

Sharon J. Saff  
Office of Financial Management Administrator

DATE \_\_\_\_\_

State Environmental/Location Engineer

DATE \_\_\_\_\_

State Traffic Safety and Design Engineer

DATE \_\_\_\_\_

District Engineer

DATE \_\_\_\_\_

Project Review Engineer

DATE \_\_\_\_\_

State Bridge and Structural Engineer

**DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA**

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Recommendation for approval:

DATE 4/25/02

*Caryn O. Van Meter*  
Project Manager

DATE 5/4/02

*Joseph P. Pledzi*  
State Urban Design Engineer

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DATE 5-10-02

*Marta V. Roman*  
State Transportation Planning Administrator

DATE \_\_\_\_\_

Office of Financial Management Administrator

DATE \_\_\_\_\_

State Environmental/Location Engineer

DATE \_\_\_\_\_

State Traffic Safety and Design Engineer

DATE \_\_\_\_\_

District Engineer

DATE \_\_\_\_\_

Project Review Engineer

DATE \_\_\_\_\_

State Bridge and Structural Engineer

**DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA**

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Recommendation for approval:

DATE 4/25/02

*Caryn O. Van Meter*  
Project Manager

DATE 5/4/02

*Jacques P. Ledet*  
State Urban Design Engineer

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DATE \_\_\_\_\_

\_\_\_\_\_  
State Transportation Planning Administrator

DATE \_\_\_\_\_

\_\_\_\_\_  
Office of Financial Management Administrator

DATE \_\_\_\_\_

\_\_\_\_\_  
State Environmental/Location Engineer

DATE \_\_\_\_\_

\_\_\_\_\_  
State Traffic Safety and Design Engineer

DATE \_\_\_\_\_

\_\_\_\_\_  
District Engineer

DATE \_\_\_\_\_

\_\_\_\_\_  
Project Review Engineer

DATE 6/3/02

*Paul V. Telle Jr.*  
State Bridge and Structural Engineer

DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA

Urban Design

Lithonia Industrial Boulevard – Phase I

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Caryn O. Van Meter  
Project Manager

DATE 5/4/02

Joseph P. Ricci  
State Urban Design Engineer

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\_\_\_\_\_  
State Transportation Planning Administrator

DATE \_\_\_\_\_

\_\_\_\_\_  
Office of Financial Management Administrator

DATE \_\_\_\_\_

\_\_\_\_\_  
State Environmental/Location Engineer

DATE \_\_\_\_\_

\_\_\_\_\_  
State Traffic Safety and Design Engineer

DATE 5-28-02

Stephen T. Newman  
District Engineer

DATE \_\_\_\_\_

\_\_\_\_\_  
Project Review Engineer

DATE \_\_\_\_\_

\_\_\_\_\_  
State Bridge and Structural Engineer