

ORIGINAL TO GENERAL FILES

D.O.T. 66

DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA

INTERDEPARTMENT CORRESPONDENCE

FILE STP-1120(2) Catoosa County **OFFICE** Preconstruction
P. I. No. 650430 **DATE** November 10, 1997

FROM 
C. Wayne Hutto, Assistant Director of Preconstruction

TO SEE DISTRIBUTION

SUBJECT PROJECT CONCEPT REPORT APPROVAL

Attached for your files is the approval for subject project.

CWH/cj

Attachment

DISTRIBUTION:

Walker Scott
Bobby Mustin
David Studstill (ATTN: Harvey Keepler)
Jerry Hobbs
Herman Griffin
Marta Rosen (ATTN: Michael Henry)
Marion Waters
Toni Dunagan
Paul Liles
Jim Hitt (Traffic Ops)
Don Mills
Charles Law
Jim Kennerly

STP-1120(2) Catoosa
October 10, 1997

The estimated costs for this project are:

	<u>PROPOSED</u>		<u>APPROVED</u>	<u>PROG</u>	<u>LET</u>
	GA	TN		DATE	DATE
Construction (includes E&C and inflation)*	\$2,106,000	\$859,000	\$655,000	1999	99-07
Right-of-Way*	\$1,720,000	\$ 65,000	\$165,000		
Utilities*	\$ 26,000	\$ 20,000	\$544,000		

*Bi-state agreement signed 6-2-94; Catoosa County refused LGPA for utilities 7-8-97.

This project is in the STIP. I recommend this project concept be approved.

WWS:JDQ/cj

Attachment

CONCUR


Frank L. Danchetz, P.E., Chief Engineer

APPROVE


Wayne Shackelford, Commissioner

DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA

INTERDEPARTMENTAL CORRESPONDENCE

FILE: STP-1120(2) CATOOSA
P.I. Number 650430

OFFICE: Atlanta, Georgia

DATE: SEPTEMBER 24, 1997

FROM: Bob Mustin, ^{DTM} Project Review Engineer

TO: C. Wayne Hutto, Assistant Director of Preconstruction

SUBJECT: PROJECT CONCEPT REPORT

RECEIVED
SEP 2 1997
PRECONSTRUCTION

We have reviewed the concept report submitted September 4, 1997 by the letter from James A. Kennerly dated August 6, 1997 and have no comments:

The estimated costs for the project are as follows:

Construction	\$ 2,451,000 (\$1,741,000 Ga. - \$710,000 Tenn.)
Inflation	\$ 245,000 (\$174,000 Ga. - \$71,000 Tenn.)
E & C	\$ 270,000 (\$191,000 Ga. - \$78,000 Tenn.)
Right of Way	\$ 1,784,497 (\$1,719,497 Ga. - \$65,000 Tenn.)
Reimbursable Utilities	\$ 45,651 (\$25,651 Ga. - \$20,000 Tenn.)

DTM

c: Jim Kennerly - Attention: Kim Fulbright

DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA
OFFICE OF ROAD AND AIRPORT DESIGN

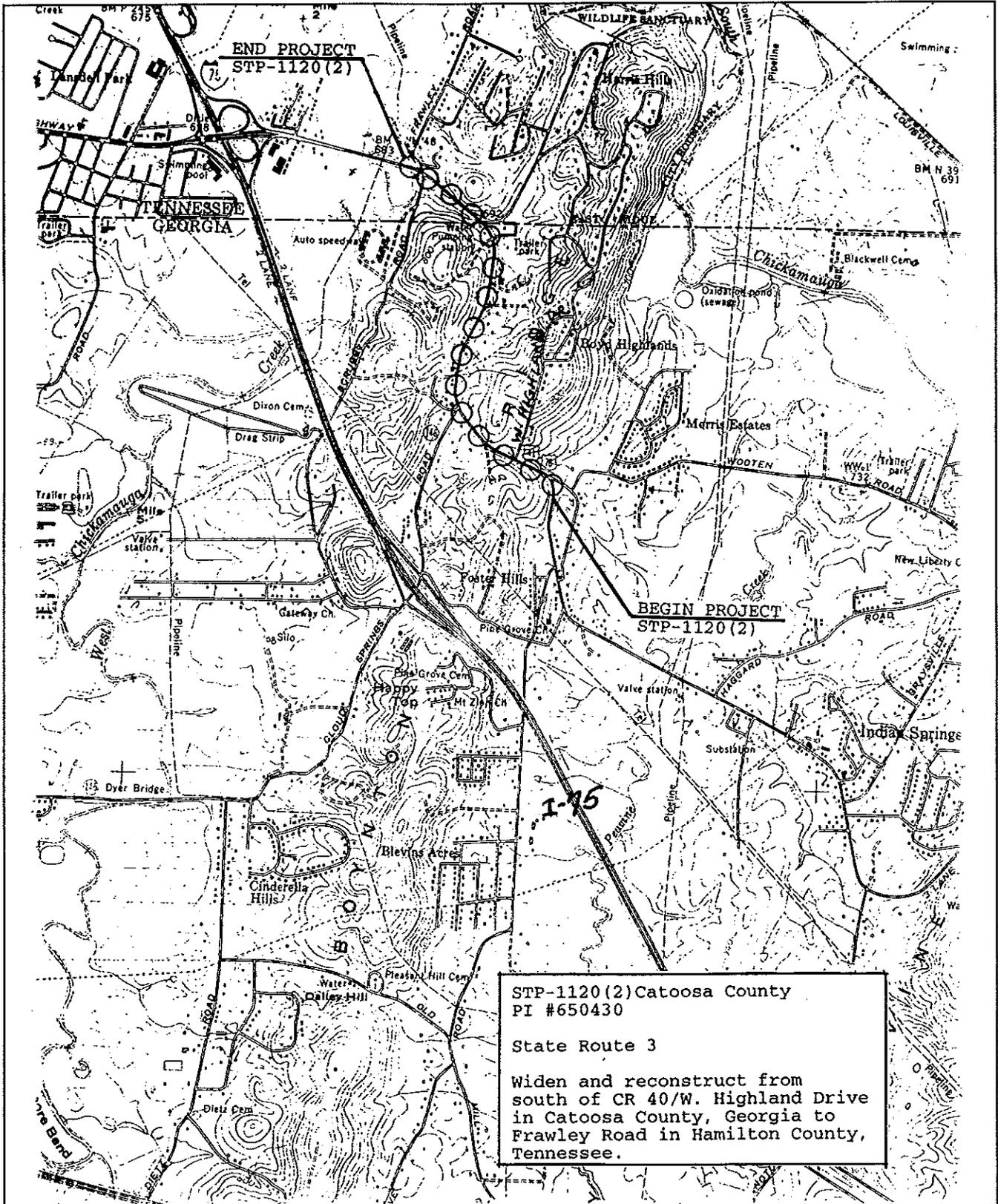
PROJECT CONCEPT REPORT

**STP-1120(2)
RINGGOLD ROAD WIDENING
CATOOSA COUNTY, GEORGIA &
HAMILTON COUNTY, TENNESSEE**

FEDERAL ROUTE NO: U.S. 41/U.S. 76 Date of Report: August 6, 1997
STATE ROUTE NO: S.R. 3/GEORGIA & S.R. 8/TENNESSEE.
GADOT P.I. NO: 650430

RECOMMENDATION FOR APPROVAL	
<u>8/29/97</u> DATE	 State Road & Airport Design Engineer
DATE	State Environmental/Location Engineer
DATE	State Traffic Operations Engineer
DATE	District Engineer
DATE	Project Review Engineer

PROJECT MAP - Project No. : STP-1120(2)



PROJECT NUMBER: STP-1120(2)

PROJECT LOCATION & DESCRIPTION

This project consist of widening and reconstructing U.S. 41 from two (3.66 m) lanes to four (3.60 m) lanes Urban Section with a (6.0 m) wide raised concrete median in Georgia and transitions to a five (3.66 m) lanes Urban Section in Tennessee.

This project begins on U.S. 41 approximately 0.62 km east of S.R. 146/Cloud Springs Road in Catoosa County, Georgia and continues northwesterly 1.76 km to the Georgia/Tennessee State Line and then proceeds on U.S. 41 to north of Frawley Road in Hamilton County, Tennessee.

PROJECT LENGTH: 1.76 km /Georgia & 0.58 km /Tennessee

TRAFFIC

CURRENT		PROJECTED	
YEAR	AADT	YEAR	AADT
1998	13900	2018	20700

PDP CLASSIFICATION

MAJOR

FUNCTIONAL CLASSIFICATION

URBAN PRINCIPAL ARTERIAL

NON-CA ()

CA ()

EXEMPT (X)

N/A ()

PROJECT NEED & PURPOSE

The proposed project is the widening of SR 3/US 41 from 0.25 km (0.15 mi.) south of Highland Road(CR 40) in Georgia to 0.15 km (0.09 mi.) north of Frawley Road in Tennessee. The southern terminus in Georgia is logical because daily traffic volumes decrease by approximately 20% south of Highland Road. At the northern terminus in Tennessee, US 41 is an existing five-lane section and this project will provide continuity along the corridor. When completed, the project should provide a safer and more efficient route for vehicles traveling this corridor. The existing pavement in Georgia, which is 7.27 m (24 ft.) wide with 1.2 m (4 ft.) shoulders, will be widened to four lanes with a 6.1 m (20 ft.)raised concrete median and turn lanes as needed. The proposed project will include travel lanes and intersection reconstruction at SR 146.

PROJECT NEED & PURPOSE (Continued)

The project portion in Tennessee will improve the route to four lanes with a continuous left turn lane and intersection reconstruction at South Sanctuary Road.

Currently, the land use on either side of the project is commercial and zoned for general business. In its year 2010 Catoosa Comprehensive Plan, the Coosa Valley Regional Development Center forecasts an increasing concentration of future commercial development along the project corridor. The same report also forecasts continued medium and high density residential development in the area adjacent to the commercial development.

Traffic volumes in the project area are expected to increase from current (1996) average annual daily traffic (AADT) of 8,112 to a projected (year 2018) AADT of 20,700. The level-of-service along this corridor, which was analyzed as a two-lane facility using the 1994 Highway Capacity Software, is almost at capacity with a level-of-service "D" and should reach a level-of-service "F" before the design year 2018.

Accident rates through the project area exceed the statewide averages for an urban principal arterial. The calculated accident rate using the 1995 data was 892 accidents per 100 million vehicle miles traveled (MVMT), which is greater than the 1995 statewide average of 549 accidents per 100 MVMT for a comparable facility. Many of the accidents that occur along this route are rear-end type accidents. Widening SR 3/US 41 to four lanes should increase safety along the route by increasing capacity and remove turning vehicles from the traffic stream.

The project is included in the Chattanooga Urban Area Transportation Study (CUATS) 2015 Long Range Transportation Plan. It is also identified in the CUATS FY1997-FY1999 Transportation Improvement Program (TIP) for Right-of-Way in FY 1997 and Construction in FY 1999. This project is one of several identified in the CUATS Long Range Transportation Plan and CUATS FY1997-FY1999 TIP that should improve access and mobility in North Georgia. Other proposed projects in the SR 3 vicinity include widening of Cloud Springs Road from Lakeview Drive to I-75 (P.I. 650440); the I-75 widening from SR 151 to the state line (P.I. 611010, 610800, & 610810); the replacement of the bridge on CR 259 over Little Chickamauga Creek (P.I. 670710); and the widening of SR 151 from SR 3 to the state line (P.I. 642190).

This project is coordinated with the Tennessee DOT project to widen US 41 from the state line to North of Frawley Road. The Tennessee project is also included in the CUATS 2015 Long Range Transportation Plan and CUATS FY1997-1999 TIP.

EXISTING ROADWAY		
TYPICAL SECTION: Two 3.66 m lanes in Georgia Five 3.66 m lanes in Tenn.		R/W WIDTH <u>60 FT</u>
POSTED SPEED <u>55 MPH Max</u>	MIN RADIUS <u>572.96 m Radius</u>	MAXIMUM GRADE <u>6.00%</u>
MAJOR STRUCTURES:		
1.		
2.		
3.		

PROPOSED ROADWAY		
TYPICAL SECTION: Four 3.60 m lanes with 6.0 m wide raised conc. median in Georgia and transition to Five 3.66 m lanes in Tennessee.		
DESIGN SPEED 70 kph	MIN RADIUS ALLOWABLE: 215 m PROPOSED: 350 m	MAX GRADE ALLOWABLE: 7.00 % PROPOSED: 6.00 %
MAJOR STRUCTURES:		

PROPOSED RIGHT OF WAY	
R/W WIDTH <u>33.6 m</u>	DISPLACEMENTS RES: <u>1</u> BUS: <u>6</u> M.H.: <u>0</u>
TYPE OF ACCESS CONTROL: Controlled by Permit	
NUMBER OF PARCELS: 44	

COORDINATION

CONCEPT TEAM MEETING DATE: JULY 1, 1997
 LOCATION INSPECTION DATE: NONE
 PERMITS REQUIRED (4f, COE, 404, etc.): NONE
 LEVEL OF PUBLIC INVOLVEMENT: Advertise opportunity for Hearing
 TIME SAVING PROCEDURES APPROPRIATE: NO
 OTHER PROJECT IN THE AREA: STP-1111(7), NH-75-3(216), NH-IM-75-3(196), NH-IM-75-3(198), BRZLB-047(5), STP-0821(6),

MISCELLANEOUS

TRAFFIC CONTROL DURING CONSTRUCTION: Construct under Traffic.
 LEVEL OF ENVIRONMENTAL ANALYSIS: Categorical Exclusion
 DESIGN EXCEPTIONS REQUIRED:

	YES	NO	UNDETERMINED
SUBST HORIZ ALIGNMENT	()	(x)	()
SUBST ROADWAY WIDTH	()	(x)	()
SUBST SHOULDER WIDTH	()	(x)	()
SUBST VERT GRADES	()	(x)	()
SUBST CROSS SLOPES	()	(x)	()
SUBST STOPPING SIGHT DIST	()	(x)	()
SUBST SUPERELEV RATES	()	(x)	()
SUBST HORIZ CLEARANCE	()	(x)	()
SUBST SPEED DESIGN	()	(x)	()
SUBST VERTICAL CLEARANCE	()	(x)	()
SUBST BRIDGE WIDTH	()	(x)	()
SUBST BR STRUCT CAPACITY	()	(x)	()

UNDERGROUND STORAGE TANKS: 4 SITES

HAZARDOUS SITES: 1 SITE.

ALTERNATIVES CONSIDERED

1. NO BUILD

ESTIMATED COST

CONSTRUCTION:	\$ 2,450,735.00	RIGHT-OF-WAY:	\$ 1,784,497.00
E & C (10) :	\$ 245,074.00	ACQUIRED BY :	D.O.T
INFLATION :	\$ 269,581	UTILITIES :	\$ 45,651
		ADJUSTED BY :	LGPA (See comment)
TOTAL CONSTRUCTION COST:		\$2,965,268	

COMMENTS: STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION SIGNED BI-STATE AGREEMENT DATED 6-2-94 (INCLUDES UTILITY ADJUSTMENTS). CATOOSA COUNTY, GEORGIA REFUSED TO DO UTILITIES DATED 7-8-97.

ATTACHMENTS: COST ESTIMATE, TYPICAL SECTIONS, CONCEPT MEETING MINUTES, PROGRAM DOCUMENT, JOINT STATE AGREEMENT, LGPA - CATOOSA COUNTY (UNSIGNED).

PRELIMINARY COST ESTIMATE

PROJECT NUMBER: STP-1120(2) COUNTY: CATOOSA COUNTY, GEORGIA &
HAMILTON COUNTY, TENNESSEE.

DATE: August 6, 1997 ESTIMATED LETTING DATE: July, 1999

PREPARED BY: Willie J. Lewis PROJECT LENGTH (Kilometers): 1.76 km Ga.
& 0.58 km Tenn.

() PROGRAMMING PROCESS (X) CONCEPT DEVELOPMENT () DURING PROJECT DEV.

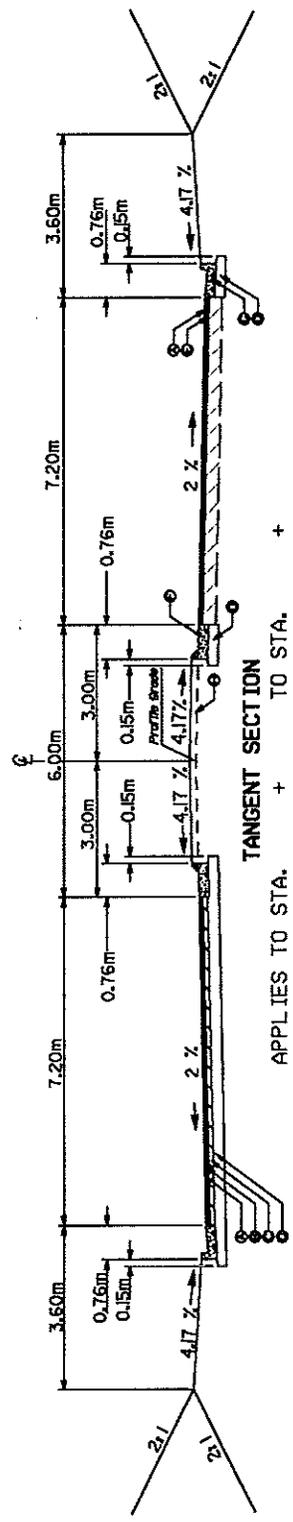
PROJECT COST	
A. RIGHT-OF-WAY:	
1. PROPERTY (LAND & EASEMENT)	\$ 139,250
2. DISPLACEMENTS; RES:1, BUS;6, M.H.:0	\$ 845,300
3. OTHER COST (ADM./COST, INFLATION)	\$ 799,947
SUBTOTAL:A	\$ 1,784,497
B. REIMBURSABLE UTILITIES:	
1. RAILROAD	\$ 0
2. TRANSMISSION LINES	\$ 45,651
3. SERVICES	\$ 0
SUBTOTAL:B	\$ 45,651
C. CONSTRUCTION:	
1. MAJOR STRUCTURES	\$ 0
a. RETAINING WALLS	\$ 0
b. BRIDGES	\$ 0
c. DETOURS BRIDGES	\$ 0
d. BOX CULVERTS	\$ 0
SUBTOTAL:C-1	\$ 0
2. GRADING AND DRAINAGE:	
a. EARTHWORK	\$ 102,275
b. DRAINAGE:	
1) Cross Drain Pipe (include box culverts)	\$ 144,727
2) Curb and Gutter	\$ 221,607
3) Longitudinal System(include catch basins)	\$ 545,578
SUBTOTAL:C-2	\$ 1,014,187

PROJECT COST		
3. BASE AND PAVING:		
a. AGGREGATE BASE		\$ 393,317
b. ASPHALT PAVING: Surface	\$ 146,830	
Binder	\$ 115,045	
Base	\$ 228,108	
SUBTOTAL:C-3.b		\$ 489,983
c. CONCRETE PAVING		\$ 0
d. OTHER b.t.c., leveling, aggr. surface crs		\$ 21,559
SUBTOTAL:C-3		\$ 904,859
4. LUMP ITEMS:		
a. TRAFFIC CONTROL		\$ 69,500
b. CLEARING AND GRUBBING		\$ 52,350
c. LANDSCAPING		\$ 0
d. EROSION CONTROL		\$ 226,502
e. DETOURS		\$ 0
SUBTOTAL:C-4		\$ 348,352
5. MISCELLANEOUS:		
a. LIGHTING		\$ 0
b. SIGNING - STRIPING - SIGNAL		\$ 14,500
c. GUARDRAIL		\$ 14,912
d. CONCRETE WORKS		\$ 120,889
SUBTOTAL:C-5		\$ 150,301
6. SPECIAL FEATURES_R/W MKRS, F.E.O. SUBTOTAL:C-6		\$ 32,936

ESTIMATE SUMMARY		
A. RIGHT-OF-WAY	\$	1,784,497
B. REIMBURSABLE UTILITIES	\$	45,651
C. CONSTRUCTION		
1. MAJOR STRUCTURES	\$	0
2. GRADING AND DRAINAGE	\$	1,014,187
3. BASE AND PAVING	\$	904,859
4. LUMP ITEMS	\$	348,352
5. MISCELLANEOUS	\$	150,301
6. SPECIAL FEATURES	\$	32,936
SUBTOTAL CONSTRUCTION COST	\$	2,450,635
E. & C. (10%)	\$	245,064
INFLATION (5% PER YEAR)	\$	269,570
NUMBER OF YEARS	2	
TOTAL CONSTRUCTION COST	\$	2,965,268
GRAND TOTAL PROJECT COST	\$	4,795,416

TIYPICAL SECTION NO. 1

U.S. 41 / U.S. 76 / S.R. 3

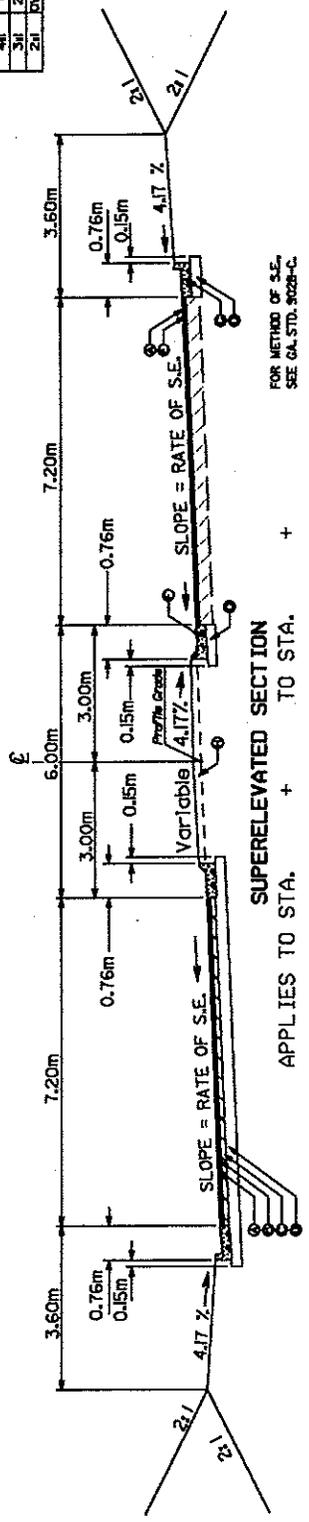


TANGENT SECTION
APPLIES TO STA. + TO STA.

- REQUIRED PAVEMENT
- ① ASPHALTIC CONCRETE "E", 38mm
 - ② ASPHALTIC CONCRETE "B", 51mm
 - ③ ASPHALTIC CONCRETE BASE, 102mm
 - ④ GRADED AGGREGATE BASE, 302mm
 - ⑤ ASPHALTIC CONCRETE LEVELING, AS REQ'D
 - ⑥ 203mm x 765mm CONC. CURB & GUTTER, GA. STD. 9032 B, TYPE 7
 - ⑦ 203mm x 765mm CONC. CURB & GUTTER, GA. STD. 9032 B, TYPE 2
 - ⑧ CONCRETE MEDIAN, 102mm

NOTE: All vegetation (i.e. trees, shrubs, grass, etc.) that is not directly affected by the actual construction limits is not to be disturbed.

SLOPE	CONTROL
4:1	CUT
3:1	FILL
2:1	OVER SHOULDER



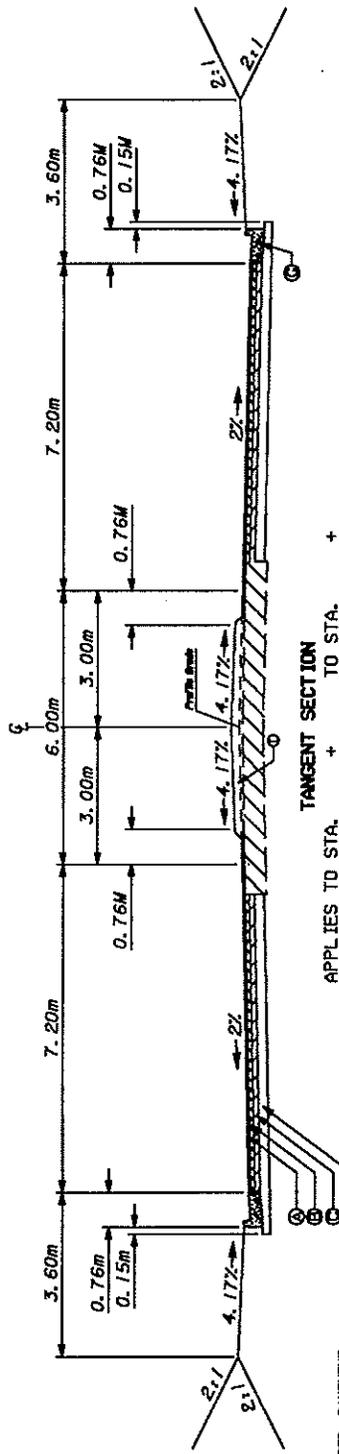
SUPERELEVATED SECTION
APPLIES TO STA. + TO STA.

FOR METHOD OF S.E. SEE GA. STD. 9028-C

TYPICAL SECTION NO. 4

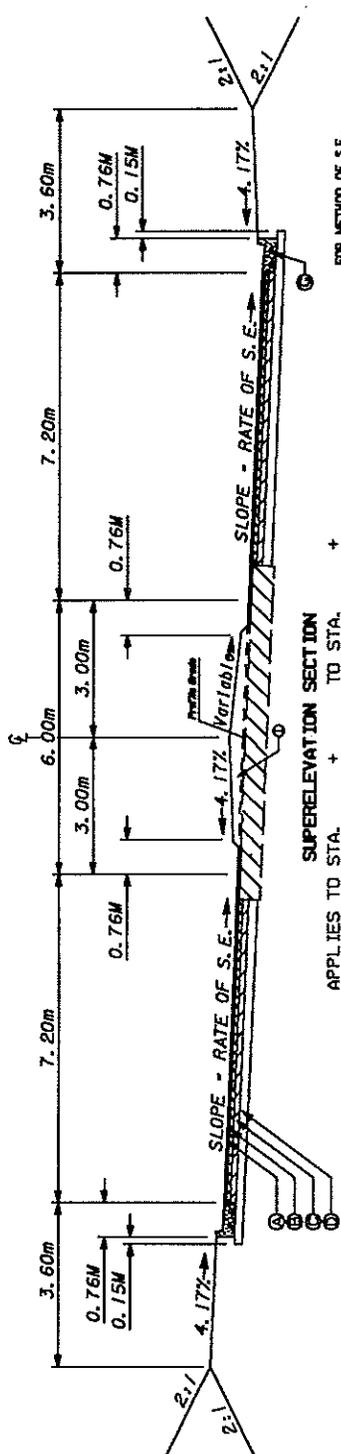
U.S. 41 / U.S. 76 / S.R. 3

PROJECT	DATE
NO. 11207	
GA.	



NOTE: All vegetation (i.e. trees, shrubs, grass, etc.) that is not directly affected by the actual construction limits is not to be disturbed.

- REQUIRED PAVEMENT**
- ① ASPHALTIC CONCRETE 'E', 38mm
 - ② ASPHALTIC CONCRETE 'B', 51mm
 - ③ ASPHALTIC CONCRETE BASE, 102mm
 - ④ GRADED AGGREGATE BASE, 305mm
 - ⑤ ASPHALTIC CONCRETE LEVELING, AS REQ'D
 - ⑥ 200mm x 750mm CONC. CURB & GUTTER, GA. STD. 9032 B, TYPE 2
 - ⑦ CONCRETE MEDIAN, 191mm, GA. STD. 9032-B, TYPE 7 FACE



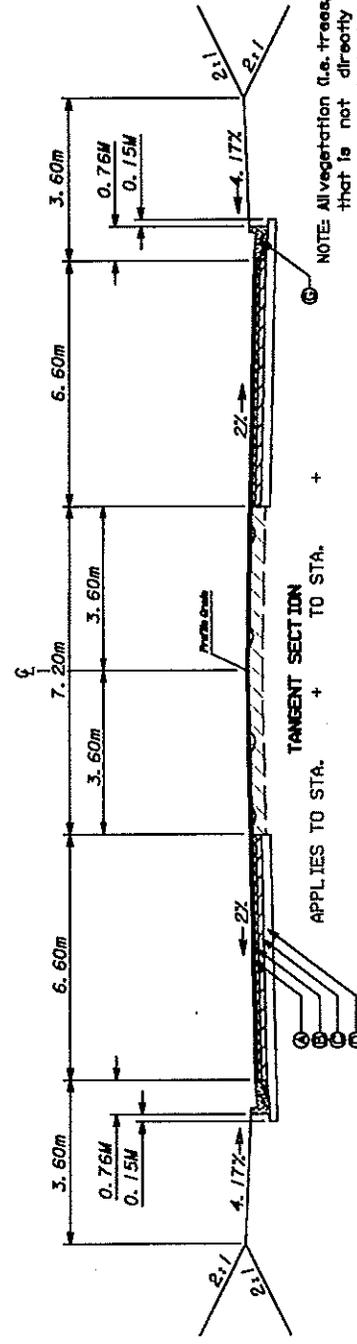
SLOPE CONTROLS	
SLOPE	CONTROL
4:1	0-3m
3:1	3m-5m
2:1	OVER 5m OVER 3m

FOR METHOD OF S.E., SEE GA. STD. 9028-C.



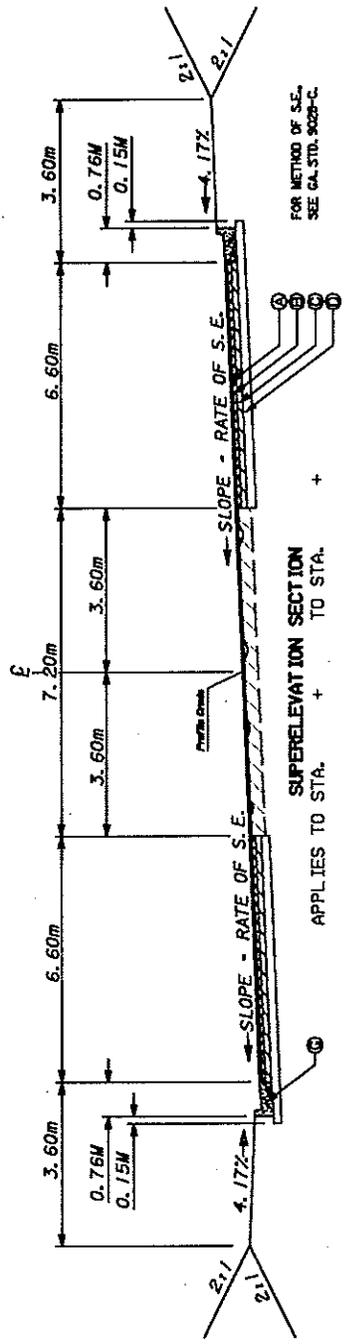
TYPICAL SECTION NO. 5

U.S. 41 / U.S. 76 / S.R. 3



NOTE: All vegetation (i.e. trees, shrubs, grass, etc.) that is not directly affected by the actual construction limits is not to be disturbed.

- REQUIRED PAVEMENT
- Ⓐ ASPHALTIC CONCRETE "E", 38mm
 - Ⓑ ASPHALTIC CONCRETE "B", 81mm
 - Ⓒ ASPHALTIC CONCRETE BASE, 102mm
 - Ⓓ GRADED AGGREGATE BASE, 305mm
 - Ⓔ ASPHALTIC CONCRETE LEVELING, AS REQ'D
 - Ⓕ 203mm X 762mm CONC. CURB & GUTTER, GA. STD. 9032 B, TYPE 2



FOR METHOD OF S.E. SEE GA. STD. 9029-C.

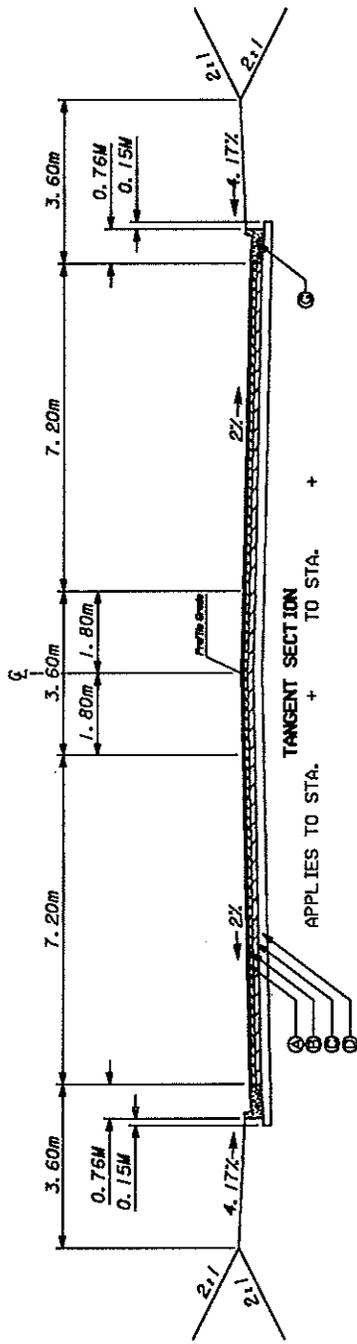
SLOPE CONTROLS	
SLOPE	CONTROL
4:1	CUT
4:1	FILL
2:1	OVER 3M OVER 3M



TYPICAL SECTION NO. 7

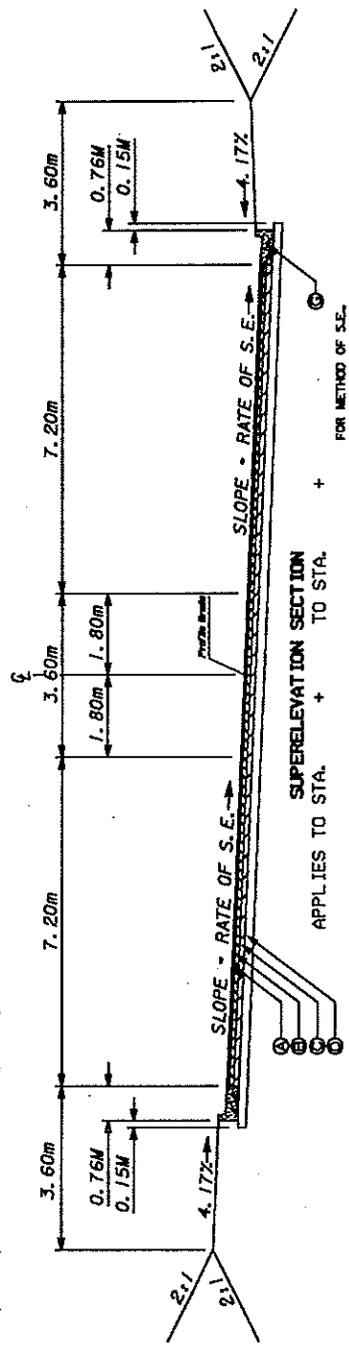
U.S. 41 / U.S. 76 / S.R. 8

GA.	NR-112X2
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- REQUIRED PAVEMENT
- Ⓐ ASPHALTIC CONCRETE "E", 38mm
 - Ⓑ ASPHALTIC CONCRETE "B", 51mm
 - Ⓒ ASPHALTIC CONCRETE BASE, 102mm
 - Ⓓ GRADED AGGREGATE BASE, 305mm
 - Ⓔ ASPHALTIC CONCRETE LEVELING, AS REVD
 - Ⓕ 203mm x 765mm CONC. CURB & GUTTER, CA. STD. 9032-B, TYPE 2
 - Ⓖ CONCRETE MEDIAN, 191mm, CA. STD. 9032-B, TYPE 7 FACE

NOTE: All vegetation (i.e. trees, shrubs, grass, etc.) that is not directly affected by the actual construction limits is not to be disturbed.



SLOPE	CUT	FILL
4:1	0-2m	0-3m
3:1	2m-3m	3m-4m
2:1	OVER 3m	OVER 4m

FOR METHOD OF S.E. SEE CA. STD. 9028-C.



**REVISION REQUEST
FOR THE
CONSTRUCTION WORK PROGRAM**

IN ACCORDANCE WITH THE BOARD RESOLUTION DATED AUGUST 16, 1973,
BOARD APPROVAL IS REQUESTED TO REVISE THE CONSTRUCTION WORK PROGRAM
FOR THE PROJECT AND ACTIVITY OUTLINED BELOW:

- ADDITION TO THE PROGRAM
 - DELETION FROM THE PROGRAM
 - SHIFT IN THE PROGRAM
 - CHANGE IN COST ESTIMATE
 - OTHER
- PE ROW CONST.
 FROM FY _____ TO FY _____

PROJECT DATA

COUNTY	PROJECT No. P.I. No.	TYPE WORK	DESCRIPTION
Catoosa	MR-1120(2) 650430	Widen & Reconstruct (4-lanes)	S.R. 3/U.S. 41: From S.R. 146 (ML 12.91) to the Tennessee State Line (ML 13.57). Length = 0.66 Mile

Fund 1 = W36
Fund 2 = 33C

ESTIMATED COST (\$1,000's)	LOW ROAD	HIGH ROAD	FISCAL YEAR	CONG. DISTRICT	FIELD DISTRICT
PE \$20	X		1992		
ROW \$165	X		1993		
CONST \$655	X		1994	7	6

REASON FOR REVISIONS:

To add this project as requested by the Director of Preconstruction.
This project moves from Preprogramming.

GDOT BOARD

FEB 20 1992

APPROVED

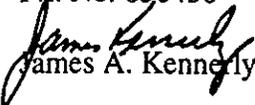
RECOMMENDED *John L. Dade*
DIRECTOR, DIVISION OF PLANNING AND PROGRAMMING

RECOMMENDED *Wayne Shackleton*
COMMISSIONER

**DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA**

INTERDEPARTMENT CORRESPONDENCE

FILE STP-1120(2) Catoosa County, Georgia **DATE** July 16, 1997
Hamilton County, Tennessee
P.I. No. 650430 **OFFICE** Atlanta, Ga.

FROM  James A. Kennerly, State Road and Airport Design Engineer ^{H.A.}

TO See Distribution List Below

SUBJECT Concept Meeting Minutes - U.S. 41/U.S. 76/S.R. 3/S.R. 8

The meeting was held on July 1, 1997 at 10:00 a.m. in the Office of Road and Airport Design Conference Room. Persons in attendance included James Kennerly, Kim Fulbright, Willie Lewis, and Chris Edmondson from the Office of Road and Airport Design; Jim Hullett, Pat Meyers, W. Paul Simmons, Andy Rikard, and Joe Fletcher from District Six; Ken Estes from Traffic Operations; Millard Mackey from Ringgold Telephone Co.; State Representative William Clark; Charles Pierce and Lee Bridges from Catoosa County; Tom McQueen from Office of Planning; Reba Scott from Office of Programming; Allen Maull, from Office of Right of Way; and Lance Irwin from EPB of Chattanooga.

Since this is a road widening project, the Office of Road and Airport Design is the Team Leader on the above project.

This project consists of widening and reconstruction of US 41/US 76/SR 3/SR 8 from 0.62 km (0.39 mile) east of SR 146/Cloud Springs Road in Catoosa County, Georgia and continues northwesterly 1.76 km (1.09 miles) to the Georgia - Tennessee State Line and then continues for 0.58 km (0.36 mile) to approximately 0.15 km (0.09 mile) north of Frawley Road in Hamilton County, Tennessee. The project length is approximately 2.34 km (1.45 miles).

This project would widen SR 3 from a two lane to a four lane facility with a 6.0 m (20 ft.) wide raised median urban section in the State of Georgia and transition to a five lanes 3.66 m (12 ft.) wide urban section in the State of Tennessee. The alignment shifts from one side of the existing roadway to the other to minimize Right of Way impacts, by shifting to undeveloped tracts of land versus developed tracts of land where possible. The proposed widening and addition of the median, along with flattening of curves, is proposed on this project because of high accident rates.

This project also consist of reconstructing the intersections at SR 3 with SR 146, Biggs Road with SR 146, and South Sanctuary Road with SR 3.

AGREEMENT
GEORGIA PROJECT STP-1120(2)
CATOOSA COUNTY, GEORGIA
HAMILTON COUNTY, TENNESSEE

This Agreement made this 2nd day of June, 1994, by and between the Department of Transportation, an agency of the State of Georgia (hereinafter "GDOT"), and the Tennessee Department of Transportation, an agency of the State of Tennessee (hereinafter "TDOT").

WHEREAS, GDOT AND TDOT desire to cooperate in the widening of U.S. 41 from S.R. 146 in Georgia to the existing 4 lane section in Tennessee.

NOW THEREFORE, in consideration of the mutual promises, covenants, responsibilities and obligations set out, GDOT AND TDOT do mutually agree as follows:

1. GDOT and TDOT, acting jointly in the manner hereinafter set forth, will widen and reconstruct U.S.. Highway 41 from S.R. 146 in Catoosa County, Georgia to the existing 4 lane roadway 1425 feet north of the boundary line between Georgia and Tennessee in Hamilton County, Tennessee (Hereinafter called the "Project").

2. The parties hereto will cooperate and participate in the design, construction and maintenance of the PROJECT as hereinafter set forth for each of them respectively.

3. Upon completion, the PROJECT shall be opened to the general public and no toll or other charge shall be levied for its use.

4. Prior to the opening of the PROJECT to traffic, the PROJECT shall be designated within the appropriate State as either a "State Highway" or a "U.S. Highway", or both by each party.

5. Funds used by TDOT and GDOT to meet their responsibilities and obligations under this agreement shall be either state or federal funds made available to each respective party for the construction of roads and bridges.

6. The engineering and preparation of plans will be undertaken as follows;

(a) GDOT will be responsible for the initial determination of the physical location and final establishment of the alignment for the PROJECT. The final

alignment will be established only after both parties agree to the particular location.

(b) GDOT will be responsible for conducting all surveys for the PROJECT in both states. TDOT shall be responsible for ensuring that GDOT, its authorized agents, employees and contractor have requisite authority to enter upon any lands in the State of Tennessee for the purposes of conducting such surveys. GDOT shall also be responsible for preparing rights-of-way plans, construction plans and specifications, advertising for receipt of bids, awarding construction contracts, constructing and providing engineering inspection during construction for the PROJECT in both states.

(c) GDOT will be responsible for preparation of the necessary Environmental documentation (C.E., E.A., or EIS) and the Location and Design Report if required. TDOT will review, provide data for, make comments upon, and approve the Environmental document and the Location and Design Report prior to circulation by GDOT.

(d) The construction plans may be broken into two or more construction contracts as determined by the parties to this agreement.

7. The plans and specifications for the PROJECT shall be subject to approval of both parties to this agreement. GDOT will submit plans, specifications and estimates to the Federal Highway Administration for approval in accordance with the normal practice, if federal funds are utilized. However, GDOT will not submit plans, specifications and estimates, on the PROJECT, to the Federal Highway Administration until those plans and specifications have first been approved by TDOT.

8. After approval of the plans, specifications and preliminary estimates for the PROJECT by the parties hereto, GDOT, will prepare proposal items for the contractors and preliminary estimates and, after the proper advertisement, shall receive bids from qualified bidders on the PROJECT. Bid proposals will not be issued to prospective contractors who are disqualified or debarred from bidding on highway work in the State of Tennessee or in the State of Georgia. No bids will be accepted from any such disqualified or debarred contractor. After the bids are tabulated by GDOT, and approved by TDOT, GDOT shall award and execute a construction contract with the lowest responsible bidder using the unit prices contained in the lowest responsible bid.

9. The construction contracts for the construction of the PROJECT shall be administered by GDOT in accordance with its normal practices and subject to the terms of this

agreement. Funds in payment for construction work will be disbursed by GDOT based upon monthly statements of amount due to the contractor. GDOT agrees to furnish copies of all estimates and monthly statements related to the construction of the PROJECT to TDOT.

10. GDOT shall acquire all necessary right-of-way for the PROJECT located within the State of Georgia and shall clear such rights-of-way of all obstructions and utility facilities which conflict with construction, with all costs and expenses thereof, including costs and expenses of acquisition, to be borne by GDOT. TDOT shall acquire all necessary rights-of-way for the PROJECT located within the State of Tennessee and shall clear such rights-of-way of all obstructions and utility facilities which conflict with construction, with all costs and expenses thereof, including costs and expenses of acquisition, to be borne by TDOT.

11. The cost for the PROJECT shall be borne as follows:

(a) Preliminary Engineering costs and the cost preparatory to the award of the construction contract shall be borne by GDOT.

(b) The cost for roadway approaches in the State of Georgia, including rights-of-way acquisition, utility relocation, construction, and construction supervision and inspection shall be borne totally by GDOT without any participation in such cost and expenses on the part of TDOT.

(c) The cost for roadway approaches in the State of Tennessee including rights-of-way acquisition, utility relocation, construction and construction supervision and inspection shall be borne totally by TDOT without any participation in such cost and expenses by GDOT.

(d) GDOT shall not let and award the construction contract or contracts for the PROJECT until both GDOT and TDOT have funds authorized to cover the estimated cost of said contract or contracts, and that authorization is approved in writing by each state.

(e) TDOT agrees to reimburse GDOT for TDOT'S share of the costs for the construction and construction supervision and inspection. Such reimbursement will be made promptly upon receipt and verification of bills submitted, which bills shall show in reasonable detail, the total construction and construction supervision and inspection costs of the PROJECT during the period covered by the bill. Such reimbursement will be no more often than monthly.

12. The parties agree that upon completion of a final audit of all costs incurred in connection with the construction of this PROJECT, each will promptly pay to, or

reimburse if appropriate, the other any balance due the other under any provision of this agreement.

13. Upon completion of the PROJECT and acceptance of the same from the contractors by all parties concerned, the responsibility for maintenance of the PROJECT will be as follows:

(a) GDOT will maintain at its expense, that portion of the PROJECT which lies within the State of Georgia.

(b) TDOT will maintain at its expense, that portion of the PROJECT which lies within the State of Tennessee.

The covenants herein contained shall, except as otherwise provided, accrue to the benefit of and be binding upon the successors and assigns of the parties hereto.

Whereupon the parties have caused their hands and seals to be set hereon by persons authorized to do so, the date and year first set out above.

RECOMMENDED AS TO GEORGIA

[Signature]
STATE PROGRAMMING ENGINEER

[Signature]
STATE HIGHWAY ENGINEER

STATE OF GEORGIA DEPARTMENT OF TRANSPORTATION

BY: *[Signature]*
COMMISSIONER

STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION

BY: *[Signature]*
Title: Commissioner

ATTEST:

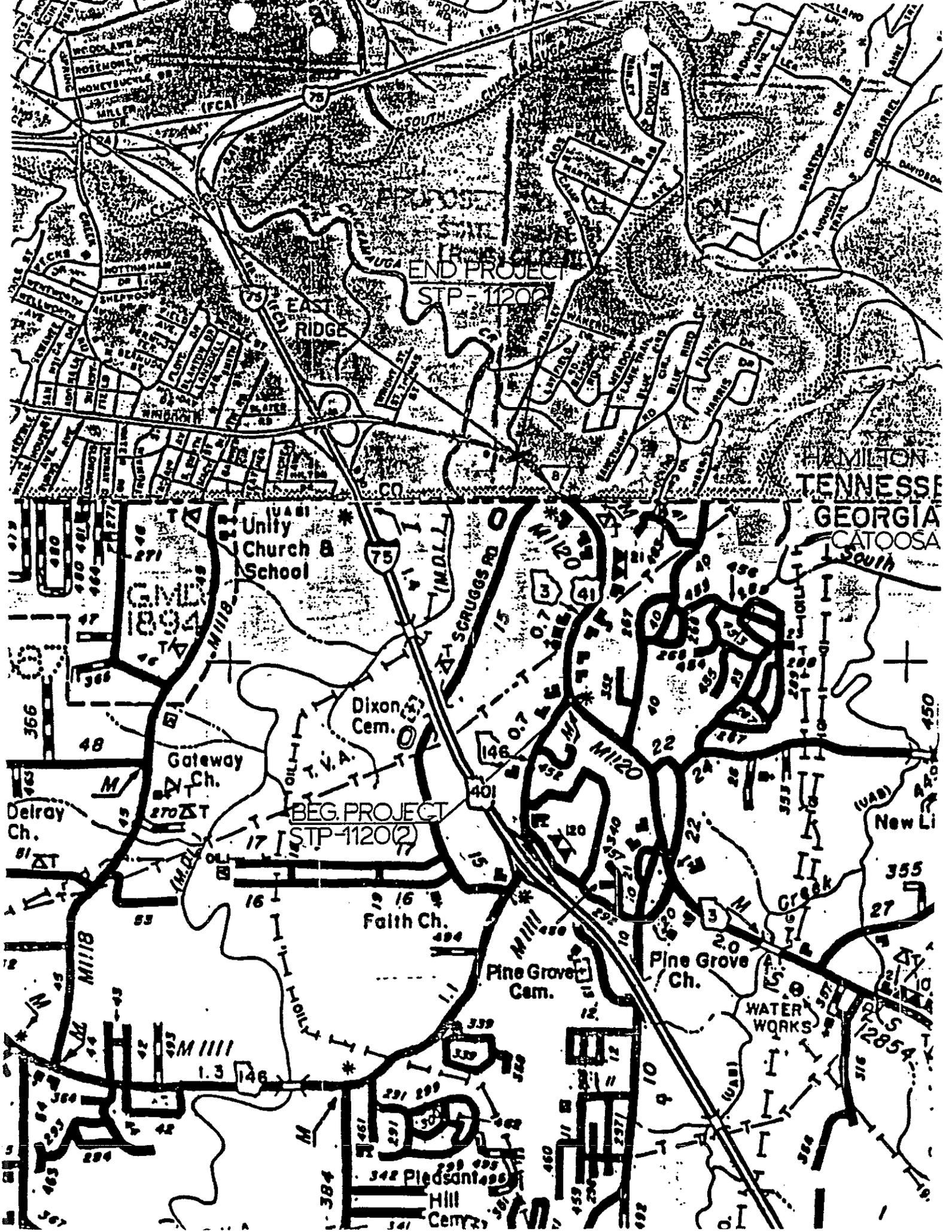
[Signature]
TREASURER

REVIEWED April 13, 1998
(DATE)

[Signature]
LEGAL - TRANSPORTATION

APPROVED AS TO LEGALITY:

BY: *[Signature]*
Henry K. Buckner, Jr.
Title: Department Attorney:



YUGA END PROJECT
STP-1120(2)

BEG. PROJECT
STP-1120(2)

Unity
Church &
School

Dixon
Cem.

Gateway
Ch.

Delray
Ch.

Falth Ch.

Pine Grove
Cam.

Pine Grove
Ch.

WATER
WORKS

Pleasant
Hill
Cem.

TENNESSEE
GEORGIA
CATOOSA

New L...

2854

July 15, 1997

ROUTE TO:

(1) K. FULBRIGHT

This project has an LGPA refusal and it will soon be ready for authorization of the next phase. Please provide your recommendation on this project moving forward to Walker Scott.

(2) WALKER SCOTT:

Please provide me with your recommendation regarding whether this project should move forward in view of the LGPA refusal.

COUNTY: CATOOSA
P.I. NO: 650430
PROJECT NUMBER: STP-1120(2)
DESCRIPTION: SR 3/US 41 FM SR 146 TO TENNESSEE STATE LINE
TYPE WORK: WIDENING

TII PE PROG DATE 9306
TII RW PROG DATE 1998
RW AUTHORIZED
RW UNAUTHORIZED \$1,784,000
TII CONSTRUCTION YEAR: 2000
CONSTRUCTION COST: \$3,000,000.00

LGPA ORGANIZATION: CATOOSA
LGPA REQUESTED DATE: 7/02/97
LGPA SIGNED DATE:
LGPA REFUSED DATE: 7/08/97
PHASES REQUESTED: U
PHASES AGREED:
REMARKS: PREVIOUSLY SENT LGPA 3/2/92,
NO RESPONSE.

COST ESTIMATE DATE: 5/01/97
UTILITY COST: \$499,000.00
LGPA: CATOOSA REF DO UTILITIES 7-8-97. |BI-STATE SGN 6-2-94.
READY TO LET: 99-07
TYPE FUNDING: 33C LOCAL TAX PROJECT?: N
ASSOCIATED PROJECTS: TIP NUMBER: STP-99-1
PRIORITY: DATE ADDED: 2/20/92
STIP CONSTRUCTION YEAR: 1999


Fsc
HERMAN T. GRIFFIN
OFFICE OF PROGRAMMING



Department of Transportation

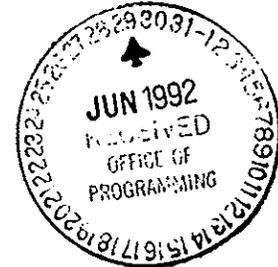
State of Georgia

District Six

Cartersville, Georgia 30120-0010

June 23, 1992

Mr. Jack D. Rowan, Commissioner
Catoosa County
206 Nashville Street
Ringgold, Georgia 30736



Dear Sir:

Subject: Local Government Project Agreement
MR-1120(2) Catoosa County
P.I. No. 650430

According to our records, you were sent a Local Government Project Agreement covering the above project. At that time you were requested to indicate your support of this project by executing the agreement. As of this date the department has not received a response from you and this delay is causing problems in resolving utility, right of way and railroad agreements on this proposed highway project.

In order to meet the scheduled letting date on this project, we need a response from you on your position regarding the Local Government Project Agreement.

We are available to answer any questions or assist you concerning any problems on this project.

Yours very truly,

A handwritten signature in cursive script that reads "Felton D. Rutledge".

Felton D. Rutledge
District Engineer

By: William H. McVey
PreConstruction Engineer
District Six

FDR:WHM:pgd

cc: Herman Griffin



File

WAYNE SHACKELFORD
COMMISSIONER

G. CHARLES LEWIS
STATE HIGHWAY ENGINEER

Department of Transportation
State of Georgia
#2 Capitol Square, S.W.
Atlanta, Georgia 30334-1002

CRAIG C. BRACK
DEPUTY COMMISSIONER

ARTHUR A. VAUGHN
TREASURER

March 2, 1992

Mr. Jack D. Rowan
Commission Chairman, County of CATOOSA
206 East Nashville Street
Ringgold, Ga. 30736

Dear Commission Chairman Rowan:

SUBJECT: LOCAL GOVERNMENT PROJECT AGREEMENT
Project number MR-1120 (2)

The Office of Programming is adding a project to the Department's
Construction Work Program for:

WIDEN & RECONSTRUCT - SR 3
SR 3/US 41 FM SR 146 TO TENNESSEE STATE LINE
CATOOSA County - P.I.NO.650430

As a prerequisite to begin design work, the County of CATOOSA is
requested to indicate support of this project by executing the attached
Local Government Project Agreement and returning it to this Office.

If you have any questions, feel free to contact either this Office or
Mr. Rutledge, District Engineer in Cartersville at your convenience.

Sincerely yours,

Herman T. Griffin, P. E.
State Transportation Programming Engineer

HTG/bl

attachment

cc: Mr. Felton D. Rutledge, Cartersville District





Department of Transportation
 State of Georgia
 #2 Capitol Square, S.W.
 Atlanta, Georgia 30334-1002

WAYNE SHACKELFORD
 COMMISSIONER

G. CHARLES LEWIS
 STATE HIGHWAY ENGINEER

CRAIG C. BRACK
 DEPUTY COMMISSIONER

ARTHUR A. VAUGHN
 TREASURER

March 2, 1992

LOCAL GOVERNMENT PROJECT AGREEMENT

In consideration of the proposed improvements, the County of CATOOSA agrees to provide or perform the following at no cost to the Georgia D.O.T. for project MR-1120(2), PI # 650430, CATOOSA County:

SR 3 - WIDEN & RECONSTRUCT
 SR 3/US 41 FM SR 146 TO TENNESSEE STATE LINE

[] Provide all rights-of-way and/or easements needed for the construction of project and remove existing structures or obstructions within the rights-of-way.

[X] Make all utility relocations, adjustments or betterments of publicly owned utilities that are in conflict with construction of this project. Reimburse Georgia D.O.T. for any damages paid to the contractor for delay of construction caused by a delay in relocating the publicly owned utilities.

[X] Relocate or adjust all privately owned utilities to clear construction of this project, including adjustments at railroad crossings if required.

[] Furnish any detours needed during construction.

This _____ day of _____, 19_____

APPROVED _____
 City/County Official



REQUEST

AT AUTHORITY

CHATTANOOGA, TENN.

TO CHATTANOOGA, TENN.



700 600

85° 10'



CATOOSA COUNTY

SCALE IN MILES

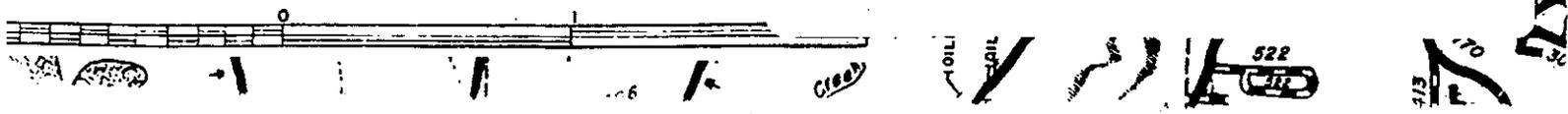
MD 1535

Mr-1120(2), Catoosa County PI #650430

State Route 3/U.S. 41:

71 Widen and reconstruct from State Route 146 to the Tennessee State Line.

33 Length = 0.66 miles



STP-1120(2) Catoosa
July 16, 1997
Page 2

The existing posted speed limit ranges from 35 mph to 55 mph in Georgia and 40 mph in Tennessee. The proposed speed design is 70 km/hr (45 mph).

Through a joint agreement between the Georgia Department of Transportation and the Tennessee Department of Transportation it was decided that GDOT would do the design and that TDOT would pay their part of construction costs..

The construction cost was estimated at \$2,450,735.00 for the above project. This project is presently scheduled for a letting in the fiscal year 2000.

Following a brief description of the project, there was a general discussion of the proposed concept.

The extent of the right of way impacts was a major point of concern. The total number of relocations, all in Georgia and based on the proposed layout, are 6 businesses and 1 residential. During the design phase of the project every effort will be made to minimize right of way impacts while maintaining the integrity of the design and meeting design standards.

A Request was made to extend the raised median to Frawley Road in Tennessee because of the traffic volumes at this location. Tennessee would have to be agreeable. They have already obtained a categorical exclusion for the proposed 5-lane section from the FHWA. Road Design will discuss this with TDOT along with all other relevant issues discussed in the concept team meeting.

Representative Clark wanted to know where the median breaks would be located and about access for people wanting to turn left onto SR 3 where there was not a median opening provided.. Median opening will be provided at public roads meeting the minimum spacing of 200 meters (660 feet) with preference given to high traffic generators. During the plan development the need for additional median openings will be investigated and discussed at the field plan reviews. Anyone wanting to turn left onto SR 3 that is not at a median opening will have to make a U-turn at the next available median opening. There were concerns about the safety of having the make a U-turn, however various studies have shown a marked reduction of accidents where a raised median was provided. Additional widening will be provided at median crossovers to accommodate U-turns.

A project has been programmed to widen SR 146 from I-75 to SR 3. Coordination between these two projects will be required as to the design of the intersection of SR 3 and SR 146. It is anticipated that this intersection will be signalized.

STP-1120(2) Catoosa
July 16, 1997
Page 3

The question of providing sidewalks was raised. No significant pedestrian traffic has been observed along this section of SR 3 nor is any anticipated, therefore sidewalks were not considered. This can be addressed during plan development.

A breakdown of comments from various offices in attendance are as follows:

Utilities: The District has provided a preliminary utility cost estimate. The utility cost was estimated at \$635,418. It was stated that the greatest utility will be relocation of the water main if required, at a cost of \$500,000, Utility impacts will be evaluated during plan development.

Traffic Operations: Ken Estes requested that intersecting roads, specifically SR 146, be redesigned to have a 90 degree skew to SR 3. He also recommended widening the median at intersections to provide separation between left turns and thru traffic. Ken will provide a detail for the wider median at intersections.

Programming: Reba Scott noted that the project designation should be STP and not MR. The MR project designation is the PE project number. Also the project schedule has been changed as follows: right of way from FY 1997 to FY 1998 and construction from FY 1999 to FY 2000. LPGA has been submitted. Bi-state agreement signed in 1994.

Right of Way: A preliminary right of way cost estimate has been provided. The right of way cost was estimated at \$1,784,497.

Environment and Location: UST investigation has not been done. No history is known of at this time. Road Design will provide 5 copies of the concept layouts for environmental investigations.

Planning: A need and purpose statement has been provided.

District Six: No further comments.

Catoosa County: No further comments.

EPB of Chattanooga: No comments.

Road Design: Additional information and layouts will be provided as needed and final concept report will be submitted.

JAK:KDF

STP-1120(2) Catoosa
July 16, 1997
Page 4

cc: Representative William Clark
Frank Danchetz
Walker W. Scott
Toni Dunagan/Tom McQueen
David Studstill/Tom Queen
Bob Mustin
David Meshberger/Allen Maull
Dudley Ellis
Charles Law/Jim Hullett/Paul Simmons/Andy Rikard/Joe Fletcher
Herman Griffin/Reba Scott
Marion Waters/Ken Estes
Wouter Gulden
Steve Henry
Tom Turner
Paul Liles
Larry Dreihaup/Keith Sinclair
Devon Woodworth/Pat Meyers
Lance Irwin
Bill Allen
Lee Bridges
Charles Pierce
Millard Mackey

Department of Transportation
State of Georgia

INTERDEPARTMENTAL CORRESPONDENCE

RECEIVED
SEP 24 1997
PRECONSTRUCTION

File: STP-1120(2)/Catoosa,Ga.-Hamilton,Tenn. Counties Office: Traffic Operations
P.I. No. 650430 Atlanta, Georgia
Date: September 19, 1997

From: *MGW P.M.C.*
M.G. Waters, III, P.E., State Traffic Operations Engineer
To: Wayne Hutto, Assistant Director of Preconstruction
Subject: Project Concept Report Review

We have reviewed the concept report on the above project for the widening and reconstruction of U.S. 41/U.S. 76(S.R. 3/S.R. 8-Tennessee). This project begins south of Highland Road(CR 40), in Catoosa County, Georgia, and extends in a northwesterly direction 2.34 km(1.45 miles) to just north of Frawley Road in Hamilton County, Tennessee.

U.S. 41/U.S.76/S.R. 3 is currently a two lane roadway with 1.2 m shoulders, and a posted speed limit of 90 km/h(55 mph).

This project will widen the road to a four lane facility, with a 6 m raised median, using the urban typical section design, in Georgia. It will then transition to the five lane roadway also with the urban typical section in Tennessee. The design speed will be 70 km/h(45 mph) therefore, an ordinance changing from the current 55mph will be necessary. We believe this concept will improve safety and operational capacity along this section of roadway.

We recommend increasing the median width to 8.4 m (28 ft.) at median openings to allow the left turn lanes to be offset or aligned directly across from each other and increase sight distance for permissive left turn movements. Where no possibility of median openings exist the median could remain the proposed width.

With the recommended statements, we find this report satisfactory for approval.

MGW:TWS

Attachment (signature page)

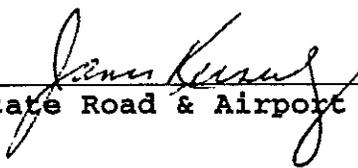
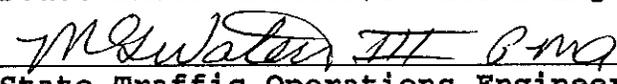
c: David Studstill
James A. Kennerly, Attn. Kim Fulbright
Bob Mustin, w/ attachment
Karl Alff, TMC
Sam Zeigler, TMC
General Files

DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA
OFFICE OF ROAD AND AIRPORT DESIGN

PROJECT CONCEPT REPORT

STP-1120 (2)
RINGGOLD ROAD WIDENING
CATOOSA COUNTY, GEORGIA &
HAMILTON COUNTY, TENNESSEE

FEDERAL ROUTE NO: U.S. 41/U.S. 76 Date of Report: August 6, 1997
STATE ROUTE NO: S.R. 3/GEORGIA & S.R. 8/TENNESSEE.
GADOT P.I. NO: 650430

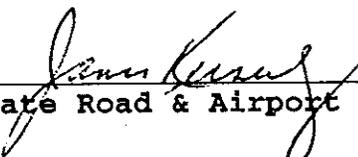
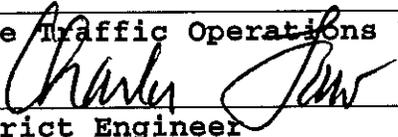
RECOMMENDATION FOR APPROVAL	
<u>8/29/97</u> DATE	 State Road & Airport Design Engineer
<u> </u> DATE	State Environmental/Location Engineer
<u>9-22-97</u> DATE	 State Traffic Operations Engineer
<u> </u> DATE	District Engineer
<u> </u> DATE	Project Review Engineer

DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA
OFFICE OF ROAD AND AIRPORT DESIGN

PROJECT CONCEPT REPORT

STP-1120(2)
RINGGOLD ROAD WIDENING
CATOOSA COUNTY, GEORGIA &
HAMILTON COUNTY, TENNESSEE

FEDERAL ROUTE NO: U.S. 41/U.S. 76 Date of Report: August 6, 1997
STATE ROUTE NO: S.R. 3/GEORGIA & S.R. 8/TENNESSEE.
GADOT P.I. NO: 650430

RECOMMENDATION FOR APPROVAL	
<u>8/29/97</u> DATE	<u></u> State Road & Airport Design Engineer
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