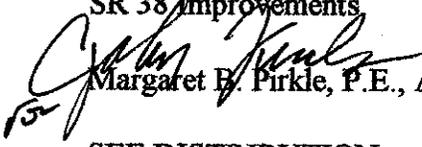


DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA

INTERDEPARTMENT CORRESPONDENCE

FILE EDS-84(26)/BHN-007-3(28) Ware County **OFFICE** Preconstruction
P.I. Nos. 522770 / 522775
SR 38 Improvements **DATE** July 23, 2004

FROM  Margaret B. Pirkle, P.E., Assistant Director of Preconstruction

TO SEE DISTRIBUTION

SUBJECT REVISED PROJECT CONCEPT REPORT APPROVAL

Attached for your files is the approval for subject project.

MBP/cj

Attachment

DISTRIBUTION:

David Mulling
Harvey Keeper
Jerry Hobbs
Jamie Simpson
Michael Henry
Phillip Allen
Joe Palladi (file copy)
Brent Story

BOARD MEMBER

DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA

INTERDEPARTMENT CORRESPONDENCE

FILE: EDS-84(26) & BHN-007-3(28) **OFFICE:** Environment/Location
P.I. No.: 522770 & 522775
Ware County **DATE:** June 30, 2004
~~SR 38 IMPROVEMENTS~~

FROM: Harvey D. Keeper, State Environmental/Location Engineer

TO: Meg Pirkle, Assistant Director of Preconstruction

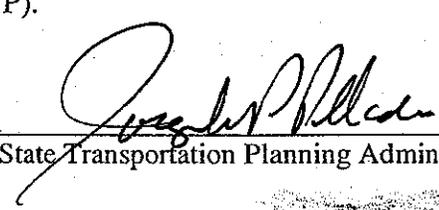
SUBJECT: Revised Project Concept Report – SR 38 Improvements, Ware County

Attached is the original copy of the revised concept Report for your further handling for approval in accordance with the Plan Development Process (PDP).

In order to minimize wetland impacts, it is recommended to revise the typical section of EDS-84(26) by replacing the previously approved 44-foot depressed median typical sections with 32-foot depressed median typical sections. It is recommended to revise the right-of-way limits to accommodate the recommended 32-foot depressed median and lessen the impact on wetland areas. The ending terminus of EDS-84(26) is also recommended for revision to prevent this project from terminating on new location based on the recommended revised alignment of EDS-84(27). The existing bridges at Greasy Branch, Little Alligator Creek, and Big Alligator Creek would be replaced under project BHN-007-3(28). New bridges will be constructed parallel to the existing bridges under EDS-84(26).

The revised concept as presented herein and submitted for approval is not listed in the current State Transportation Improvement Program (STIP), but is consistent with that which is included in the Department's Construction Work Program (CWP).

DATE: 7/12/04


State Transportation Planning Administrator

HDK/KET/dle

Attachments

Distribution:

David Mulling, Project Review Engineer

Phillip Allen, State Traffic Safety & Design Engineer

Joe Palladi, State Transportation Planning Administrator

Jamie Simpson, State Transportation Financial Management Administrator

Brent Story, State Consultant Design Engineer

Gary Priestler, Jesup District Engineer

Paul Liles, State Bridge & Structural Design Engineer

REVISED PROJECT CONCEPT REPORT

EDS-84(26) – WARE CO.

Need and Purpose: The US 84/SR 38 Improvements are part of the Governors Road Improvement Program (G.R.I.P.) and involves the multi-laning of this primary east-west corridor in south Georgia, serving as a catalyst for the development of this region. The improvements will aid in the economic development of sparsely populated rural areas and small towns along this route. Traffic carrying capacity will be increased; safety and operational characteristics along this segment will be improved.

Project location: The proposed concept would be located along US 84/SR 38 in Ware County beginning approximately at mile post 3.9 and ending approximately at mile post 12.6. The total length of the project is approximately 8.7 miles. This project ties into EDS-84(23) at the beginning terminus and EDS-84(27) at the ending terminus.

Description of the approved concept: The project would begin approximately 900 feet west of Greasy Branch and would widen US 84/SR 38 on the south side, adding a 44-foot depressed grass median with two 12-foot lanes of pavement, while utilizing the existing right-of-way on the north side of US 84/SR 38. It would continue to approximately 1,850 feet east of CR 615/Sutton Lane. At that point, the median narrows to a 32-foot depressed grass median and would continue eastward to approximately 2,300 feet west of CR 71/Ammons Road. The 32-foot depressed grass median complies with the agreement between the Department and coordinating federal resource agencies to reduce impacts to wetlands. From there, the median widens to 44 feet and continues to approximately 2,600 feet east of CR 71/Ammons Road. At that point, the median narrows to 32 feet and continues eastward 3,700 feet. The alignment would then shift to the north side while holding the existing right-of-way on the south side of US 84/SR 38 and would continue to approximately 1,000 feet east of CR 88/Ruskin Road. There, the median widens to 44 feet and the alignment would shift to the south side of US 84/SR 38 and continue to the end of the project at CR 79/Needham Road. Through this section, it is proposed to construct four new 12-foot lanes, while holding the existing right-of-way on the north side. This concept would avoid a historical site along the north side of US 84/SR 38. The existing bridges at Greasy Branch Creek, Little Alligator Creek, and Big Alligator Creek would be widened and new ones built parallel to the existing bridges. Additional required right-of-way would vary from 70 to 118 feet. The speed design is 65 miles per hour.

PDP Classification: Major/Construction on existing roadway and new construction

Federal Oversight: Full Oversight (), Exempt(X), State Funded(), or Other ()

Functional Classification:

U. S. Route Number(s): 84

State Route Number(s): 38

Traffic (AADT) as shown in the approved concept:

Current Year: 2001	AADT: 5,700 to	Design Year: 2021	AADT: 9,050 to
	6,450		10,250

Proposed features to be revised:

- **Typical Section:** To facilitate the connection between this project and EDS-84(23) and lessen the impact on wetland areas, it is recommended that the typical section be revised.
- **Right-of-Way:** It is recommended that the right-of-way limits for this project be revised to accommodate the revised typical section and lessen the impact on wetland areas.
- **Termini:** It is also recommended that the ending terminus be revised to prevent EDS-84(26) from terminating on new location based on the recommended revised alignment for EDS-84(27).

Describe the revised feature(s) to be approved:

- **Typical Section:** The beginning of this project will tie into the recommended revised concept of EDS-84(23). EDS-84(23) ends with a 32-foot depressed median typical section constructed by adding two 12-foot lanes on the south side of US 84/SR 38. EDS-84(26) begins approximately 900 feet west of Greasy Branch continuing with the addition of two 12-foot lanes on the south side of US 84/SR 38 as well as the 32-foot depressed median. Widening to the south would continue until approximately 1.3 miles west of CR 88/Ruskin Road. At this point, widening would shift to the north side of US 84/SR 38, maintaining the 32-foot depressed median typical section until EDS-84(26) reaches its ending terminus. When possible throughout the project, the existing pavement would be utilized to reduce cost. The existing bridges at Greasy Branch, Little Alligator Creek, and Big Alligator Creek would be replaced under project BHN-007-3(28). New bridges will be constructed parallel to the existing bridges under EDS-84(26).
- **Right-of-Way:** The proposed right-of-way widths vary between 200 to 250 feet.
- **Termini:** EDS-84(26) reaches its ending terminus approximately (mile post 11.3) 3,600 feet west of CR 88/Ruskin Road. The total length of revised concept is approximately 7.4 miles. The speed design remains at 65 mph.

Updated traffic data (AADT):

Current Year: 2007 AADT: 7,200 Design Year: 2027 AADT: 11,500

Programmed/Schedule:

P.E. 2003 R/W: ~~2008~~ LR Construction: ~~2009~~ LR

Revised cost estimates:

1. BHN-007-3(28): Construction cost including inflation and E&C: \$1,738,000
2. EDS-84(26): Construction cost including inflation and E&C: \$15,346,000
3. Right-of-way: \$1,605,000
4. Utilities: LPGA (Ware signed 03-11-02)

Is the project located in a Non-attainment area?Yes**X**.....No.

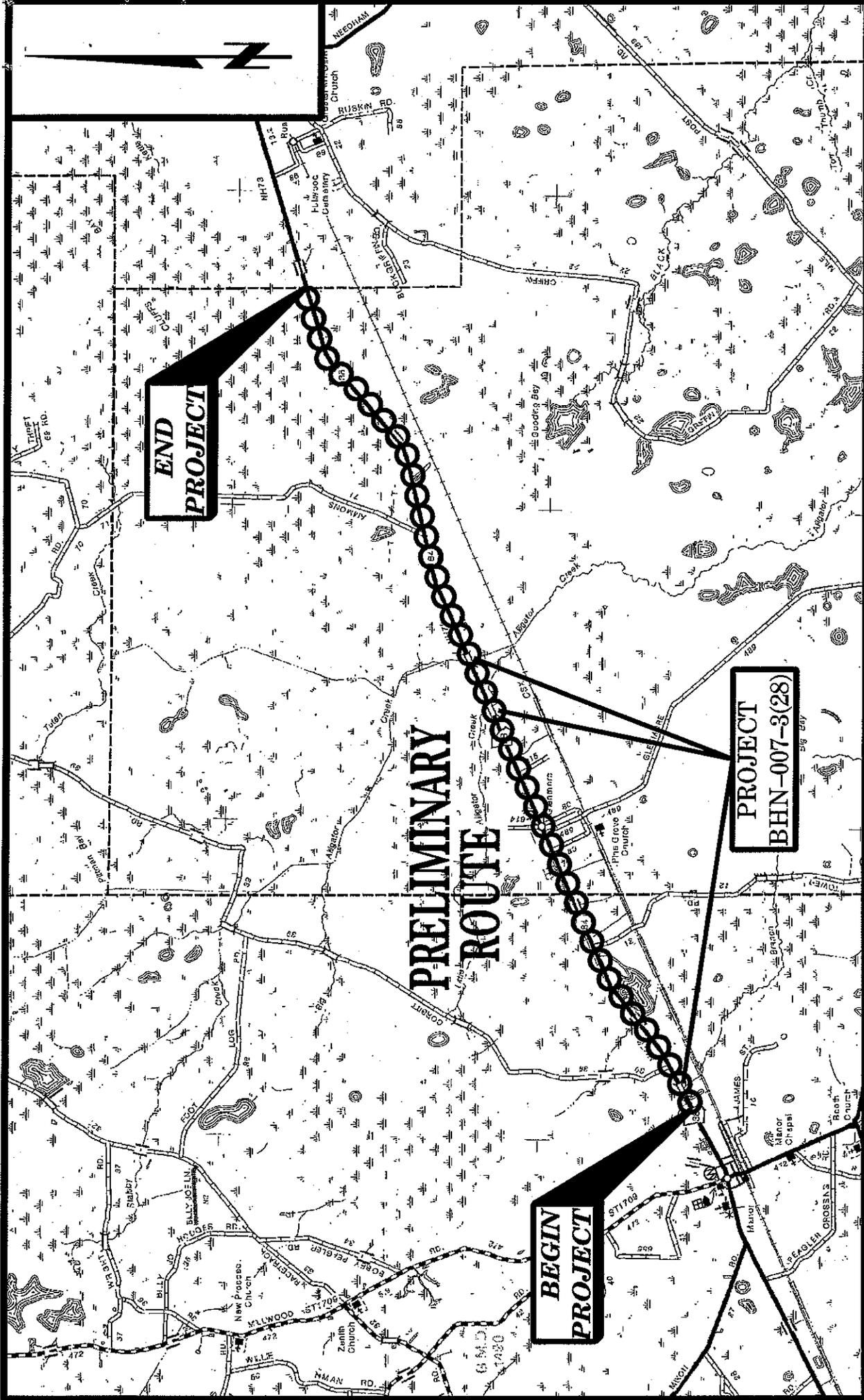
Recommendation: It is recommended that the proposed revision to this concept be approved for implementation.

Attachments:

1. Sketch Map,
2. Cost Estimate,
3. Typical Sections

Concur: Thomas L. Johnson
Director of Preconstruction

Approve: Paul V. Mullins
Chief Engineer *(initials)*



**END
PROJECT**

**PRELIMINARY
ROUTE**

**BEGIN
PROJECT**

**PROJECT
BHN-007-3(28)**



SCALE IN MILES



LOCATION

STRIPMAP

EDS-84(26) & BHN-007-3(28)
US 84/SR 38 IMPROVEMENTS
WARE COUNTY
P.I.# 522770 & 522775

SOURCE: GENERAL HIGHWAY MAP, WARE CO., GEORGIA
PREPARED BY THE GEORGIA DEPARTMENT OF TRANSPORTATION, 1988

CONCEPT COST ESTIMATE

Office of Environment/Location

June 29, 2004 2:26 PM

County(s)

PI Number Project Number

Project Name Project Length Miles

Project Description

Widening of existing US 84/SR 38 with a 32-foot depressed median and 65 mph speed design.

Existing Roadway

2-lane section, posted 55 mph

Comments

TRAFFIC:

Current Design Year Daily Volume (AADT)

Future Design Year Daily Volume (AADT)

Concept Estimate Feasibility Estimate

Typical Section(s) Used in Estimate

Typical Section Length

Typical Section(s) Used in Estimate	Typical Section Length
<input type="text"/>	<input type="text"/> Miles

Prepared By

MAJOR STRUCTURES*Note! All distances are in feet***Bridges: Stream Crossings & Grade Separations**

NO	LOCATION	QTY	CROSSING TYPE	WIDTH	LENGTH	UNIT COST	TOTAL
1	Greasy Branch Creek	1	Stream-New	43.00	125.0	54.00	290,000
2	Little Alligator Creek	1	Stream-New	43.00	100.0	54.00	232,000
3	Big Alligator Creek	1	Stream-New	43.00	160.0	54.00	372,000
4							
5							
6							
7							
8							
9							
10							
11							
12							

Bridge Culverts

NO	LOCATION	TYPE / W x H / FILL	LENGTH	UNIT COST	TOTAL
1	Approx. 2,200 ft east of CR 39	Single / 6 x 6 / 10	200.0	429.63	86,000
2	Approx. 2,500 ft west of CR 80	Double / 6 x 3 / 10	200.0	524.01	105,000
3	Approx. 2,200 ft west of CR 80	Double / 6 x 3 / 10	200.0	524.01	105,000
4	Approx. 3,000 ft east of Gooding Bay Rd.	Triple / 6 x 4 / 10	200.0	841.74	168,000
5					
6					
7					
8					

Walls

NO	LOCATION	TYPE	HEIGHT	LENGTH	UNIT COST	TOTAL
1						
2						
3						
4						
5						
6						

MAJOR STRUCTURES SUBTOTAL **\$ 1,358,000**

Typical Section

Rural Widening: 2 To 4-Lanes with 32 ft Divided Median

Typical Section Length Miles

Right-of-Way Width Feet

GRADING AND DRAINAGE

- 1. EARTHWORK
 - a. Unclassified Excavation Soil
 - b. Unclassified Excavation Rock
 - c. Borrow Excavation
- 2. MINOR DRAINAGE

QUANTITY		UNIT COST	TOTAL
34,820	CY	2.30	80,000
	CY		
89,367	CY	6.23	557,000
7.40	MI	77,231	572,000
GRADING AND DRAINAGE SUBTOTAL			\$1,209,000

BASE AND PAVING

- 1. GRADED AGGREGATE BASE
- 2. ASPHALT PAVING
 - a. Asph Conc 9.5 mm Superpave
 - b. Asph Conc 19 mm Superpave
 - c. Asph Conc 25 mm Superpave
 - d. Bituminous Tack Coat
- 3. CONCRETE PAVING
 - a. Curb and Gutter
 - b. Miscellaneous
- 4. OTHER PAVING

THICKNESS and SPREAD RATE	QUANTITY		UNIT COST	TOTAL
10"	83,279	TN	16.07	1,338,000
1 1/2" (165 LB/SY)	14,000	TN	46.23	647,000
3" (330 LB/SY)	24,721	TN	44.34	1,096,000
4" (440 LB/SY)	30,062	TN	41.59	1,250,000
	18,296	GL	1.08	20,000
		LF		
	7.40	MI	36,421	270,000
				462,000
BASE AND PAVING SUBTOTAL				\$5,083,000

LUMP ITEMS

- 1. TRAFFIC CONTROL
- 2. CLEARING AND GRUBBING
- 3. EROSION CONTROL
- 4. SIGNING & MARKING
- 5. MISCELLANEOUS

QUANTITY	UNIT COST	TOTAL
7.40 MI	76,522	566,000
179.39 AC	6,000	1,076,000
7.40 MI	124,197	919,000
7.40 MI	19,967	148,000
7.40 MI	120,781	894,000
LUMP ITEM SUBTOTAL		\$3,603,000

MISCELLANEOUS PROJECT ITEMS

- 1. GUARDRAIL
- 2. GUARDRAIL ANCHORS
- 3. DETOURS
- 4. SPECIAL FEATURES

QUANTITY	UNIT COST	TOTAL
5,000 LF	9.35	47,000
32 EA	390.54	12,000
	MI	331,252
MISCELLANEOUS SUBTOTAL		\$59,000

Typical Section

Rural New Location: 2-Lanes with 24 ft Pavement

Typical Section Length Miles

Right-of-Way Width Feet

GRADING AND DRAINAGE

- 1. EARTHWORK
 - a. Unclassified Excavation Soil
 - b. Unclassified Excavation Rock
 - c. Borrow Excavation
- 2. MINOR DRAINAGE

QUANTITY	UNIT COST	TOTAL
	CY	
	CY	
	CY	
0.60	MI	28,217
GRADING AND DRAINAGE SUBTOTAL		\$209,000

BASE AND PAVING

- 1. GRADED AGGREGATE BASE
- 2. ASPHALT PAVING
 - a. Asph Conc 9.5 mm Superpave
 - b. Asph Conc 19 mm Superpave
 - c. Asph Conc 25 mm Superpave
 - d. Bituminous Tack Coat
- 3. CONCRETE PAVING
 - a. Curb and Gutter
 - b. Miscellaneous
- 4. OTHER PAVING

THICKNESS and SPREAD RATE	QUANTITY	UNIT COST	TOTAL
10"	6,588 TN	16.07	106,000
1 1/2" (165 LB/SY)	1,074 TN	46.23	50,000
3" (330 LB/SY)	2,166 TN	44.34	96,000
4" (440 LB/SY)	1,912 TN	41.59	80,000
	1,370 GL	1.08	1,000
	0.60 MI	20,313	12,000
			35,000
BASE AND PAVING SUBTOTAL			\$380,000

LUMP ITEMS

- 1. TRAFFIC CONTROL
- 2. CLEARING AND GRUBBING
- 3. EROSION CONTROL
- 4. SIGNING & MARKING
- 5. MISCELLANEOUS

QUANTITY	UNIT COST	TOTAL
0.60	MI	10,696
7.27	AC	6,000
0.60	MI	122,347
0.60	MI	7,939
0.60	MI	31,480
LUMP ITEM SUBTOTAL		\$147,000

ESTIMATE SUMMARY

TYPICAL SECTION	COST (per mile)
1. Rural Widening: 2 To 4-Lanes with 32 ft Divided Median	\$ 1,337,000
2. Rural New Location: 2-Lanes with 24 ft Pavement	\$ 1,227,000
PROJECT COST	
A. MAJOR STRUCTURES	\$ 1,358,000
B. GRADING AND DRAINAGE	\$ 1,418,000
C. BASE AND PAVING	\$ 5,463,000
D. LUMP ITEMS	\$ 3,750,000
E. MISCELLANEOUS	\$ 59,000
SUBTOTAL CONSTRUCTION COST	\$ 12,048,000
ENGINEERING & CONTINGENCIES (10%)	\$ 1,205,000
INFLATION <u>2</u> yr(s) @ <u>5</u> % per yr	\$ 1,358,000
GRAND TOTAL CONSTRUCTION COST	\$ 14,611,000

CONCEPT COST ESTIMATE

Office of Environment/Location

June 29, 2004 4:39 PM

County(s)

PI Number Project Number

Project Name Project Length Miles

Project Description

Replacement of existing bridges at Greasy Branch Creek, Little Alligator Creek, and Big Alligator Creek.

Existing Roadway

2-lane section, posted 55 mph

Comments

Conjunction with EDS-84(26).

TRAFFIC:

Current Design Year Daily Volume (AADT)

Future Design Year Daily Volume (AADT)

Concept Estimate

Feasibility Estimate

Typical Section(s) Used in Estimate

Typical Section Length

<input type="text"/>	<input type="text"/> Miles

Prepared By

MAJOR STRUCTURES

Note! All distances are in feet

Bridges: Stream Crossings & Grade Separations

NO	LOCATION	QTY	CROSSING TYPE	WIDTH	LENGTH	UNIT COST	TOTAL
1	Greasy Branch Creek	1	Stream-New	43.00	125.0	54.00	290,000
2	Little Alligator Creek	1	Stream-New	43.00	100.0	54.00	232,000
3	Big Alligator Creek	1	Stream-New	43.00	160.0	54.00	372,000
4							
5							
6							
7							
8							
9							
10							
11							
12							

Bridge Culverts

NO	LOCATION	TYPE / W x H / FILL	LENGTH	UNIT COST	TOTAL
1					
2					
3					
4					
5					
6					
7					
8					

Walls

NO	LOCATION	TYPE	HEIGHT	LENGTH	UNIT COST	TOTAL
1						
2						
3						
4						
5						
6						

MAJOR STRUCTURES SUBTOTAL \$ 894,000

Typical Section

Typical Section Length Miles

Right-of-Way Width Feet

GRADING AND DRAINAGE

- 1. EARTHWORK
 - a. Unclassified Excavation Soil
 - b. Unclassified Excavation Rock
 - c. Borrow Excavation
- 2. MINOR DRAINAGE

QUANTITY	UNIT COST	TOTAL
	CY	
	CY	
	CY	
	MI	
GRADING AND DRAINAGE SUBTOTAL		

BASE AND PAVING

- 1. GRADED AGGREGATE BASE
- 2. ASPHALT PAVING
 - a. Asph Conc 9.5 mm Superpave
 - b. Asph Conc 19 mm Superpave
 - c. Asph Conc 25 mm Superpave
 - d. Bituminous Tack Coat
- 3. CONCRETE PAVING
 - a. Curb and Gutter
 - b. Miscellaneous
- 4. OTHER PAVING

THICKNESS and SPREAD RATE	QUANTITY	UNIT COST	TOTAL
	TN		
1 1/2" (165 LB/SY)	TN		
3" (330 LB/SY)	TN		
4" (440 LB/SY)	TN		
	GL		
	LF		
	MI		
BASE AND PAVING SUBTOTAL			

LUMP ITEMS

- 1. TRAFFIC CONTROL
- 2. CLEARING AND GRUBBING
- 3. EROSION CONTROL
- 4. SIGNING & MARKING
- 5. MISCELLANEOUS

QUANTITY	UNIT COST	TOTAL
	MI	
	AC	
	MI	
	MI	
	MI	
LUMP ITEM SUBTOTAL		

MISCELLANEOUS PROJECT ITEMS

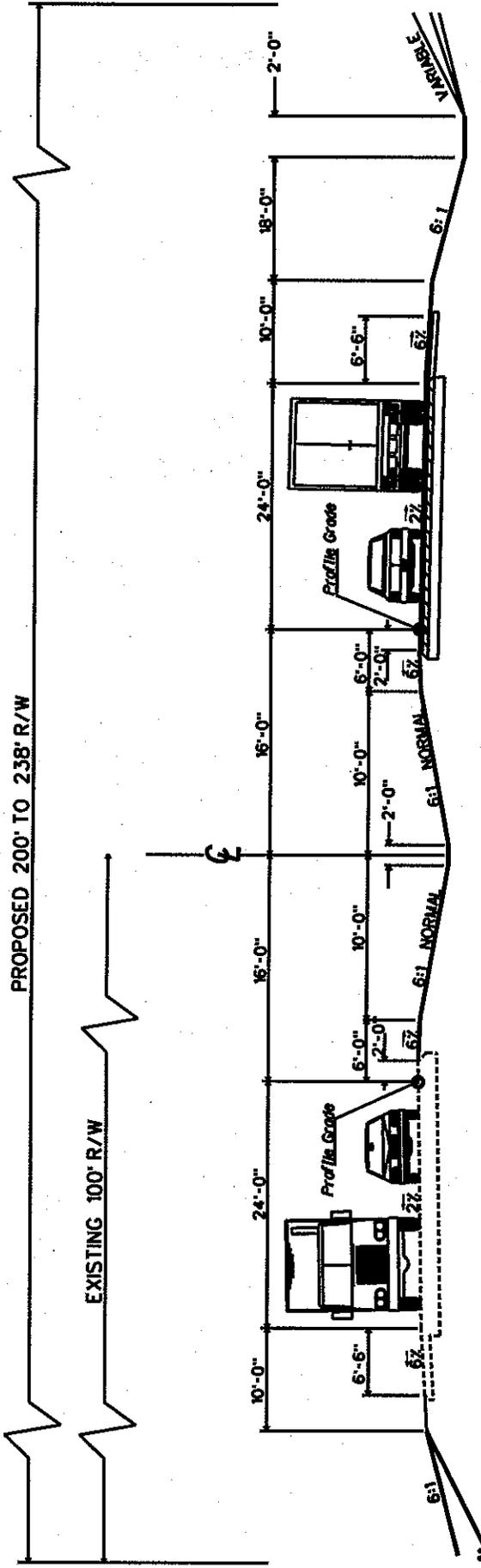
- 1. GUARDRAIL
- 2. GUARDRAIL ANCHORS
- 3. DETOURS
- 4. SPECIAL FEATURES

QUANTITY	UNIT COST	TOTAL
2,400	LF 9.35	22,000
12	EA 390.54	5,000
0.60	MI 331,252	199,000
Tie into existing roadway		313,000
MISCELLANEOUS SUBTOTAL		\$539,000

ESTIMATE SUMMARY

TYPICAL SECTION	COST (per mile)
PROJECT COST	
A. MAJOR STRUCTURES	\$ 894,000
B. GRADING AND DRAINAGE	\$ -
C. BASE AND PAVING	\$ -
D. LUMP ITEMS	\$ -
E. MISCELLANEOUS	\$ 539,000
SUBTOTAL CONSTRUCTION COST	\$ 1,433,000
ENGINEERING & CONTINGENCIES (10%)	\$ 143,000
INFLATION <u>2</u> yr(s) @ <u>5</u> % per yr	\$ 162,000
GRAND TOTAL CONSTRUCTION COST	\$ 1,738,000

G.R.I.P. TYPICAL SECTION
 32-FOOT DEPRESSED GRASS MEDIAN RURAL SECTION
 65 MPH SPEED DESIGN

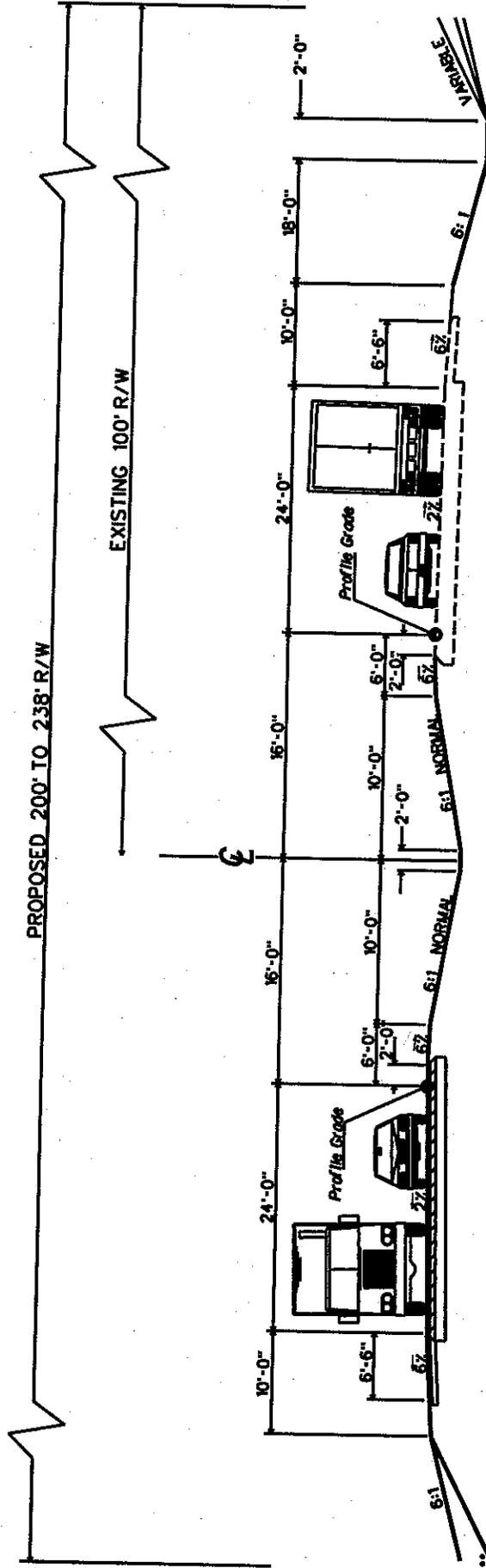


** Guardrail Required when steeper than 6:1

US 84 / SR 88 IMPROVEMENTS
 EDS-84(26) WARE CO.
 HOLDING EXISTING NORTH RIGHT-OF-WAY
 FROM 900 FEET WEST OF GREASY BRANCH
 TO 1.3 MILES WEST OF CR 88 / RUSKIN ROAD

NOT TO SCALE

G.R.I.P. TYPICAL SECTION
 32-FOOT DEPRESSED GRASS MEDIAN RURAL SECTION
 65 MPH SPEED DESIGN



** Guardrail Required when steeper than 6:1

US 84 / SR 38 IMPROVEMENTS
 EDS-84(26) WARE CO.
 HOLDING EXISTING SOUTH RIGHT-OF-WAY
 FROM 1.3 MILES WEST OF CR 88 / RUSKIN ROAD
 TO 3,600 FEET WEST OF CR 88 / RUSKIN ROAD

NOT TO SCALE

ORIGINAL TO GENERAL FILES

D.O.T. 66

DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA

INTERDEPARTMENT CORRESPONDENCE

FILE EDS-84(26)/BHN-007-3(28) Ware County **OFFICE** Preconstruction
P. I. Nos. 522770/522775
DATE November 24, 1999

CWH
FROM C. Wayne Hutto, Assistant Director of Preconstruction

TO SEE DISTRIBUTION

SUBJECT PROJECT CONCEPT REPORT APPROVAL

Attached for your files is the approval for subject project.

CWH/cj

Attachment

DISTRIBUTION:

Tom Turner
David Mulling
David Studstill (ATTN: Harvey Keeper)
Jerry Hobbs
Herman Griffin
Georgene Geary (ATTN: Michael Henry)
Marion Waters
Marta Rosen
Paul Liles
Don Mills
Jim Kennerly
Gary Priester

**DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA**

INTERDEPARTMENT CORRESPONDENCE

FILE EDS-84(26)/BHN-007-3(28) Ware County **OFFICE** Preconstruction
P.I. No. 522770/522775

DATE November 9, 1999

FROM 
Thomas L. Turner, P.E., Director of Preconstruction

TO Wayne Shackelford, Commissioner

SUBJECT PROJECT CONCEPT REPORT

These combined projects are the widening and reconstruction of US 84/SR 38 from 900' west of Greasy Branch Creek eastward to CR 79/Needham Road for a total of 8.70 miles. The existing roadway consists of two, 12' lanes with 10' rural shoulders on a variable 60' to 100' of existing right-of-way. The existing major structures are: (1) Greasy Branch Creek - 96' x 38' bridge with a sufficiency rating of 60.7; (2) Little Alligator Creek - 72' x 38' bridge with a sufficiency rating of 58; (3) Big Alligator Creek - 144' x 38' bridge with a sufficiency rating of 58. This project is part of the Governor's Road Improvement Program (GRIP). As part of this program, the existing two lane US 84/SR 38 will be multilaned to stimulate growth via an improved transportation network. The base year traffic (2001) varies from 5,700 VPD to 6,450 VPD and the design year traffic (2021) varies from 9,050 VPD to 10,250 VPD. The proposed design speed is 65 MPH.

The proposed construction will provide two, 12' lanes in each direction divided by a 32' and 44' depressed grassed median. The proposed 32' grassed median (used in certain areas) complies with the agreement between the Department of ^{DOT} federal resource agencies to reduce impacts to wetlands. Additional right-of-way required will vary from 110' to 118'. The existing bridges at Greasy Branch Creek, Little Alligator Creek, and Big Alligator Creek will be widened to 38' under project BHN-007-3(28) Ware County. New parallel bridges will be constructed over Greasy Branch Creek, Little Alligator Creek, and Big Alligator Creek to accommodate the new lanes. Additionally, three existing culverts will be extended to accommodate the new lanes. This roadway will remain open to traffic during construction.

The west terminus and east terminus ties into projects EDS-84(23) Clinch/Ware and EDS 84(27) Ware, respectively with similar typical sections.

Environmental concerns include requiring a COE 404 Permit; 41 acres of wetlands impacted; an Environmental Assessment will be prepared; 10 displacements - 7 residences, 3 mobile homes; a public hearing will be held; time saving procedures are not appropriate.

Wayne Shackelford

Page 2

EDS-84(26)/BHN-007-3(28) Ware

November 9, 1999

The estimated costs for these projects are:

EDS-84(26) Ware County

	<u>PROPOSED</u>	<u>APPROVED</u>	<u>PROG DATE</u>	<u>LET DATE</u>
Construction (includes E&C and inflation)	\$12,848,000	\$10,895,000	2004	03-07
Right-of-Way	\$ 1,250,000	----		
Utilities*	----	----		

*LGPA to be sent.

BHN-007-3(28) Ware County

	<u>PROPOSED</u>	<u>APPROVED</u>	<u>PROG DATE</u>	<u>LET DATE</u>
Construction (includes E&C and inflation)	\$1,316,000	\$1,085,000	2004	03-07
Right-of-Way	----	----		
Utilities*	----	----		

*Ware County signed LGPA for utilities 4-24-96.

This project is part of the Governor's Road Improvement Program. I recommend this project concept be approved.

TLT:JDQ/cj

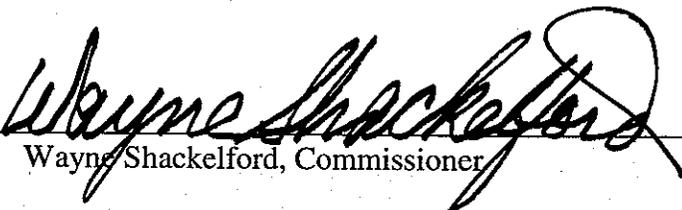
Attachment

CONCUR



Frank L. Danchetz, P.E., Chief Engineer

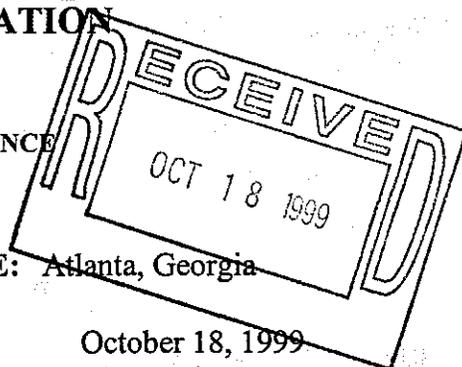
APPROVE



Wayne Shackelford, Commissioner

DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA

INTERDEPARTMENTAL CORRESPONDENCE



FILE: EDS-84(26) Ware
P.I. Number 522770

OFFICE: Atlanta, Georgia

DATE: October 18, 1999

FROM: David Mulling, Project Review Engineer *DM*

TO: Wayne Hutto, Assistant Director of Pre-construction

SUBJECT: CONCEPT REPORT

We have reviewed the concept report submitted October 14, 1999 by the letter from David Studstill dated October 12, 1999, and have the following comment:

1. Reimbursable utility cost information was not provided.

The costs for the project are:

Construction	\$10,618,000
Inflation	\$ 1,062,000
E&C	\$ 1,168,000
Reimbursable Utilities	\$?
Right of Way	\$ 1,250,000

EDS-84(26) BHN-007-3(28)
\$ 1,316,000 *del.*

DTM

c: David Studstill

DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA
OFFICE OF ENVIRONMENT/LOCATION

PROJECT CONCEPT REPORT
EDS-84(26)
WARE COUNTY
P.I. NO. 522770

Federal Route No.: F-7-3

Date of Report: September 2, 1999

State Route No.: 38

RECOMMENDATION FOR APPROVAL

10/7/99
Date

Oil S. Hittell
State Environmental/Location Engineer

Date

State Road & Airport Design Engineer

Date

State Traffic Operations Engineer

Date

District Engineer/Jesup

Date

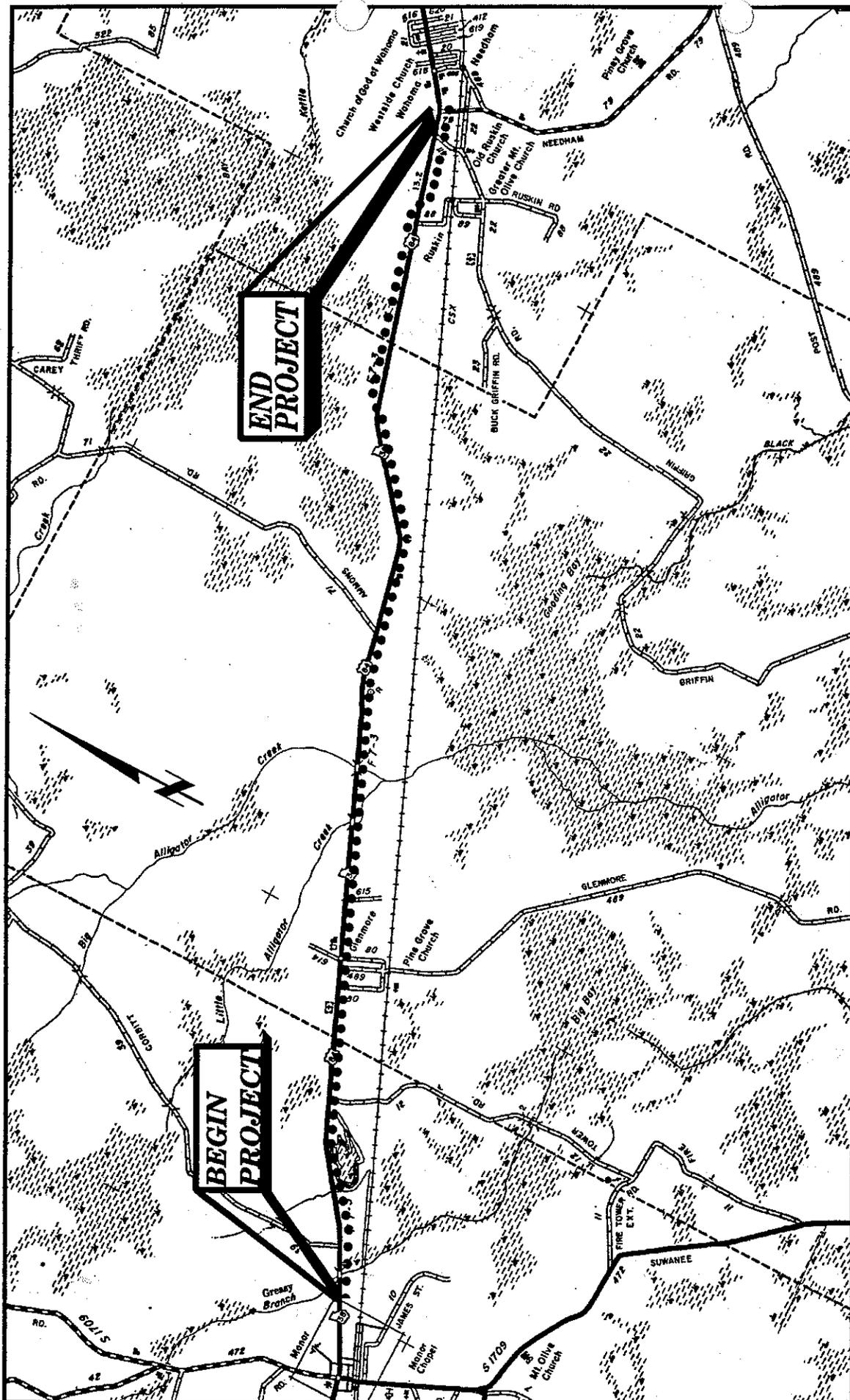
State Bridge & Structural Engineer

Date

State Transportation Planning Administrator

Date

State Transportation Programming Administrator



LOCATION

EDS-84(26)
 US 84SR 38 IMPROVEMENTS
 WARE COUNTY
 P.I. NO.: 522770



SCALE IN MILES

SOURCE, GENERAL HIGHWAY MAP, WARE, CO, GEORGIA
 PREPARED BY THE GEORGIA DEPARTMENT OF TRANSPORTATION, 1988

5/1/99

PROJECT CONCEPT REPORT

PROJECT NUMBER: EDS-84(26)WARE

PROJECT LOCATION AND DESCRIPTION

Project EDS-84 (26), in Ware County, would widen and reconstruct US 84/SR 38 from a point approximately 900 feet west of Greasy Branch Creek to C.R. 79/Needham Road for a distance of approximately 8.7 miles. This project ties into EDS-84(23) at the beginning terminus and EDS-84(27) at the ending terminus.

The project would begin approximately 900 feet west of Greasy Branch and would widen US 84/SR 38 on the south side, adding a 44-foot grassed median with two 12-foot lanes of pavement, while utilizing the existing right of way on the north side of US 84/SR 38. It would continue to approximately 1,850 feet east of CR 615/Sutton Lane. At that point, the median narrows to a 32-foot grassed median and would continue eastward to approximately 2,300 feet west of CR 71/Ammons Road. The 32-foot grassed median complies with the agreement between the Department and federal resource agencies to reduce impacts to wetlands. At that point, the median widens to 44 feet and continues to approximately 2,600 feet east of CR 71/Ammons Road. At that point, the median narrows to 32 feet and continues eastward 3,700 feet. The alignment would then shift to the north side while holding the existing right of way on the south side of US 84/SR 38 and would continue to approximately 1,000 feet east of CR 88/Ruskin Road. There, the median widens to 44 feet and the alignment would shift to the south side of US 84/SR 38 and continue to the end of the project at CR 79/Needham Road. Through this section, it is proposed to construct four new 12-foot lanes, while holding the existing right of way on the north side. This concept would avoid a historical site along the north side of US 84/SR 38. The existing bridges at Greasy Branch Creek, Little Alligator Creek and Big Alligator Creek would be widened and new ones built parallel to the existing bridges. Additional required R/W would vary from 70 feet to 118 feet. The speed design is 65 miles per hour.

TRAFFIC

CURRENT		PROJECTED	
YEAR	AADT	YEAR	AADT
2001	5,700	2021	9,050
	6,450		10,250

PDP CLASSIFICATION	FUNCTIONAL CLASSIFICATION
MAJOR / NEW CONSTRUCTION & CONSTRUCTION ALONG EXISTING ROADWAY	RURAL ARTERIAL
MAJOR / NEW CONSTRUCTION & CONSTRUCTION ALONG EXISTING ROADWAY FOS ()	N/A (X) EXEMPT ()

NEED AND PURPOSE

The US 84/SR 38 Improvements are part of the Governors Road Improvement Program (G.R.I.P.) and involves the multi-laning of this primary east-west corridor in south Georgia, serving as a catalyst for the development of this region. The improvements will aid in the economic development of sparsely populated rural areas and small towns along this route. Traffic carrying capacity will be increased; safety and operational characteristics along this segment will be improved.

EXISTING ROADWAY

TYPICAL SECTION:	2 - 12 foot lanes with 10 foot shoulders-rural	R/W WIDTH 60 feet - 100 feet
POSTED SPEED	MINIMUM ^{DEGREE} RADIUS OF CURVE	MAX GRADE
45.55 MPH	4°-00'	0.38 %

MAJOR STRUCTURES

FEATURES INTERSECTED/TYPE	LENGTH	WIDTH	PRIORITY RATING	SUFF. RATING
Greasy Branch Creek Bridge - I.D. N.:299-0008-0	96 feet	38 feet	00000	60.7
Little Alligator Creek - I.D. N.:299-0009-0	72 feet	38 feet	00000	58.0
Big Alligator Creek - I.D. N.:299-0010-0	144 feet	38 feet	00000	58.0
6X2 double culvert located approx. 1700 feet east of CR 120/Fire Tower Rd.	46 feet	12 feet		
6X3 triple culvert located approx. 3100 feet east of Gooding Bay Rd.	45 feet	19 feet		
6X3 triple culvert located approx. 3200 feet west of CR 88/Ruskin Rd.	48 feet	19 feet		

PROPOSED ROADWAY

TYPICAL SECTION:	4 -12-foot lanes w/32 & 44-foot depressed grassed median rural			
DESIGN SPEED	MINIMUM ^{DEGREE} RADIUS OF CURVE		MAX GRADE	
65 MPH	ALLOWABLE	3°-45'	ALLOWABLE	4.0 %
	PROPOSED	3°-45'	PROPOSED	0.38 %

MAJOR STRUCTURES

FEATURES INTERSECTED/TYPE	LENGTH	WIDTH
Greasy Branch Creek (Widen Existing Bridge & Build 1 New Bridge)	96 feet	38 feet
Little Alligator Creek (Widen Existing Bridge & Build 1 New Bridge)	72 feet	38 feet
Big Alligator Creek (Widen Existing Bridge & Build 1 New Bridge)	144 feet	38 feet
Extend 6X2 double culvert located approx. 1700 feet east of CR 120/Fire Tower Rd.	100 feet	12 feet
Extend 6X3 triple culvert located approx. 3100 feet east of Gooding Bay Rd.	88 feet	19 feet
Extend 6X3 triple culvert located approx. 3200 feet west of CR 88/Ruskin Rd.	88 feet	19 feet

RIGHT-OF-WAY

ADDITIONAL RIGHT-OF WAY WIDTH	PARCELS IMPACTED	DISPLACEMENTS
Varies - 110 feet to 118 feet	62	BUS. : 0 RES.: 7 M.H.: 3

TYPE OF ACCESS CONTROL: By permit

ESTIMATED COST

CONSTRUCTION:	\$ 10,618,000	RIGHT-OF-WAY:	\$ 1,250,000
E & C (10%):	\$ 1,062,000	ACQUIRED BY:	DOT
INFLATION:	\$ 1,197,000	UTILITIES:	To be requested
(2 yrs at 5% per yr):		ADJUSTED BY:	LGPA & D.O.T.
TOTAL CONS'T COST:	\$ 12,877,000		

COORDINATION

CONCEPT TEAM MEETING DATE: 6-23-99

ENVIRONMENTAL CONCERNS: None

PERMITS REQUIRED: C.O.E. 404 (Approx. 21 wetland acres)

LEVEL OF PUBLIC INVOLVEMENT: Public Hearing to be scheduled

TIME SAVING PROCEDURES APPROPRIATE: NO

OTHER PROJECTS IN THE AREA: EDS-84(23)Clinch/Ware, EDS-84(27)Ware

MISCELLANEOUS

TRAFFIC CONTROL DURING CONSTRUCTION: Maintain existing traffic during construction

LEVEL OF ENVIRONMENTAL ANALYSIS: (EA) Environmental Assessment

UNDERGROUND STORAGE TANKS: Study to be requested

HAZARDOUS WASTE SITES: Study to be requested

DESIGN VARIATIONS REQUESTED:

	YES	NO	UNDETERMINED
SUBST HORIZ ALIGNMENT	()	(X)	()
SUBST ROADWAY WIDTH	()	(X)	()
SUBST SHOULDER WIDTH	()	(X)	()
SUBST VERTICAL GRADES	()	(X)	()
SUBST CROSS SLOPES	()	(X)	()
SUBST STOPPING SIGHT DIST	()	(X)	()
SUBST SUPERELEV RATES	()	(X)	()
SUBST HORIZONTAL CLEARANCE	()	(X)	()
SUBST SPEED DESIGN	()	(X)	()
SUBST VERTICAL CLEARANCE	()	(X)	()
SUBST BRIDGE WIDTH	()	(X)	()
SUBST BR STRUCT CAPACITY	()	(X)	()

ALTERNATIVES CONSIDERED

1. No build.
 2. Widen on opposing side of proposed roadway. Was eliminated due to residential displacements or history impacts.
-

COMMENTS

1. This project will match existing vertical alignment.
2. This alignment has not been approved through the 404-B(1) guideline process with the Federal resource agencies and is subject to change.

ATTACHMENTS: COST ESTIMATE, TYPICAL SECTIONS, TEAM MEETING MINUTES

PREPARED BY: Radney Simpson, L.E. I

PRELIMINARY COST ESTIMATE
OFFICE OF ENVIRONMENT/LOCATION

P.I. NO: 522770

DATE: 09-03-1999

PROJECT NO: EDS-84(26)

PROJECT NAME: SR 38 IMPROVEMENTS

COUNTY: WARE

PROJECT DESCRIPTION:

SR 38 IMPROVEMENTS FROM JUST EAST OF MANOR TO THE
INTERSECTION OF SR 38 AND NEEDHAM ROAD.

PROJECT LENGTH: 8.700 MILES

SECTION LENGTH: 1.900 MILES

TYPICAL SECTION:

RURAL NEW LOCATION-4-LANES WITH 44' DIV MEDIAN (48' PAV'T)

PROPOSED R / W = 250 ft

EXISTING ROADWAY (If Applicable):

SR 38

TRAFFIC:

INITIAL DESIGN YEAR: 2001

DAILY VOLUME (AADT): 6,450

FINAL DESIGN YEAR: 2021

DAILY VOLUME (AADT): 10,250

COMMENTS:

THIS COST ESTIMATE IS FOR ALL ROADWAY ON UNIT 26 WHERE THE
HORIZONTAL ALIGNMENT DID NOT MEET 65 MPH & NEW LOCATION

PREPARED BY: RADNEY SIMPSON L.E.I

PROJECT COSTS

A. RIGHT - OF - WAY

1. PROPERTY (Land and Easements)	\$	0
2. DISPLACEMENTS	\$	0
3. OTHER COST	\$	0
SUBTOTAL	\$	0

B. REIMBURSABLE UTILITIES

1. RAILROAD	\$	0
2. TRANSMISSION LINES	\$	0
3. SERVICES	\$	0
SUBTOTAL	\$	0

C. MAJOR STRUCTURES

1. WALLS	\$	0
<hr/>		
2. BRIDGE STREAM CROSSING	\$	0
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3. BRIDGE OVER/UNDERPASS	\$	0
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4. BOX CULVERTS	\$	0
<hr/>		
SUBTOTAL	\$	0

D. GRADING AND DRAINAGE

1. EARTHWORK		
a. UNCLASSIFIED EXCAVATION SOIL	\$	19,000
5,720 CY @ \$3.35		
b. UNCLASSIFIED EXCAVATION ROCK	\$	0
0 CY @ \$4.00		
c. BORROW EXCAVATION	\$	78,000
19,397 CY @ \$4.00		
2. DRAINAGE		
a. MINOR DRAINAGE (Including Cross Drain Pipes & Longitudinal System)	\$	156,000
1.900 MILES @ \$82,080		
b. CURB AND GUTTER	\$	0
0 LF @ \$9.27		
SUBTOTAL	\$	253,000

PROJECT COSTS

con't.

E. BASE AND PAVING

1. GRADED AGGREGATE BASE	\$	537,000
12.00" -- 43,338 T @ \$12.40		
2. ASPHALT PAVING		
a. ASPHALTIC CONCRETE "E"	\$	186,000
1.50" -- 5,768 T @ \$32.22		
b. ASPHALTIC CONCRETE "B"	\$	245,000
2.00" -- 7,691 T @ \$31.87		
c. ASPHALTIC CONCRETE BASE	\$	664,000
7.00" -- 21,535 T @ \$30.85		
d. BITUMINOUS TACK COAT	\$	7,000
7,854 G @ \$0.85		
3. CONCRETE PAVING	\$	0
4. OTHER PAVING	\$	164,000
	SUBTOTAL \$	1,803,000

F. LUMP ITEMS

1. TRAFFIC CONTROL	\$	0
2. CLEARING AND GRUBBING	\$	271,000
58 ACRES @ \$4,700		
3. LANDSCAPING	\$	97,000
1.900 MILES @ \$51,000		
4. EROSION CONTROL	\$	91,000
1.900 MILES @ \$48,000		
5. DETOURS (Including Temporary Bridges)	\$	0
	SUBTOTAL \$	459,000

G. MISCELLANEOUS

1. SIGNING/STRIPING	\$	70,000
1.900 MILES @ \$37,000		
2. GUARDRAIL	\$	0
0 LF @ \$11.46 + 0 Anchors @ \$1,326.82		
3. OTHER	\$	306,000
1.900 MILES @ \$161,100		
	SUBTOTAL \$	376,000

H. SPECIAL FEATURES	\$	0
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ESTIMATE SUMMARY

		SECTION COST (per mile)
A. RIGHT-OF-WAY	\$ 0	\$ 0
B. REIMBURSABLE UTILITIES	\$ 0	\$ 0

CONSTRUCTION COST SUMMARY

C. MAJOR STRUCTURES	\$ 0	
D. GRADING AND DRAINAGE	\$ 253,000	
E. BASE AND PAVING	\$ 1,803,000	
F. LUMP ITEMS	\$ 459,000	
G. MISCELLANEOUS	\$ 376,000	
H. SPECIAL FEATURES	<u>\$ 0</u>	
SUBTOTAL CONSTRUCTION COST	\$ 2,891,000	\$ 1,522,000
E. & C. (10%)	\$ 289,000	
INFLATION 2 yr(s) @ 5% per year	<u>\$ 326,000</u>	
TOTAL CONSTRUCTION COST	\$ 3,506,000	\$ 1,845,000

GRAND TOTAL CONSTRUCTION COST	\$ 3,506,000	\$ 1,845,000
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RURAL NEW LOCATION-4-LANES WITH 44' DIV MEDIAN (48' PAV'T)

PRELIMINARY COST ESTIMATE
OFFICE OF ENVIRONMENT/LOCATION

P.I. NO: 522770

DATE: 06-01-1999

PROJECT NO: EDS-84(26)

PROJECT NAME: SR 38 Improvements

COUNTY: WARE

PROJECT DESCRIPTION:

SR 38 IMPROVEMENTS FROM JUST EAST OF MANOR TO THE
INTERSECTION OF SR 38 AND NEEDHAM ROAD.

PROJECT LENGTH: 8.700 MILES

SECTION LENGTH: 6.800 MILES

TYPICAL SECTION:

RURAL WIDENING-2 TO 4 LANES WITH 44' DIV MEDIAN (48' PAV'T)
PROPOSED R / W = 100 ft

EXISTING ROADWAY (If Applicable):

S.R. 38

TRAFFIC:

INITIAL DESIGN YEAR: 2001

DAILY VOLUME (AADT): 6,450

FINAL DESIGN YEAR: 2021

DAILY VOLUME (AADT): 10,250

COMMENTS:

THIS COST ESTIMATE IS FOR ALL ROADWAY ON UNIT 26 HOLDING
EXISTING PAVING AND ADDING TWO LANES OF PAVING.

PREPARED BY: Radney Simpson L.E.I

PROJECT COSTS

A. RIGHT - OF - WAY

1. PROPERTY (Land and Easements)	\$	0
2. DISPLACEMENTS	\$	0
3. OTHER COST	\$	0
SUBTOTAL	\$	0

B. REIMBURSABLE UTILITIES

1. RAILROAD	\$	0
2. TRANSMISSION LINES	\$	0
3. SERVICES	\$	0
SUBTOTAL	\$	0

C. MAJOR STRUCTURES

1. WALLS	\$	0
<hr/>		
2. BRIDGE STREAM CROSSING WIDENING AND NEW BRIDGE COST FOR ALL BRIDGES ON UNIT 26	\$	781,750
*p1800X 3. BRIDGE OVER/UNDERPASS	\$	0
<hr/>		
4. BOX CULVERTS ALL BOX CULVERTS ON UNIT 26	\$	205,000
SUBTOTAL	\$	987,000

D. GRADING AND DRAINAGE

1. EARTHWORK		
a. UNCLASSIFIED EXCAVATION SOIL 29,100 CY @ \$3.35	\$	98,000
b. UNCLASSIFIED EXCAVATION ROCK 0 CY @ \$4.00	\$	0
c. BORROW EXCAVATION 69,970 CY @ \$4.00	\$	280,000
2. DRAINAGE		
a. MINOR DRAINAGE (Including Cross Drain Pipes & Longitudinal System) 6.800 MILES @ \$39,960	\$	272,000
b. CURB AND GUTTER 0 LF @ \$9.27	\$	0
SUBTOTAL	\$	650,000

PROJECT COSTS

con't.

E. BASE AND PAVING

1. GRADED AGGREGATE BASE	\$ 1,069,000
12.00" -- 86,170 T @ \$12.40	
2. ASPHALT PAVING	
a. ASPHALTIC CONCRETE "E"	\$ 798,000
1.50" -- 24,774 T @ \$32.22	
b. ASPHALTIC CONCRETE "B"	\$ 1,053,000
2.00" -- 33,032 T @ \$31.87	
c. ASPHALTIC CONCRETE BASE	\$ 1,529,000
9.00" -- 49,548 T @ \$30.85	
d. BITUMINOUS TACK COAT	\$ 20,000
24,094 G @ \$0.85	
3. CONCRETE PAVING	\$ 0
4. OTHER PAVING	\$ 447,000
	SUBTOTAL \$ 4,916,000

F. LUMP ITEMS

1. TRAFFIC CONTROL	\$ 59,000
2. CLEARING AND GRUBBING	\$ 387,000
82 ACRES @ \$4,700	
3. LANDSCAPING	\$ 122,000
6.800 MILES @ \$18,000	
4. EROSION CONTROL	\$ 163,000
6.800 MILES @ \$24,000	
5. DETOURS (Including Temporary Bridges)	\$ 0
	SUBTOTAL \$ 731,000

G. MISCELLANEOUS

1. SIGNING/STRIPING	\$ 204,000
6.800 MILES @ \$30,000	
2. GUARDRAIL	\$ 0
0 LF @ \$11.46 + 0 Anchors @ \$1,326.82	
3. OTHER	\$ 239,000
6.800 MILES @ \$35,100	
	SUBTOTAL \$ 443,000

H. SPECIAL FEATURES

\$ 0

ESTIMATE SUMMARY

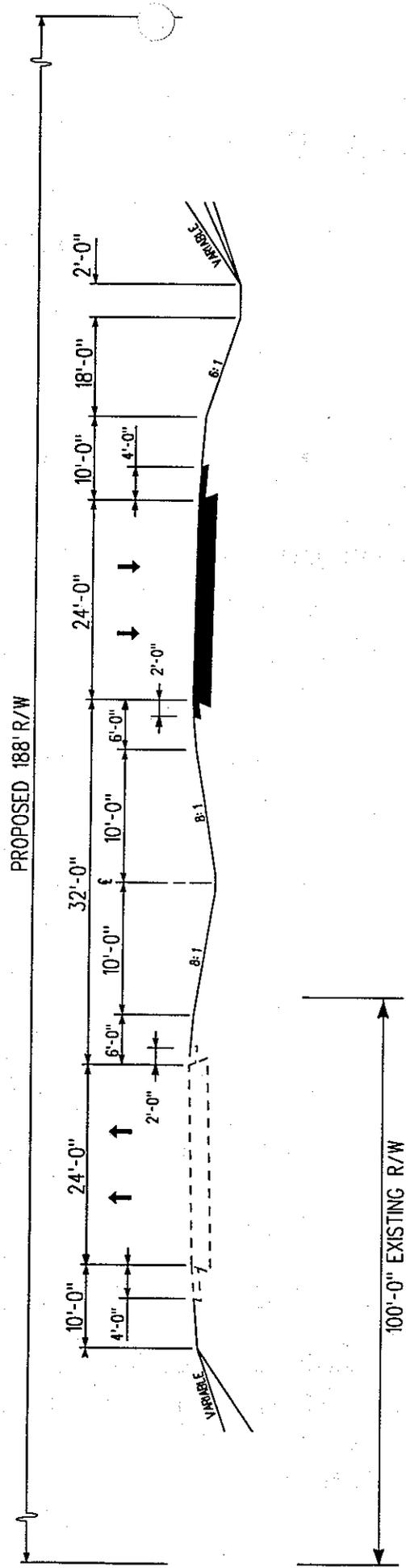
		SECTION COST (per mile)
A. RIGHT-OF-WAY	\$ 0	\$ 0
B. REIMBURSABLE UTILITIES	\$ 0	\$ 0

CONSTRUCTION COST SUMMARY

C. MAJOR STRUCTURES	\$ 987,000	
D. GRADING AND DRAINAGE	\$ 650,000	
E. BASE AND PAVING	\$ 4,916,000	
F. LUMP ITEMS	\$ 731,000	
G. MISCELLANEOUS	\$ 443,000	
H. SPECIAL FEATURES	<u>\$ 0</u>	
SUBTOTAL CONSTRUCTION COST	\$ 7,727,000	\$ 1,136,000
E. & C. (10%)	\$ 773,000	
INFLATION 2 yr(s) @ 5% per year	<u>\$ 871,000</u>	
TOTAL CONSTRUCTION COST	\$ 9,371,000	\$ 1,378,000

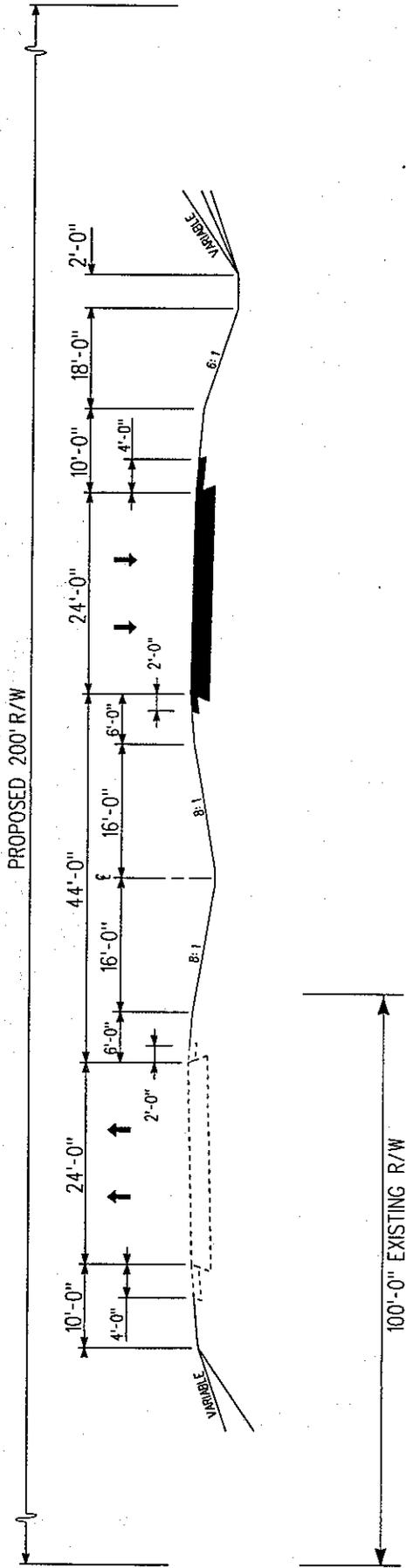
GRAND TOTAL CONSTRUCTION COST	\$ 9,371,000	\$ 1,378,000
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RURAL WIDENING-2 TO 4 LANES WITH 44' DIV MEDIAN (48' PAV'T)



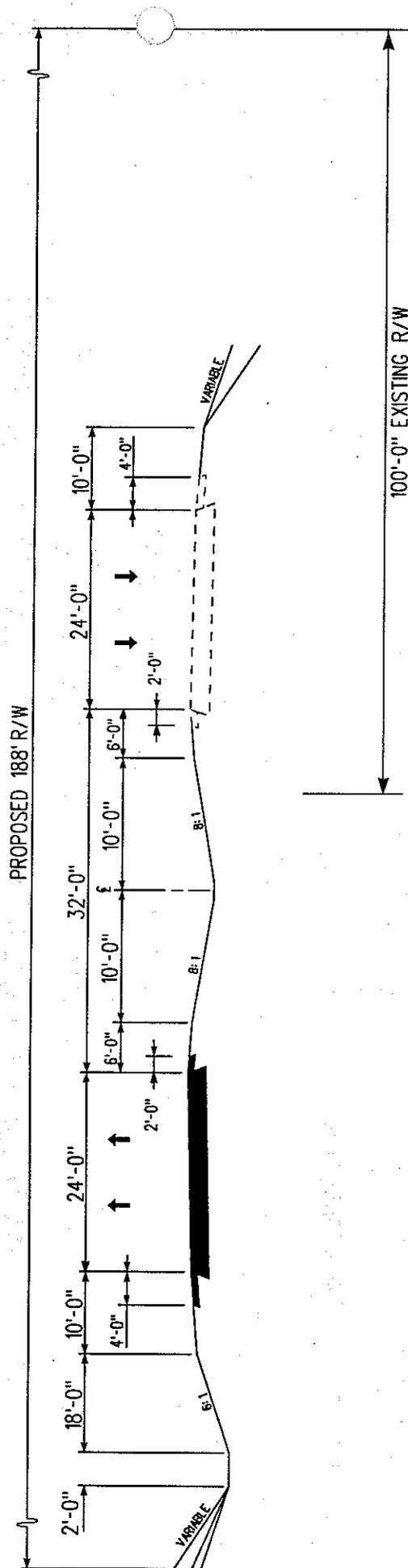
SR 38 IMPROVEMENTS
 EDS-84(26) WARE CO.
 LIMITS SECTION APPLIES
 FROM 1,850 FEET EAST OF CR 615/SUTTON LANE
 TO 2,300 FEET WEST OF CR 71/AMMONS ROAD
 FROM 2,600 FEET EAST OF CR 71/AMMONS ROAD
 TO 1.5 MILES WEST OF CR 88/RUSKIN ROAD

NOT TO SCALE



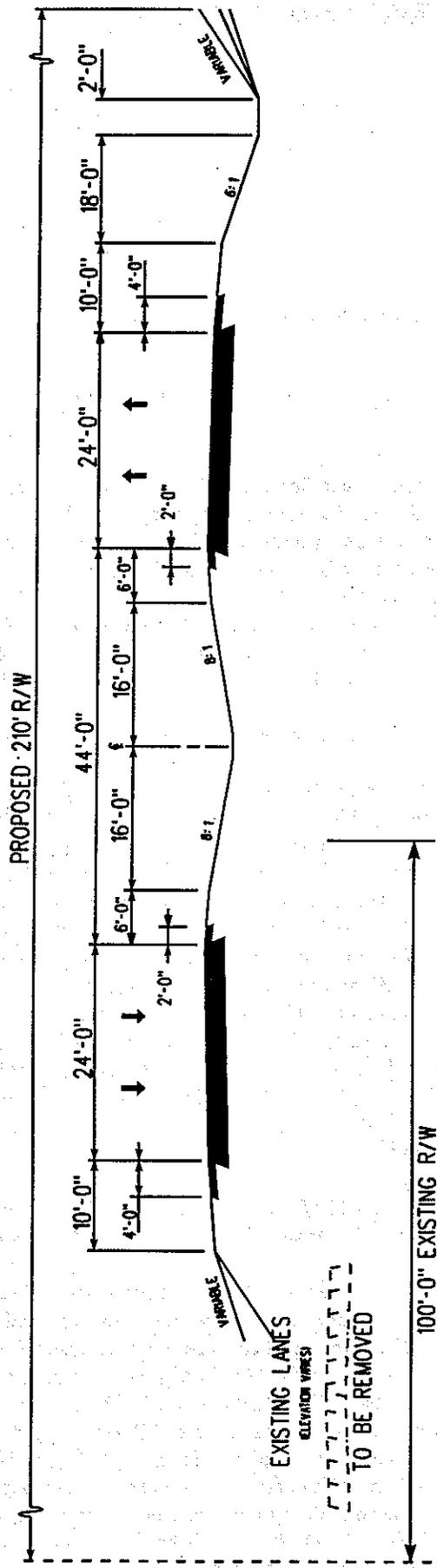
SR 38 IMPROVEMENTS
 EDS-84(26) WARE CO.
 LIMITS SECTION APPLIES
 FROM 900 FEET WEST OF GREASY BRANCH
 TO 1,850 FEET EAST OF CR 615/SUTTON LANE
 FROM 2,300 FEET WEST OF CR 71/AMMONS ROAD
 TO 2,600 EAST OF CR 71/AMMONS ROAD

NOT TO SCALE



SR 38 IMPROVEMENTS
 EDS-84(26) WARE CO.
 LIMITS SECTION APPLIES
 FROM 1.5 MILES WEST OF CR 88/RUSKIN ROAD
 TO 1,000 FEET EAST OF CR 88/RUSKIN ROAD.

NOT TO SCALE



**S.R. 38 IMPROVEMENTS
EDS-84(26) WARE CO.
LIMITS SECTION APPLIES
FROM 1,000 FEET EAST OF CR 88/RUSKIN ROAD
TO CR 79/NEEDHAM ROAD**

NOT TO SCALE

DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA

INTERDEPARTMENTAL CORRESPONDENCE

FILE EDS-84(26,27) Ware County OFFICE Environment/Location
P.I. Nos. 522770,522780 DATE June 25, 1999

FROM Radney Simpson, LE I

TO Distribution Below

SUBJECT CONCEPT TEAM MEETING MINUTES - S.R.38 Improvements

DATE: June 23, 1999

PLACE: City Hall in Waycross

IN ATTENDANCE: Representing Ware County: Carlos Nelson, Frank Sherrill & Wayne Kilmark. Representing Ware County commission: Rodger Strickland, Marshall Monk, Ralph Tyson & Jerry Pope. Representing Waycross: Robert L. Odum-Mayor, Bucky Heys-City Manager, Jennifer Carter & A.A. Karle-Chamber Commerce. Representing Waycross City Commission: Leonard Burse Jr., William Sumurein & Gene Ward. Representing State Representative Harry Dixon: Bryant Clake. Representing Utilities: Ralph Bennet & David Norfolk -Bell South, J.C. Humphrey-GA.Power. Representing Waycross Area office: William R. Strickland-Waycross Asst. Construction Area Engineer. Representing the Jesup District Office: Zoe Hardenbrook-Preconstruction, Stephen Thomas-Utilities & Allen Troha-Traffic Operations. Representing the Office of State Road and Airport Design: Jim Simpson & Nicholas Castronova. Representing the Office of Environment/Location: Zewdie Bekele-EAB. Jerry Hobbs, Fred Matheny & Radney Simpson-Location.

Jerry Hobbs opened the meeting and Radney Simpson gave a detailed description of the projects.

Project EDS-84(26) would begin approximately 900 feet west of Greasy Branch and would widen US 84/SR 38 on the south side, adding a 44-foot grassed median with two 12-foot lanes of pavement, while utilizing the existing right of way on the north side of US 84/SR 38. It would continue to approximately 1,850 feet east of CR 615/Sutton Lane. At that point, the median narrows to a 32-foot grassed median and would continue eastward to approximately 2,300 feet west of CR 71/Ammons Road. The 32-foot grassed median complies with the agreement between the Department and federal resource agencies to reduce impacts to wetlands. At that point, the median widens to 44 feet and continues to approximately 2,600 feet east of CR 71/Ammons Road. At that point, the median narrows to 32 feet and continues to approximately 1.5 miles west of CR 88/Ruskin Road. The alignment would then shift to the north side while holding the existing right of way on the south side of US 84/SR 38 and would continue to approximately 1,000 feet east of CR 88/Ruskin Road. There, the median widens to 44 feet and the alignment would shift to the south side of US 84/SR 38 and continue to the end of the project at CR 79/Needham Road. Through this section, it is proposed to construct four new 12-foot lanes, while holding the existing right of way on the north side. This concept would avoid a historical site along the north side of US 84/SR 38. The existing bridges at Greasy Branch Creek, Little Alligator Creek and Big Alligator Creek would be widened and new ones built parallel to the existing bridges. Access throughout the project would be by permit along the existing road. Additional required R/W vary from 70 feet to 150 feet. The speed design is 65 miles per hour.

Project EDS-84(27), in Ware county, would widen and reconstruct US 84/SR 38 from the CR 79/Needham Road to the intersection SR 38C and US 82/SR 50 for a distance of approximately 4.8 miles. This project ties into the EDS-84(26) at the beginning terminus.

The project would begin at the intersection of US 84/SR 38 and CR 79/Needham Road. The project would reconstruct US 84/SR 38 along the south side, adding a 44-foot grassed median with four 12-foot lanes of pavement, acquiring proposed right of way along both sides of the US 84/SR 38. It would continue to approximately 400 hundred feet east of CR 79/Needham Road. This continues the concept proposed for EDS-84(26). At that point the, project would widen US 84/SR 38 along the south side, adding two 12-foot lanes of pavement, using the existing pavement to approximately 3,000 feet east of CR 91/Smith Road. There, the concept makes a transition to an urban section with a 20 foot raised median. This concept would continue eastward to CR 290/Oregon Avenue. At that point, the alignment would shift onto new location south of US 84/SR 38 and a rural typical section would begin and continue eastward for a distance of approximately 1.2 miles. The alignment would tie back into Glenmore Avenue and continue along SR 38C/Victory Drive. The concept would hold the existing right of way along the west side and widen to the east side, constructing a 20 foot raised median, adding two 12-foot lanes of pavement in an urban section. SR 38C/Victory Drive currently has four 12-foot lanes with curb and gutter and a 14-foot flush median. This alignment would continue northward to the intersection of SR 38C/Victory Drive and US 82/SR

50/Corridor "Z", where the project would end. This concept would use existing pavement. The additional R/W varies from 40 to 105 feet. The speed design is 45 and 65 MPH.

After review of the concept, the following comments were made:

Local Officials- City-County-State

Waycross Mayor, Robert L. Odum- Mayor Odum asked to review the proposed right of way needs just prior to the intersection of SR 38/Glenmore. Radney Simpson explained that the 20-foot raised median in a rural section needed 150 of total right of way. Mayor Odum asked that the construction of the projects begin at the end of EDS-84(27), near Waycross and continue westward towards Manor. Mayor Odum expressed his appreciation to the Department for coming and his approval of the projects.

Ware County Commissioner, Rodger Strickland-Chairman- Commissioner Strickland asked that the construction of the projects begin at the end of EDS-84(27), near Waycross and continue westward towards Manor. Commissioner Strickland inquired about the possibility of placing an additional median opening on SR 38C/Victory Drive. Jerry Hobbs explained Location's policy of placing median openings at state routes and county routes. Once the project was transferred to Design, the Design office would determine where additional median openings, if any, would be needed. Upon review with Jim Simpson from the office of Road Design, it was decided that an additional median opening could be placed on SR 38C/Victory Drive. Commissioner Strickland expressed his appreciation for the Department for coming and his approval of the projects. On a side note, he asked that Bypass projects in the area of Waycross be referred to as perimeter roads.

Ware County Commissioner, Marshall Monk - Commissioner Monk inquired how long it would take to get environmental clearance for the projects. Zewdie Bekele-EAB, reminded everyone that because the project has yet to be cleared environmentally that the project was subject to change. Zewdie stated that once a consultant was selected, it could take approximately 18 months to get environmental clearance.

Waycross City Commissioner, Gene Ward - Commissioner Ward expressed a concern about projected truck traffic increase due to new industrial development. Reiterated a need to begin construction of the projects at Waycross and continue westward towards Manor. Expressed a need for a perimeter road to relieve truck traffic in the town of Waycross. Zoe Hardenbrook stated that a "quick fix" was being looked into for the truck turning movement problem in at the intersection of Francis St. and Nicholls St. Jerry Hobbs explained that the Concept Report for EDS-84(27) is recommending building a perimeter road around Waycross starting from US 1/SR 50 to US 84/SR 38 in Pierce County.

Waycross City Manager, Bucky Heys - Mr. Heys inquired how the existing right of way was determined. Jerry Hobbs stated that SR 38 existing construction plans and tax maps were used to determine the existing right of way. Mr. Heys asked to review the required right of way, starting in Waycross and continuing to just west of Wahoma. Jerry Hobbs suggested that after the comment phase of the meeting, everyone could review the project layouts. At the review, Radney Simpson reviewed the area of interest with Mr. Heys.

Chamber of Commerce- Representative inquired if the project was designed for 65 MPH. Radney Simpson informed the representative that in rural areas the project was designed for 65 MPH and 45 MPH in urban areas. The representative voiced concern over truck traffic in the town of Waycross and asked how long it would take to construct the project. Design stated it would take approximately 18-24 months.

Road Design

After the comment phase was over, everyone was invited to look over the projects. Location requested that Design match the existing vertical profiles as much as possible, even though the maximum grade is 0.38%. Jim Simpson stated that the cross slope grades along the mainline could be adjusted to address any drainage issues and it should not be a problem to match existing vertical as much as possible.

Local Utilities

Bell South - Would like to replace existing utilities with fiber optics. Wanted to know proposed right of way impacts as soon as possible.

GA. Power - Would like some layouts. Jerry Hobbs instructed the layouts could be requested through the Department's District utilities department.

District Office

Traffic Operations - Need sufficient right of way for radius turning movements. Road Design noted the comment.

Planning- Projects would function well with Waycross' Planning philosophy.

ASSIGNMENTS- Planning is to provide need and purpose to Location.

Location is to provide one copy of Team Meeting layouts to District 5

ERS/ers

Distribution:

Wayne Hutto

Wouter Gulden /Attn.: Warren Bailey

Herman Griffin/Attn.: Meg Pirkle

Jim Kennerly/Attn: Jim Simpson

Marta Rosen

Marion Waters

Bobby Mustin

Dudley Ellis

David Meshberger

Steve Henry

Gary D. Priestler /Jesup District

Luke Cousin

Bascombe Hughes

Harvey Keeper

Ron Brown

Rowe Bowen/Attn: Bob Entorf

/Attn: Tal Stanfield

DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA
OFFICE OF ENVIRONMENT/LOCATION

PROJECT CONCEPT REPORT
EDS-84(26)
WARE COUNTY
P.I. NO. 522770

Federal Route No.: F-7-3

Date of Report: September 2, 1999

State Route No.: 38

RECOMMENDATION FOR APPROVAL

10/7/99
Date

[Signature]
State Environmental/Location Engineer

Date

State Road & Airport Design Engineer

Date

State Traffic Operations Engineer

Date

District Engineer/Jesup

10/16/99
Date

Paul V. Hill Jr.
State Bridge & Structural Engineer

Date

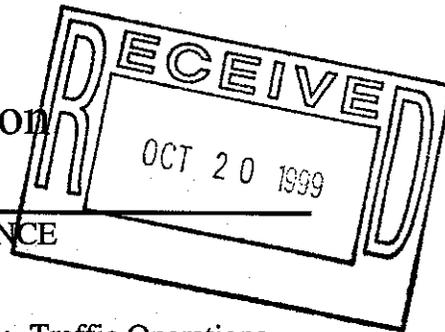
State Transportation Planning Administrator

Date

State Transportation Programming Administrator

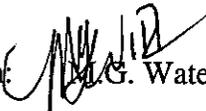
Department of Transportation
State of Georgia

INTERDEPARTMENTAL CORRESPONDENCE



File: EDS-84(26)/Ware County
P.I. No. 522770

Office: Traffic Operations
Atlanta, Georgia
Date: October 15, 1999

From:  M.G. Waters, III, P.E., State Traffic Operations Engineer

To: Wayne Hutto, Assistant Director of Preconstruction

Subject: Project Concept Report Review

We have reviewed the concept report on the above project for the widening and reconstruction of US 84/SR 38, from approximately 900 feet west of Greasy Branch Creek to Needham Road(CR 79), a distance of 8.7 miles. The improvements are part of the Governors Road Improvement Program(GRIP) that involves the multi-laning of an east-west corridor in south Georgia.

US 84/SR 38 is currently a two lane rural roadway with 10 foot shoulders. The posted speed limit varies between 45 and 55mph.

This project will widen US 84/SR 38 to a four lane divided roadway with a 32 foot and 44 foot wide grassed median. The 32 foot grassed median, used in certain areas, complies with an agreement between the Department and federal agencies to reduce impacts to wetlands. The speed design will be 65mph and traffic will be maintained during construction.

We are in agreement with our district office recommendation for sufficient right-of-way for radius turning movements but also we remind designers of revised MOG 6638-1, Auxiliary Lanes, dated July 2,1999.

We request that conduit be installed within the limits of this project as part of this project. The conduit would be used for the future interconnection of the Advanced Transportation Management System components in this area. Our Traffic Operations Design Office can provide details cost estimates for inclusion in the project

We believe this concept will improve safety and operational capacity along this section of roadway.

We therefore find this report satisfactory for approval.

MGW:TWS

Attachment (signature page)

c: David Studstill

James A. Kennerly, State Road and Airport Design Engineer

David Mulling, w/ attachment

Marta Rosen

Chuck Hasty, TMC

Keith Golden, P.E., TMC

Paul Liles, State Bridge Design Engineer

General Files

DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA
OFFICE OF ENVIRONMENT/LOCATION

PROJECT CONCEPT REPORT
EDS-84(26)
WARE COUNTY
P.I. NO. 522770

Federal Route No.: F-7-3

Date of Report: September 2, 1999

State Route No.: 38

RECOMMENDATION FOR APPROVAL

10/7/99
Date

[Signature]
State Environmental/Location Engineer

Date

State Road & Airport Design Engineer

10-18-99
Date

Marion H. Carter
State Traffic Operations Engineer

Date

District Engineer/Jesup

Date

State Bridge & Structural Engineer

Date

State Transportation Planning Administrator

Date

State Transportation Programming Administrator

DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA
OFFICE OF ENVIRONMENT/LOCATION

PROJECT CONCEPT REPORT
EDS-84(26)
WARE COUNTY
P.I. NO. 522770

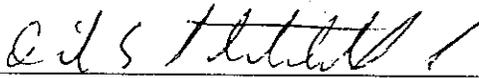
Federal Route No.: F-7-3

Date of Report: September 2, 1999

State Route No.: 38

RECOMMENDATION FOR APPROVAL

10/7/99
Date


State Environmental/Location Engineer

Date

State Road & Airport Design Engineer

Date

State Traffic Operations Engineer

Date

District Engineer/Jesup

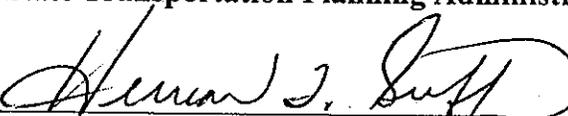
Date

State Bridge & Structural Engineer

Date

State Transportation Planning Administrator

10/14/99
Date


State Transportation Programming Administrator

DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA
OFFICE OF ENVIRONMENT/LOCATION

PROJECT CONCEPT REPORT
EDS-84(26)
WARE COUNTY
P.I. NO. 522770

Federal Route No.: F-7-3

Date of Report: September 2, 1999

State Route No.: 38

RECOMMENDATION FOR APPROVAL

10/7/99
Date


State Environmental/Location Engineer

Date

State Road & Airport Design Engineer

Date

State Traffic Operations Engineer

Date

District Engineer/Jesup

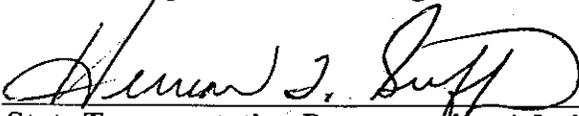
Date

State Bridge & Structural Engineer

Date

State Transportation Planning Administrator

10/14/99
Date


State Transportation Programming Administrator

DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA
OFFICE OF ENVIRONMENT/LOCATION

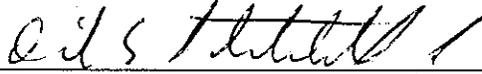
PROJECT CONCEPT REPORT
EDS-84(26)
WARE COUNTY
P.I. NO. 522770

Federal Route No.: F-7-3

Date of Report: September 2, 1999

State Route No.: 38

RECOMMENDATION FOR APPROVAL

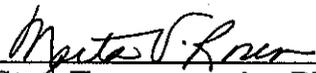
10/7/99 
Date State Environmental/Location Engineer

Date State Road & Airport Design Engineer

Date State Traffic Operations Engineer

Date District Engineer/Jesup

Date State Bridge & Structural Engineer

10/24/99 
Date State Transportation Planning Administrator

Date State Transportation Programming Administrator

DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA
OFFICE OF ENVIRONMENT/LOCATION

PROJECT CONCEPT REPORT
EDS-84(26)
WARE COUNTY
P.I. NO. 522770

Federal Route No.: F-7-3

Date of Report: September 2, 1999

State Route No.: 38

RECOMMENDATION FOR APPROVAL

10/7/99
Date

[Signature]
State Environmental/Location Engineer

Date

State Road & Airport Design Engineer

Date

State Traffic Operations Engineer

11/17/99
Date

Mary Q. Pruitt
District Engineer/Jesup

Date

State Bridge & Structural Engineer

Date

State Transportation Planning Administrator

Date

State Transportation Programming Administrator

DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA
OFFICE OF ENVIRONMENT/LOCATION

PROJECT CONCEPT REPORT
EDS-84(26)
WARE COUNTY
P.I. NO. 522770

Federal Route No.: F-7-3

Date of Report: September 2, 1999

State Route No.: 38

RECOMMENDATION FOR APPROVAL

10/7/99
Date

[Signature]
State Environmental/Location Engineer

10/20/99
Date

[Signature]
State Road & Airport Design Engineer

Date

State Traffic Operations Engineer

Date

District Engineer/Jesup

Date

State Bridge & Structural Engineer

Date

State Transportation Planning Administrator

Date

State Transportation Programming Administrator

MONTH: JULY 1993
LOCATION/RODEN ⁴¹⁰

REVISION REQUEST
for the
CONSTRUCTION WORK PROGRAM

In accordance with the Board Resolution dated August 16, 1973, Board approval is requested to revise the Construction Work Program for the project and activity outlined below:

Action Requested: ADDITION TO THE PROGRAM

PROJECT DATA

County	Project No. P.I. No.	Type Work
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WARE	EDS-84(26) 522770	RECST. OR REHAB. WIDENING
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Description: SR 38/US 84 FROM CR 472/MANOR TO CR 79

Project Length = 9.25 Miles

FUNDING INFORMATION

Estimated Cost (\$1,000's)	DOT Share	Other Share	Fiscal Year	Cong. District	Field District
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PE	\$200		1995		
ROW	\$1,000		1997	1	5
CONST	\$10,895	\$10,895	1999		

Fund 1 = EDS
Fund 2 = EDS

REASON FOR REVISION:

Requested by the Director of Preconstruction
This project should be coordinated with BRN-007-3(28), P.I. # 522775.

RECOMMENDED

Gregg Bonline

DIRECTOR, DIVISION OF PLANNING AND PROGRAMMING

RECOMMENDED

Wayne Shackelford

COMMISSIONER

