

ORIGINAL TO GENERAL FILES

D.O.T. 66

**DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA**

**INTERDEPARTMENT CORRESPONDENCE**

**FILE** HPPN-EDS-84(27) Ware County **OFFICE** Preconstruction  
P. I. No. 522780  
*CWHutto* **DATE** November 24, 1999  
**FROM** C. Wayne Hutto, Assistant Director of Preconstruction  
**TO** SEE DISTRIBUTION

**SUBJECT PROJECT CONCEPT REPORT APPROVAL**

Attached for your files is the approval for subject project.

CWH/cj

Attachment

**DISTRIBUTION:**

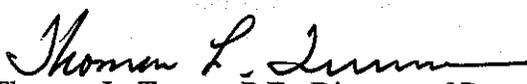
Tom Turner  
David Mulling  
David Studstill (ATTN: Harvey Keepler)  
Jerry Hobbs  
Herman Griffin  
Georgene Geary (ATTN: Michael Henry)  
Marion Waters  
Marta Rosen  
Paul Liles  
Don Mills  
Jim Kennerly  
Gary Priester

**DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA**

**INTERDEPARTMENT CORRESPONDENCE**

**FILE** HPPN-EDS-84(27) Ware County **OFFICE** Preconstruction  
P.I. No. 522780

**DATE** November 9, 1999

**FROM**   
Thomas L. Turner, P.E., Director of Preconstruction

**TO** Wayne Shackelford, Commissioner

**SUBJECT** PROJECT CONCEPT REPORT

This project is the widening and reconstruction of US 84/SR 38 from Needham Road (CR 79) to the intersection of SR 38C and US 82/SR 50/Corridor "Z", for a total of 4.80 miles. The existing roadway consists of two, 12' lanes with 10' rural shoulders on a variable 60' to 100' of existing right-of-way. The SR 38 improvements are part of the Governor's Road Improvement Program (GRIP) and involves the multi-laning of this primary east-west corridor in south Georgia, serving as a catalyst for the development of this region. As part of this program, the existing two lane SR 38/US 84 will be multi-laned to stimulate growth via an improved transportation network. The base year traffic (2001) varies from 6,050 - 8,300 VPD and the design year traffic (2021) ies from 9,600 - 13,300 VPD. The posted speeds are 45 MPH and 55 MPH. The proposed design speeds are 45 MPH and 65 MPH.

The proposed construction will provide two, 12' lanes in each direction separated by a median consisting of a 20' raised or 44' grassed transitioning back and forth throughout the entire project length. The project will require an additional 135' to 150' of right-of-way. No design exceptions are required to implement this project. The western terminus of this project ties to project EDS-84(26) Ware County, with a similar typical section. Traffic will be maintained utilizing stage construction.

Environmental concerns include requiring a COE 404 permit; approximately 19 acres of wetlands impacted; an Environmental Assessment will be prepared; 30 displacements - 20 residences; 1 business; 9 mobile homes; a public hearing will be held; time saving procedures are not appropriate.

The estimated costs for this project are:

	<u>PROPOSED</u>	<u>APPROVED</u>	<u>PROG DATE</u>	<u>LET DATE</u>
Construction (includes E&C and inflation)	\$6,916,000	\$3,600,000	2003	02-06
Right-of-Way	\$4,285,000	\$1,400,000		
Utilities*	-----	-----		

Wayne Shackelford  
Page 2

EDS-84(27) Ware  
November 9, 1999

\*LGPA to be sent.

This project is part of the Governor's Road Improvement Program (GRIP). I recommend this project concept be approved.

TLT:JDQ/cj

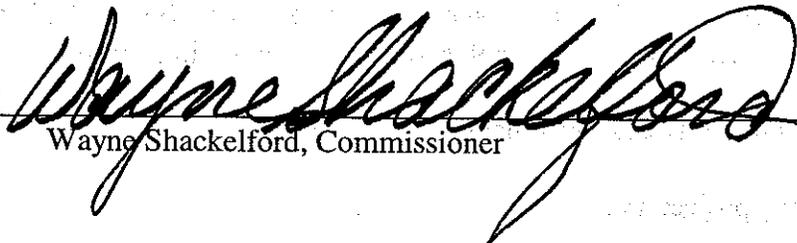
Attachment

CONCUR



Frank L. Danchetz, P.E., Chief Engineer

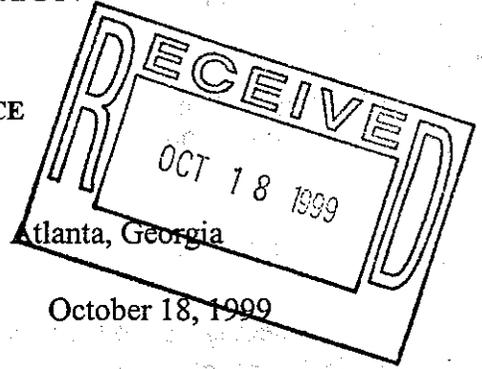
APPROVE



Wayne Shackelford, Commissioner

DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA

-----  
INTERDEPARTMENTAL CORRESPONDENCE



**FILE:** EDS-84(27) Ware  
P.I. Number 522780

**OFFICE:** Atlanta, Georgia  
**DATE:** October 18, 1999

**FROM:** David Mulling, Project Review Engineer *DM*

**TO:** Wayne Hutto, Assistant Director of Pre-construction

**SUBJECT: CONCEPT REPORT**

We have reviewed the concept report submitted October 14, 1999 by the letter from David Studstill dated October 12, 1999, and have the following comment:

1. Reimbursable utility cost information was not provided.

The costs for the project are:

Construction	\$5,715,000
Inflation	\$ 572,000
E&C	\$ 629,000
Reimbursable Utilities	\$ ?
Right of Way	\$ 4,285,000

DTM

c: David Studstill

DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA  
OFFICE OF ENVIRONMENT/LOCATION

**PROJECT CONCEPT REPORT**  
**EDS-84(27)**  
**WARE COUNTY**  
**P.I. NO. 522780**

Federal Route No.: F-7-3

Date of Report: June 1, 1999

State Route No.: 38

**RECOMMENDATION FOR APPROVAL**

10/2/99  
Date

*[Signature]*  
State Environmental/Location Engineer

\_\_\_\_\_  
Date

\_\_\_\_\_  
State Road & Airport Design Engineer

\_\_\_\_\_  
Date

\_\_\_\_\_  
State Traffic Operations Engineer

\_\_\_\_\_  
Date

\_\_\_\_\_  
District Engineer/Jesup

\_\_\_\_\_  
Date

\_\_\_\_\_  
State Bridge & Structural Engineer

\_\_\_\_\_  
Date

\_\_\_\_\_  
State Transportation Planning Administrator

\_\_\_\_\_  
Date

\_\_\_\_\_  
State Transportation Programming Administrator

DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA  
OFFICE OF ENVIRONMENT/LOCATION

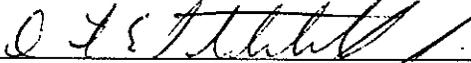
**PROJECT CONCEPT REPORT**  
**EDS-84(27)**  
**WARE COUNTY**  
**P.I. NO. 522780**

Federal Route No.: F-7-3

Date of Report: June 1, 1999

State Route No.: 38

**RECOMMENDATION FOR APPROVAL**

10/7/99   
Date State Environmental/Location Engineer

\_\_\_\_\_  
Date State Road & Airport Design Engineer

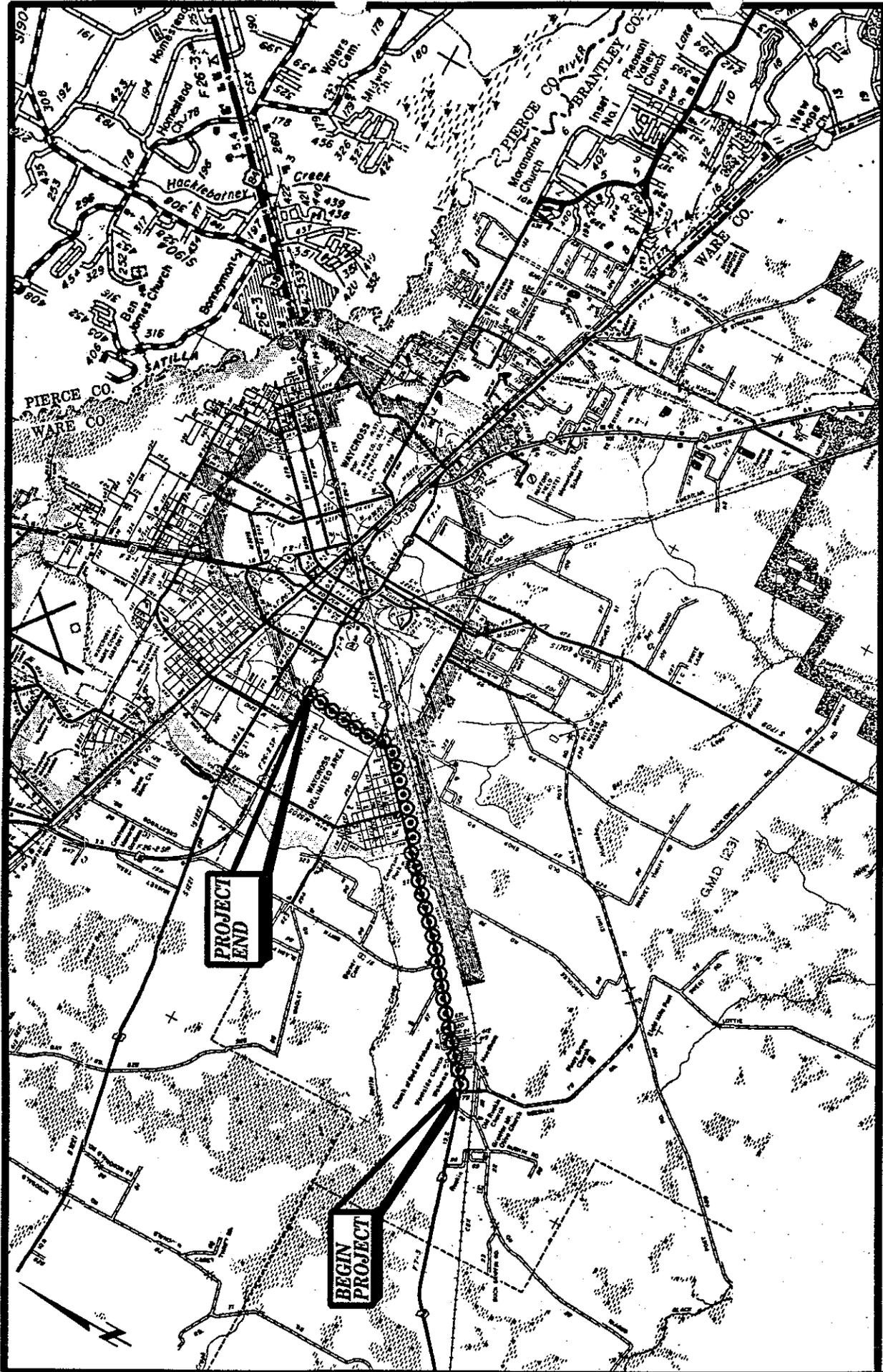
\_\_\_\_\_  
Date State Traffic Operations Engineer

\_\_\_\_\_  
Date District Engineer/Jesup

\_\_\_\_\_  
Date State Bridge & Structural Engineer

\_\_\_\_\_  
Date State Transportation Planning Administrator

\_\_\_\_\_  
Date State Transportation Programming Administrator



LOCATION

STRIPMAP  
 EDS-84(27)  
 SR 38SR 38CCORRIDOR Z  
 WARE COUNTY  
 P.I.# 522780  
 5/999



SOURCE: GENERAL HIGHWAY MAP, WARE CO. 1988, PIERCE CO. 1998, GEORGIA  
 PREPARED BY THE GEORGIA DEPARTMENT OF TRANSPORTATION.

**PROJECT CONCEPT REPORT**

**PROJECT NUMBER: EDS-84(27)WARE**

**PROJECT LOCATION AND DESCRIPTION**

Project EDS-84(27), in Ware county, would widen and reconstruct US 84/SR 38 from the CR 79/Needham Road to the intersection SR 38C and US 82/SR 50 for a distance of approximately 4.8 miles. This project ties into the EDS-84(26) at the beginning terminus.

The project would begin at the intersection of US 84/SR 38 and CR 79/Needham Road. The project would reconstruct US 84/SR 38 along the south side, adding a 44-foot grassed median with four 12-foot lanes of pavement, acquiring proposed right of way along both sides of the US 84/SR 38. It would continue to approximately 400 hundred feet east of CR 79/Needham Road. This continues the concept proposed for EDS-84(26). At that point the, project would widen US 84/SR 38 along the south side, adding two 12-foot lanes of pavement, using the existing pavement to approximately 3,000 feet east of CR 91/Smith Road. There, the concept makes a transition to an urban section with a 20 foot raised median. This concept would continue eastward to CR 290/Oregon Avenue. At that point, the alignment would shift onto new location south of US 84/SR 38 and a rural typical section would begin and continue eastward for a distance of approximately 1.2 miles. The alignment would tie back into Glenmore Avenue and continue along SR 38C/Victory Drive. The concept would hold the existing right of way along the west side and widen to the east side, constructing a 20 foot raised median, adding two 12-foot lanes of pavement in an urban section. SR 38C/Victory Drive currently has four 12-foot lanes with curb and gutter and a 14-foot flush median. This alignment would continue northward to the intersection of SR 38C/Victory Drive and US 82/SR 50/Corridor "Z", where the project would end. (This concept would use existing pavement.) The additional R/W would vary from 135 feet to 150 feet. The speed design is 45 and 65 MPH.

**TRAFFIC**

CURRENT		PROJECTED	
YEAR	AADT	YEAR	AADT
2001	6,050	2021	9,600
	8,300		13,300

PDP CLASSIFICATION	FUNCTIONAL CLASSIFICATION
MAJOR / NEW CONSTRUCTION & CONSTRUCTION ALONG EXISTING ROADWAY	RURAL ARTERIAL
<del>NON-CA ( )</del> CA ( )	N/A (X) EXEMPT ( )
FOS ( )	

**NEED AND PURPOSE**

The S.R 38 Improvements are part of the Governors Road Improvement Program(G.R.I.P.) and involves the multi-laning of this primary east-west corridor in south Georgia, serving as a catalyst for the development of this region. The improvements will aid in the economic development of sparsely populated rural areas and small towns along this route. Traffic carrying capacity will be increased; safety and operational characteristics along this segment will be improved.

**EXISTING ROADWAY**

<b>TYPICAL SECTION:</b>	2 – 12 foot lanes 10 foot shoulders-rural	<b>R/W WIDTH</b> 60 feet – 100 feet
<b>POSTED SPEED</b>	<sup>DEGREE</sup> <b>MINIMUM RADIUS OF CURVE</b>	<b>MAX GRADE</b>
45 & 55 MPH	5°-45' <del>Radius</del>	0.38 %

**MAJOR STRUCTURES**

FEATURES INTERSECTED/TYPE	LENGTH	WIDTH	PRIORITY RATING	SUFF. RATING
None				

**PROPOSED ROADWAY**

<b>TYPICAL SECTION:</b>	Four 12-foot lanes w/44-foot depressed grassed median /Four 12-foot lanes w/20-foot raised median in an urban and rural sections			
<b>DESIGN SPEED</b>	<sup>DEGREE</sup> <b>MINIMUM RADIUS OF CURVE</b>	<b>MAX GRADE</b>		
45 & 65 MPH	<b>ALLOWABLE</b>	3°-45' <del>Radius</del>	<b>ALLOWABLE</b>	4.0 %
	<b>PROPOSED</b>	3°-45' <del>Radius</del>	<b>PROPOSED</b>	0.38 % <span style="float: right;">N.L. 0.5%</span>

**MAJOR STRUCTURES**

FEATURES INTERSECTED/TYPE	LENGTH	WIDTH

**RIGHT-OF-WAY**

ADDITIONAL RIGHT-OF WAY WIDTH	PARCELS IMPACTED	DISPLACEMENTS
Varies from 135 feet to 150 feet	149	BUS. : 1 RES.: 20 M.H.: 9

**TYPE OF ACCESS CONTROL:** By permit

---

**ESTIMATED COST**

---

<b>CONSTRUCTION:</b>	\$ 5,715,000	<b>RIGHT-OF-WAY:</b>	\$ 4,285,000
<b>E &amp; C (10%):</b>	\$ 571,000	<b>ACQUIRED BY:</b>	DOT
<b>INFLATION:</b>	\$ 646,000	<b>UTILITIES:</b>	To be requested
<b>(2 yrs at 5% per yr):</b>		<b>ADJUSTED BY:</b>	LGPA & D.O.T.
<b>TOTAL CONST COST:</b>	\$ 6,932,000		

---

**COORDINATION**

---

**CONCEPT TEAM MEETING DATE:** June 23, 1999

**ENVIRONMENTAL CONCERNS** None

**PERMITS REQUIRED:** C.O.E. 404 (Approx. 19 acres of wetlands)

**LEVEL OF PUBLIC INVOLVEMENT:** Public Hearing to be scheduled

**TIME SAVING PROCEDURES APPROPRIATE:** NO

**OTHER PROJECTS IN THE AREA:** EDS-84(23)Clinch/Ware, EDS-84(26)Ware

---

**MISCELLANEOUS**

---

**TRAFFIC CONTROL DURING CONSTRUCTION:** Maintain existing traffic during construction

**LEVEL OF ENVIRONMENTAL ANALYSIS:** (EA) Environmental Assessment

**UNDERGROUND STORAGE TANKS:** Study to be requested.

**HAZARDOUS WASTE SITES:** Study to be requested.

**DESIGN VARIATIONS REQUESTED:**

	YES	NO	UNDETERMINED
SUBST HORIZ ALIGNMENT	( )	(X)	( )
SUBST ROADWAY WIDTH	( )	(X)	( )
SUBST SHOULDER WIDTH	( )	(X)	( )
SUBST VERTICAL GRADES	( )	(X)	( )
SUBST CROSS SLOPES	( )	(X)	( )
SUBST STOPPING SIGHT DIST	( )	(X)	( )
SUBST SUPERELEV RATES	( )	(X)	( )
SUBST HORIZONTAL CLEARANCE	( )	(X)	( )
SUBST SPEED DESIGN	( )	(X)	( )
SUBST VERTICAL CLEARANCE	( )	(X)	( )
SUBST BRIDGE WIDTH	( )	(X)	( )
SUBST BR STRUCT CAPACITY	( )	(X)	( )

---

---

### ALTERNATIVES CONSIDERED

---

1. No build.
  2. Widen on opposing side of proposed roadway. Was eliminated due to residential displacements or history impacts
  3. Southeast bypass around Waycross. Because of wetland impacts, decided to consider a portion of Southeast bypass in a possible future project.
  4. SR 38-Smith Road-Ramping system along corridor 'Z'. Was eliminated because of history impacts along ramping system.
  5. SR 38-38C/Victory Drive-Ramping system along corridor 'Z'. Was eliminated because of history impacts along ramping system.
- 

### COMMENTS

---

1. This project will match existing vertical alignment.
2. This alignment has not been approved through the 404-B(1) guideline process with the Federal resource agencies and is subject to change.
3. Consider building a southeast bypass around Waycross starting from US 1/SR 50 to US 84/SR 38 in Pierce County.

**ATTACHMENTS:** COST ESTIMATE, TYPICAL SECTIONS AND TEAM MINUTES

**PREPARED BY:** Radney Simpson, L.E. I

**PRELIMINARY COST ESTIMATE**  
**OFFICE OF ENVIRONMENT/LOCATION**

**P.I. NO:** 522780

**DATE:** 05-26-1999

**PROJECT NO:** EDS-84(27)

**PROJECT NAME:** SR 38 IMPROVEMENTS

**COUNTY:** WARE

**PROJECT DESCRIPTION:**

SR 38 IMPROVEMENTS FROM THE INTERSECTION OF SR  
38/NEEDHAM ROAD TO THE INTERSECTION OF SR 38C/VICTORY DRIVE

**PROJECT LENGTH:** 4.800 MILES

**SECTION LENGTH:** 2.620 MILES

**TYPICAL SECTION:**

RURAL WIDENING-2 TO 4 LANES WITH 44' DIV MEDIAN (48' PAV'T)

PROPOSED R / W = 200 ft

**EXISTING ROADWAY (If Applicable):**

US 84/SR 38

**TRAFFIC:**

**INITIAL DESIGN YEAR:** 2001

**DAILY VOLUME (AADT):** 8,300

**FINAL DESIGN YEAR:** 2021

**DAILY VOLUME (AADT):** 13,300

**COMMENTS:**

**PREPARED BY:** Radney Simpson LE I

# PROJECT COSTS

## A. RIGHT - OF - WAY

1. PROPERTY (Land and Easements)	\$	0
2. DISPLACEMENTS	\$	0
3. OTHER COST	\$	0
SUBTOTAL	\$	0

## B. REIMBURSABLE UTILITIES

1. RAILROAD	\$	0
2. TRANSMISSION LINES	\$	0
3. SERVICES	\$	0
SUBTOTAL	\$	0

## C. MAJOR STRUCTURES

1. WALLS	\$	0
<hr/>		
2. BRIDGE STREAM CROSSING	\$	0
<hr/>		
3. BRIDGE OVER/UNDERPASS	\$	0
<hr/>		
4. BOX CULVERTS	\$	0
<hr/>		
SUBTOTAL	\$	0

## D. GRADING AND DRAINAGE

### 1. EARTHWORK

a. UNCLASSIFIED EXCAVATION SOIL	\$	16,000
4,700 CY @ \$3.35		
<hr/>		
b. UNCLASSIFIED EXCAVATION ROCK	\$	0
0 CY @ \$4.00		
<hr/>		
c. BORROW EXCAVATION	\$	45,000
11,260 CY @ \$4.00		
<hr/>		

### 2. DRAINAGE

a. MINOR DRAINAGE (Including Cross Drain Pipes & Longitudinal System)	\$	105,000
2.620 MILES @ \$39,960		
<hr/>		
b. CURB AND GUTTER	\$	0
0 LF @ \$9.27		
<hr/>		

SUBTOTAL \$ 166,000

**PROJECT COSTS**

con't.

**E. BASE AND PAVING**

1. GRADED AGGREGATE BASE	\$	412,000
12.00" -- 33,201 T @ \$12.40		
2. ASPHALT PAVING		
a. ASPHALTIC CONCRETE "E"	\$	308,000
1.50" -- 9,545 T @ \$32.22		
b. ASPHALTIC CONCRETE "B"	\$	406,000
2.00" -- 12,727 T @ \$31.87		
c. ASPHALTIC CONCRETE BASE	\$	524,000
8.00" -- 16,969 T @ \$30.85		
d. BITUMINOUS TACK COAT	\$	7,000
8,807 G @ \$0.85		
3. CONCRETE PAVING	\$	0
4. OTHER PAVING	\$	166,000
	<b>SUBTOTAL \$</b>	<b>1,823,000</b>

**F. LUMP ITEMS**

1. TRAFFIC CONTROL	\$	44,000
2. CLEARING AND GRUBBING	\$	299,000
64 ACRES @ \$4,700		
3. LANDSCAPING	\$	47,000
2.620 MILES @ \$18,000		
4. EROSION CONTROL	\$	63,000
2.620 MILES @ \$24,000		
5. DETOURS (Including Temporary Bridges)	\$	0
	<b>SUBTOTAL \$</b>	<b>453,000</b>

**G. MISCELLANEOUS**

1. SIGNING/STRIPING	\$	79,000
2.620 MILES @ \$30,000		
2. GUARDRAIL	\$	0
0 LF @ \$11.46 + 0 Anchors @ \$1,326.82		
3. OTHER	\$	92,000
2.620 MILES @ \$35,100		
	<b>SUBTOTAL \$</b>	<b>171,000</b>

**H. SPECIAL FEATURES**

\$ 0

## ESTIMATE SUMMARY

		SECTION COST (per mile)
A. RIGHT-OF-WAY	\$ 0	\$ 0
B. REIMBURSABLE UTILITIES	\$ 0	\$ 0

### CONSTRUCTION COST SUMMARY

C. MAJOR STRUCTURES	\$ 0	
D. GRADING AND DRAINAGE	\$ 166,000	
E. BASE AND PAVING	\$ 1,823,000	
F. LUMP ITEMS	\$ 453,000	
G. MISCELLANEOUS	\$ 171,000	
H. SPECIAL FEATURES	<u>\$ 0</u>	
SUBTOTAL CONSTRUCTION COST	\$ 2,613,000	\$ 997,000
E. & C. (10%)	\$ 261,000	
INFLATION 2 yr(s) @ 5% per year	<u>\$ 295,000</u>	
TOTAL CONSTRUCTION COST	\$ 3,169,000	\$ 1,210,000

---

<b>GRAND TOTAL CONSTRUCTION COST</b>	<b>\$ 3,169,000</b>	<b>\$ 1,210,000</b>
--------------------------------------	---------------------	---------------------

RURAL WIDENING-2 TO 4 LANES WITH 44' DIV MEDIAN (48' PAV'T)

**PRELIMINARY COST ESTIMATE**  
**OFFICE OF ENVIRONMENT/LOCATION**

P.I. NO: 522780

DATE: 05-26-1999

PROJECT NO: EDS-84(27)

PROJECT NAME: SR 38 IMPROVEMENTS

COUNTY: WARE

**PROJECT DESCRIPTION:**

SR 38 IMPROVEMENTS FROM THE INTERSECTION OF SR  
38/NEEDAM RD TO THE INTERSECTION OF SR38C/VICTORY DR.

PROJECT LENGTH: 4.800 MILES

SECTION LENGTH: 1.000 MILES

**TYPICAL SECTION:**

RECONSTRUCTING 2 TO 4 LANES WITH 20' RAISED MEDIAN WIDEN ON ONE SIDE (48' PAVT)

**EXISTING ROADWAY (If Applicable):**

US 84/SR 38

**TRAFFIC:**

INITIAL DESIGN YEAR: 2001

DAILY VOLUME (AADT): 8,300

FINAL DESIGN YEAR: 2021

DAILY VOLUME (AADT): 13,300

**COMMENTS:**

Cost to upgrade 14 foot flush median to 20 foot raised

PREPARED BY: Radney Simpson LE I

**PROJECT COSTS**

**A. RIGHT - OF - WAY**

1. PROPERTY (Land and Easements)	\$	0
2. DISPLACEMENTS	\$	0
3. OTHER COST	\$	0

**SUBTOTAL \$ 0**

**B. REIMBURSABLE UTILITIES**

1. RAILROAD	\$	0
2. TRANSMISSION LINES	\$	0
3. SERVICES	\$	0

**SUBTOTAL \$ 0**

**C. MAJOR STRUCTURES**

1. WALLS	\$	0
----------	----	---

---

2. BRIDGE STREAM CROSSING	\$	0
---------------------------	----	---

---

3. BRIDGE OVER/UNDERPASS	\$	0
--------------------------	----	---

---

4. BOX CULVERTS	\$	0
-----------------	----	---

---

**SUBTOTAL \$ 0**

**D. GRADING AND DRAINAGE**

**1. EARTHWORK**

a. UNCLASSIFIED EXCAVATION SOIL	\$	0
---------------------------------	----	---

0 CY @ \$2.00

---

b. UNCLASSIFIED EXCAVATION ROCK	\$	0
---------------------------------	----	---

0 CY @ \$4.00

---

c. BORROW EXCAVATION	\$	0
----------------------	----	---

0 CY @ \$3.00

**2. DRAINAGE**

a. MINOR DRAINAGE (Including Cross Drain Pipes & Longitudinal System)	\$	158,000
---	----	---------

1.000 MILES @ \$157,950

---

b. CURB AND GUTTER	\$	147,000
--------------------	----	---------

15,840 LF @ \$9.27

---

**SUBTOTAL \$ 305,000**

# PROJECT COSTS

con't.

## E. BASE AND PAVING

1. GRADED AGGREGATE BASE	\$	126,000
12.00" -- 10,138 T @ \$12.40		
2. ASPHALT PAVING		
a. ASPHALTIC CONCRETE "E"	\$	78,000
1.50" -- 2,429 T @ \$32.22		
b. ASPHALTIC CONCRETE "B"	\$	103,000
2.00" -- 3,238 T @ \$31.87		
c. ASPHALTIC CONCRETE BASE	\$	200,000
8.00" -- 6,477 T @ \$30.85		
d. BITUMINOUS TACK COAT	\$	2,000
2,726 G @ \$0.85		
3. CONCRETE PAVING	\$	0
4. OTHER PAVING	\$	51,000
	SUBTOTAL \$	560,000

## F. LUMP ITEMS

1. TRAFFIC CONTROL	\$	26,000
2. CLEARING AND GRUBBING	\$	57,000
12 ACRES @ \$4,700		
3. LANDSCAPING	\$	16,000
1.000 MILES @ \$15,960		
4. EROSION CONTROL	\$	19,000
1.000 MILES @ \$19,050		
5. DETOURS (Including Temporary Bridges)	\$	0
	SUBTOTAL \$	118,000

## G. MISCELLANEOUS

1. SIGNING/STRIPING	\$	37,000
1.000 MILES @ \$37,000		
2. GUARDRAIL	\$	0
0 LF @ \$11.46 + 0 Anchors @ \$1,326.82		
3. OTHER	\$	80,000
1.000 MILES @ \$80,100		
	SUBTOTAL \$	117,000

## H. SPECIAL FEATURES

\$ 0

**ESTIMATE SUMMARY**

		<b>SECTION COST (per mile)</b>
<b>A. RIGHT-OF-WAY</b>	\$ 0	\$ 0
<b>B. REIMBURSABLE UTILITIES</b>	\$ 0	\$ 0

**CONSTRUCTION COST SUMMARY**

<b>C. MAJOR STRUCTURES</b>	\$ 0	
<b>D. GRADING AND DRAINAGE</b>	\$ 305,000	
<b>E. BASE AND PAVING</b>	\$ 560,000	
<b>F. LUMP ITEMS</b>	\$ 118,000	
<b>G. MISCELLANEOUS</b>	\$ 117,000	
<b>H. SPECIAL FEATURES</b>	<u>\$ 0</u>	
<b>SUBTOTAL CONSTRUCTION COST</b>	<b>\$ 1,100,000</b>	<b>\$ 1,100,000</b>
<b>E. &amp; C. (10%)</b>	\$ 110,000	
<b>INFLATION 2 yr(s) @ 5% per year</b>	<u>\$ 125,000</u>	
<b>TOTAL CONSTRUCTION COST</b>	<b>\$ 1,335,000</b>	<b>\$ 1,335,000</b>
<hr/>		
<b>GRAND TOTAL CONSTRUCTION COST</b>	<b>\$ 1,335,000</b>	<b>\$ 1,335,000</b>

URBAN WIDENING-2 TO 4 LANES WITH 20' RAISED MEDIAN WIDEN ON ONE SIDE (48' PAV'T)

**PRELIMINARY COST ESTIMATE**  
**OFFICE OF ENVIRONMENT/LOCATION**

**P.I. NO:** 522780

**DATE:** 05-26-1999

**PROJECT NO:** EDS-84(27)

**PROJECT NAME:** SR 38 IMPROVEMENTS

**COUNTY:** WARE

**PROJECT DESCRIPTION:**

SR 38 IMPROVEMENTS FROM THE INTERSECTION OF SR  
38/NEEDHAM ROAD TO THE INTERSECTION OF SR 38C/VICTORY DRIVE

**PROJECT LENGTH:** 4.800 MILES

**SECTION LENGTH:** 1.180 MILES

**TYPICAL SECTION:**

RURAL NEW LOCATION-4-LANES WITH 20' RAISED MEDIAN (48' PAV'T)

\*PROPOSED R / W = 150 ft

**EXISTING ROADWAY (If Applicable):**

US 84/SR 38

**TRAFFIC:**

**INITIAL DESIGN YEAR:** 2001

**DAILY VOLUME (AADT):** 8,300

**FINAL DESIGN YEAR:** 2021

**DAILY VOLUME (AADT):** 13,300

**COMMENTS:**

**PREPARED BY:** Radney Simpson LE I

# PROJECT COSTS

## A. RIGHT - OF - WAY

1. PROPERTY (Land and Easements)	\$	0
2. DISPLACEMENTS	\$	0
3. OTHER COST	\$	0
<b>SUBTOTAL</b>	<b>\$</b>	<b>0</b>

## B. REIMBURSABLE UTILITIES

1. RAILROAD	\$	0
2. TRANSMISSION LINES	\$	0
3. SERVICES	\$	0
<b>SUBTOTAL</b>	<b>\$</b>	<b>0</b>

## C. MAJOR STRUCTURES

1. WALLS	\$	0
<hr/>		
2. BRIDGE STREAM CROSSING	\$	0
<hr/>		
3. BRIDGE OVER/UNDERPASS	\$	0
<hr/>		
4. BOX CULVERTS	\$	0
<hr/>		
<b>SUBTOTAL</b>	<b>\$</b>	<b>0</b>

## D. GRADING AND DRAINAGE

### 1. EARTHWORK

a. UNCLASSIFIED EXCAVATION SOIL	\$	44,000
13,000 CY @ \$3.35		
b. UNCLASSIFIED EXCAVATION ROCK	\$	0
0 CY @ \$4.00		
c. BORROW EXCAVATION	\$	152,000
38,000 CY @ \$4.00		

### 2. DRAINAGE

a. MINOR DRAINAGE (Including Cross Drain Pipes & Longitudinal System)	\$	80,000
1.180 MILES @ \$68,040		
b. CURB AND GUTTER	\$	116,000
12,461 LF @ \$9.27		

**SUBTOTAL \$ 392,000**

**PROJECT COSTS**

con't.

**E. BASE AND PAVING**

<b>1. GRADED AGGREGATE BASE</b>	\$	<u>321,000</u>
12.00" -- 25,918 T @ \$12.40		
<b>2. ASPHALT PAVING</b>		
<b>a. ASPHALTIC CONCRETE "E"</b>	\$	<u>108,000</u>
1.50" -- 3,344 T @ \$32.22		
<b>b. ASPHALTIC CONCRETE "B"</b>	\$	<u>142,000</u>
2.00" -- 4,458 T @ \$31.87		
<b>c. ASPHALTIC CONCRETE BASE</b>	\$	<u>472,000</u>
8.00" -- 15,285 T @ \$30.85		
<b>d. BITUMINOUS TACK COAT</b>	\$	<u>4,000</u>
5,182 G @ \$0.85		
<b>3. CONCRETE PAVING</b>	\$	<u>0</u>
<b>4. OTHER PAVING</b>	\$	<u>105,000</u>
	<b>SUBTOTAL \$</b>	<b><u>1,152,000</u></b>

**F. LUMP ITEMS**

<b>1. TRAFFIC CONTROL</b>	\$	<u>15,000</u>
<b>2. CLEARING AND GRUBBING</b>	\$	<u>101,000</u>
21 ACRES @ \$4,700		
<b>3. LANDSCAPING</b>	\$	<u>102,000</u>
1.180 MILES @ \$86,040		
<b>4. EROSION CONTROL</b>	\$	<u>54,000</u>
1.180 MILES @ \$46,050		
<b>5. DETOURS</b> (Including Temporary Bridges)	\$	<u>0</u>
	<b>SUBTOTAL \$</b>	<b><u>272,000</u></b>

**G. MISCELLANEOUS**

<b>1. SIGNING/STRIPING</b>	\$	<u>44,000</u>
1.180 MILES @ \$37,000		
<b>2. GUARDRAIL</b>	\$	<u>0</u>
0 LF @ \$11.46 + 0 Anchors @ \$1,326.82		
<b>3. OTHER</b>	\$	<u>142,000</u>
1.180 MILES @ \$120,000		
	<b>SUBTOTAL \$</b>	<b><u>186,000</u></b>

**H. SPECIAL FEATURES**

\$ 0

**ESTIMATE SUMMARY**

		<b>SECTION COST (per mile)</b>
<b>A. RIGHT-OF-WAY</b>	\$ 0	\$ 0
<b>B. REIMBURSABLE UTILITIES</b>	\$ 0	\$ 0

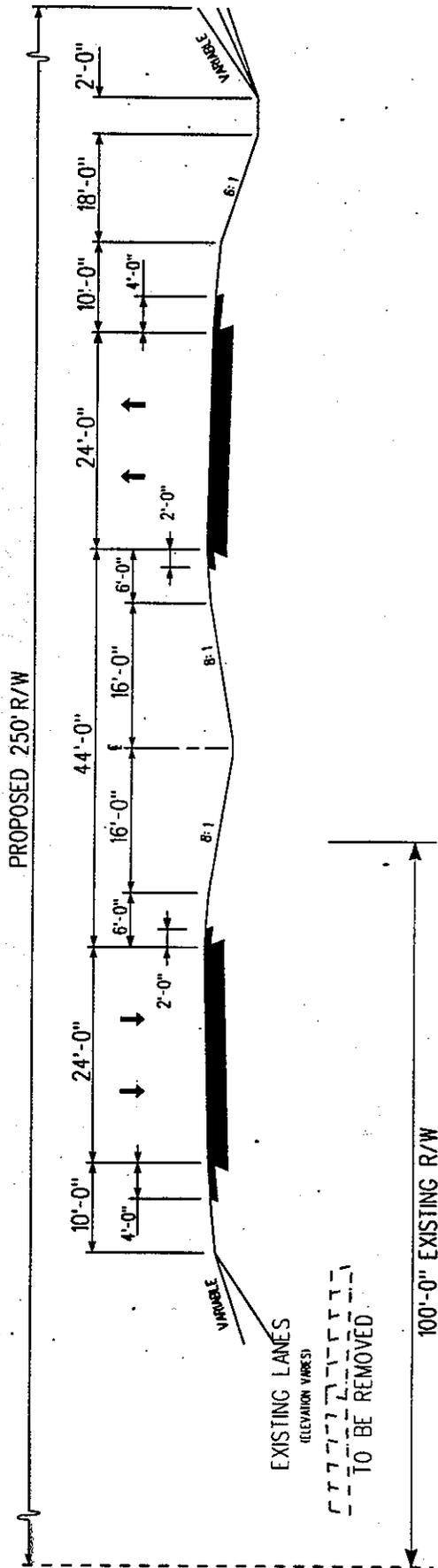
**CONSTRUCTION COST SUMMARY**

<b>C. MAJOR STRUCTURES</b>	\$ 0	
<b>D. GRADING AND DRAINAGE</b>	\$ 392,000	
<b>E. BASE AND PAVING</b>	\$ 1,152,000	
<b>F. LUMP ITEMS</b>	\$ 272,000	
<b>G. MISCELLANEOUS</b>	\$ 186,000	
<b>H. SPECIAL FEATURES</b>	<u>\$ 0</u>	
<b>SUBTOTAL CONSTRUCTION COST</b>	<b>\$ 2,002,000</b>	<b>\$ 1,697,000</b>
<b>E. &amp; C. (10%)</b>	\$ 200,000	
<b>INFLATION 2 yr(s) @ 5% per year</b>	<u>\$ 226,000</u>	
<b>TOTAL CONSTRUCTION COST</b>	<b>\$ 2,428,000</b>	<b>\$ 2,058,000</b>

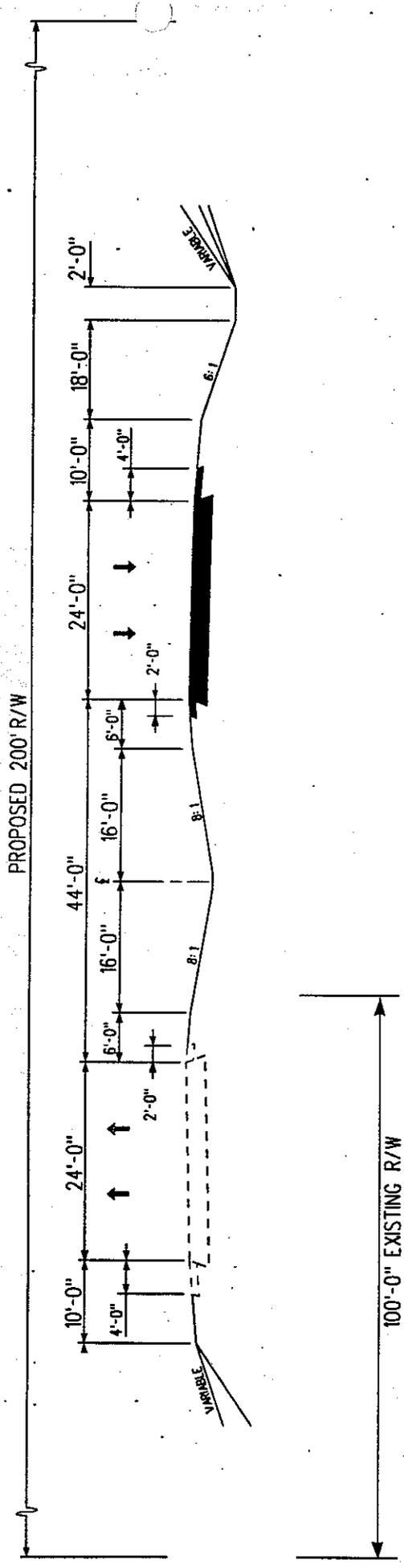
---

<b>GRAND TOTAL CONSTRUCTION COST</b>	<b>\$ 2,428,000</b>	<b>\$ 2,058,000</b>
--------------------------------------	---------------------	---------------------

RURAL NEW LOCATION-4-LANES WITH 20' RAISED MEDIAN (48' PAV'T)

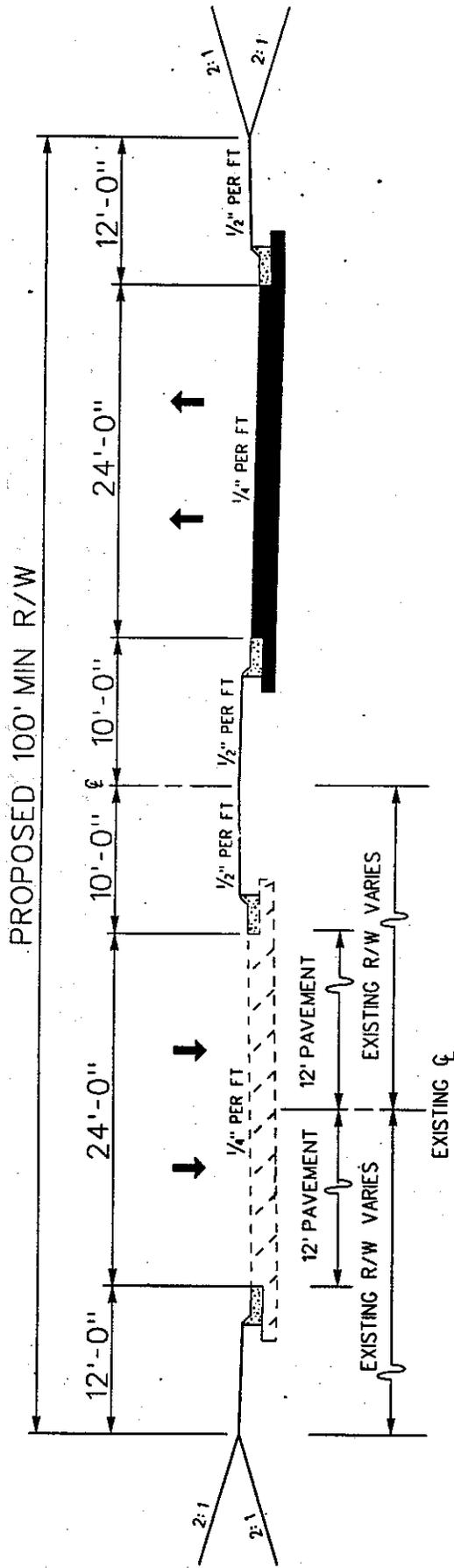


S.R. 38 IMPROVEMENTS  
 EDS-84(27) WARE CO.  
 LIMITS SECTION APPLIES  
 FROM CR 79/NEEDHAM ROAD  
 TO 400 FEET EAST CR 79/NEEDHAM ROAD  
 NOT TO SCALE

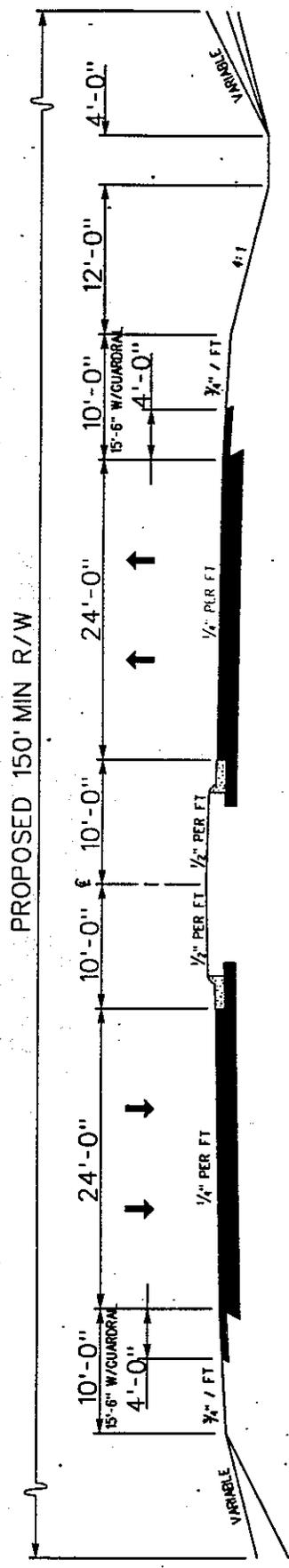


SR 38 IMPROVEMENTS  
 EDS-84(27) WARE CO.  
 LIMITS SECTION APPLIES  
 FROM 400 FEET EAST OF CR 79/NEEDHAM ROAD  
 TO 3,000 FEET EAST OF CR 91/SMITH ROAD

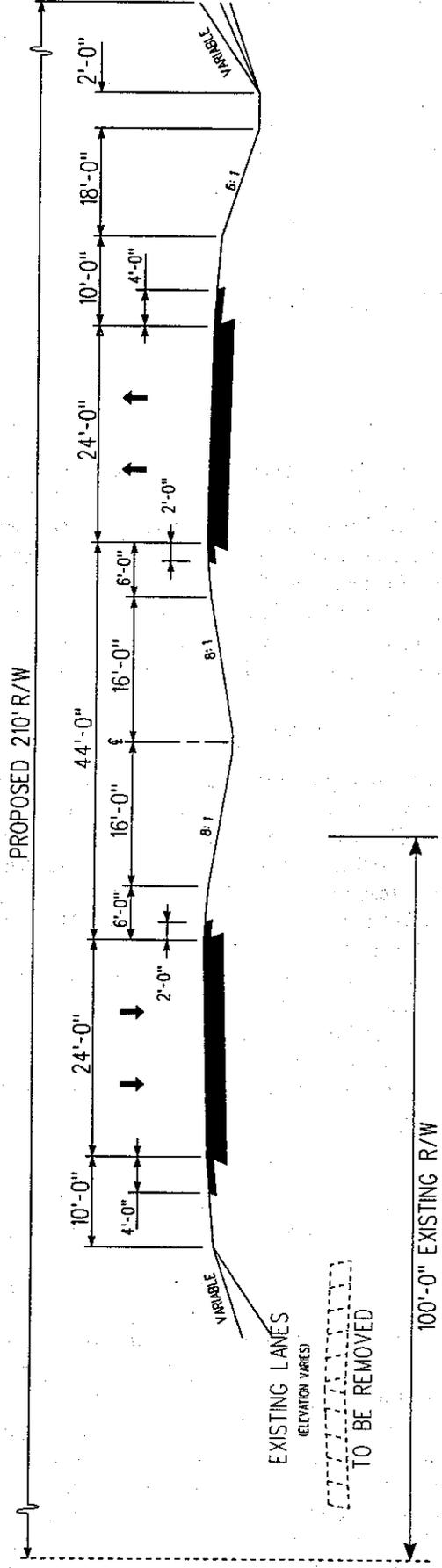
NOT TO SCALE



**TYPICAL CROSS SECTION**  
**US 84/SR 38 IMPROVEMENTS**  
 FROM 3,000 FEET EAST OF CR 91/SMITH ROAD  
 TO CR 290/OREGON AVENUE  
 FROM THE INTERSECTION OF US84/SR 38 & GLENMORE AVENUE  
 TO THE INTERSECTION OF SR 38C/VICTORY DRIVE & SR 50/CORRIDOR 'Z'  
 EDS-84(27) WARE COUNTY  
**NOT TO SCALE**



US 84/SR 38 IMPROVEMENTS  
 EDS-84(27) WARE CO.  
 LIMITS SECTION APPLIES  
 FROM CR 290/OREGON AVENUE  
 TO THE INTERSECTION OF US84/SR 38 & GLENMORE AVENUE  
 NOT TO SCALE



S.R. 38 IMPROVEMENTS  
 EDS-84(27) WARE CO.  
 LIMITS SECTION APPLIES  
 FROM CR 79/NEEDHAM ROAD  
 TO 400 FEET EAST CR 79/NEEDHAM ROAD  
 NOT TO SCALE



50/Corridor "Z", where the project would end. This concept would use existing pavement. The additional R/W varies from 40 to 105 feet. The speed design is 45 and 65 MPH.

After review of the concept, the following comments were made:

**Local Officials- City-County-State**

**Waycross Mayor, Robert L. Odum-** Mayor Odum asked to review the proposed right of way needs just prior to the intersection of SR 38/Glenmore. Radney Simpson explained that the 20-foot raised median in a rural section needed 150 of total right of way. Mayor Odum asked that the construction of the projects begin at the end of EDS-84(27), near Waycross and continue westward towards Manor. Mayor Odum expressed his appreciation to the Department for coming and his approval of the projects.

**Ware County Commissioner, Rodger Strickland-Chairman-** Commissioner Strickland asked that the construction of the projects begin at the end of EDS-84(27), near Waycross and continue westward towards Manor. Commissioner Strickland inquired about the possibility of placing an additional median opening on SR 38C/Victory Drive. Jerry Hobbs explained Location's policy of placing median openings at state routes and county routes. Once the project was transferred to Design, the Design office would determine where additional median openings, if any, would be needed. Upon review with Jim Simpson from the office of Road Design, it was decided that an additional median opening could be placed on SR 38C/Victory Drive. Commissioner Strickland expressed his appreciation for the Department for coming and his approval of the projects. On a side note, he asked that Bypass projects in the area of Waycross be referred to as perimeter roads.

**Ware County Commissioner, Marshall Monk -** Commissioner Monk inquired how long it would take to get environmental clearance for the projects. Zewdie Bekele-EAB, reminded everyone that because the project has yet to be cleared environmentally that the project was subject to change. Zewdie stated that once a consultant was selected, it could take approximately 18 months to get environmental clearance.

**Waycross City Commissioner, Gene Ward -** Commissioner Ward expressed a concern about projected truck traffic increase due to new industrial development. Reiterated a need to begin construction of the projects at Waycross and continue westward towards Manor. Expressed a need for a perimeter road to relieve truck traffic in the town of Waycross. Zoe Hardenbrook stated that a "quick fix" was being looked into for the truck turning movement problem in at the intersection of Francis St. and Nicholls St. Jerry Hobbs explained that the Concept Report for EDS-84(27) is recommending building a perimeter road around Waycross starting from US 1/SR 50 to US 84/SR 38 in Pierce County.

**Waycross City Manager, Bucky Heys -** Mr. Heys inquired how the existing right of way was determined. Jerry Hobbs stated that SR 38 existing construction plans and tax maps were used to determine the existing right of way. Mr. Heys asked to review the required right of way, starting in Waycross and continuing to just west of Wahoma. Jerry Hobbs suggested that after the comment phase of the meeting, everyone could review the project layouts. At the review, Radney Simpson reviewed the area of interest with Mr. Heys.

**Chamber of Commerce- Representative** inquired if the project was designed for 65 MPH. Radney Simpson informed the representative that in rural areas the project was designed for 65 MPH and 45 MPH in urban areas. The representative voiced concern over truck traffic in the town of Waycross and asked how long it would take to construct the project. Design stated it would take approximately 18-24 months.

**Road Design**

After the comment phase was over, everyone was invited to look over the projects. Location requested that Design match the existing vertical profiles as much as possible, even though the maximum grade is 0.38%. Jim Simpson stated that the cross slope grades along the mainline could be adjusted to address any drainage issues and it should not be a problem to match existing vertical as much as possible.

**Local Utilities**

**Bell South -** Would like to replace existing utilities with fiber optics. Wanted to know proposed right of way impacts as soon as possible.

**GA. Power -** Would like some layouts. Jerry Hobbs instructed the layouts could be requested through the Department's District utilities department.

**District Office**

**Traffic Operations -** Need sufficient right of way for radius turning movements. Road Design noted the comment.

**Planning-** Projects would function well with Waycross' Planning philosophy.

**ASSIGNMENTS-** Planning is to provide need and purpose to Location.

Location is to provide one copy of Team Meeting layouts to District 5

**ERS/ers**

**Distribution:**

Wayne Hutto

Wouter Gulden /Attn.: Warren Bailey

Herman Griffin/Attn.: Meg Pirkle

Jim Kennerly/Attn: Jim Simpson

Marta Rosen

Marion Waters

Bobby Mustin

Dudley Ellis

David Meshberger

Steve Henry

Gary D. Priester /Jesup District

Luke Cousin

Bascombe Hughes

Harvey Keepler

Ron Brown

Rowe Bowen/Attn: Bob Entorf

/Attn: Tal Stanfield

DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA  
OFFICE OF ENVIRONMENT/LOCATION

PROJECT CONCEPT REPORT  
EDS-84(27)  
WARE COUNTY  
P.I. NO. 522780

Federal Route No.: F-7-3

Date of Report: June 1, 1999

State Route No.: 38

RECOMMENDATION FOR APPROVAL

10/7/99  
Date

*[Signature]*  
State Environmental/Location Engineer

\_\_\_\_\_  
Date

\_\_\_\_\_  
State Road & Airport Design Engineer

\_\_\_\_\_  
Date

\_\_\_\_\_  
State Traffic Operations Engineer

\_\_\_\_\_  
Date

\_\_\_\_\_  
District Engineer/Jesup

10/16/99  
Date

*Paul V. Tills Jr.*  
State Bridge & Structural Engineer

\_\_\_\_\_  
Date

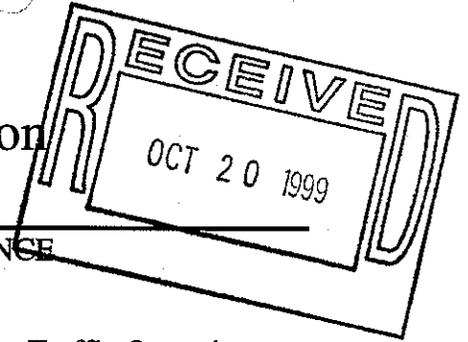
\_\_\_\_\_  
State Transportation Planning Administrator

\_\_\_\_\_  
Date

\_\_\_\_\_  
State Transportation Programming Administrator

Department of Transportation  
State of Georgia

INTERDEPARTMENTAL CORRESPONDENCE



File: EDS-84(27)/Ware County  
P.I. No. 522780

Office: Traffic Operations  
Atlanta, Georgia  
Date: October 15, 1999

From:  M.G. Waters, III, P.E., State Traffic Operations Engineer

To: Wayne Hutto, Assistant Director of Preconstruction

Subject: Project Concept Report Review

We have reviewed the concept report on the above project for the widening and reconstruction of US 84/SR 38, from Needham Road(CR 79) to the intersection of SR 38C and US 82/SR 50, a distance of 4.8 miles. This project ties into the EDS-84(26) project at its beginning terminus. The improvements are part of the Governors Road Improvement Program(GRIP) that involves the multi-laning of an east-west corridor in south Georgia.

US 84/SR 38 is currently a two lane rural roadway with 10 foot shoulders. The posted speed limit varies between 45 and 55mph.

This project will widen US 84/SR 38 to a four lane divided roadway with a 44 foot wide grassed median. At the intersection with Smith Road(CR 91) the project will transition to an urban section with a 20 foot raised median. Continuing eastward to Oregon Avenue(CR 290), it will then shift on new location, back to a rural typical design, for approximately 1.2 miles. The project will then tie into Glenmore Avenue, revert to an urban typical design with a 20 foot raised median, continuing to the end of the project. The speed design will be 45 and 65mph. Traffic will be maintained during construction.

We are in agreement with our district office recommendation for sufficient right-of-way for radius turning movements but also we remind designers of revised MOG 6638-1, Auxiliary Lanes, dated July 2,1999.

We request that conduit be installed within the limits of this project as part of this project. The conduit would be used for the future interconnection of the Advanced Transportation Management System components in this area. Our Traffic Operations Design Office can provide details cost estimates for inclusion in the project

We believe this concept will improve safety and operational capacity along this section of roadway.

We therefore find this report satisfactory for approval.

MGW:TWS

Attachment (signature page)

c: David Studstill

James A. Kennerly, State Road and Airport Design Engineer

David Mulling, w/ attachment

Marta Rosen

Chuck Hasty, TMC

Keith Golden, P.E., TMC

General Files

DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA  
OFFICE OF ENVIRONMENT/LOCATION

PROJECT CONCEPT REPORT  
EDS-84(27)  
WARE COUNTY  
P.I. NO. 522780

Federal Route No.: F-7-3

Date of Report: June 1, 1999

State Route No.: 38

RECOMMENDATION FOR APPROVAL

10/7/99  
Date

*[Signature]*  
State Environmental/Location Engineer

\_\_\_\_\_  
Date

\_\_\_\_\_  
State Road & Airport Design Engineer

10-18-99  
Date

*[Signature]*  
State Traffic Operations Engineer

\_\_\_\_\_  
Date

\_\_\_\_\_  
District Engineer/Jesup

\_\_\_\_\_  
Date

\_\_\_\_\_  
State Bridge & Structural Engineer

\_\_\_\_\_  
Date

\_\_\_\_\_  
State Transportation Planning Administrator

\_\_\_\_\_  
Date

\_\_\_\_\_  
State Transportation Programming Administrator

DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA  
OFFICE OF ENVIRONMENT/LOCATION

PROJECT CONCEPT REPORT  
EDS-84(27)  
WARE COUNTY  
P.I. NO. 522780

Federal Route No.: F-7-3

Date of Report: June 1, 1999

State Route No.: 38

RECOMMENDATION FOR APPROVAL

10/2/99  
Date

*[Signature]*  
State Environmental/Location Engineer

Date

State Road & Airport Design Engineer

Date

State Traffic Operations Engineer

Date

District Engineer/Jesup

Date

State Bridge & Structural Engineer

10/24/99  
Date

*[Signature]*  
State Transportation Planning Administrator

Date

State Transportation Programming Administrator

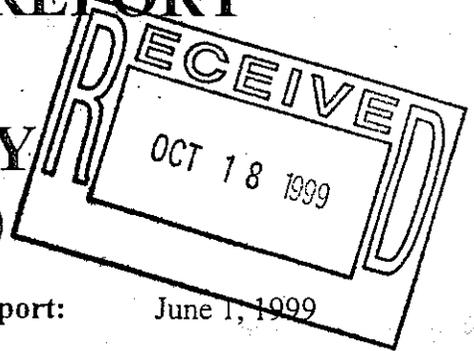
DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA  
OFFICE OF ENVIRONMENT/LOCATION

# PROJECT CONCEPT REPORT

EDS-84(27)

WARE COUNTY

P.I. NO. 522780



Federal Route No.: F-7-3

Date of Report: June 1, 1999

State Route No.: 38

## RECOMMENDATION FOR APPROVAL

10/2/99  
Date *[Signature]*  
State Environmental/Location Engineer

\_\_\_\_\_  
Date State Road & Airport Design Engineer

\_\_\_\_\_  
Date State Traffic Operations Engineer

\_\_\_\_\_  
Date District Engineer/Jesup

\_\_\_\_\_  
Date State Bridge & Structural Engineer

\_\_\_\_\_  
Date State Transportation Planning Administrator

10/14/99  
Date *[Signature]*  
State Transportation Programming Administrator

DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA  
OFFICE OF ENVIRONMENT/LOCATION

**PROJECT CONCEPT REPORT**  
**EDS-84(27)**  
**WARE COUNTY**  
**P.I. NO. 522780**

Federal Route No.: F-7-3

Date of Report: June 1, 1999

State Route No.: 38

RECOMMENDATION FOR APPROVAL

10/7/99  
Date

*[Signature]*  
State Environmental/Location Engineer

Date

State Road & Airport Design Engineer

Date

State Traffic Operations Engineer

Date

District Engineer/Jesup

Date

State Bridge & Structural Engineer

Date

State Transportation Planning Administrator

10/14/99  
Date

*[Signature]*  
State Transportation Programming Administrator

DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA  
OFFICE OF ENVIRONMENT/LOCATION

PROJECT CONCEPT REPORT  
EDS-84(27)  
WARE COUNTY  
P.I. NO. 522780

Federal Route No.: F-7-3

Date of Report: June 1, 1999

State Route No.: 38

RECOMMENDATION FOR APPROVAL

10/7/99  
Date

*[Signature]*  
State Environmental/Location Engineer

10/26/99  
Date

*[Signature]*  
State Road & Airport Design Engineer

\_\_\_\_\_  
Date

\_\_\_\_\_  
State Traffic Operations Engineer

\_\_\_\_\_  
Date

\_\_\_\_\_  
District Engineer/Jesup

\_\_\_\_\_  
Date

\_\_\_\_\_  
State Bridge & Structural Engineer

\_\_\_\_\_  
Date

\_\_\_\_\_  
State Transportation Planning Administrator

\_\_\_\_\_  
Date

\_\_\_\_\_  
State Transportation Programming Administrator

DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA  
OFFICE OF ENVIRONMENT/LOCATION

**PROJECT CONCEPT REPORT**  
**EDS-84(27)**  
**WARE COUNTY**  
**P.I. NO. 522780**

Federal Route No.: F-7-3

Date of Report: June 1, 1999

State Route No.: 38

**RECOMMENDATION FOR APPROVAL**

10/2/99  
Date

*[Signature]*  
State Environmental/Location Engineer

\_\_\_\_\_  
Date

\_\_\_\_\_  
State Road & Airport Design Engineer

\_\_\_\_\_  
Date

\_\_\_\_\_  
State Traffic Operations Engineer

11/12/99  
Date

*Mary D. Smith*  
District Engineer/Jesup

\_\_\_\_\_  
Date

\_\_\_\_\_  
State Bridge & Structural Engineer

\_\_\_\_\_  
Date

\_\_\_\_\_  
State Transportation Planning Administrator

\_\_\_\_\_  
Date

\_\_\_\_\_  
State Transportation Programming Administrator

File: EDC-84(27) Ware Co.

MEMO  
FROM:



RECEIVED  
MAY 10 1994  
PRECONSTRUCTION

DATE: May 9, 1994

TO: David Studstill, State Environmental/Location Engineer

US84 FROM WAYCROSS TO HOMERVILLE

Transmitted herewith for your information are a set of Resolutions from various officials in Ware County wanting the above corridor to be completed as soon as possible.

Attachment

xc: Jim Kennerly w/attachment  
Wayne Hutto w/attachment

John Lively  
5.5.94

A RESOLUTION

The Four-laning of Highway 84 from  
Waycross to Manor to Homerville

- WHEREAS,** Ware County is the largest county in land area in the State of Georgia, and,
- WHEREAS,** Ware County is one of the top three timber producing counties in the State of Georgia and timber produced from stumpage is vital to Georgia's \$12 billion timber industry, which brings in more money to our State than any other enterprise, and
- WHEREAS,** transportation and distribution is vital to the economy of our State with veneering, pulp mills, and chipping mills from Savannah, Jesup, Waycross, Brunswick, Saint Mary's, Fernandina Beach, Florida, Riceboro, Valdosta, Pearson, Macon, Lumber City, Baxley, Swainsboro, Claxton, Blackshear, Hoboken, and a new one being built in Fitzgerald, and
- WHEREAS,** these pulp mills, chip mills, and veneer mills furnish thousands of jobs in Ware County and surrounding areas, and
- WHEREAS,** adequate and safe transportation is vital to forest and agricultural industries, and
- WHEREAS,** US Highway 84 from Waycross to Valdosta is a vital transportation link for timber and agricultural products, but a most inadequate highway with only two lanes with poor shoulders to serve heavy traffic of logging trucks, and other traffic, and
- WHEREAS,** a Magnet School has been built in the Manor Community of Ware County where 362 students are transported over a two-lane highway twice a day putting them in jeopardy as well as many of the parents who transport them daily, and
- WHEREAS,** a substantial number of area citizens also travel regularly to Valdosta State University to obtain college degrees, or to continue graduate or undergraduate work, and
- WHEREAS,** some 1,100 CSX Railroad workers use US 84 each day to go to rice yard and the Railroad Engine House for their employment, and
- WHEREAS,** the new consolidated Ware County High School is located off US Highway 84, and some 1,800 students will daily be transported over this highly-congested road, and

**WHEREAS,** the four-laning of Highway 84 will link the major port cities of Savannah, Brunswick, and Bainbridge, and

**WHEREAS,** the Georgia Department of Transportation should be <sup>Commended</sup> for already four-laning 84 East from Waycross to <sup>Waycross</sup> Metshon, Hinesville to Fleming, Jesup to Ludowici, Valdosta to Thomasville and Cairo, and

**WHEREAS,** the request of four-laning of US 84 from Waycross to homerville is a top priority for four-laning in our area, and help and aid of the GA DOT in making this a top priority to four-lane Highway 84 West from Waycross to Homerville and urge DOT to give the project top billing,

**THEREFORE, BE IT RESOLVED** by the Ware County Commission that the GA DOT Board is called upon to initiate the four-laning of US Highway 84 from Waycross to Homerville at the earliest possible date to improve the safety of our children and students and the transportation of forest and agricultural products and other business and commerce over said highway in our area.

So Resolved this 7th day of March, 1994.

Ware County Board of Commissioners

Attest:

Gail Barron  
Gail Barron, County Clerk

By:

S. Roger Strickland  
S. Roger Strickland, Chairman

A RESOLUTION  
The Four-laning of Highway 84 from  
Waycross to Manor to Homerville

WHEREAS, Ware County is the largest county in land area in the State of Georgia; and

WHEREAS, Ware County is one of the top three timber producing counties in the State of Georgia and timber produced from stumpage is vital to Georgia's \$12 billion timber industry, which brings in more money to our state than any other enterprise; and

WHEREAS, Transportation and distribution is vital to the economy of our state with veneering, pulp mills, and chipping mills from Savannah, Jesup, Waycross, Brunswick, St. Marys, Fernandina Beach (Florida), Riceboro, Valdosta, Pearson, Macon, Lumber City, Baxley, Swainsboro, Claxton, Blackshear, Hoboken, and a new one being built in Fitzgerald; and

WHEREAS, these pulp mills, chip mills, and veneer mills furnish thousands of jobs in Ware County and surrounding areas; and

WHEREAS, adequate and safe transportation is vital to forest and agricultural industries; and

WHEREAS, U.S. Highway 84 from Waycross to Valdosta is a vital transportation link for timber and agricultural products, but a most inadequate highway with only two lanes with poor shoulders to serve heavy traffic of logging trucks and other traffic that is generated particularly by General Dollar Distribution Center in Homerville; and

WHEREAS, a Magnet School has been built in the Manor Community of Ware County where 362 students are transported over a two-lane highway twice a day putting them in jeopardy as well as many of the parents who transport them daily; and

WHEREAS, a substantial number of area citizens also travel regularly to Valdosta State University to obtain college degrees, or to continue graduate or undergraduate work; and

WHEREAS, some 1,100 CSXT railroad workers use U.S. 84 each day to go to Rice Yard and the CSXT Engine House for their employment; and

WHEREAS, the new consolidated Ware County High School is located off U.S. Highway 84 and some 1,800 students will be daily transported over this highly congested road; and

WHEREAS, the four-laning of U.S. 84 will link the major port cities of Savannah, Brunswick and Bainbridge; and

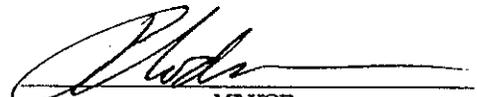
WHEREAS, the Georgia Department of Transportation has four-laned U.S. Highway 84 East from Waycross to Offerman, Hinesville to Fleming, Jesup to Ludowici, and from Valdosta to Thomasville and Cairo; and

WHEREAS, the request of four-laning of U.S. 84 from Waycross to Homerville is a top priority for four-laning in our area; and

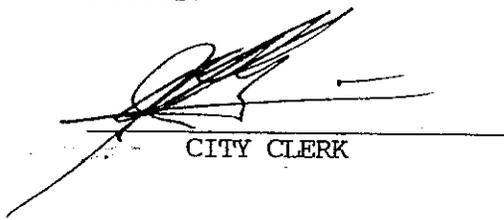
WHEREAS, the help and aid of the Georgia Department of Transportation is requested by the Commission of the City of Waycross in making the four-laning of U.S. Highway 84 West (from Waycross to Homerville) a top priority Department of Transportation project.

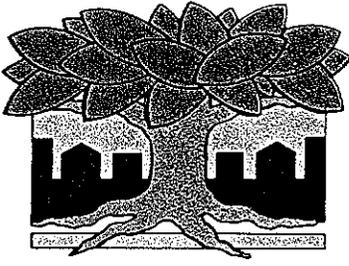
NOW, THEREFORE, BE IT RESOLVED by the Commission of the City of Waycross that the Georgia Department of Transportation Board is called upon to initiate the four-laning of U.S. Highway 84 West (from Waycross to Homerville) at the earliest possible date to improve the safety of our children and students and the transportation of forest and agricultural products and other business and commerce over said highway in our area.

Resolution adopted this 22nd day of March, 1994.

  
MAYOR

ATTEST:

  
CITY CLERK



**WAYCROSS  
W · A · R · E**

**A RESOLUTION**

The Four-Laning of Highway 84 from  
Waycross to Manor to Homerville

WHEREAS, Ware County is the largest county in land area in the State of Georgia, and

WHEREAS, Ware County is one of the top three timber producing counties in the State of Georgia and timber produced from stumpage is vital to Georgia's \$12 billion timber industry, which brings in more money to our State than any other enterprise, and

WHEREAS, the transportation and distribution is vital to the economy of our State with veneering, pulp mills, and chipping mills from Savannah, Jesup, Waycross, Brunswick, Saint Mary's, Fernandina Beach, Florida, Riceboro, Valdosta, Pearson, Macon, Lumber City, Baxley, Swainsboro, Claxton, Blackshear, Hoboken, and a new one being built in Fitzgerald, and

WHEREAS, these pulp mills, chip mills, and veneer mills furnish thousands of jobs in our towns and urban surrounding areas, and

WHEREAS, transportation is so vital to the forest and agricultural industries to prosper and have safe transportation which is so vital to primary, secondary, and tertiary manufacturing, and

**Waycross/Ware County Development Authority**

P.O. Box 137 • 200 Lee Avenue • Waycross, Georgia 31502  
Phone 912/283-3742 • FAX 912/283-0121

WHEREAS, the Highway 84 from Waycross to Valdosta is one of the most used for timber and agricultural products, and the most used and inadequate in the State with only two lanes, poor shoulders, and heavy traffic of logging trucks, and other traffic use, and

WHEREAS, a Magnet School has been built in Manor where 362 students are required to use a two-lane highway twice a day going back and forth from school putting them in jeopardy and many of the parents who transport them daily to and from school, along with school buses, and

WHEREAS, a substantial number of area citizens travel daily and weekly to Valdosta State University to obtain college degrees, or to continue graduate or undergraduate work, and

WHEREAS, some 1,100 CSX Railroad workers use U.S. 84 each day to go to Rice Yard and the Railroad Engine House for their employment, and

WHEREAS, a new consolidated Ware County High School is located on the 84 connector, and some 1,800 students daily have to be transported on this highly-congested road, and

WHEREAS, we also would like to point out that with the four-laning of Highway 84 we can link the major port cities of Savannah, Brunswick, and Bainbridge, and

WHEREAS, we would like to commend the DOT for already four-laning 84 East from Waycross to Mershon, Hinesville to Fleming, Jesup to Ludowici, Valdosta to Thomasville and Cairo, and

WHEREAS, this request of 84 four-laning from Waycross to Homerville is our top priority for four-laning in our area, and we sincerely solicit your help and your aid in making this a top priority to four-lane Highway 84 West from Waycross to Homerville and urge DOT to give the project top billing,

THEREFORE, BE IT RESOLVED by the Waycross/Ware County Development Authority that we earnestly and conscientiously request the Highway Commissioners and the DOT Board to use their influence, good judgment, and compassion in initiating the four-laning of Highway 84 to ensure more safety for our children and students being transported and the expeditious transportation of forest and agricultural products and other business and commerce in our area.

A RESOLUTION  
The Four-laning of Highway 84 from  
Waycross to Manor to Homerville

- WHEREAS*, Ware County is the largest county in land area in the State of Georgia, and,
- WHEREAS*, Ware County is one of the top three timber producing counties in the State of Georgia and timber produced from stumpage is vital to Georgia's \$12 billion timber industry, which brings in more money to our State than any other enterprise, and
- WHEREAS*, transportation and distribution is vital to the economy of our State with veneering, pulp mills, and chipping mills from Savannah, Jesup, Waycross, Brunswick, Saint Mary's, Fernandina Beach, Florida, Riceboro, Valdosta, Pearson, Macon, Lumber City, Baxley, Swainsboro, Claxton, Blackshear, Hoboken, and a new one being built in Fitzgerald, and
- WHEREAS*, these pulp mills, chip mills, and veneer mills furnish thousands of jobs in Ware County and surrounding areas, and
- WHEREAS*, adequate and safe transportation is vital to forest and agricultural industries, and
- WHEREAS*, US Highway 84 from Waycross to Valdosta is a vital transportation link for timber and agricultural products, but a most inadequate highway with only two lanes with poor shoulders to serve heavy traffic of logging trucks, and other traffic, and
- WHEREAS*, a Magnet School has been built in the Manor Community of Ware County where 362 students are transported over a two-lane highway twice a day putting them in jeopardy as well as many of the parents who transport them daily, and
- WHEREAS*, a substantial number of area citizens also travel regularly to Valdosta State University to obtain college degrees, or to continue graduate or undergraduate work, and
- WHEREAS*, some 1,100 CSX Railroad workers use US 84 each day to go to rice yard and the Railroad Engine House for their employment, and
- WHEREAS*, the new consolidated Ware County High School is located off US Highway 84, and some 1,800 students will daily be transported over this highly-congested road, and

**WHEREAS,** the four-laning of Highway 84 will link the major port cities of Savannah, Brunswick, and Bainbridge, and

**WHEREAS,** the Georgia Department of Transportation should be for already four-laning 84 East from Waycross to ~~Macon~~<sup>Statham</sup>, Hinesville to Fleming, Jesup to Ludowici, Valdosta to Thomasville and Cairo, and

**WHEREAS,** the request of four-laning of US 84 from Waycross to homerville is a top priority for four-laning in our area, and help and aid of the GA DOT in making this a top priority to four-lane Highway 84 West from Waycross to Homerville and urge DOT to give the project top billing,

**THEREFORE, BE IT RESOLVED** by the Ware County Board of Education that the GA DOT Board is called upon to initiate the four-laning of US Highway 84 from Waycross to Homerville at the earliest possible date to improve the safety of our children and students and the transportation of forest and agricultural products and other business and commerce over said highway in our area.

So Resolved this 20th day of APRIL, 1994.

Ware County Board of Education

Attest:

*B. Donald Blair*  
~~B. Donald Blair~~  
Superintendent

By:

*H. Talmadge Booth Jr.*  
H. Talmadge Booth, Jr.  
Chairperson

A RESOLUTION  
The Four-laning of Highway 84 from  
Waycross to Manor to Homerville

- WHEREAS,** Ware County is the largest county in land area in the State of Georgia, and,
- WHEREAS,** Ware County is one of the top three timber producing counties in the State of Georgia and timber produced from stumpage is vital to Georgia's \$12 billion timber industry, which brings in more money to our State than any other enterprise, and
- WHEREAS,** transportation and distribution is vital to the economy of our State with veneering, pulp mills, and chipping mills from Savannah, Jesup, Waycross, Brunswick, Saint Mary's, Fernandina Beach, Florida, Riceboro, Valdosta, Pearson, Macon, Lumber City, Baxley, Swainsboro, Claxton, Blackshear, Hoboken, and a new one being built in Fitzgerald, and
- WHEREAS,** these pulp mills, chip mills, and veneer mills furnish thousands of jobs in Ware County and surrounding areas, and
- WHEREAS,** adequate and safe transportation is vital to forest and agricultural industries, and
- WHEREAS,** US Highway 84 from Waycross to Valdosta is a vital transportation link for timber and agricultural products, but a most inadequate highway with only two lanes with poor shoulders to serve heavy traffic of logging trucks, and other traffic, and
- WHEREAS,** a Magnet School has been built in the Manor Community of Ware County where 362 students are transported over a two-lane highway twice a day putting them in jeopardy as well as many of the parents who transport them daily, and
- WHEREAS,** a substantial number of area citizens also travel regularly to Valdosta State University to obtain college degrees, or to continue graduate or undergraduate work, and
- WHEREAS,** some 1,100 CSX Railroad workers use US 84 each day to go to rice yard and the Railroad Engine House for their employment, and
- WHEREAS,** the new consolidated Ware County High School is located off US Highway 84, and some 1,800 students will daily be transported over this highly-congested road, and

**WHEREAS,** the four-laning of Highway 84 will link the major port cities of Savannah, Brunswick, and Bainbridge, and

**WHEREAS,** the Georgia Department of Transportation should be <sup>Commended</sup> for already four-laning 84 East from Waycross to ~~Morison~~ <sup>Merston</sup>, Hinesville to Fleming, Jesup to Ludowici, Valdosta to Thomasville and Cairo, and

**WHEREAS,** the request of four-laning of US 84 from Waycross to homerville is a top priority for four-laning in our area, and help and aid of the GA DOT in making this a top priority to four-lane Highway 84 West from Waycross to Homerville and urge DOT to give the project top billing,

**THEREFORE, BE IT RESOLVED** by the Waycross-Ware County Planning Commission that the GA DOT Board is called upon to initiate the four-laning of US Highway 84 from Waycross to Homerville at the earliest possible date to improve the safety of our children and students and the transportation of forest and agricultural products and other business and commerce over said highway in our area.

So Resolved this 13 day of April, 1994

Waycross-Ware County Planning  
Commission

Attest: R. Wayne Kilpatrick

By: Evelyn Gartham Holland

*44*  
*LOCATION/RO DEN*

REVISION REQUEST  
for the  
CONSTRUCTION WORK PROGRAM

In accordance with the Board Resolution dated August 16, 1973, Board approval is requested to revise the Construction Work Program for the project and activity outlined below:

Action Requested: ADDITION TO THE PROGRAM

PROJECT DATA

County	Project No. P.I. No.	Type Work
WARE	EDS-84 (27) 522780	RECST. OR REHAB. WIDENING
Description: SR38/US84 FM CR 79 TO EXITING 5 LN IN WAYCROSS@ SR 4N		

Project Length = 6.20 Miles

FUNDING INFORMATION

Estimated Cost (\$1,000's)	DOT Share	Other Share	Fiscal Year	Cong. District	Field District
PE \$140			1995		
ROW \$1,400			1997	1	5
CONST \$7,303	\$7,303	\$0	1999		

Fund 1 = EDS  
Fund 2 = EDS

REASON FOR REVISION:

Requested by the Director of Preconstruction

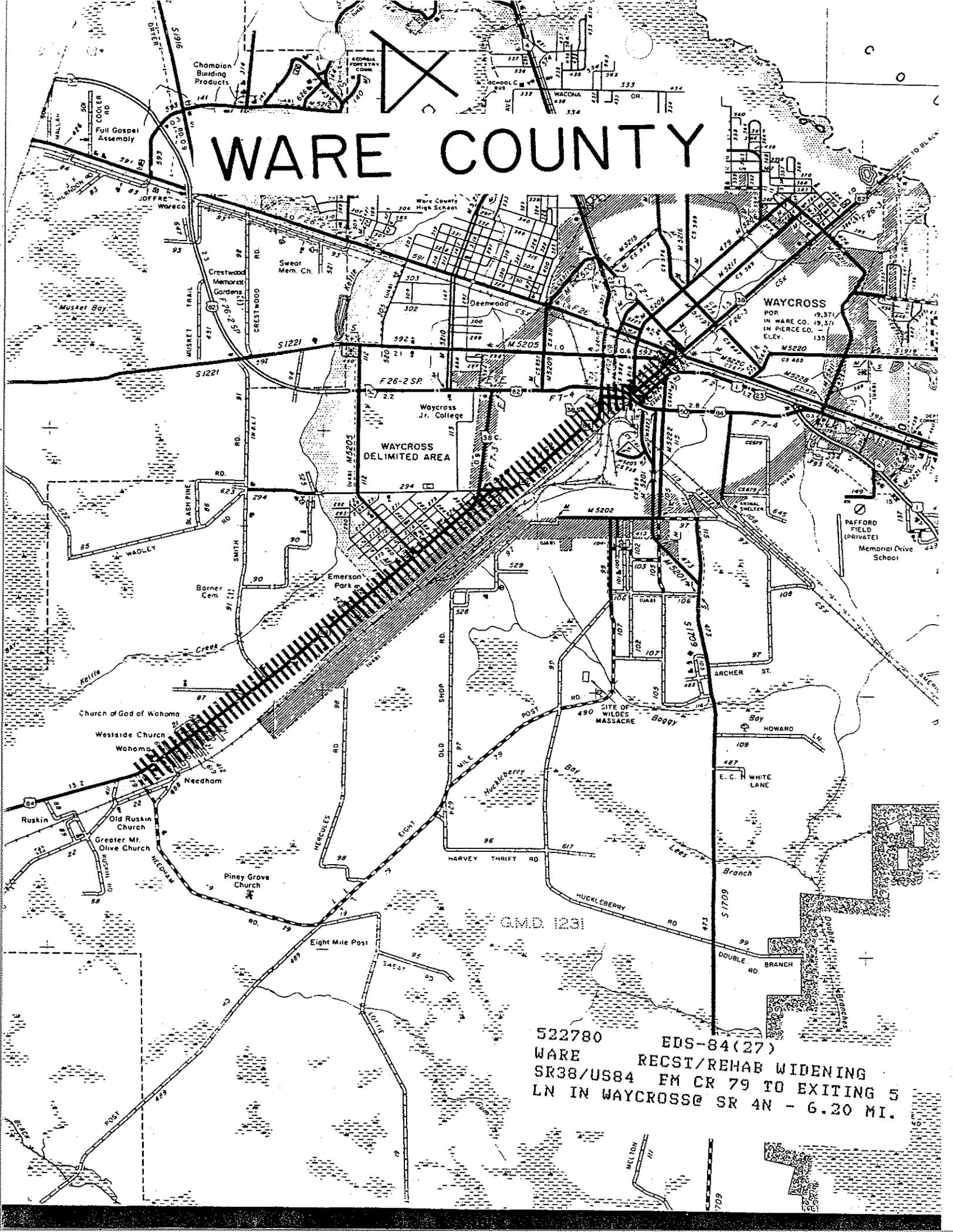
RECOMMENDED

*Gregory Bonline*  
DIRECTOR, DIVISION OF PLANNING AND PROGRAMMING

RECOMMENDED

*Wayne Shackelford*  
COMMISSIONER

# WARE COUNTY



522780 EDS-84(27)  
WARE RECST/REHAB WIDENING  
SR38/US84 FM CR 79 TO EXITING 5  
LN IN WAYCROSS@ SR 4N - 6.20 MI.