

D.O.T. 66

**DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA**

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**INTERDEPARTMENT CORRESPONDENCE**

**FILE** STP-012-1(71) Bartow County **OFFICE** Preconstruction  
P. I. No. 621350  
*CWH* **DATE** February 11, 2000  
**FROM** C. Wayne Hutto, Assistant Director of Preconstruction  
**TO** SEE DISTRIBUTION

**SUBJECT PROJECT CONCEPT REPORT APPROVAL**

Attached for your files is the approval for subject project.

CWH/cj

Attachment

**DISTRIBUTION:**

Tom Turner  
David Mulling  
David Studstill (ATTN: Harvey Keeper)  
Jerry Hobbs  
Herman Griffin  
Georgene Geary (ATTN: Michael Henry)  
Marion Waters  
Marta Rosen  
Paul Liles  
Don Mills  
Charles Law

**DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA**

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**INTERDEPARTMENT CORRESPONDENCE**

**FILE** STP-012-1(71) Bartow County **OFFICE** Preconstruction  
P.I. No. 621350

**DATE** January 20, 2000

**FROM**   
Thomas L. Turner, P.E., Director of Preconstruction

**TO** Wayne Shackelford, Commissioner

**SUBJECT** PROJECT CONCEPT REPORT

This project is the widening, reconstruction and relocation of SR 20 from I-75 to US 411/SR 61 in Bartow County. The project will also include the reconstruction of the existing interchange at SR 61 and US 411/SR 3. The SR 20/SR 3/SR 61 interchange is of substandard design based on today's design guidelines. This contributes to congestion and confusion at the convergence point of three of the heavily traveled routes in Bartow County. This portion of SR 20 is a major access point to I-75 for both the SR 20 and SR 3 corridors. Traffic volumes on SR 20 in the project area are expected to increase from 33,190 VPD (2003) to 49,130 VPD (2023). Currently the projected land use for the area is commercial, office and light industry. A planned community college is proposed to be built on the north side of SR 20 east of Market Place Boulevard. Capacity analysis of the existing conditions for the intersections of SR 20/SR 61, SR 20 ramp from US 41, and SR 20/Market Place Boulevard indicated that the Level of Service (LOS) in the PM peak would operate at and inadequate LOS F. In addition, accident history indicated a substantial increase from 1995 to 1996 for both the accident rate (+60%) and injury rate (+197%).

The proposed construction will widen SR 20 to four lanes with a 44' median, and US 411/SR 61 will be widened to four lanes with a 20' raised median within the limits of its reconstruction. A portion of SR 20 will be relocated (4,000'±) to intersect SR 61 approximately 700' north of the existing intersection of SR 61 and CR 629. County Road 629 will be realigned to tie into SR 61 directly across from the relocated SR 20. Construction of SR 20 will begin at the relocated SR 20, SR 61, and CR 529 intersection and end at the existing four lane at I-75.

The existing interchange at SR 61 and US 41 will be reconstructed using a partial diamond, partial clover leaf interchange design. This reconstruction includes the addition of a loop ramp for northbound SR 61 traffic to northbound US 41/SR 3 without traffic interfering with southbound SR 61 traffic to US 41/SR 3 northbound. Some relocation of all ramps will be required. The existing bridges over SR 61 will be replaced and the existing bridges on SR 3 over the CSX Railroad will be widened. Traffic signals will be required at the ramps north and south of US 41 on SR 61 and at CR 629. The existing median break on US 41 at Sta. 185+30 will be

Wayne Shackelford

Page 2

STP-012-1(71) Bartow

January 20, 2000

closed because of its proximity to the entrance and exit ramps at the interchange. A frontage road will be constructed along the north side of US 41 in this area, with access at a new median break approximately 0.25 mile west of the existing opening.

This report constitutes a revision of the original concept approved in March, 1993.

Environmental concerns include requiring a COE 404 Permit; a Categorical Exclusion will be prepared; seven (7) displacements - 5 residences and 2 businesses; a public hearing will be held; time saving procedures are not appropriate.

The estimated costs for this project are:

	<u>PROPOSED</u>	<u>APPROVED</u>	<u>PROG DATE</u>	<u>LET DATE</u>
Construction (includes E&C and inflation)*	\$19,751,000	\$5,755,000	2001	01-02
Right-of-Way*	\$ 3,456,000	\$2,533,000		
Utilities*	\$ 630,000	-----		

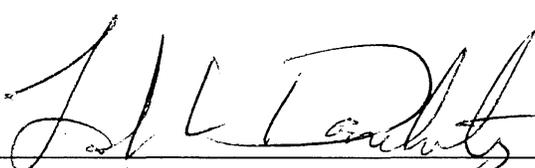
\*Bartow County signed contract on 5-27-98 for PE, right-of-way, and utilities costs not to exceed \$2.8 million.

This project is in the STIP. I recommend this project concept be approved.

TLT:JDQ/cj

Attachment

CONCUR

  
\_\_\_\_\_  
Frank L. Danchetz, P.E., Chief Engineer

APPROVE

  
\_\_\_\_\_  
Wayne Shackelford, Commissioner

**DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA**

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**INTERDEPARTMENTAL CORRESPONDENCE**

**FILE:** STP-012-1(71) Bartow  
P.I. Number 621350

**OFFICE:** Atlanta, Georgia

**DATE:** January 5, 2000

**FROM:** David Mulling, Project Review Engineer *DM*

**TO:** Wayne Hutto, Assistant Director of Pre-construction

**SUBJECT: CONCEPT REPORT**



We have reviewed the revised concept report submitted December 30, 1999 by the letter from James A. Kennerly dated December 29, 1999, and have the following comments:

1. Alternates 1 and 2, and the recommended alternate, Alternate 4, all reference a 20 foot raised median for State Route 20. This conflicts with the 44 foot median shown for State Route 20 in the project description on Page 3, in the proposed roadway section on Page 5, in the typical sections, and on the plan view layouts.

The costs for the project are:

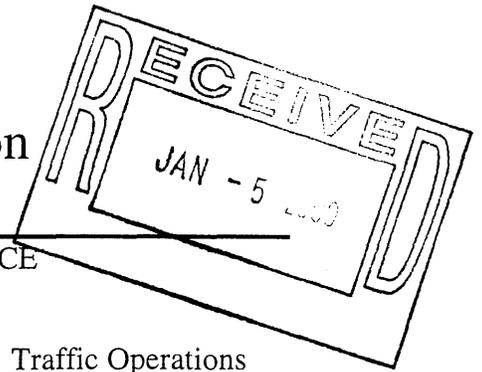
Construction	\$17,100,000
Inflation	\$ 855,000
E&C	\$ 1,796,000
Reimbursable Utilities	\$ 630,000
Right of Way	\$ 3,456,000

DTM

c: Jim Kennerly

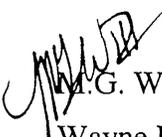
Department of Transportation  
State of Georgia

INTERDEPARTMENTAL CORRESPONDENCE



File: STP-012-1(71)/Bartow County  
P.I. No. 621350

Office: Traffic Operations  
Atlanta, Georgia  
Date: January 3, 2000

From:  M.G. Waters, III, P.E., State Traffic Operations Engineer  
To: Wayne Hutto, Assistant Director of Preconstruction

Subject: Project Concept Report Review

We have reviewed the revised concept report on the above project for the widening of SR 20 and the improvements to the SR 61 interchange at US 41/SR 3. The SR 20 widening will begin at I-75 extending to the US 411/SR 61 intersection, a distance of approximately 2.47 miles. The original concept was approved in March of 1993. This revision includes the addition of a loop ramp for northbound SR 61 traffic to northbound US 41/SR 3 without traffic interfering with the southbound SR 61 traffic to US 41/SR 3 northbound.

SR 20 will be widened from a two lane roadway to a four lane roadway, two lanes in each direction, separated by a 44 foot depressed median. The design speed will be 55mph from I-75 to Market Place Blvd., and 45mph from Market Place Blvd. to US 411/SR 61. US 411/SR 61 will be widened to four lanes with a 20 foot raised median, within the limits of it's reconstruction. We recommend increasing the median width to 28 feet at median openings to allow the left turn lanes to be offset or aligned directly across from each other and increase sight distance for permissive left turn movements. Traffic is to be maintained on the existing roadways during construction.

We also recommend right turn deceleration lanes be included at intersecting roadways as required by MOG 6638-1, dated July 2,1999.

We request that conduit be installed within the limits of this project as part of this project. The conduit would be used for the future interconnection of the Advanced Transportation Management System components in this area. Our Traffic Operations Design Office can provide details and cost estimates for inclusion in the project.

We believe this concept will improve safety and traffic operations along this section of roadway.

With the recommended statements, we find this report satisfactory for approval.

MGW:TWS

Attachment (signature page)

c: David Studstill

James A. Kennerly, State Road and Airport Design Engineer

Attn.: Kim Fulbright

David Mulling, w/ attachment

Marta Rosen

Charles Law, P.E., District Engineer, Cartersville

Attn.: Joe Fletcher

Chuck Hasty, TMC

Keith Golden, P.E., TMC

Paul Liles, State Bridge Design Engineer

General Files

DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA  
OFFICE OF ROAD AND AIRPORT DESIGN

**REVISED**  
**PROJECT CONCEPT REPORT**

**STATE ROUTE 20**

**STP-012-1(71)**  
**P.I. NO. 621350**  
**BARTOW COUNTY**

FEDERAL ROUTE NO: F 12-1  
STATE ROUTE NO: 20, 61, 3  
GADOT P.I. NO: 621350

Date of Report: DECEMBER 20, 1999

**RECOMMENDATION FOR APPROVAL**

\_\_\_\_\_  
DATE

\_\_\_\_\_  
State Transportation Planning Administrator

\_\_\_\_\_  
DATE

\_\_\_\_\_  
State Programming Engineer

*12/29/99*

\_\_\_\_\_  
DATE

*James Kennedy*  
\_\_\_\_\_  
State Road and Airport Design Engineer

\_\_\_\_\_  
DATE

\_\_\_\_\_  
District Engineer

\_\_\_\_\_  
DATE

\_\_\_\_\_  
Project Review Engineer

\_\_\_\_\_  
DATE

\_\_\_\_\_  
State Traffic Operations Engineer

\_\_\_\_\_  
DATE

\_\_\_\_\_  
State Bridge & Structural Design Engineer

\_\_\_\_\_  
DATE

\_\_\_\_\_  
State Environmental/Location Engineer



**PROJECT NUMBER: STP-012-1(71) Bartow County**

<b>PROJECT LOCATION &amp; DESCRIPTION</b>
<p>This project encompasses the widening of SR 20 from 2 lanes to 4 lanes with a 44 ft. depressed median and widening SR 61 to provide 4 through lanes with turn lanes to accommodate projected turning movements. It also includes the relocation of the SR 20 intersection with SR 61 and improvements to the SR 61 and SR 3/US 41 interchange in Bartow County.</p>
<p>PROJECT LENGTH: 2.47 Miles</p>

<b>TRAFFIC</b>				
<b>CURRENT</b>		<b>PROJECTED</b>		
<u>YEAR</u>	<u>AADT</u>	<u>YEAR</u>	<u>AADT</u>	
2003	33190	2023	49130	

<b>ACCIDENT HISTORY</b>			
<u>YEAR</u>	<u>Accident Rate</u>	<u>Injury Rate</u>	<u>Fatality Rate</u>
1995	222 (661)	94 (316)	0 (1.59)
1996	354 (671)	279 (319)	0 (1.56)
1997	326 (N/A)	264 (N/A)	0 (N/A)

Note: All rates are per 100 million vehicle miles of travel. Numbers in parentheses are statewide average rates.

<b>PDP CLASSIFICATION</b>	<b>FUNCTIONAL CLASSIFICATION</b>
MAJOR	PRINCIPAL ARTERIAL

FOS ( )	EXEMPT ( X )	SF ( )
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## PROJECT NEED & PURPOSE

The proposed project is to provide a multi-lane facility by widening and relocating the existing SR 20, to four lanes from I-75 to US 411/SR61 in Bartow County. The project would also involve the reconstruction of the existing interchange at SR 61 and US 411/SR 3. This portion of SR 20 is a major access point to I-75 for both the SR 20 and SR 3 Corridors.

Traffic volumes on SR 20 in the project area are expected to increase from 33,190 AADT (2003) average annual daily traffic to 49,130 (2023). Currently, the projected land use for the area is Commercial, office and light industry. A planned community college is projected to be built on the north side of State route 20 east of Market Place Boulevard. Property owned by the Georgia Board of Regents adjoins the proposed widening of SR 20 to the north and south. Low density housing is projected for an area north SR 20 and east of Cline-Smith Road.

Capacity analysis of the existing conditions for the intersections of SR 20/SR 61, SR 20 ramp from US 41, and SR 20/Market Place Boulevard, concluded that the level of service in the PM peak would operate at an inadequate level of service (LOS=F). This level would create delays exceeding the threshold set for that level of service.

The 2023 Level of Service for this project is 'D'. To maintain a LOS D, the Northern Arc would have to be built and accommodate a percentage of the proposed traffic for SR 20. If the outer perimeter is not constructed, an additional lane in each direction for SR 20, from I-75 to SR 61, would have to be constructed to insure a level of service "D. This is documented in a report by Day-Wilburn Associates for Bartow County. The report is available for review.

Accident history shows that the accident rate witnessed a substantial increase from 1995 to 1996 for both the accident rate (+60%) and the injury rate (+197 %), although the arterial is well below the state average for a principal arterial. There was no state average for 1997.

From a 1985 Origin and Destination Survey, it was discovered that over 60% of all through traffic surveyed in the SR 20 Corridor west of SR 3 utilized the segment of SR 20 east of SR 61 to enter or exit the area. Also 50% of all trucks interviewed either entered or exited the area via this segment of SR 20 and passed through the interchange at SR 61. The SR 20/SR 3/SR 61 interchange is of substandard design when compared to today's design guidelines. This contributes to congestion and confusion at the convergence point of three of the most heavily traveled routes in Bartow County.

This project has been included in the Bartow Countywide Transportation Study for the City of Cartersville and Bartow County. The Study will be used to identify future transportation facility needs and their estimated costs. All transportation modes including roads, aviation, transit, rail, bike and pedestrian facilities will be addressed in this study. This study is a joint project between the City of Cartersville and Bartow County. Geographically, the study also includes the towns of Emerson, Euharlee, Adairsville and Kingston. Construction of this project would alleviate the congestion at this vital intersection in Bartow County and improve capacity and safety for the traveling public along this corridor.

**EXISTING ROADWAY**

TYPICAL SECTION: 2-12' lanes with ± 8 ft. shoulders from SR 61 to County Road 227

R/W WIDTH: Variable - 100' to 200'

POSTED SPEED	MAX DEGREE OF CURVE	MAXIMUM GRADE
<u>45 to 55 mph</u>	<u>6° - 0'</u>	<u>4.5%</u>

MAJOR STRUCTURES:

1. 27.8' x 118' Bridge - N. SR 3 over SR 61, 1992 Sufficiency Rating = 76.3
2. 27.8' x 118' Bridge - S. SR 3 over SR 61, 1992 Sufficiency Rating = 76.3
3. 28.0' x 142' Bridge - N. SR 3 over R.R., 1992 Sufficiency Rating = 75.3
4. 28.0' x 142' Bridge - N. SR 3 over R.R., 1992 Sufficiency Rating = 58.8
5. 28.0' x 152' Bridge - N. SR 3 over Pettit Creek, 1992 Sufficiency Rating = 77.9

**PROPOSED ROADWAY**

TYPICAL SECTION: The basic typical section for SR 20 consists of 2-12 ft. lanes in each direction separated by a 44 ft. depressed median with rural shoulders between I-75 and Market Place Blvd. and urban shoulders between Market Place Blvd. and SR 61. SR 61 will include 2-12 ft. lanes in each direction separated by a variable width median and urban shoulders. Additional lanes will be provided at intersections to accommodate turning movements.

LOCATION	DESIGN SPEED	MAX DEGREE OF CURVE	MAX GRADE
SR 20 from I-75 to Market Place Blvd. US 41 from SR 20 to Massell Dr.	<u>55 mph</u>	<u>4° - 30'</u>	<u>4.5%</u>
SR 20 From Market Pl. Blvd. to SR 61 SR 61 From US 41 to Relocated SR 20	<u>45 mph</u>	<u>8° - 00'</u>	<u>5%</u>

**PROPOSED ROADWAY**

MAJOR STRUCTURES:

1. Existing Bridge - N. SR 3 over SR 61, Remove and replace
2. Existing Bridge - S. SR 3 over SR 61, Remove and replace
3. Existing Bridge - N. SR 3 over R.R., Widen to allow for 3 lanes and tapers on SR 3 northbound
4. Existing Bridge - S. SR 3 over R.R., Widen to allow for 3 lanes and tapers on SR 3 southbound
5. Existing Bridge - S. SR 3 over Pettit Cr., Widen to allow for 3 lanes and tapers on SR 3 northbound

**PROPOSED RIGHT OF WAY**

R/W WIDTH: Variable - 140' to 200'

DISPLACEMENTS

RES: 5      BUS: 2      M.H.: 0

TYPE OF ACCESS CONTROL: By Permit

NUMBER OF PARCELS: 66

**COORDINATION**

CONCEPT TEAM MEETING DATE: April 19, 1991    SUPPLEMENTAL: June 3, 1999

CONFORMS TO TIP/STIP: Yes

MEETS LOGICAL TERMINI REQUIREMENTS: Yes

P.A.R. MEETING: To Be Determined

LOCATION INSPECTION DATE: None

PERMITS REQUIRED (4f,COE,404,etc.): Nationwide Permit (under current requirements)

LEVEL OF PUBLIC INVOLVEMENT: :    Public Hearing

TIME SAVING PROCEDURES APPROPRIATE: No

OTHER PROJECT IN THE AREA: STP 018-1(51) Bartow County

**SCHEDULING CONSIDERATIONS**

TIME TO COMPLETE ENVIRONMENTAL:	6 MONTHS
TIME TO COMPLETE PRELIMINARY RD/RW PLANS:	8 MONTHS
TIME TO COMPLETE 404 PERMIT:	6 MONTHS
TIME TO COMPLETE FINAL CONSTRUCTION PLANS:	10 MONTHS
TIME TO BUY RIGHTS-OF-WAY:	12 MONTHS

**MISCELLANEOUS**

TRAFFIC CONTROL DURING CONSTRUCTION: Traffic to be maintained on existing roadways during construction.

LEVEL OF ENVIRONMENTAL ANALYSIS: Categorical Exclusion Anticipated

DESIGN VARIATIONS REQUIRED:

	YES	NO	UNDETERMINED
SUBST HORIZ ALIGNMENT	( )	(X)	( )
SUBST ROADWAY WIDTH	( )	(X)	( )
SUBST SHOULDER WIDTH	( )	(X)	( )
SUBST VERT GRADES	( )	(X)	( )
SUBST CROSS SLOPES	( )	(X)	( )
SUBST STOPPING SIGHT DIST	( )	(X)	( )
SUBST SUPERELEV RATES	( )	(X)	( )
SUBST HORIZ CLEARANCE	( )	(X)	( )
SUBST SPEED DESIGN	( )	(X)	( )
SUBST VERTICAL CLEARANCE	( )	(X)	( )
SUBST BRIDGE WIDTH	( )	(X)	( )
SUBST BR STRUCT CAPACITY	( )	(X)	( )

UNDERGROUND STORAGE TANKS: To be determined by survey

HAZARDOUS SITES: To be determined by survey

**ALTERNATIVES CONSIDERED**

1. Widen SR 20 to four lanes, with a <sup>44'</sup> ~~20 ft. raised~~ <sup>del.</sup> median, using existing lanes as eastbound lanes. Begin construction on SR 20 at the intersection of SR 61 and end construction at existing four-lane section at I-75. Reconstruct the interchange at SR 20 and US 41 using a partial diamond design, with a short section of SR 20 relocated to intersect with the westbound ramp from SR 61 to US 41. There would be a stop condition at the intersection of the northbound off ramp of US 41 and SR 20. Traffic signals would be required at the ramps north and south of US 41 on SR 61. Some relocation of all ramps would be required. Relocate the intersection of SR 20 and County Road 227 for a 90° intersection.

Replace the existing bridges over SR 61 to allow for four lanes on SR 61. Widen the existing bridges on SR 3 over the CSX Railroad for three lanes on SR 3 north and south bound.

Not selected since obliteration of the loop ramps would cause more congestion.

2. Widen SR 20 to four lanes, with a <sup>44'</sup> ~~20 ft. raised~~ <sup>del.</sup> median, using existing lanes as eastbound lanes. Construct two lanes and raised median north of the existing lanes. Relocate a section of SR 20 (4000 ft. ±) to intersect SR 61 approximately 700 ft. north of the existing location at the intersection of SR 61 and County Road 629. Construction on SR 20 would begin at the intersection of the relocated SR 20, SR 61 and County Road 629 and end at the existing four-lane section at I-75. Reconstruct the interchange at SR 20 and US 41 using a diamond interchange design. Some relocation of all ramps would be required. Traffic signals would be required at the ramps north and south of US 41 on SR 61 and at County Road 629. Relocate the intersection of SR 20 and County Road 227 for a 90° intersection.

Replace the existing bridges over SR 61 to allow for four lanes on SR 61. Widen the existing bridges on SR 3 over the CSX Railroad for three lanes on SR 3 north and south bound.

Not selected since obliteration of the loop ramps would cause more congestion.

3. Relocate State Route 20 south of it's present location, connecting it to US 41/SR 3 running east and west through a reconstructed tri-level interchange at SR 20 and US. 411/SR 61. This would introduce a loop ramp in the southwest quadrant, relocate the existing entrance ramps in the northwest and southeast quadrants, and eliminating the loop ramp in the northeast quadrant, replacing it with a westbound off ramp from US 41 to US 411.

This alternate was not chosen because of the high cost for construction, inadequate access to the General Hospital, and other operational problems.

**ALTERNATIVES CONSIDERED**

- ~~120'~~  
44'
4. Widen SR 20 to four lanes, with a ~~20 ft.~~ raised median, using existing lanes as eastbound lanes. Construct two lanes with a raised median north of the existing SR 20. Relocate a section of SR 20 (4000 ft. ±) to intersect SR 61 approximately 700 ft. north of the existing location at the intersection of SR 61 and County Road 629. Construction on SR 20 would begin at the intersection of the relocated SR 20, SR 61 and County Road 629 and end at the existing four-lane section at I-75. Relocate the intersection of SR 20 and County Road 227 for a 90° intersection. Reconstruct the interchange at SR 20 and US 41 using a partial diamond, partial cloverleaf interchange design. Some relocation of all ramps would be required. Traffic signals would be required at the ramps north and south of US 41 on SR 61 and at County Road 629.

Replace the existing bridges over SR 61 to allow for six lanes on SR 61. Widen the existing bridges on SR 3 over the CSX Railroad for three lanes on SR 3 north and south bound.

Revised to provide a loop ramp in the northeast quadrant of the interchange to adequately handle all traffic movements. This is the recommended alternate.

**COMMENTS:** This revision to the approved concept includes addition of a loop ramp to carry northbound SR 61 traffic to US 41/SR 3 northbound without interfering with southbound SR 61 traffic to US 41/SR 3 northbound traffic. In order to preclude an increase of travel time for emergency vehicles traveling to the hospital, the existing median opening from US 41/SR 3 is to be retained. The US 41 westbound bridge over Pettit Creek would be widened to provide for an additional northbound lane.

**Additional Required Right of Way:** In order to properly reconstruct the existing Westbound entrance ramp from U.S. 411/S.R. 61, controlled access to the north of the ramp is necessary. The two lane improved westbound entrance ramp, which requires approximately 3100' for tapers and signing, extends beyond the existing nine drives along U.S. 41 and west of the bridge at Pettit Creek.

Three alternates have been introduced to accommodate the Westbound ramp and are as follows:

1. **Proposed Right of way with Limited access rights ( No frontage road connector).** This alternate would require that limited access be acquired from Pettit Creek to the CSX Railroad with no access to U. S. 41 from any of the property on the north side of the ramp. **Cost \$4,781,000.00**
2. **Proposed Right of way with Access at Median Break (sta. 171+90):** This alternate also requires limited access along the proposed right of way , but would permit access from a local street at the median opening at Sta. 171+90. **Cost: \$2,263,750.00**
3. **Proposed Right of Way with Access Road:** This alternate introduces a frontage road along the proposed right of way with access at the median opening (171+90). This is the preferred alternate. **Cost : \$1,295,150.00 \***

- \*This cost includes the cost of construction for the proposed frontage road.

It is recommended that additional right of way, along with limited access, be acquired for controlled access along the north side of the improved westbound ramp from US 411. In doing so, access from the north side will be limited at the median break ( sta. 171+90). The existing median opening at sta. 185+30 will be closed because the location falls within the function boundaries of the entrance and exit ramps along US 41. Closing the median opening is also necessary to eliminate turning movements to the north side of US 41. The proposed frontage road, access point, and proposed right of way are shown on the attached plan view layout.

ESTIMATED COST			
CONSTRUCTION:	\$17,103,000	RIGHT-OF-WAY:	\$3,455,713
E & C (10) :	\$1,710,300	ACQUIRED BY :	GDOT
INFLATION :	\$855,000	UTILITIES :	\$,630,324
SUB-TOTAL	\$ 19,668,450	ADJUSTED BY :	LGPA
TOTAL CONSTRUCTION COST:		\$23,754,487	

ATTACHMENTS:

- Need and Purpose Statement
- Detailed Cost Estimate
- Typical Sections
- Traffic Data
- Concept Team Meeting Minutes
- LGPA
- Programming Documents

**PRELIMINARY COST ESTIMATE**

PROJECT NUMBER: STP-012-1(71)

COUNTY: BARTOW

DATE: FEBRUARY 1, 1999

ESTIMATED LETTING DATE: 2001

PREPARED BY: EDWARD L. BRAGG, P.E.

PROJECT LENGTH: 3.0 MI. ±

( )PROGRAMMING PROCESS (X)CONCEPT DEVELOPMENT ( )DURING PROJECT DEV.

<b>PROJECT COST</b>	
<b>A. RIGHT-TO-WAY:</b>	
1. PROPERTY (LAND & EASEMENT)	\$ 640,250
2. DISPLACEMENTS; RES:5, BUS;5, M.H.:0	\$ 1,266,350
3. OTHER COST (ADM./COST, INFLATION)	\$ 1,549,113
SUBTOTAL:A	\$ 3,455,713
<b>B. REIMBURSABLE UTILITIES:</b>	
1. RAILROAD	\$ 0
2. TRANSMISSION LINES	\$ 30,000
3. SERVICES	\$ 600,324
SUBTOTAL:B	\$ 630,324
<b>C. CONSTRUCTION:</b>	
1. MAJOR STRUCTURES	\$
a. OVERPASSES - SR 61	\$ 2,520,000
b. OTHER - CSX RAILROAD	\$ 1,750,000
SUBTOTAL:C-1	\$ 4,270,000
2. GRADING AND DRAINAGE:	
a. EARTHWORK	\$ 2,570,000
b. DRAINAGE:	\$
1) Cross Drain Pipe	\$ 143,000
2) Curb and Gutter	\$ 282,000

<b>PROJECT COST</b>		
3) Longitudinal System(include catch basins)		\$ 655,000
SUBTOTAL:C-2		\$ 3,650,000
3. BASE AND PAVING:		
a. AGGREGATE BASE		\$ 2,625,000
b. ASPHALT PAVING: Surface	\$ 863,000	
Binder	\$ 710,000	
Base	\$1,699,000	
SUBTOTAL:C-3.b		\$ 3,272,000
c. CONCRETE PAVING		\$
d. OTHER		\$ 90,000
SUBTOTAL:C-3		\$ 6,042,000
4. LUMP ITEMS:		
a. GRASSING		\$ 218,000
b. CLEARING AND GRUBBING		\$ 1,269,000
c. LANDSCAPING		\$
d. EROSION CONTROL		\$ 223,000
e. TRAFFIC CONTROL		\$ 496,000
SUBTOTAL:C-4		\$ 2,206,000
5. MISCELLANEOUS:		
a. LIGHTING		\$
b. SIGNING - MARKING		\$ 125,000
c. GUARDRAIL – MODIFY END OF BRIDGE AND HANDRAIL		\$ 56,000
d. SIDEWALK		\$ 250,000
SUBTOTAL:C-5		\$ 431,000
6. SPECIAL FEATURES	SUBTOTAL:C-6	\$ 504,000

<b>ESTIMATE SUMMARY</b>		
A. RIGHT-OF-WAY	\$	3,455,713
B. REIMBURSABLE UTILITIES	\$	630,324
C. CONSTRUCTION		
1. MAJOR STRUCTURES	\$	4,270,000
2. GRADING AND DRAINAGE	\$	3,650,000
3. BASE AND PAVING	\$	6,042,000
4. LUMP ITEMS	\$	2,206,000
5. MISCELLANEOUS	\$	431,000
6. SPECIAL FEATURES	\$	504,000
SUBTOTAL CONSTRUCTION COST	\$	17,103,000
E. & C. (10%)	\$	1,710,300
INFLATION (5% PER YEAR)	\$	855,000
NUMBER OF YEARS	2	
TOTAL CONSTRUCTION COST		<b>19,668,450</b>
<b>GRAND TOTAL PROJECT COST</b>		<b>23,754,487</b>



## MEMORANDUM MEETING MINUTES

**DATE:** June 7, 1999  
**TO:** Attendees  
**FROM:** Chris Marsengill  
**SUBJECT:** Supplemental Concept Team Meeting  
SR 20 Relocation – STP-012-1(71) Bartow County P.I. No. 621350

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On June 3, 1999, a supplemental concept team meeting was held in the Georgia Department of Transportation Road Design Conference Room No. 444 to discuss the concept design for the above referenced project and to gather data for further project development.

**Attendees:**

Mr. Wayne Sorrow, Road Design - Georgia DOT  
Mr. James Tidwell, Road Design - Georgia DOT  
Mr. Greg Mayo, Road Design - Georgia DOT  
Mr. Ken Estes, Traffic Operations - Georgia DOT  
Mr. Peter Hickey, Georgia Board of Regents  
Mr. Mike McDonald, Tunnel-Spangler & Associates  
Mr. Clarence Brown, Bartow County Commissioner  
Mr. Edward L. Bragg, Jordan, Jones and Goulding, Inc.  
Mr. Chris Marsengill, Jordan, Jones and Goulding, Inc.  
Ms. Janet Harvey, Office of Planning, Georgia DOT  
Mr. Mike Jones, Office of Environment/Location, Georgia DOT  
Rep. Jeff Lewis, State House of Representatives  
Mr. James Kennerly, State Road and Airport Design Engineer, Georgia DOT  
Ms. Dania Aponte, Office of Environment/Location, Georgia DOT  
Ms. Sharon Sharp, SRC, Georgia Power Company  
Mr. Rick Wells, Georgia Power Company  
Mr. Doug Jones, Georgia Power Company  
Mr. Al Jordan, Georgia Power Company  
Ms. Michelle Brouillette, Office of Environment/Location, Georgia DOT  
Mr. Andy Rikard, District 6 Location Office, Georgia DOT  
Mr. James Hullett, District 6 Pre-construction Engineer, Georgia DOT  
Mr. David Ashley, Jordan, Jones and Goulding, Inc.  
Mr. Bill King, Public Works Assistant Director, City of Cartersville  
Mr. Ray Southern, Public Works Director, City of Cartersville  
Mr. Jerry Milam, Assistant City Manager, City of Cartersville

Mr. Sorrow called the meeting to order and apologized for the absence of the Office of Right of Way and the Office of Utilities. Mr. Sorrow then asked Mr. Bragg to give a brief description of the project.

Mr. Bragg described the roadway typical sections, discussed the project termini, located the two

## Meeting Minutes

June 7, 1999

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proposed bridge replacements over US 411/SR 61, and located the two proposed bridge widenings over CSX Railroad. Mr. Bragg stated that he is unaware of any design exceptions. He also stated that Jordan, Jones & Goulding, Inc., JJ&G, would complete the environmental assessment for the project.

Mr. Ashley stated that JJ&G has completed a preliminary environmental field reconnaissance. Further, he does not expect any major environmental issues. He also stated that JJ&G has not executed a complete Phase I archaeological study. Mr. Ashley stated that JJ&G would obtain any required environmental permits and would complete any required mitigation planning.

Mr. Sorrow stated that a Public Hearing would be required for the project, but one has not been scheduled.

Ms. Aponte asked if a historical survey report had been submitted to the Office of Environment/Location. Mr. Ashley stated that Edwards-Pittman is in the process of defining the historical boundary for the Georgia Institute of Genetics. Once this boundary is defined, the report will be forwarded to Georgia DOT.

Mr. McDonald asked how the Georgia Institute of Genetics historical boundary was being defined. Mr. Ashley stated that the main objective in drawing the boundary was to encompass enough of the property to preserve its historical significance. This requires a boundary that includes the structure, a portion of the driveway, a portion of the fields, a portion of the wooded area, etc.

Mr. Estes asked how the location and spacing of the proposed median openings at the eastern end of SR 20 was established. Mr. Kennerly stated that the development of adjacent properties depended on the location of the median openings. Therefore, the median openings were coordinated with the adjacent property owners. Mr. Bragg stated that the spacing of the median openings satisfied Georgia DOT median opening spacing requirements for an urban typical section.

Mr. Estes questioned the 3-lane drop along westbound US 41 at the merge point of the 2-lane ramp from southbound US 411/SR 61. Mr. Kennerly suggested evaluating a one lane drop on the ramp. Mr. Sorrows recommended consulting with Mr. Kim Fulbright on this issue.

Mr. Estes also questioned the proximity of the weave at the merge point of the northbound US 411/SR 61 ramp to eastbound US 41 and the signal at US 41 and Market Place Boulevard. Mr. Kennerly stated that this is an existing problem. Further, this type of situation, although not desirable, is common where a major "interstate-type" interchange is required in a non-limited access area because improvement options are limited.

Mr. Estes questioned the lane drop adjacent to the accel/decel lane at the hospital on US 41 north of the intersection of Market Place and US 41. Mr. Kennerly stated that, once again, this is an existing problem.

Commissioner Brown questioned converting the hospital's existing driveway on US 41 to a right-in-right-out configuration and removing the existing signal. He is concerned that this will increase emergency response times to destinations south of the hospital. Mr. Kennerly stated that further investigation of emergency vehicle traffic patterns is warranted. Further, it may be necessary to extend the project along US 41 to incorporate improvements to the intersection of US 41 and Market Place Boulevard.

Mr. McDonald asked if any of the median opening locations had been moved since he had last received

Meeting Minutes

June 7, 1999

Page 3

drawing files. Mr. Bragg informed him that they had not.

Mr. McDonald asked if the proposed right-in-right-out driveway along SR 20 at the Wal-Mart shopping center could be used to access the Georgia Board of Regents' property adjacent to the shopping center. This would require an agreement between property owners.

Commissioner Brown asked if Wal-Mart delivery trucks traveling on SR 20 would be required to use the same entrance as Wal-Mart customers. Mr. Bragg confirmed that trucks arriving from the east would, while trucks arriving from the west would not.

Rep. Lewis asked if the traffic signal at the existing intersection of US 411/SR 61 and SR 20 would be removed. Mr. Bragg stated that the left turn movements from the proposed westbound US 41 ramp to US 411/SR 61 would require a signalized intersection.

Rep. Lewis asked if Peebles Valley Road would be two or four lanes. Mr. Bragg stated that Peebles Valley Road must be widened to a multi-lane section at the proposed intersection with US 411/SR 61 in order to match the laneage of SR 20. Mr. Bragg also stated that the realigned portion of Peebles Valley Road would taper back to a 2-lane section as quickly as possible.

Mr. King asked if the existing Old Tennessee Road would be left in place. Mr. Bragg stated that Old Tennessee Road would be terminated between the proposed northern Right of Way of SR 20 and the first driveway to the north. It is JJ&G's understanding that the portion of Old Tennessee Road south of the proposed southern Right of Way of SR 20 will be abandoned per the stipulations of construction permits granted by the City of Cartersville to the current property owner. Access to the Felton Property was also addressed as a stipulation of the same permits.

Mr. McDonald asked if the width of the existing SR 20 Right of Way between the Market Place Boulevard intersection and the proposed southern Right of Way SR 20 could be reduced. In the absence of the Right of Way Office, Mr. Kennerly advised that laws regarding Right of Way abandonment require that, once it is determined the Right of Way will be of no future value to Georgia DOT, the land be first offered to the previous property owners. If the previous property owner refuses the offer, the land is then sold at auction.

Ms. Harvey stated that the need and purpose statement will be expanded. She will review the need and purpose statement and make recommendations.

The Georgia Power Company representatives advised that Georgia Power was currently designing a transmission line that would parallel the proposed southern Right of Way of SR 20 through the Rogers property and then turn northeast to parallel the northern side of existing lines on the Georgia Power easement through the Georgia Board of Regents property. Georgia Power is in the process of acquiring an easement from Henderson-Bowen along the proposed southern Right of Way of SR 20. Georgia Power is scheduled to have the transmission lines operational by June 1, 2000. Approximately ten transmission poles will be placed along the proposed Right of Way. Relocation of a single pole would cost approximately \$100,000. Mr. Kennerly stated that the Georgia Power transmission line would be a control in the design of SR 20 through this area.

Mr. McDonald recommended extending the reduced Right of Way width to the end of the urban typical section in the interest of Georgia Power's transmission design. Mr. Kennerly concurred.

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Ms. Brouillette stated that the three proposed cross drains would be subject to new Preconstruction Notification requirements that may be put into place September 15, 1999. In addition, stream impacts may require mitigation.

Mr. Ashley stated that a complete environmental survey would be completed following concept approval.

Mr. King requested the construction of as much urban section as possible along the SR 20 corridor.

Mr. Southern had no comments.

Mr. Milam advised that the city had issued construction permits for a bank in the northeast quadrant of the existing intersection of US 41 and Market Place Boulevard. He asked if there would be a traffic signal at the proposed intersection of Market Place Boulevard and the existing SR 20. Mr. Marsengill advised that this intersection would be signalized.

Mr. Milam asked who would be responsible for the relocation cost of City of Cartersville utilities. Mr. Kennerly advised that an LGPA would be issued requesting that the City absorb this cost.

Commissioner Brown advised that Bartow County, the City of Cartersville, the hospital and the adjacent Winn-Dixie shopping center funded the existing signal at the hospital driveway on US 41.

Mr. Kennerly concluded that more work must be done on the concept. He advised that this concept must be more comprehensive than normal due to the number of issues that must be considered. There are many existing safety problems that should be addressed. Many businesses and property owners will be impacted. Emergency access to the hospital must be further investigated.

Mr. Kennerly advised that the Georgia Institute of Genetics historical boundary must be established as soon as possible.

cc: Jim McGee, Jordan, Jones & Goulding, Inc.  
Tommy Crochet, Jordan, Jones & Goulding, Inc.

AGREEMENT  
BY AND BETWEEN  
DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA  
AND  
BARTOW COUNTY  
FOR  
SR 20 FROM I-75 TO US 41

THIS AGREEMENT, is made and entered into this 27<sup>th</sup> day of May, 1998, by and between the DEPARTMENT OF TRANSPORTATION an agency of the State of Georgia, hereinafter referred to as the "DEPARTMENT", and BARTOW COUNTY, GEORGIA, acting by and through its Sole Commissioner, hereinafter referred to as the "LOCAL GOVERNMENT".

WHEREAS, the LOCAL GOVERNMENT has represented to the DEPARTMENT a desire to improve the roadway facilities along State Route (SR) 20 from I-75 to US 41 including the interchange with SR 61 at US 41 and described as Project STP-012-1(71), P.I. No. 621350, hereafter sometimes referred to as the "PROJECT"; and

WHEREAS, the LOCAL GOVERNMENT has represented to the DEPARTMENT a desire to provide preliminary engineering to develop the alignment, to design the PROJECT as identified by the DEPARTMENT, develop right of way plans, provide all environmental documentation, relocating utilities, and participate in the right of way acquisition, as specified in the Agreement, and the DEPARTMENT has relied upon such representation; and

WHEREAS, the DEPARTMENT has indicated a willingness to participate in the funding of the construction for the PROJECT with funds of the DEPARTMENT, funds apportioned to the DEPARTMENT by the Federal Highway Administration, hereinafter referred to as "FHWA", under Title 23, United States Code, Section 104, or a combination of funds from any of the above sources, subject to those certain conditions set forth in the Agreement.

NOW, THEREFORE, in consideration of the mutual promises made and of the benefits to flow from one to the other, the DEPARTMENT and the LOCAL GOVERNMENT hereby agree

each with the other as follows:

1. The DEPARTMENT shall:
  - a. Furnish or make the following services or information to the LOCAL GOVERNMENT:
    - i. Soil borings, as determined necessary by the LOCAL GOVERNMENT and approved by the DEPARTMENT, analyze and test such soil boring samples and prepare necessary soil engineering reports and design recommendations based on such soil data. It being further understood and agreed that the DEPARTMENT does not warrant such recommendations and the LOCAL GOVERNMENT, shall use such information at its sole risk.
    - ii. Provide standard drawings and construction details applicable to the PROJECT.
    - iii. Provide all applicable Standard Specifications, Supplemental Specifications and Special Provisions currently published by the DEPARTMENT.
2. The LOCAL GOVERNMENT shall:
  - a. Upon a written request by the DEPARTMENT, contribute toward the PROJECT by providing funds for the design engineering, prepare all Environmental Documentation and design traffic data required to implement the PROJECT, participate in the acquisition of rights of way, and adjustment and relocation of utilities, an amount equal to but not to exceed TWO MILLION EIGHT HUNDRED FIFTY SEVEN THOUSAND DOLLARS (\$2,857,000).
  - b. The LOCAL GOVERNMENT's responsibility for design shall include, but is not limited to the following items:
    - i. Prepare the PROJECT concept report in accordance with the format used by the DEPARTMENT. The concept for the PROJECT shall be developed to accommodate the future traffic volumes as generated by the LOCAL GOVERNMENT and approved by the DEPARTMENT.

The concept report shall be approved by the DEPARTMENT prior to the LOCAL GOVERNMENT beginning further development of the PROJECT plans. It is recognized by the parties to this Agreement that the approved concept may be modified by the LOCAL GOVERNMENT during the course of design due to the public input, environmental requirements, or right of way considerations. However, any project modifications will require the approval of the DEPARTMENT.

- ii. Validate the approved PROJECT concept and prepare a Project Design Book for approval by the DEPARTMENT prior to the beginning of preliminary plans.
- iii. Prepare all public hearing and public information displays and conduct all required public hearings and public information meetings in accordance with established DEPARTMENT practice.
- iv. Perform all surveys and mapping needed for design of the PROJECT.
- v. Prepare the PROJECT's drainage design including erosion control plans and the development of the hydraulic studies for the Federal Emergency Management Agency Floodways and acquisition of all necessary permits, such as, FEMA and COE 404 for the PROJECT.
- vi. Prepare traffic studies, preliminary construction plans, preliminary and final utility plans, preliminary and final right of way plans, staking of the required right of way, preliminary and final bridge plans, and final construction plans including erosion control, traffic handling, and construction sequence plans and specifications including special provisions for the PROJECT.
- vii. Provide certification, by a Georgia Registered Professional Engineer, that the construction plans have been prepared under the guidance of the professional engineer and are in accordance with GDOT Standard Specifications.

3. The LOCAL GOVERNMENT shall develop the plans in keeping with the studies encompassing the PROJECT area and in accordance with the DEPARTMENT's Plan Development Process and Plan Presentation Guide. The construction plans shall be developed in metric units and the project right-of-way plans shall be prepared in dual units - English and Metric utilizing CAiCE software and in accordance with DEPARTMENT metric guidelines.
4. Upon the LOCAL GOVERNMENT's determination of the rights of way required for the PROJECT and the approval of the right of way plans by the DEPARTMENT, the DEPARTMENT shall acquire the necessary rights of way for the PROJECT and fund the acquisition costs which are in excess of the amounts available in paragraph 2(a) of this Agreement. Right of way acquisition shall be in accordance with the rules and regulations of the FHWA including, but not limited to, Title 23 United States Code Annotated and 49 CFR Part 24 and the rules and regulations of the DEPARTMENT.
5. The LOCAL GOVERNMENT shall be responsible for all utility relocation costs necessary for the construction of the PROJECT subject to the limitations in paragraph 2(a) of this agreement. Any funds remaining, subject to the limitations in paragraph 2(a) of this Agreement, after all utility relocations are completed will be contributed toward the right of way acquisition for the PROJECT.
6. The DEPARTMENT shall review and has approval authority for all aspects of the PROJECT. The DEPARTMENT will work with the FHWA to obtain all needed approvals with information furnished by the LOCAL GOVERNMENT.
7. The DEPARTMENT shall review all aspects of the LOCAL GOVERNMENT's work on the PROJECT. The DEPARTMENT shall have sole discretion over the PROJECT consistent with providing access to the general motoring public. When the PROJECT plans are finalized, and approved by the DEPARTMENT, the DEPARTMENT shall let the PROJECT for construction. The DEPARTMENT shall participate in the cost of construction of the PROJECT to the extent of providing the total bid.
8. The DEPARTMENT shall be solely responsible for securing and awarding the

construction contract for the PROJECT.

9. This Agreement is made and entered into in Fulton County, Georgia and shall be governed and construed under the laws of the State of Georgia. The covenants herein contained shall, except as otherwise provided, accrue to the benefit of and be binding upon the successors and assigns of the parties hereto.

IN WITNESS WHEREOF, the DEPARTMENT and the LOCAL GOVERNMENT have caused these presents to be executed under seal by their duly authorized representatives.

RECOMMENDED:

James A. Kennerly  
James A. Kennerly  
State Road & Airport Design Engineer

Walker Scott  
Walker Scott, P.E.  
Director of Preconstruction

Frank L. Danchetz  
Frank L. Danchetz  
Chief Engineer

SOLE COMMISSIONER  
BARTOW COUNTY  
BY: Clarence Brown  
Commissioner

Stephen R. Bradley  
Witness

Kathy Hill  
Notary Public  
Notary Public, Bartow County, Georgia  
My Commission Expires Jan. 2, 2002

This Agreement approved by the SOLE  
COUNTY COMMISSIONER on: 5/1/98

DEPARTMENT OF TRANSPORTATION

BY: Wayne Shackelford  
Wayne Shackelford,  
Commissioner

ATTEST

Billy F. Sharp  
Billy F. Sharp  
Treasurer

REVIEWED 5-13-98  
(DATE)

Tandra S. Bury  
LEGAL - TRANSPORTATION

No Pre-Award Examination  
Required Jim Starnel

33E A \$71,200 needs -0-

<b>TO BE COMPLETED BY FHWA</b>  <div style="font-size: 2em; opacity: 0.5; transform: rotate(-15deg); position: absolute; top: 20px; left: 20px;">File</div>	 U.S. Department of Transportation <b>FEDERAL-AID PROJECT AGREEMENT</b> Federal Highway Administration	<b>STATE</b> <b>GEORGIA</b> <b>COUNTY</b> <b>BARTOW</b> <b>PROJECT NO.</b> <b>STP-012-1(71)</b>
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The State, through its Highway Agency, having complied, or hereby agreeing to comply, with the applicable terms and conditions set forth in (1) Title 23, U.S. Code, Highways, (2) the Regulations issued pursuant thereto and, (3) the policies and procedures promulgated by the Federal Highway Administrator relative to the above designated project, and the Federal Highway Administration having authorized certain work to proceed as evidenced by the date entered opposite the specific item of work, Federal funds are obligated for the project not to exceed the amount shown herein, the balance of the estimated total cost being an obligation of the State. Such obligation of Federal funds extends only to project costs incurred by the State after the Federal Highway Administration authorization to proceed with the project involving such costs.

**PROJECT TERMINI**  
 SR 20 FROM SR 61 TO I-75 (INCL. INTCH W/SR 61 & US 41)

**FED. ITEM NO. 020643** **D.O.T. P.I. NO. 621350**

PROJECT CLASSIFICATION OR PHASE OF WORK	EFFECTIVE DATE OF AUTHORIZATION	APPROXIMATE LENGTH (Miles)
HIGHWAY PLANNING AND RESEARCH (HP & R)		
PRELIMINARY ENGINEERING	3-27-92	2.47
RIGHTS-OF-WAY		
CONSTRUCTION		
OTHER (Specify)		

FUNDS	
ESTIMATED TOTAL COST OF PROJECT	FEDERAL FUNDS
\$ 89,000.00	\$ 71,200.00

The State further stipulates that as a condition to payment of the Federal funds obligated, it accepts and will comply with the applicable provisions set forth on the following pages.

<p><b>GEORGIA DEPARTMENT OF TRANSPORTATION</b>  <small>(Official name of Highway Agency)</small></p> <p>By <u>Wayne Stuckelberg</u>  <b>COMMISSIONER, GA. DEPT. OF TRANSPORTATION</b>  <small>(Title)</small></p> <p>By _____  <small>(Title)</small></p> <p>By _____  <small>(Title)</small></p>	<p><b>U.S. DEPARTMENT OF TRANSPORTATION</b>  <b>FEDERAL HIGHWAY ADMINISTRATION</b></p> <p>By _____  <small>(Division Administrator)</small></p> <p>Date executed by        Division Administrator _____</p>
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GEORGIA DEPARTMENT OF TRANSPORTATION  
WORK AUTHORIZATION

*Program*

REF: A. 23 CFR 630 (FHFM 6-3-2-2).  
B. Ga. DOT Construction Work Program P.I.#: 621350  
C. Sec 105 Annual Work Program Fed Item #: 020643

SYMBOL	NO	SEC	UNIT	COUNTY	CD	FY	PHASE
SIP-012-1	(71)			BARTOW	7	FY92	P/E

- \* CHARACTER OF PROPOSED WORK AND REMARKS/STIPULATIONS
- \*
- \* PRELIMINARY ENGINEERING AND ASSOCIATED INCIDENTALS FOR THE FUTURE WIDEN & RECONSTRUCT
- \*
- \*
- \* ROW APPRAISALS AND ASSOCIATED INCIDENTALS. FA PARTICIPATION WILL BE LIMITED TO THE AREAS INCORPORATED INTO THE FINAL ROW FOR PROJECT.
- \*

STATE ROUTE: 20 MILEAGE: 2.47  
 SR 20 FM SR 61 TO I-75 (INCL INCH W/SR 61 & US 41)  
 REQUEST: Federal State Other Fund/Appn Code  
 \$89,000 \$71,200 \$17,800 -0- SIP (33E)

- \*
- \*
- \* 4R Classification:
- \*
- \*
- \*

Advance Project by CA Procedures:  Yes  No

\* TRAFFIC: Present: \_\_\_\_\_ Future: \_\_\_\_\_

Authorization requested for amount of federal funds shown above.

\* Railroad crossing name and location

Signed: *Kevin J. Griffin* Date: 3-9-92  
 State Transportation Programming Engineer

\* Air-Highway Clearance Involved:  Yes  No

Authorization granted to proceed.

\* Items marked below with [XX] are applicable to this phase of work.

Signed: *J.S. Hemant* Date: 3/27/92  
 For the Division Administrator, FHWA

\* [ ] Urban Transportation planning actions per 23 CFR 450., Sub B, Annual Element/TIP Identifying # \_\_\_\_\_

\* [ ] State Clearinghouse Actions # \_\_\_\_\_

Work is authorized and is to proceed as scheduled. The Director of Administration is requested to open the necessary accounts.

\* [ ] Addition to Section 105 Program

\*  Categorical Exclusion per 23 CFR 771.

\* [ ] Public Involvement/Hearings Not Required

Signed: *Wayne Mackelberg* Date: 3/27/92  
 Commissioner, Ga. DOT

\* [ ] Work to be accomplished by Ga. DOT Maintenance Forces.

REVIEWED AND RECORDED BY THE OFFICE OF PROGRAMMING DATE <u>3-9-92</u> BY <i>[Signature]</i>
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REQUEST  
FOR  
PRE-PROGRAMMING AUTHORIZATION

AUTHORIZATION IS REQUESTED TO PROCEED WITH DEVELOPMENT OF A PROJECT  
CONCEPT ON THE FOLLOWING PROJECT:

PROJECT DATA

621850 SIP-012-1(71)  
BARTON SR 20  
SR 20 FM SR 61 TO I-75 (INCL I  
NTCH W/SP 61 & US 41)  
OLD PROJ: PR-012-1(71)

Fund 1 = 010  
Fund 2 = MLP

TYPE WORK	DESCRIPTION
Ident & Reconstruct	State Route 20: From S.R. 61 to I-75. (including the interchange with U.S. 41 & S.R. 61) Length = 2.47 Miles

PRELIMINARY COST ESTIMATE (\$1,000's)	PROPOSED FISCAL YEAR	ROW TO BE PROVIDED BY	CONG. DIST.	FIELD DIST.
ROW CONST. \$2,964	1994	D.O.T.	7	6

NEEDS RATING:  
SUFFICIENCY RATING:  
COMMENTS:

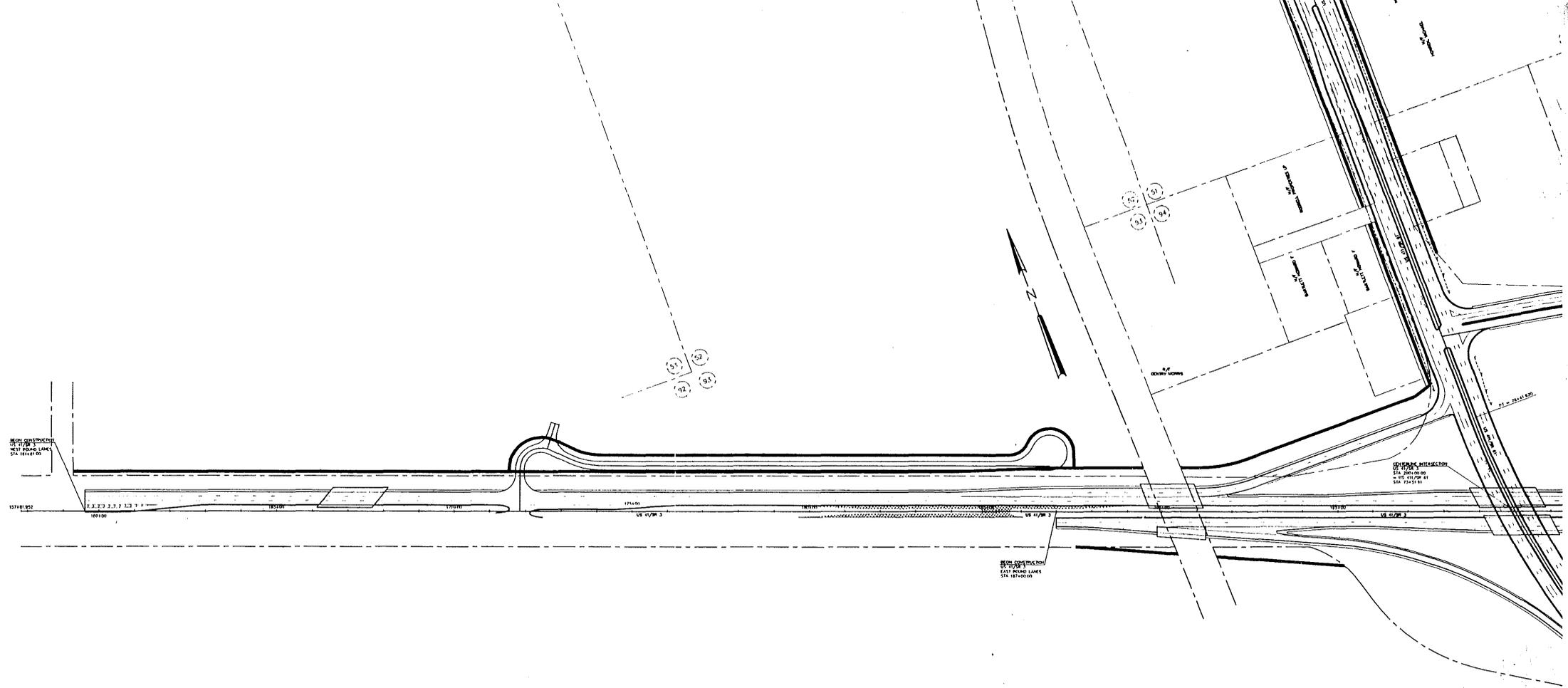
It is proposed to add this project to the Construction Work Program after the Project  
Concept Report has been approved. This project was recommended by the S.H.I.P.  
Committee on May 23, 1989.

RECOMMENDED

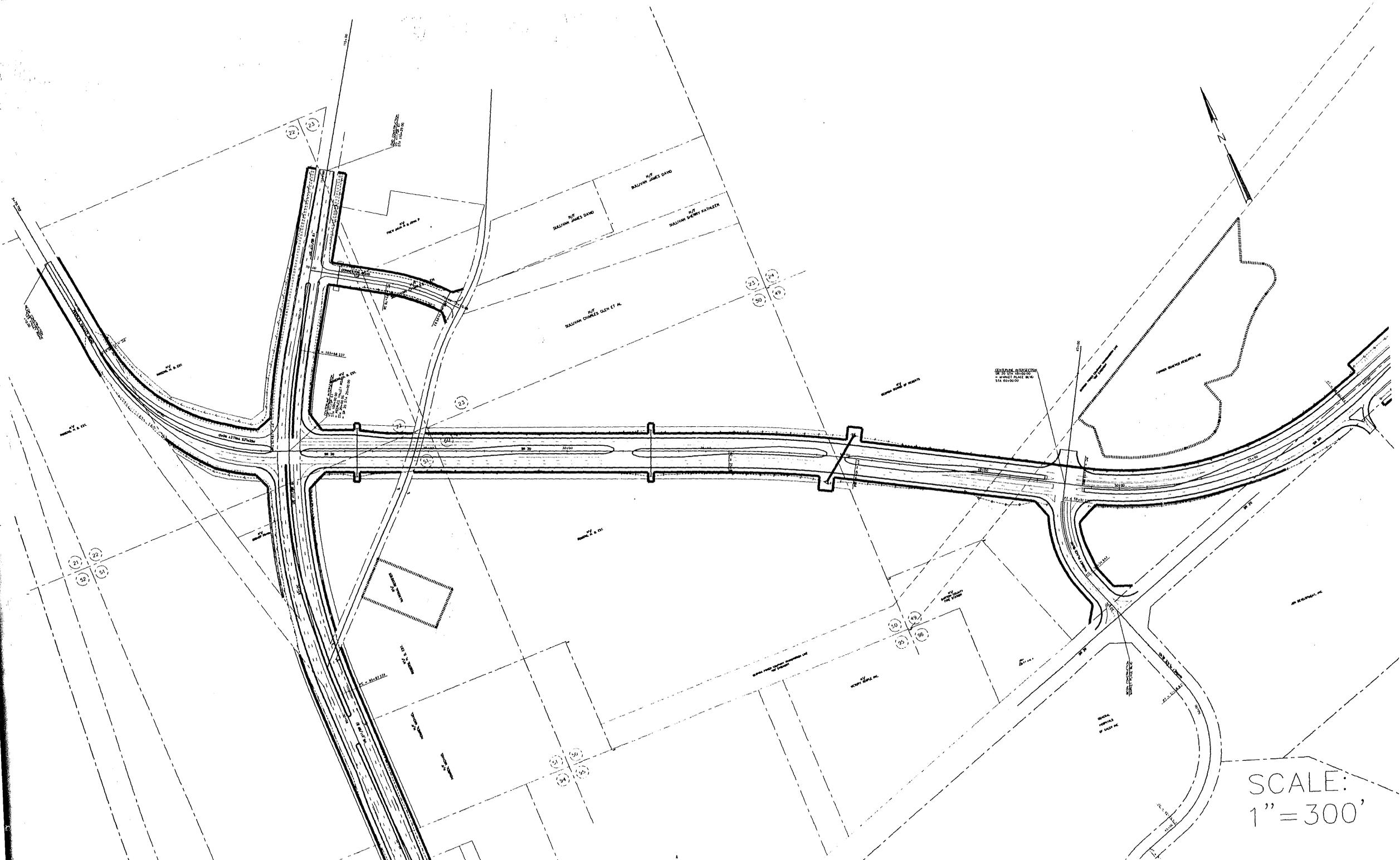
*David E. Hardy*  
DIRECTOR, DIVISION OF PLANNING AND PROGRAMMING

APPROVED

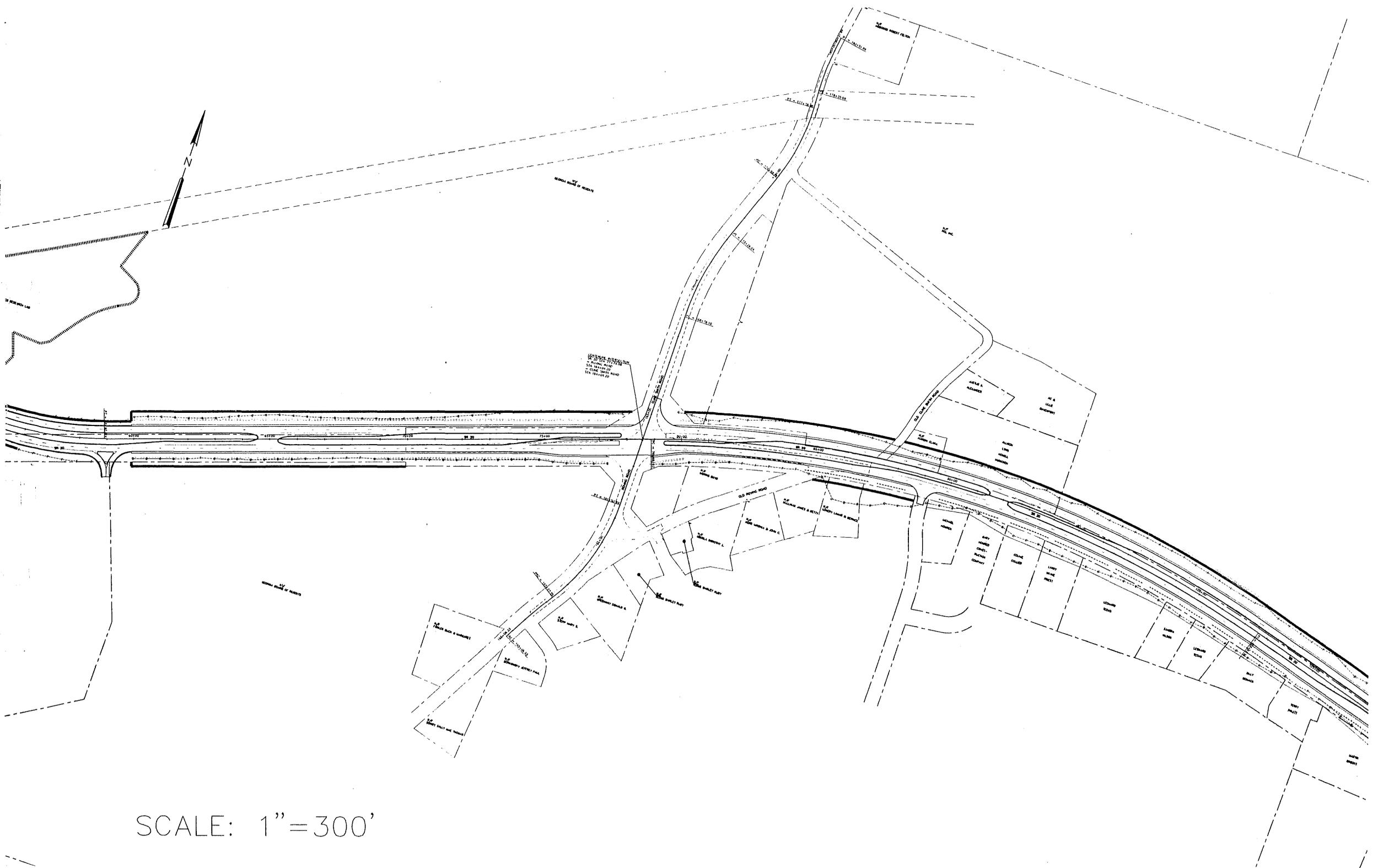
*[Signature]*  
COMMISSIONER



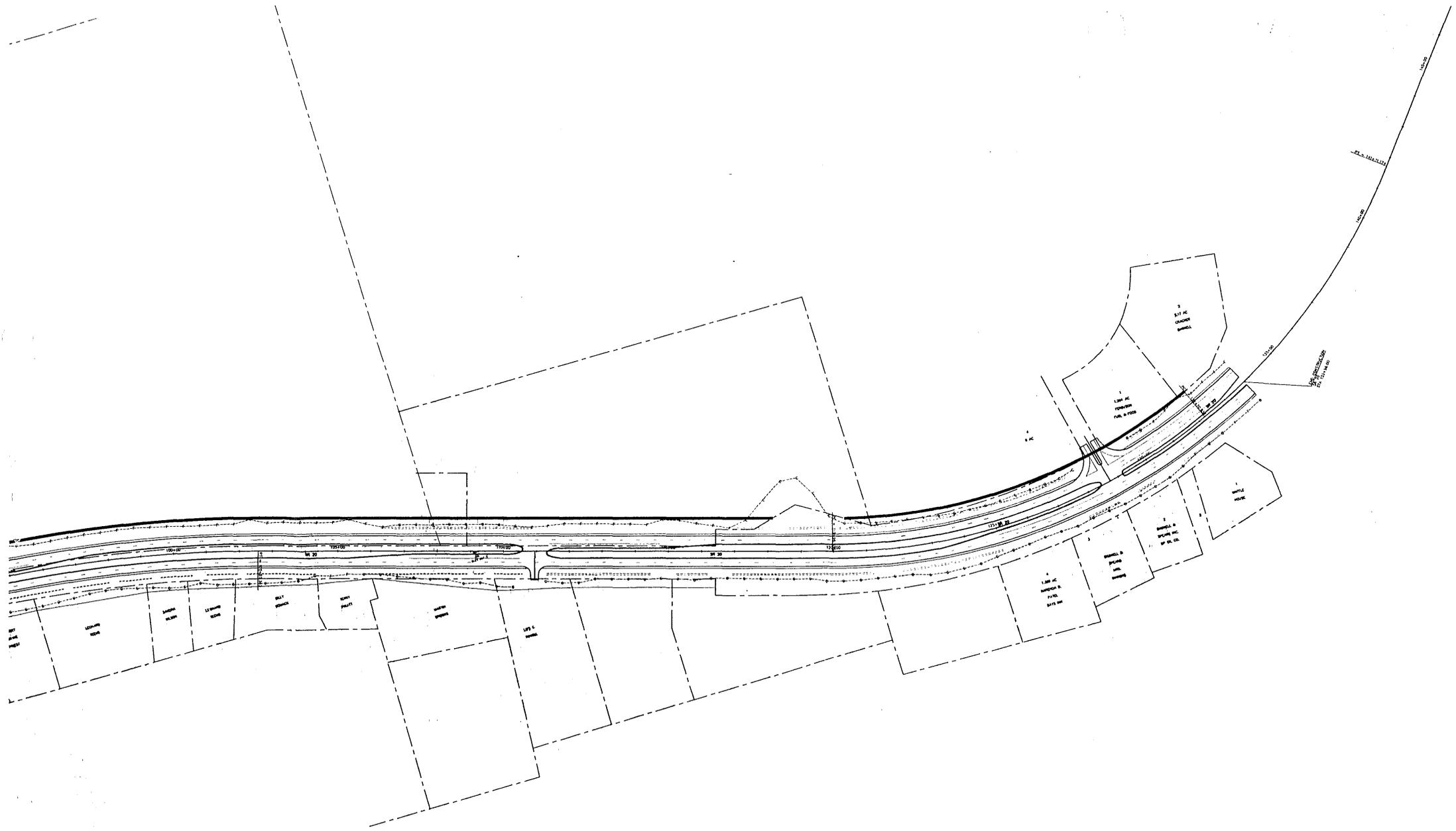
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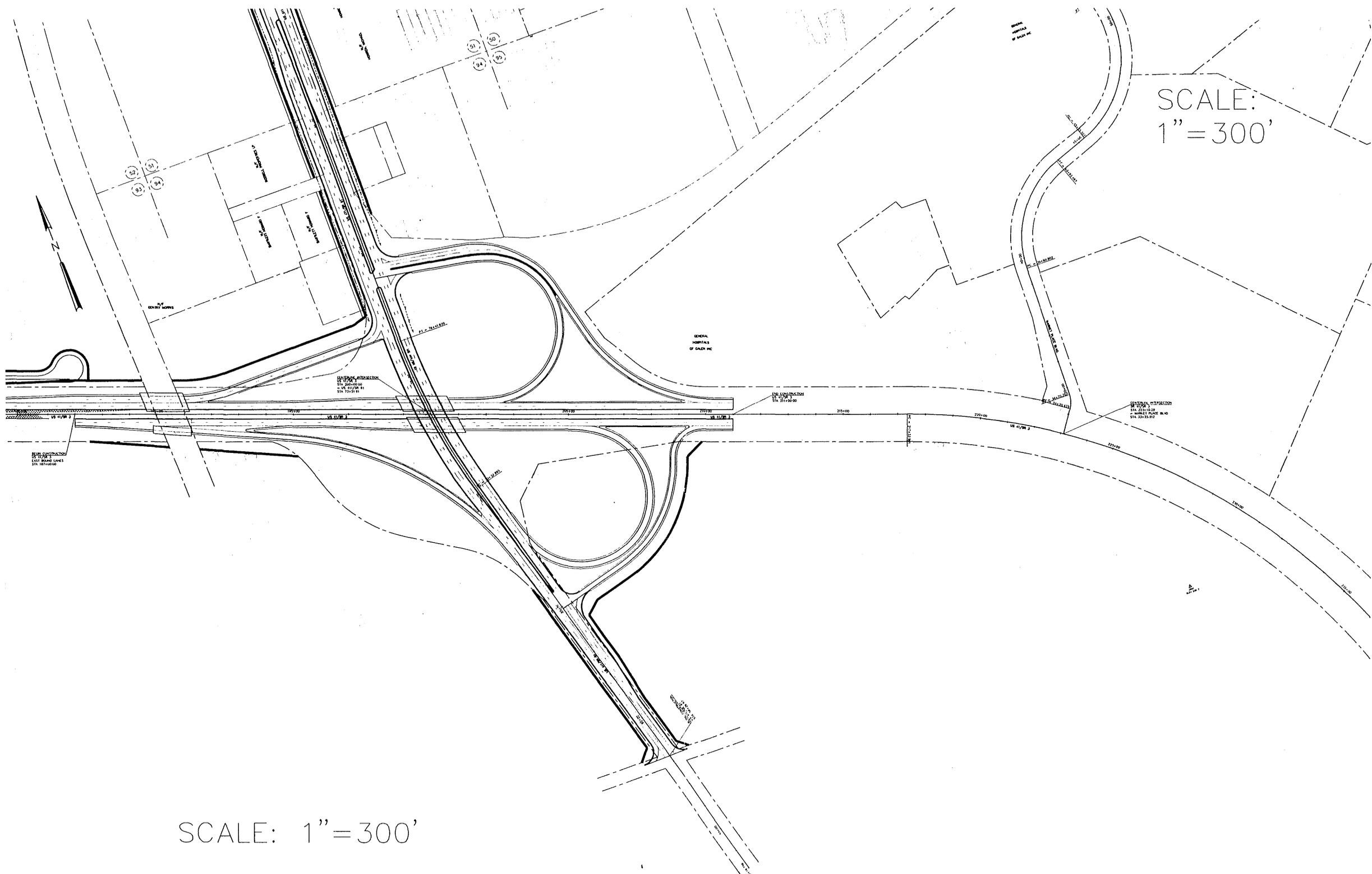


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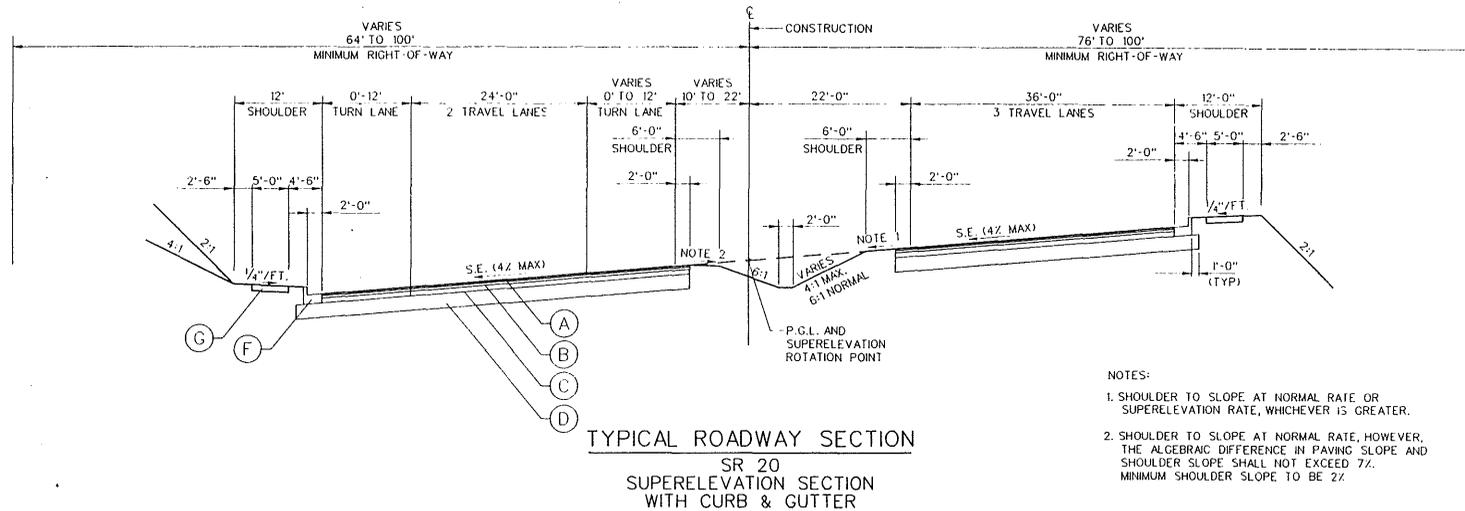


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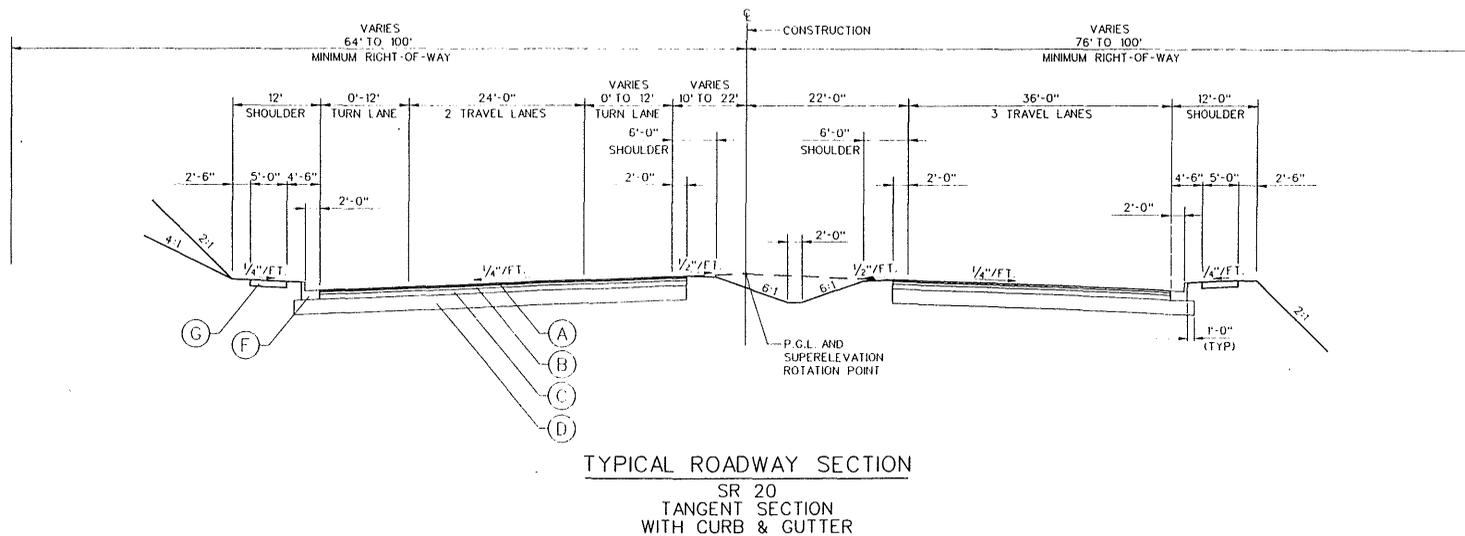


SCALE: 1" = 300'



NOTES:

- SHOULDER TO SLOPE AT NORMAL RATE OR SUPERELEVATION RATE, WHICHEVER IS GREATER.
- SHOULDER TO SLOPE AT NORMAL RATE, HOWEVER, THE ALGEBRAIC DIFFERENCE IN PAVING SLOPE AND SHOULDER SLOPE SHALL NOT EXCEED 7%. MINIMUM SHOULDER SLOPE TO BE 2%.



**PAVEMENT DESIGN**

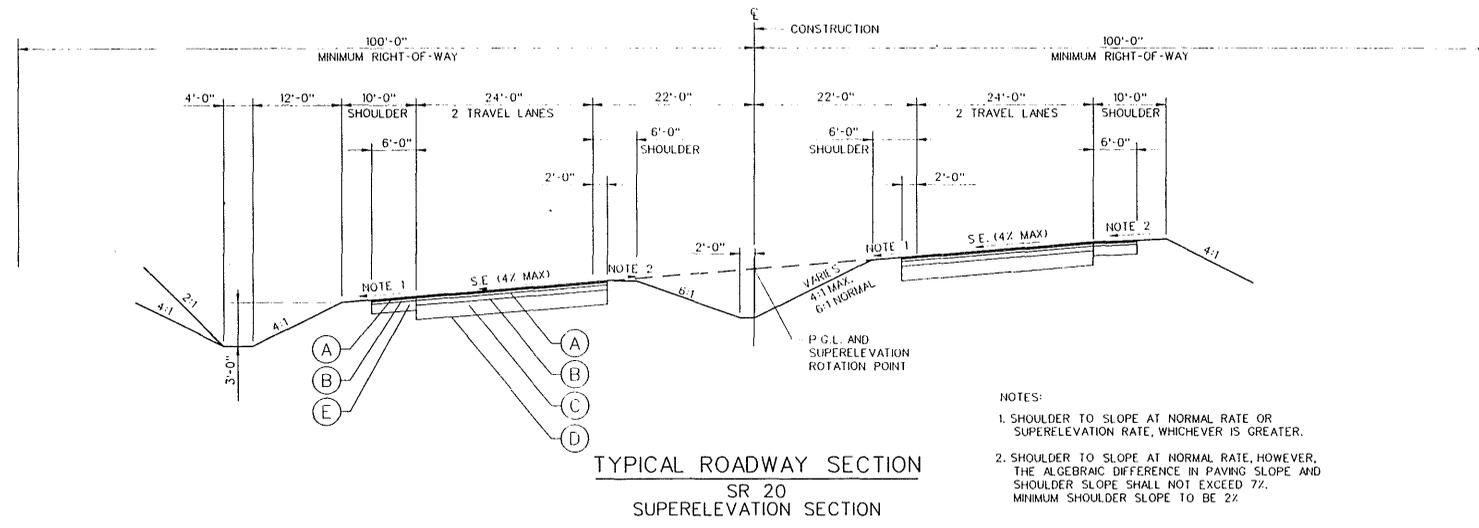
- (A) 165 LBS/YD<sup>2</sup> ASPHALTIC CONCRETE "E" SUPERPAVE
- (B) 220 LBS/YD<sup>2</sup> ASPHALTIC CONCRETE "B" SUPERPAVE
- (C) 440 LBS/YD<sup>2</sup> ASPHALTIC CONCRETE BASE SUPERPAVE
- (D) 16" GRADED AGGREGATE BASE
- (E) 8" GRADED AGGREGATE BASE
- (F) 8" x 30" TYPE 2 CONCRETE CURB & GUTTER
- (G) GDOT STD 9031-W 4" CONCRETE SIDEWALK

TYPICAL SECTIONS SHOWING SUPERELEVATION AND TURN LANES ALSO APPLY TO OPPOSITE HAND SECTIONS - SEE CONSTRUCTION PLAN SHEETS FOR LOCATIONS AND DIRECTION OF SUPERELEVATION AND LOCATIONS AND DIMENSIONS OF TURN LANES.

12/19/99 3:01:01 PM \\GDOT\TEMP\RA\1\TECH2\04EJES\Trans\decf\_jchu\p c:\ocadtemp\62350301.pr.f

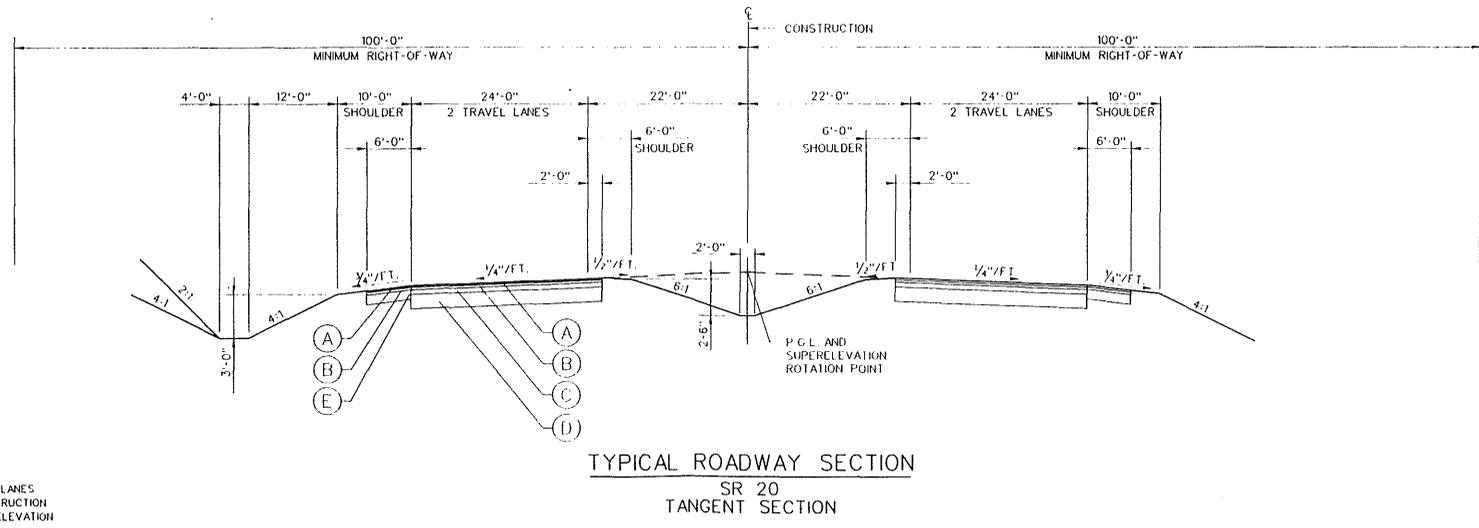
THIS LINE IS ONE INCH LONG WHEN PLOTTED FULL SCALE

		<p>NOT RELEASED FOR CONSTRUCTION</p>			<p>SR 20 FROM SR 61 TO I-75 - INCL. SR 3 &amp; SR 61 INTERSECTIONS</p> <p>TYPICAL SECTIONS</p>										
		<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td>DESIGNED RCM</td> <td>CHECKED</td> <td>DATE</td> </tr> <tr> <td>DRAWN JAK</td> <td>JOH NO 2184 016</td> <td>SCALE THREE</td> </tr> </table>		DESIGNED RCM	CHECKED	DATE	DRAWN JAK	JOH NO 2184 016	SCALE THREE	<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td>1</td> </tr> </table>	1	<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td>1</td> </tr> </table>	1	<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td>REV</td> </tr> </table>	REV
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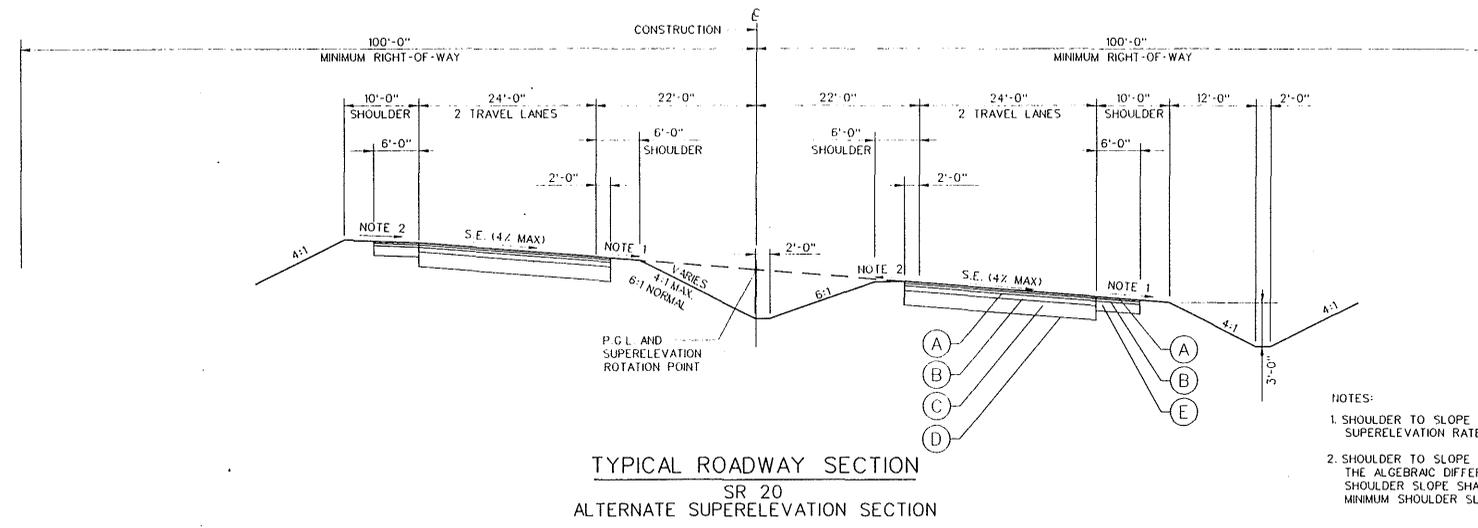
- PAVEMENT DESIGN**
- (A) 165 LBS/YD<sup>2</sup> ASPHALTIC CONCRETE "E" SUPERPAVE
  - (B) 220 LBS/YD<sup>2</sup> ASPHALTIC CONCRETE "B" SUPERPAVE
  - (C) 440 LBS/YD<sup>2</sup> ASPHALTIC CONCRETE BASE SUPERPAVE
  - (D) 16" GRADED AGGREGATE BASE
  - (E) 8" GRADED AGGREGATE BASE

TYPICAL SECTIONS SHOWING SUPERELEVATION AND TURN LANES. ALSO APPLY TO OPPOSITE HAND SECTIONS - SEE CONSTRUCTION PLAN SHEETS FOR LOCATIONS AND DIRECTION OF SUPERELEVATION AND LOCATIONS AND DIMENSIONS OF TURN LANES.

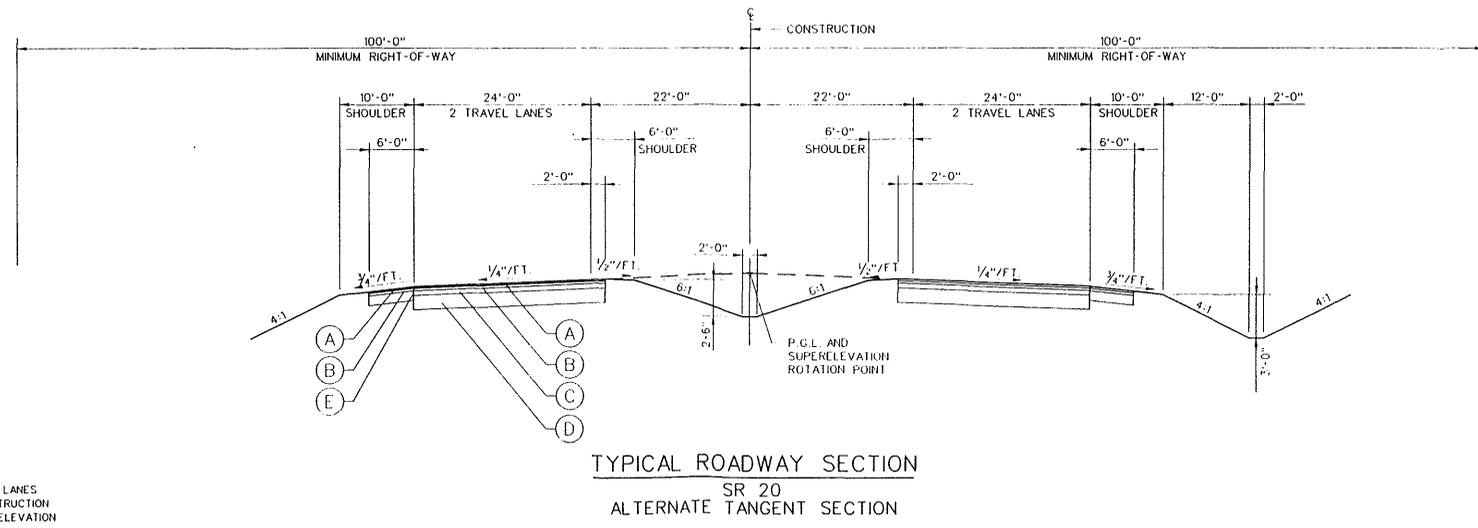
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THIS LINE IS ONE INCH LONG WHEN PLOTTED FULL SCALE

		<p>NOT RELEASED FOR CONSTRUCTION</p>		<p>SR 20 FROM SR 61 TO I-75 - INCL. SR 3 &amp; SR 61 INTERSECTIONS</p>
TYPICAL SECTIONS				
DESIGNED RCM	CHECKED	DATE	SCALE	SHEET
DRAWN JAC	JOB NO. 2384 GR	2	2	REV



NOTES:  
 1. SHOULDER TO SLOPE AT NORMAL RATE OR SUPERELEVATION RATE, WHICHEVER IS GREATER.  
 2. SHOULDER TO SLOPE AT NORMAL RATE, HOWEVER, THE ALGEBRAIC DIFFERENCE IN PAVING SLOPE AND SHOULDER SLOPE SHALL NOT EXCEED 7%. MINIMUM SHOULDER SLOPE TO BE 2%.



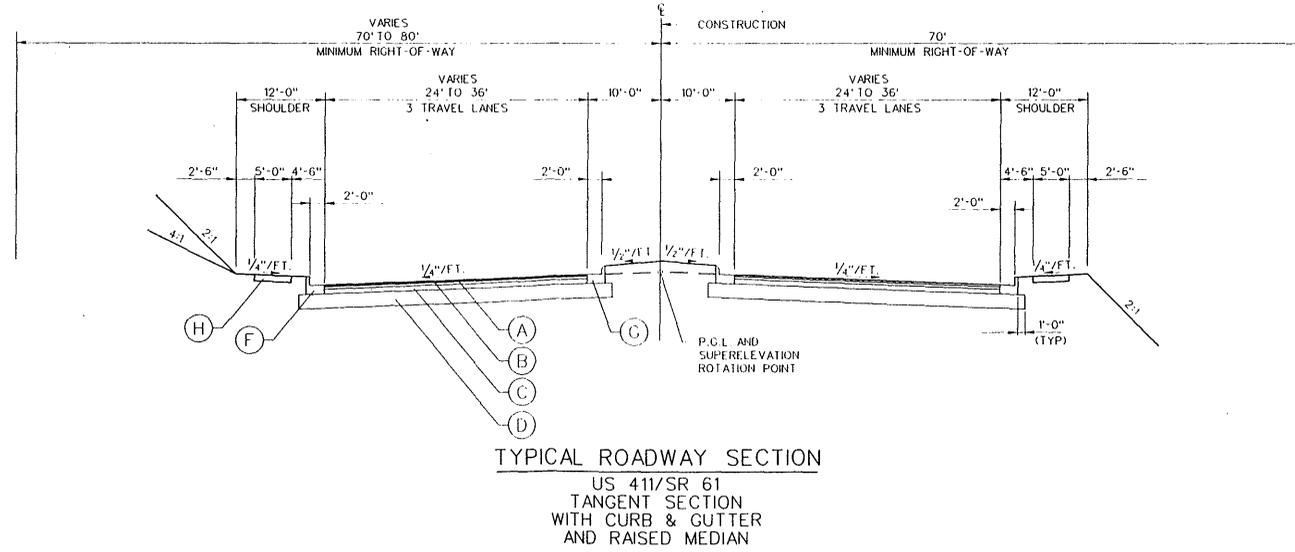
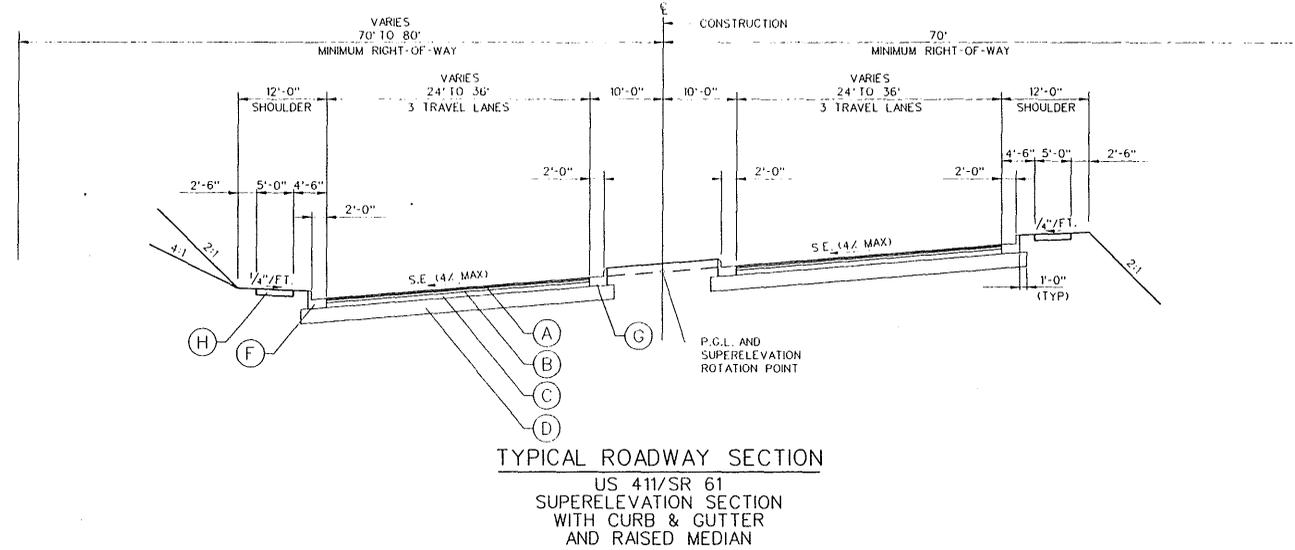
- PAVEMENT DESIGN**
- (A) 165 LBS/YD2 ASPHALTIC CONCRETE "E" SUPERPAVE
  - (B) 220 LBS/YD2 ASPHALTIC CONCRETE "B" SUPERPAVE
  - (C) 440 LBS/YD2 ASPHALTIC CONCRETE BASE SUPERPAVE
  - (D) 16" GRADED AGGREGATE BASE
  - (E) 8" GRADED AGGREGATE BASE

TYPICAL SECTIONS SHOWING SUPERELEVATION AND TURN LANES. ALSO APPLY TO OPPOSITE HAND SECTIONS - SEE CONSTRUCTION PLAN SHEETS FOR LOCATIONS AND DIRECTION OF SUPERELEVATION AND LOCATIONS AND DIMENSIONS OF TURN LANES.

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THIS LINE IS ONE INCH LONG WHEN PLOTTED FULL SCALE

		<p>NOT RELEASED FOR CONSTRUCTION</p>	<p>SR 20 FROM SR 61 TO I-75 - INCL. SR 3 &amp; SR 61 INTERSECTIONS</p>	<p>TYPICAL SECTIONS</p>														
<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td>DESIGNED RCM</td> <td>CHECKED</td> <td>DATE</td> </tr> <tr> <td>DRAWN JAC</td> <td>JOB NO. 2184 (16)</td> <td>SCALE 1/4"=1'-0"</td> </tr> </table>		DESIGNED RCM	CHECKED	DATE	DRAWN JAC	JOB NO. 2184 (16)	SCALE 1/4"=1'-0"	<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td>NO.</td> <td>DATE</td> <td>DESCRIPTION OF REVISION</td> </tr> <tr> <td> </td> <td> </td> <td> </td> </tr> </table>		NO.	DATE	DESCRIPTION OF REVISION				<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td style="text-align: center;">3</td> </tr> <tr> <td style="text-align: center;">SHEET</td> </tr> </table>	3	SHEET
DESIGNED RCM	CHECKED	DATE																
DRAWN JAC	JOB NO. 2184 (16)	SCALE 1/4"=1'-0"																
NO.	DATE	DESCRIPTION OF REVISION																
3																		
SHEET																		



**PAVEMENT DESIGN**

- (A) 165 LBS/YD<sup>2</sup> ASPHALTIC CONCRETE "E" SUPERPAVE
- (B) 220 LBS/YD<sup>2</sup> ASPHALTIC CONCRETE "B" SUPERPAVE
- (C) 440 LBS/YD<sup>2</sup> ASPHALTIC CONCRETE BASE SUPERPAVE
- (D) 16" GRADED AGGREGATE BASE
- (E) 8" GRADED AGGREGATE BASE
- (F) 8" x 30" TYPE 2 CONCRETE CURB & GUTTER
- (G) 8" x 30" TYPE 7 CONCRETE CURB & GUTTER
- (H) GDOT STD 9031-W 4" CONCRETE SIDEWALK

TYPICAL SECTIONS SHOWING SUPERELEVATION AND TURN LANES. ALSO APPLY TO OPPOSITE HAND SECTIONS - SEE CONSTRUCTION PLAN SHEETS FOR LOCATIONS AND DIRECTION OF SUPERELEVATION AND LOCATIONS AND DIMENSIONS OF TURN LANES.

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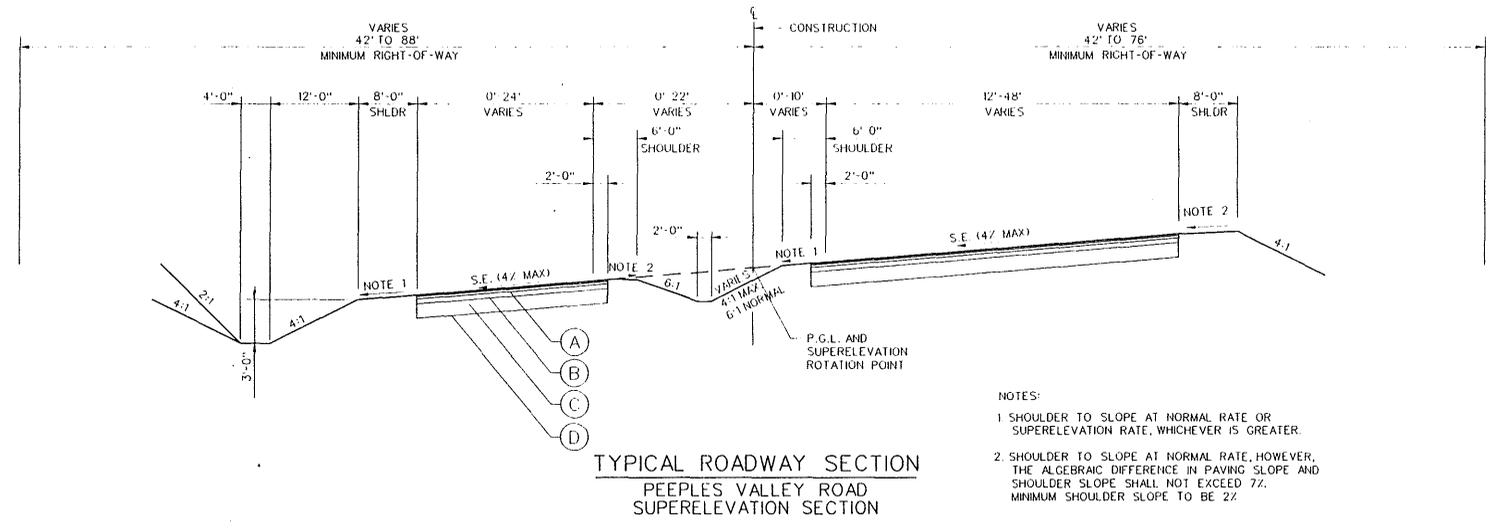
NOT RELEASED FOR CONSTRUCTION

SR 20 FROM SR 61 TO I-75 - INCL. SR 3 & SR 61 INTERSECTIONS

TYPICAL SECTIONS

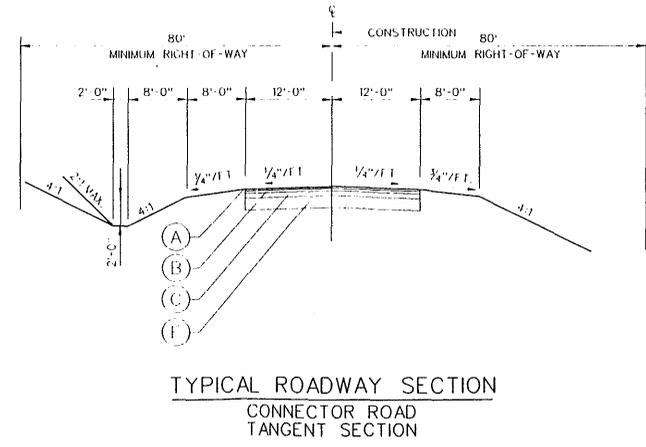
DESIGNED RCM	CHECKED	DATE	4	REV
DRAWN JAC	JOB NO. 2184 016	SCALE NONE		

THIS LINE IS ONE INCH LONG WHEN PLOTTED FULL SCALE



NOTES:

1. SHOULDER TO SLOPE AT NORMAL RATE OR SUPERELEVATION RATE, WHICHEVER IS GREATER.
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**PAVEMENT DESIGN**

- (A) 165 LBS/YD<sup>2</sup> ASPHALTIC CONCRETE "E" SUPERPAVE
- (B) 220 LBS/YD<sup>2</sup> ASPHALTIC CONCRETE "B" SUPERPAVE
- (C) 440 LBS/YD<sup>2</sup> ASPHALTIC CONCRETE BASE SUPERPAVE
- (D) 16" GRADED AGGREGATE BASE
- (E) 8" GRADED AGGREGATE BASE
- (F) 12" GRADED AGGREGATE BASE

TYPICAL SECTIONS SHOWING SUPERELEVATION AND TURN LANES ALSO APPLY TO OPPOSITE HAND SECTIONS - SEE CONSTRUCTION PLAN SHEETS FOR LOCATIONS AND DIRECTION OF SUPERELEVATION AND LOCATIONS AND DIMENSIONS OF TURN LANES.



NOT RELEASED FOR CONSTRUCTION

SR 20 FROM SR 61 TO I-75 - INCL. SR 3 & SR 61 INTERSECTIONS

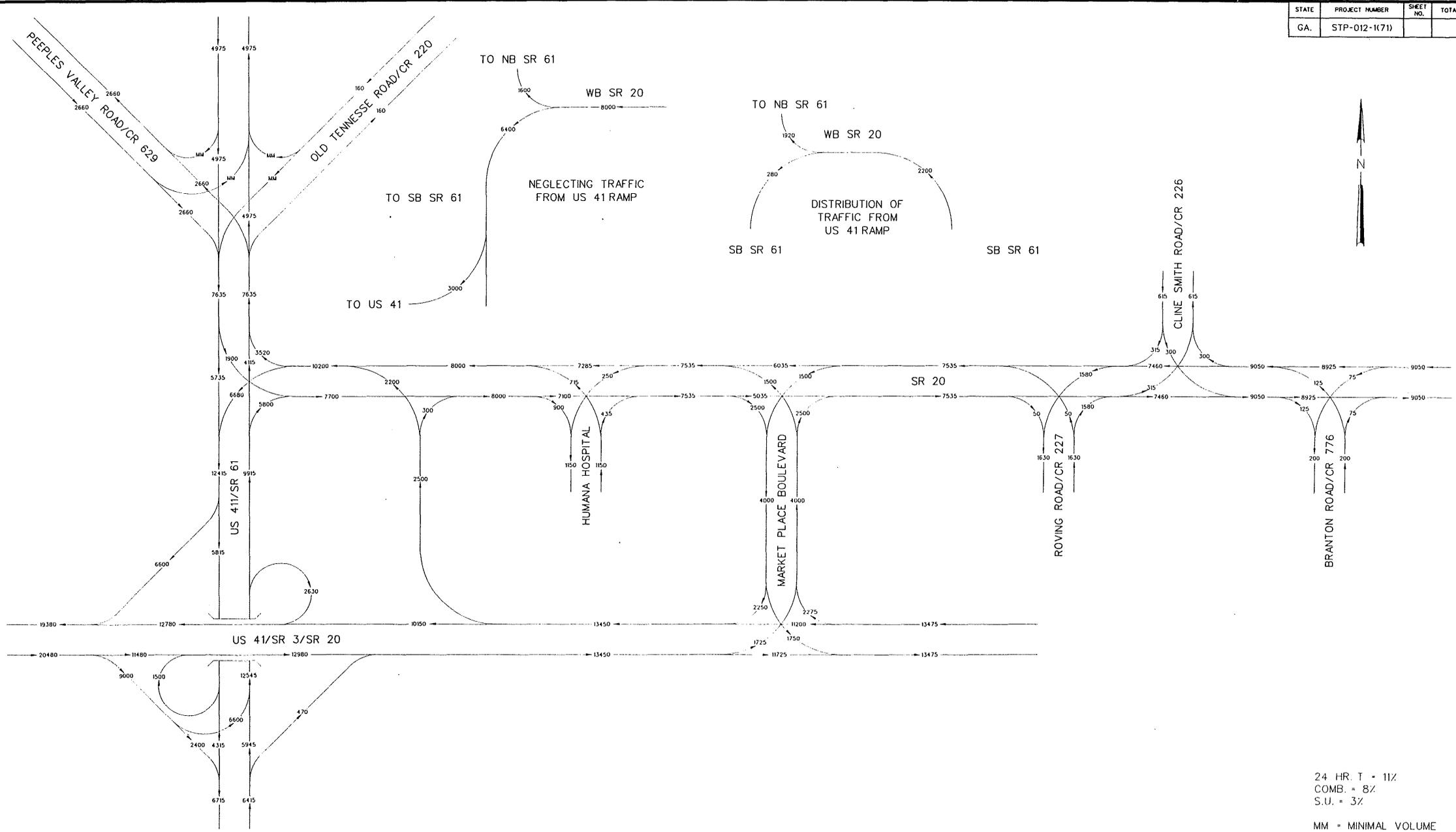
**TYPICAL SECTIONS**

DESIGNED - BCM	CHECKED	DATE	5
DRAWN - JAC	JOB NO. 2184 016	SCALE - NONE	SHEET
			REV

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THIS LINE IS ONE INCH LONG WHEN PLOTTED FULL SCALE

STATE	PROJECT NUMBER	SHEET NO.	TOTAL
GA.	STP-012-1(71)		



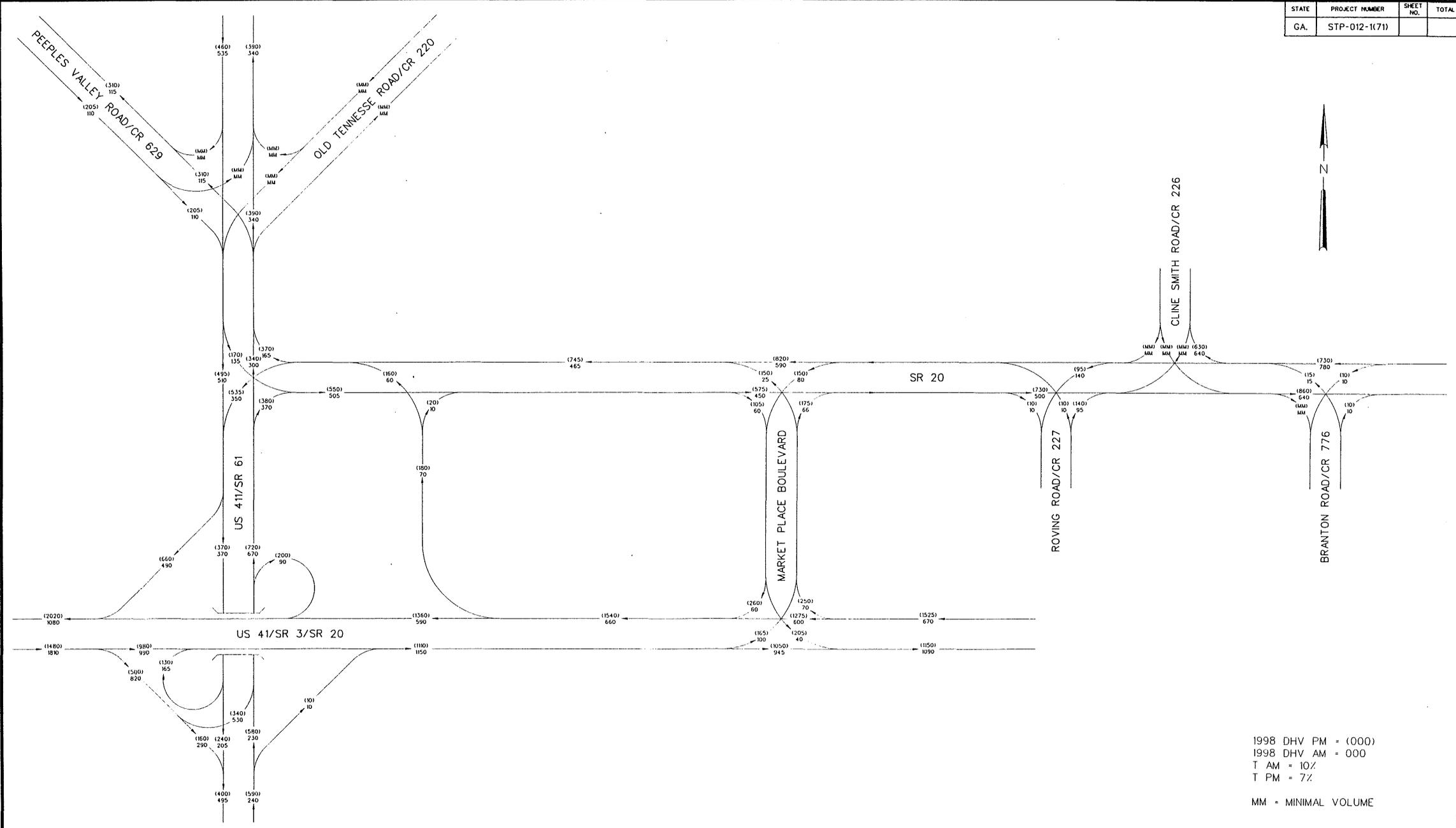
24 HR. T = 11%  
 COMB. = 8%  
 S.U. = 3%  
 MM = MINIMAL VOLUME

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SR 20 FROM SR 61 TO I-75 - INCL. SR 3 & SR 61 INTERSECTIONS			
TRAFFIC DIAGRAM 1998 AVERAGE DAILY TRAFFIC			
DESIGNED: MA	CHECKED:	DATE: JANUARY 1999	FIGURE 1.2
DRAWN: JAC	JOB NO: 2184.016	SCALE: NONE	

STATE	PROJECT NUMBER	SHEET NO.	TOTAL
GA.	STP-012-1(71)		



1998 DHV PM = (000)  
 1998 DHV AM = 000  
 T AM = 10%  
 T PM = 7%  
 MM = MINIMAL VOLUME

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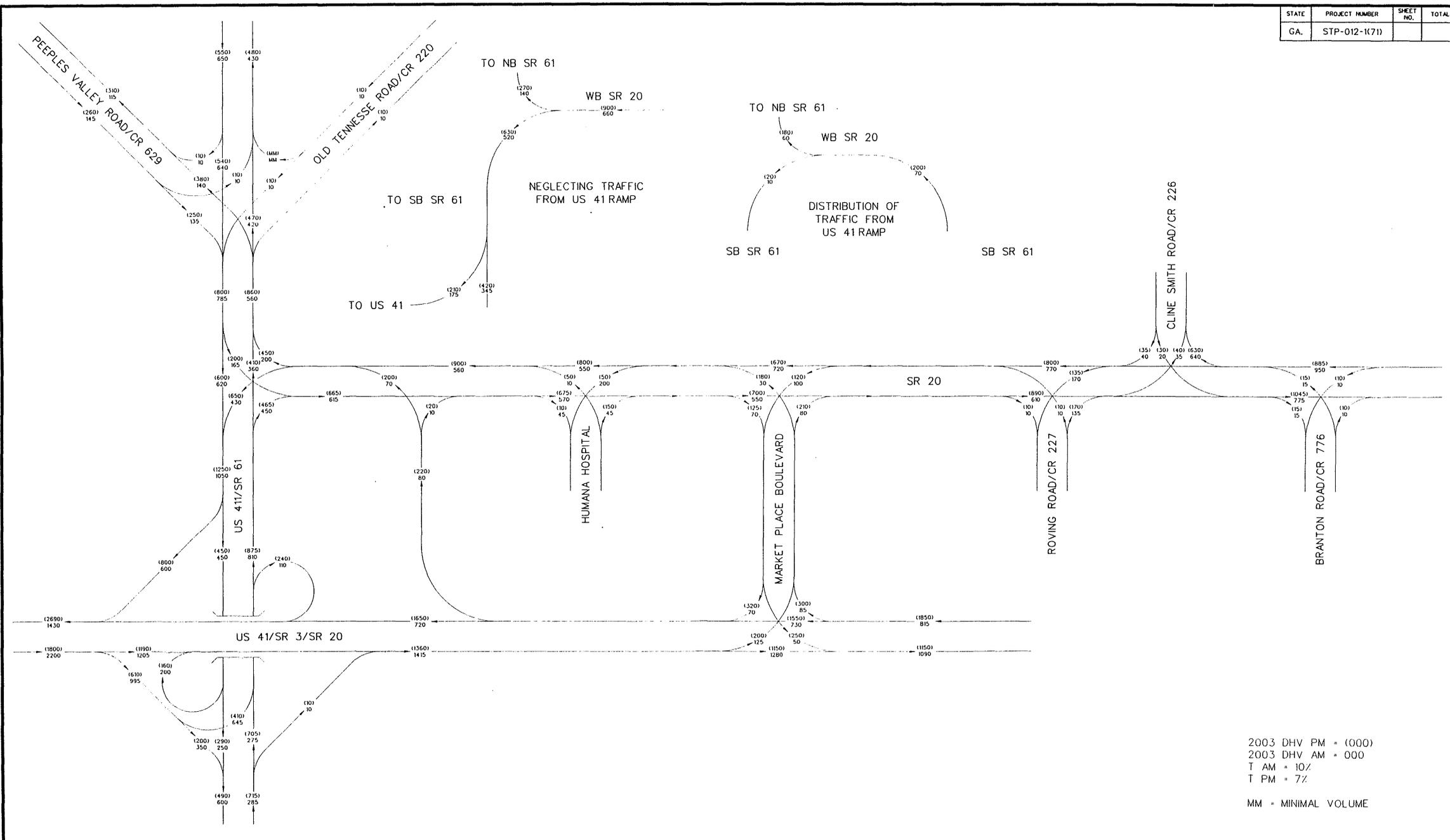
SR 20 FROM SR 61 TO I-75 - INCL. SR 3 & SR 61 INTERSECTIONS

TRAFFIC DIAGRAM  
 1998  
 HOURLY PEAK COUNTS

DESIGNED: MA	CHECKED:	DATE: JANUARY 1999	FIGURE 1.3
DRAWN: JAC	JOB NO: 2184 01E	SCALE: NONE	



STATE	PROJECT NUMBER	SHEET NO.	TOTAL
GA.	STP-012-1(71)		



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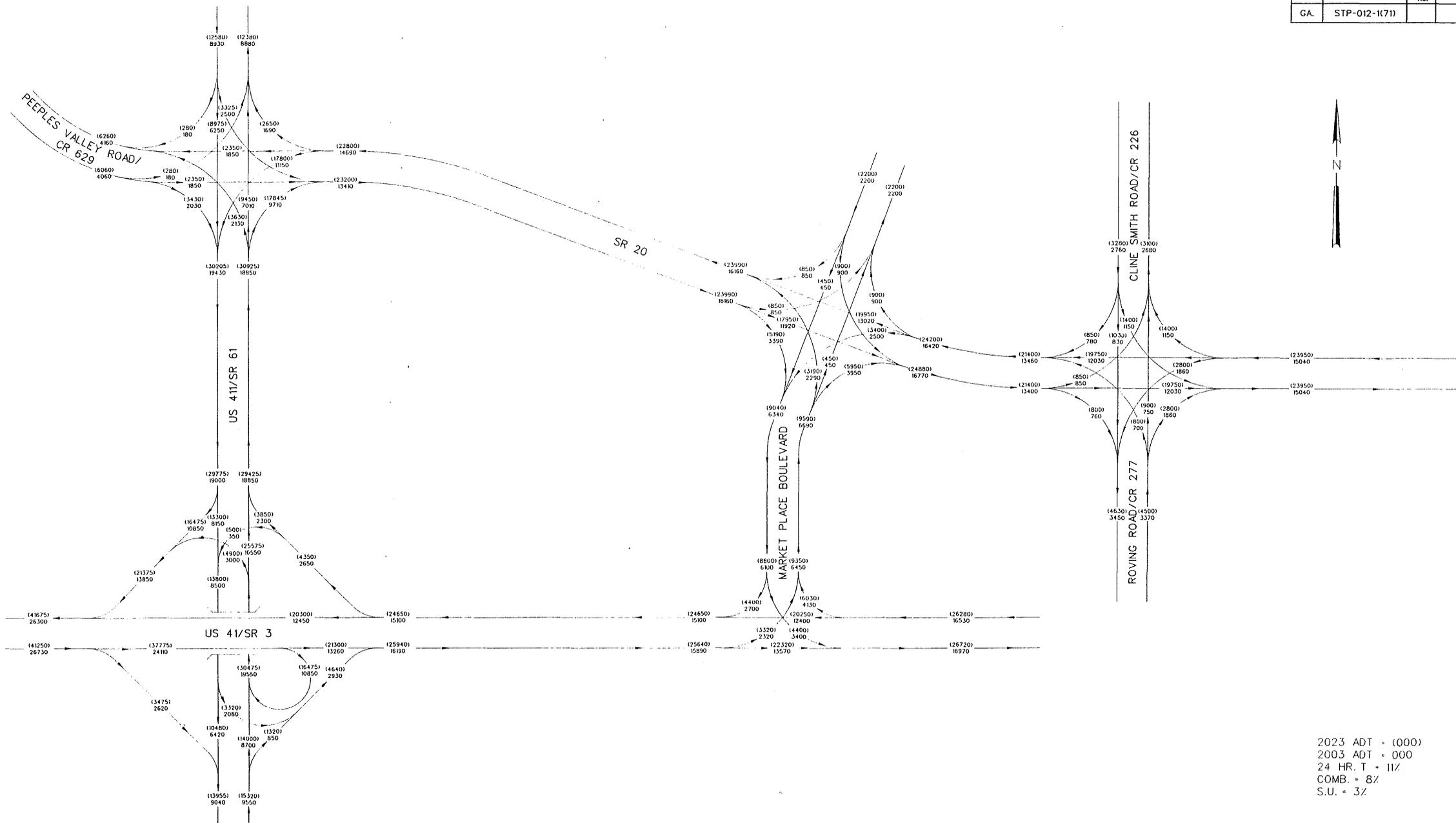
SR 20 FROM SR 61 TO I-75 - INCL. SR 3 & SR 61 INTERSECTIONS

TRAFFIC DIAGRAMS  
2003 NO BUILD  
HOURLY PEAK COUNTS

DESIGNED: MA	CHECKED:	DATE: JANUARY 1999
DRAWN: JAC	JOB NO: 2184.01L	SCALE: NONE

FIGURE 3.1

STATE	PROJECT NUMBER	SHEET NO.	TOTAL
GA.	STP-012-1(71)		



2023 ADT = (000)  
 2003 ADT = 000  
 24 HR. T = 11/2  
 COMB. = 8%  
 S.U. = 3%

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Jordan  
Jones &  
Goulding



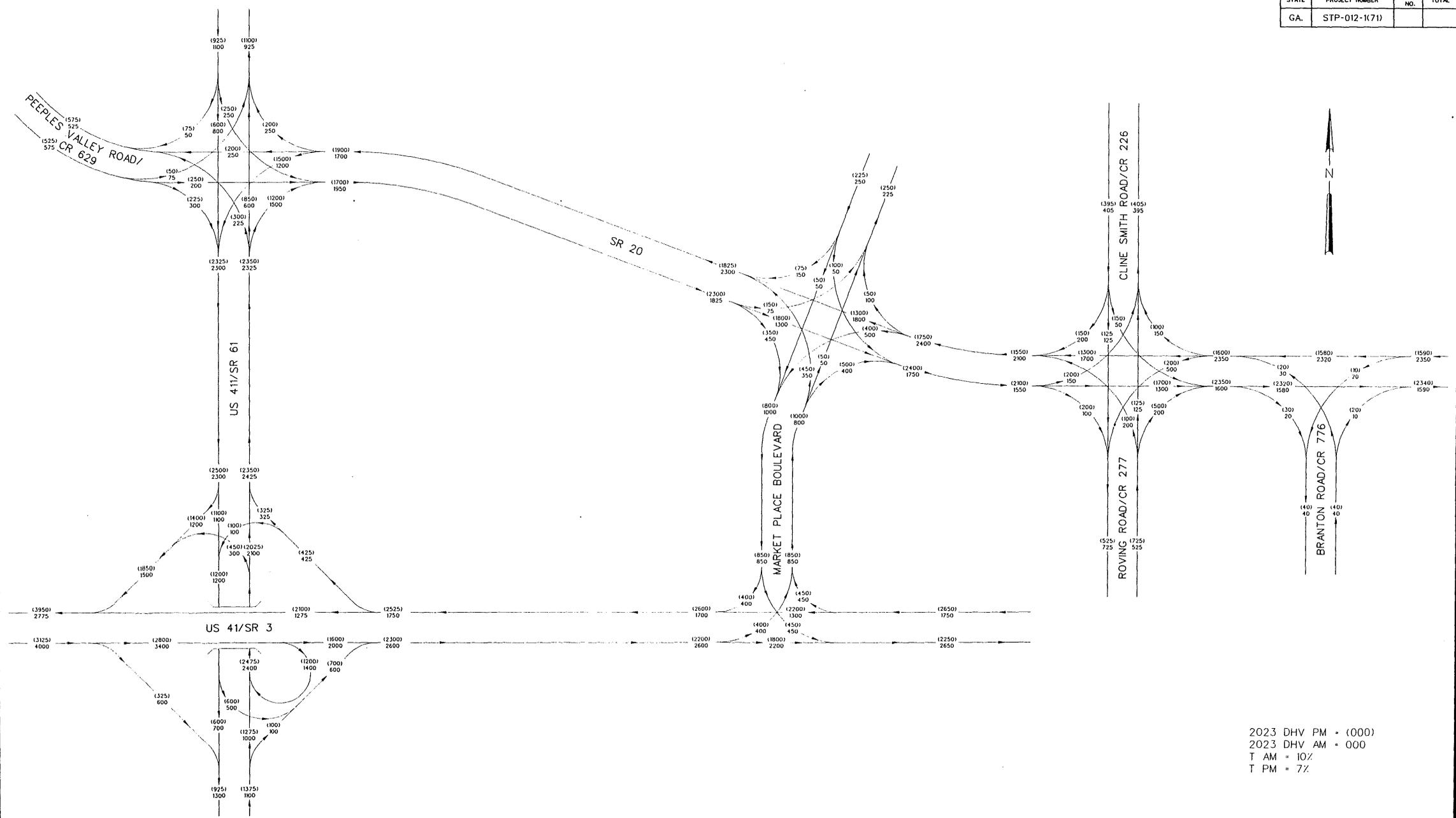
DWA  
DATA ENGINEERING ASSOCIATES, P.C.



BARTOW

SR 20 FROM SR 61 TO I-75 - INCL. SR 3 & SR 61 INTERSECTIONS			
TRAFFIC DIAGRAMS ALTERNATE #1 AVERAGE DAILY TRAFFIC			
DESIGNED: MA	CHECKED:	DATE: JANUARY, 1999	FIGURE 4.1
DRAWN: JAC	JOB NO: C184 016	SCALE: NONE	

STATE	PROJECT NUMBER	SHEET NO.	TOTAL
GA.	STP-012-1(71)		



2023 DHV PM = (000)  
 2023 DHV AM = 000  
 T AM = 10%  
 T PM = 7%

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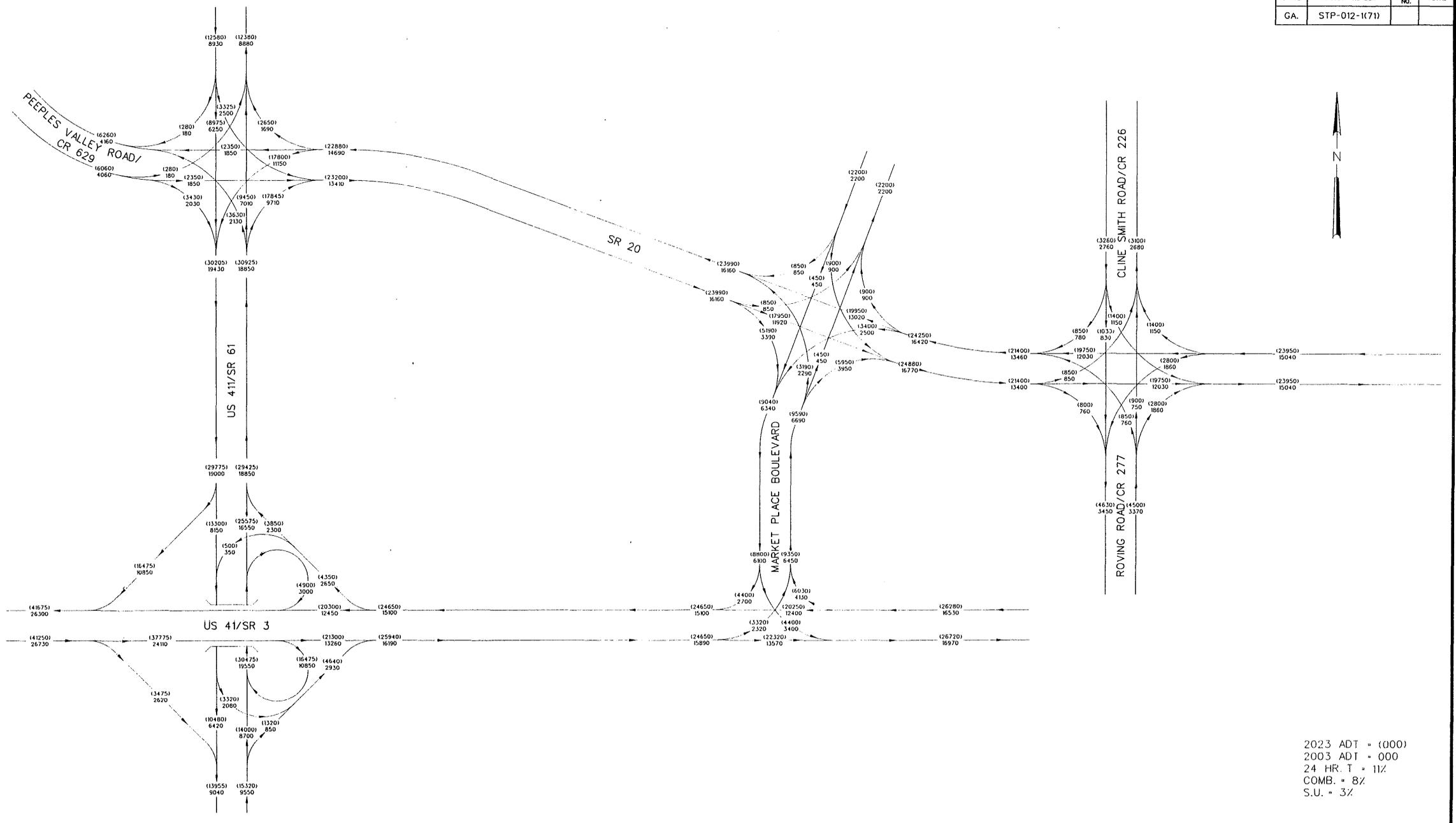
SR 20 FROM SR 61 TO I-75 - INCL. SR 3 & SR 61 INTERSECTIONS

TRAFFIC DIAGRAMS  
 ALTERNATE #1  
 DESIGN HOURLY VOLUMES

DESIGNED MA	CHECKED	DATE JANUARY 1999
DRAWN JAC	JOB NO. 2184.016	SCALE NONE

FIGURE 4.2

STATE	PROJECT NUMBER	SHEET NO.	TOTAL
GA.	STP-012-1(71)		



2023 ADT = (000)  
 2003 ADT = 000  
 24 HR. T = 11%  
 COMB. = 8%  
 S.U. = 3%

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SR 20 FROM SR 61 TO I-75 - INCL. SR 3 & SR 61 INTERSECTIONS			
TRAFFIC DIAGRAMS ALTERNATE • 2 AVERAGE DAILY TRAFFIC			
DESIGNED: MA	CHECKED:	DATE: JANUARY 1999	FIGURE 4.3
DRAWN: JAC	JOB NO: 2184.016	SCALE: NONE	



DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA  
OFFICE OF ROAD AND AIRPORT DESIGN  
**REVISED**  
**PROJECT CONCEPT REPORT**

**STATE ROUTE 20**

**STP-012-1(71)**

**P.I. NO. 621350**

**BARTOW COUNTY**

FEDERAL ROUTE NO: F 12-1  
STATE ROUTE NO: 20, 61, 3  
GADOT P.I. NO: 621350

Date of Report: DECEMBER 20, 1999

**RECOMMENDATION FOR APPROVAL**

\_\_\_\_\_  
DATE

\_\_\_\_\_  
State Transportation Planning Administrator

\_\_\_\_\_  
DATE

\_\_\_\_\_  
State Programming Engineer

\_\_\_\_\_  
DATE

\_\_\_\_\_  
State Road and Airport Design Engineer

\_\_\_\_\_  
DATE

\_\_\_\_\_  
District Engineer

\_\_\_\_\_  
DATE

\_\_\_\_\_  
Project Review Engineer

1-4-2000

*Maria L. L. L.*

\_\_\_\_\_  
DATE

\_\_\_\_\_  
State Traffic Operations Engineer

\_\_\_\_\_  
DATE

\_\_\_\_\_  
State Bridge & Structural Design Engineer

\_\_\_\_\_  
DATE

\_\_\_\_\_  
State Environmental/Location Engineer

DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA  
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1/10/00  
DATE

Marta V. Riser  
State Transportation Planning Administrator

\_\_\_\_\_  
DATE

\_\_\_\_\_  
State Programming Engineer

\_\_\_\_\_  
DATE

\_\_\_\_\_  
State Road and Airport Design Engineer

\_\_\_\_\_  
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\_\_\_\_\_  
District Engineer

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Project Review Engineer

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\_\_\_\_\_  
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\_\_\_\_\_  
State Road and Airport Design Engineer

*1/10/00*

\_\_\_\_\_  
DATE

*Charles Jones*  
\_\_\_\_\_  
District Engineer

\_\_\_\_\_  
DATE

\_\_\_\_\_  
Project Review Engineer

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DATE

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State Traffic Operations Engineer

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State Bridge & Structural Design Engineer

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State Environmental/Location Engineer

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State Road and Airport Design Engineer

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District Engineer

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DATE

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Project Review Engineer

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DATE

\_\_\_\_\_  
State Traffic Operations Engineer

*12/30/99*  
\_\_\_\_\_  
DATE

*Paul V. Tulus Jr*  
\_\_\_\_\_  
State Bridge & Structural Design Engineer

\_\_\_\_\_  
DATE

\_\_\_\_\_  
State Environmental/Location Engineer

DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA  
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STATE ROUTE NO: 20, 61, 3  
GADOT P.I. NO: 621350

Date of Report: DECEMBER 20, 1999

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DATE <i>1/3/00</i>	State Transportation Planning Administrator <i>[Signature]</i>
DATE	State Programming Engineer
DATE	State Road and Airport Design Engineer
DATE	District Engineer
DATE	Project Review Engineer
DATE	State Traffic Operations Engineer
DATE	State Bridge & Structural Design Engineer
DATE	State Environmental/Location Engineer