

ORIGINAL TO GENERAL FILES

D.O.T. 66

**DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA**

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**INTERDEPARTMENT CORRESPONDENCE**

**FILE** EDS-441(27) Coffee County                      **OFFICE** Preconstruction  
P. I. No. 421910  
**DATE** January 3, 2000  
**FROM**   
C. Wayne Hutto, Assistant Director of Preconstruction  
**TO** SEE DISTRIBUTION

**SUBJECT REVISED PROJECT CONCEPT REPORT APPROVAL**

Attached for your files is the approval for subject project.

CWH/cj

Attachment

DISTRIBUTION:

Tom Turner  
David Mulling  
David Studstill (ATTN: Harvey Keeper)  
Jerry Hobbs  
Herman Griffin  
Georgene Geary (ATTN: Michael Henry)  
Marion Waters  
Marta Rosen  
David Crim

DEPARTMENT OF TRANSPORTATION  
**STATE OF GEORGIA**

INTERDEPARTMENT CORRESPONDENCE

FILE EDS-441(27) -US 441/SR 31 Improvements OFFICE Environment/Location  
Coffee County

P.I. No. 421910

DATE November 23, 1999

FROM *David E. Studstill* P.E., State Environment/Location Engineer

TO Wayne Hutto, P.E., Assistant Director of Preconstruction

SUBJECT Revised Project Concept Report

Project EDS-441(27), in Coffee County, would widen and reconstruct US 441/SR 31 from approximately 1,000 feet north of the Broxton city limits to SR 107W, just north of Mill Creek Bridge, for a project length of approximately 12 miles.

**Approved Concept:** The project would begin approximately 1,000 feet north of the Broxton city limits and would add a 44 foot grassed median and two 12-foot lanes of pavement along the east side. It would continue northward approximately 12 miles to the intersection of SR 107W. This project would tie into proposed project EDS-441(37) at the ending terminus.

**Proposed Revised Concept:** It is proposed to revise the 55 MPH speed design to 55 MPH desirable which meets 65 MPH minimum. This will allow a posted speed limit of 65 MPH. It is also proposed to revise the concept based upon the latest environmental studies.

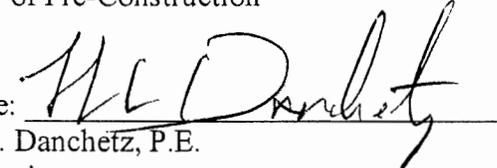
The concept would begin approximately one mile north of the Broxton City limits. The concept would construct four 12-foot lanes and a 44 foot grassed median, while continuing northward on a skew crossing the existing roadway to the east side and continue approximately 2,100 feet north of CR 336/Bro Smith Road. This concept would avoid impacting a cemetery along the east side of the existing roadway just south of CR 342/Gasman Roads and avoid a historical resource on the east side just north of CR 342/Gasman Road. The concept would impact a historical resource along the west side resulting in a programmatic 4F. The alignment would continue northward holding the existing right of way widening to the east to approximately 1,100 feet north of CR 118/Lindsey Merrit Road. This concept would avoid impacting a historical resource along the west side of the roadway at CR 336/Bro Smith Road. The median would reduce to a 20 foot raised median in an urban section adding two 12-foot lanes widening from the existing edge of pavement on the west side. It would continue approximately 800 feet north of CR 124/Earnest Pridgen Road. This concept would avoid impacting a community center located along the west side of the existing roadway near CR 124/Earnest Pridgen Road and minimize impacts to a church located along the east side of the existing roadway just north of CR 333/Pridgen Church Road. The median would widen to a 44-foot grassed median while constructing four 12-foot lanes towards the east side. It would continue to just south of CR 498/Sapps Sill Road. This concept would minimize impacts to wetlands. The concept would shift to the west side holding the existing right of way on the east side and continue northward to approximately 1,850 feet north of SR 107 east. This concept would minimize impacts to wetlands and reduce displacements. The concept would shift to the east side and continue adding a 44-foot grassed median and two 12 foot lanes of pavement to a point just north of Mill Creek bridge. The existing bridge at Mill Creek will be replaced under project BRN 023-2(7) at an estimated cost, including

inflation and E & C, of \$2,001,000. SR 107 located just north of Mill Creek Bridge will be relocated to provide an intersection skewed nearer to 90°.

<b>Estimated Cost:</b>	<b>Proposed</b>	<b>Approved</b>	<u>Prog.Date</u>
Construction (incl.E&C + infl.)	\$ 20,757,000.00	\$ 21,912,000	Long Range
Right-of-way	\$ 2,145,500.00	\$ 2,641,000	
Utilities	not available	\$ 21,500	

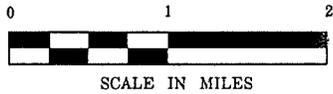
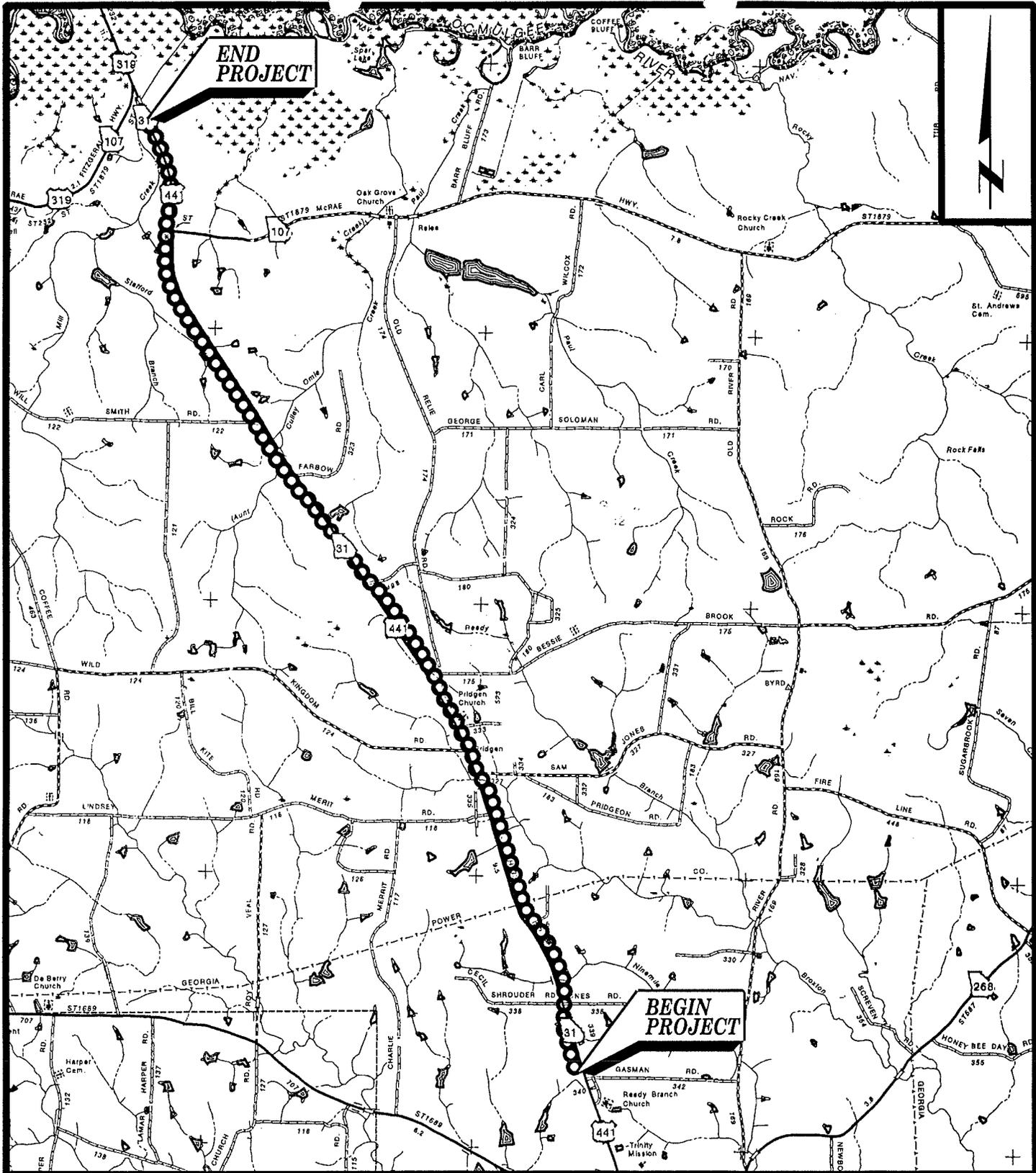
**Recommendation:** It is recommended that the proposed revision to the concept be approved for implementation.

Concur:   
Thomas L. Turner, P.E.  
Director of Pre-Construction

Approve:   
Frank L. Danchetz, P.E.  
Chief Engineer

DES/ERS

Attachments: Sketch Map  
Cost Estimates  
Typical Sections



STRIPMAP  
 EDS-441(27)  
 IMPROVEMENTS TO US 441/SR 31  
 COFFEE COUNTY  
 P.I.# 421910

SOURCE: GENERAL HIGHWAY MAP, COFFEE CO., GEORGIA  
 PREPARED BY THE GEORGIA DEPARTMENT OF TRANSPORTATION, 1996

**PRELIMINARY COST ESTIMATE**  
**OFFICE OF ENVIRONMENT/LOCATION**

**P.I. NO:** 421910

**DATE:** 11-17-1999

**PROJECT NO:** EDS-441(27)

**PROJECT NAME:** SR 31 Improvement

**COUNTY:** COFFEE

**PROJECT DESCRIPTION:**

From the northern terminus of the proposed Broxton

Bypass to just north Mill Creek bridge

**PROJECT LENGTH:** 12.000 MILES

**SECTION LENGTH:** 11.700 MILES

**TYPICAL SECTION:**

RURAL NEW LOCATION-4-LANES WITH 44' DIV MEDIAN (48' PAV'T)

PROPOSED R / W = 250 ft

**EXISTING ROADWAY (If Applicable):**

US 441/SR 31

**TRAFFIC:**

**INITIAL DESIGN YEAR:** 1997

**DAILY VOLUME (AADT):** 5,700

**FINAL DESIGN YEAR:** 2017

**DAILY VOLUME (AADT):** 8,900

**COMMENTS:**

**PREPARED BY:** Radney Simpson LE I

## PROJECT COSTS

### A. RIGHT - OF - WAY

1. PROPERTY (Land and Easements)	\$	0
2. DISPLACEMENTS	\$	0
3. OTHER COST	\$	0
SUBTOTAL	\$	0

### B. REIMBURSABLE UTILITIES

1. RAILROAD	\$	0
2. TRANSMISSION LINES	\$	0
3. SERVICES	\$	0
SUBTOTAL	\$	0

### C. MAJOR STRUCTURES

1. WALLS	\$	0
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2. BRIDGE STREAM CROSSING	\$	0
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3. BRIDGE OVER/UNDERPASS	\$	0
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4. BOX CULVERTS	\$	0
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SUBTOTAL	\$	0

### D. GRADING AND DRAINAGE

1. EARTHWORK		
a. UNCLASSIFIED EXCAVATION SOIL	\$	1,405,000
419,300 CY @ \$3.35		
b. UNCLASSIFIED EXCAVATION ROCK	\$	0
0 CY @ \$4.00		
c. BORROW EXCAVATION	\$	718,000
179,500 CY @ \$4.00		
2. DRAINAGE		
a. MINOR DRAINAGE (Including Cross Drain Pipes & Longitudinal System)	\$	960,000
11.700 MILES @ \$82,080		
b. CURB AND GUTTER	\$	0
0 LF @ \$9.80		
SUBTOTAL	\$	3,083,000

## PROJECT COSTS

con't.

### E. BASE AND PAVING

1. GRADED AGGREGATE BASE	\$ 2,930,000
10.00" -- 227,336 T @ \$12.89	
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2. ASPHALT PAVING	
a. ASPHALTIC CONCRETE "E"	\$ 1,245,000
1.50" -- 35,521 T @ \$35.05	
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b. ASPHALTIC CONCRETE "B"	\$ 1,587,000
2.00" -- 47,362 T @ \$33.50	
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c. ASPHALTIC CONCRETE BASE	\$ 1,928,000
3.00" -- 56,834 T @ \$33.93	
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d. BITUMINOUS TACK COAT	\$ 26,000
31,358 G @ \$0.84	
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3. CONCRETE PAVING	\$ 0
4. OTHER PAVING	\$ 772,000
SUBTOTAL	<u>\$ 8,488,000</u>

### F. LUMP ITEMS

1. TRAFFIC CONTROL	\$ 60,000
2. CLEARING AND GRUBBING	\$ 1,666,000
355 ACRES @ \$4,700	
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3. LANDSCAPING	\$ 597,000
11.700 MILES @ \$51,000	
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4. EROSION CONTROL	\$ 562,000
11.700 MILES @ \$48,000	
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5. DETOURS (Including Temporary Bridges)	\$ 0
SUBTOTAL	<u>\$ 2,885,000</u>

### G. MISCELLANEOUS

1. SIGNING/STRIPING	\$ 433,000
11.700 MILES @ \$37,000	
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2. GUARDRAIL	\$ 0
0 LF @ \$10.88 + 0 Anchors @ \$1,632.38	
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3. OTHER	\$ 1,885,000
11.700 MILES @ \$161,100	
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SUBTOTAL	<u>\$ 2,318,000</u>

### H. SPECIAL FEATURES

\$ 0

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**ESTIMATE SUMMARY**

		<b>SECTION COST (per mile)</b>
<b>A. RIGHT-OF-WAY</b>	\$ 0	\$ 0
<b>B. REIMBURSABLE UTILITIES</b>	\$ 0	\$ 0

**CONSTRUCTION COST SUMMARY**

<b>C. MAJOR STRUCTURES</b>	\$ 0	
<b>D. GRADING AND DRAINAGE</b>	\$ 3,083,000	
<b>E. BASE AND PAVING</b>	\$ 8,488,000	
<b>F. LUMP ITEMS</b>	\$ 2,885,000	
<b>G. MISCELLANEOUS</b>	\$ 2,318,000	
<b>H. SPECIAL FEATURES</b>	\$ <u>0</u>	
<b>SUBTOTAL CONSTRUCTION COST</b>	<b>\$ 16,774,000</b>	<b>\$ 1,434,000</b>
<b>E. &amp; C. (10%)</b>	\$ 1,677,000	
<b>INFLATION 2 yr(s) @ 5% per year</b>	<u>\$ 1,892,000</u>	
<b>TOTAL CONSTRUCTION COST</b>	<b>\$ 20,343,000</b>	<b>\$ 1,739,000</b>
<hr/>		
<b>GRAND TOTAL CONSTRUCTION COST</b>	<b>\$ 20,343,000</b>	<b>\$ 1,739,000</b>

RURAL NEW LOCATION-4-LANES WITH 44' DIV MEDIAN (48' PAV'T)

**PRELIMINARY COST ESTIMATE**  
**OFFICE OF ENVIRONMENT/LOCATION**

**P.I. NO:** 421910

**DATE:** 11-17-1999

**PROJECT NO:** EDS-441(27)

**PROJECT NAME:** SR 31 Improvements

**COUNTY:** COFFEE

**PROJECT DESCRIPTION:**

From the northern terminus of the proposed Broxton

Bypass to just north of Mill Creek bridge

**PROJECT LENGTH:** 12.000 MILES

**SECTION LENGTH:** 0.300 MILES

**TYPICAL SECTION:**

URBAN WIDENING 2-4 LANES WITH 20' RAISED MEDIAN WIDEN ON ONE SIDE (48' PAV'T)

**EXISTING ROADWAY (If Applicable):**

US 441/SR 31

**TRAFFIC:**

**INITIAL DESIGN YEAR:** 1997

**DAILY VOLUME (AADT):** 5,700

**FINAL DESIGN YEAR:** 2017

**DAILY VOLUME (AADT):** 8,900

**COMMENTS:**

**PREPARED BY:** Radney Simpson LE I

## PROJECT COSTS

### A. RIGHT - OF - WAY

1. PROPERTY (Land and Easements)	\$	<u>0</u>
2. DISPLACEMENTS	\$	<u>0</u>
3. OTHER COST	\$	<u>0</u>
SUBTOTAL	\$	<u>0</u>

### B. REIMBURSABLE UTILITIES

1. RAILROAD	\$	<u>0</u>
2. TRANSMISSION LINES	\$	<u>0</u>
3. SERVICES	\$	<u>0</u>
SUBTOTAL	\$	<u>0</u>

### C. MAJOR STRUCTURES

1. WALLS	\$	<u>0</u>
<hr/>		
2. BRIDGE STREAM CROSSING	\$	<u>0</u>
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3. BRIDGE OVER/UNDERPASS	\$	<u>0</u>
<hr/>		
4. BOX CULVERTS	\$	<u>0</u>
<hr/>		
SUBTOTAL	\$	<u>0</u>

### D. GRADING AND DRAINAGE

#### 1. EARTHWORK

a. UNCLASSIFIED EXCAVATION SOIL	\$	<u>0</u>
0 CY @ \$2.00		
b. UNCLASSIFIED EXCAVATION ROCK	\$	<u>0</u>
0 CY @ \$4.00		
c. BORROW EXCAVATION	\$	<u>0</u>
0 CY @ \$3.00		

#### 2. DRAINAGE

a. MINOR DRAINAGE (Including Cross Drain Pipes & Longitudinal System)	\$	<u>47,000</u>
0.300 MILES @ \$157,950		
b. CURB AND GUTTER	\$	<u>62,000</u>
6,336 LF @ \$9.80		
SUBTOTAL	\$	<u>109,000</u>

## PROJECT COSTS

con't.

### E. BASE AND PAVING

1. GRADED AGGREGATE BASE	\$	33,000
10.00" -- 2,534 T @ \$12.89		
2. ASPHALT PAVING		
a. ASPHALTIC CONCRETE "E"	\$	26,000
1.50" -- 729 T @ \$35.05		
b. ASPHALTIC CONCRETE "B"	\$	65,000
4.00" -- 1,943 T @ \$33.50		
c. ASPHALTIC CONCRETE BASE	\$	0
0.00" -- 0 T @ \$33.93		
d. BITUMINOUS TACK COAT	\$	1,000
600 G @ \$0.84		
3. CONCRETE PAVING	\$	0
4. OTHER PAVING	\$	13,000
SUBTOTAL	\$	138,000

### F. LUMP ITEMS

1. TRAFFIC CONTROL	\$	31,000
2. CLEARING AND GRUBBING	\$	17,000
4 ACRES @ \$4,700		
3. LANDSCAPING	\$	5,000
0.300 MILES @ \$15,960		
4. EROSION CONTROL	\$	6,000
0.300 MILES @ \$19,050		
5. DETOURS (Including Temporary Bridges)	\$	0
SUBTOTAL	\$	59,000

### G. MISCELLANEOUS

1. SIGNING/STRIPING	\$	11,000
0.300 MILES @ \$37,000		
2. GUARDRAIL	\$	0
0 LF @ \$10.88 + 0 Anchors @ \$1,632.38		
3. OTHER	\$	24,000
0.300 MILES @ \$80,100		
SUBTOTAL	\$	35,000

### H. SPECIAL FEATURES

\$ 0

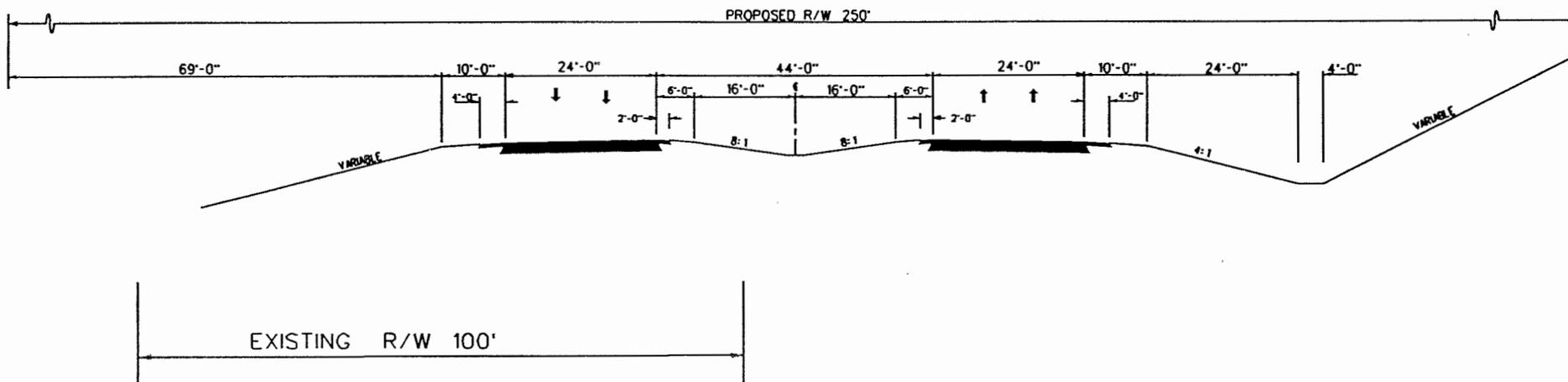
## ESTIMATE SUMMARY

		SECTION COST (per mile)
A. RIGHT-OF-WAY	\$ 0	\$ 0
B. REIMBURSABLE UTILITIES	\$ 0	\$ 0

### CONSTRUCTION COST SUMMARY

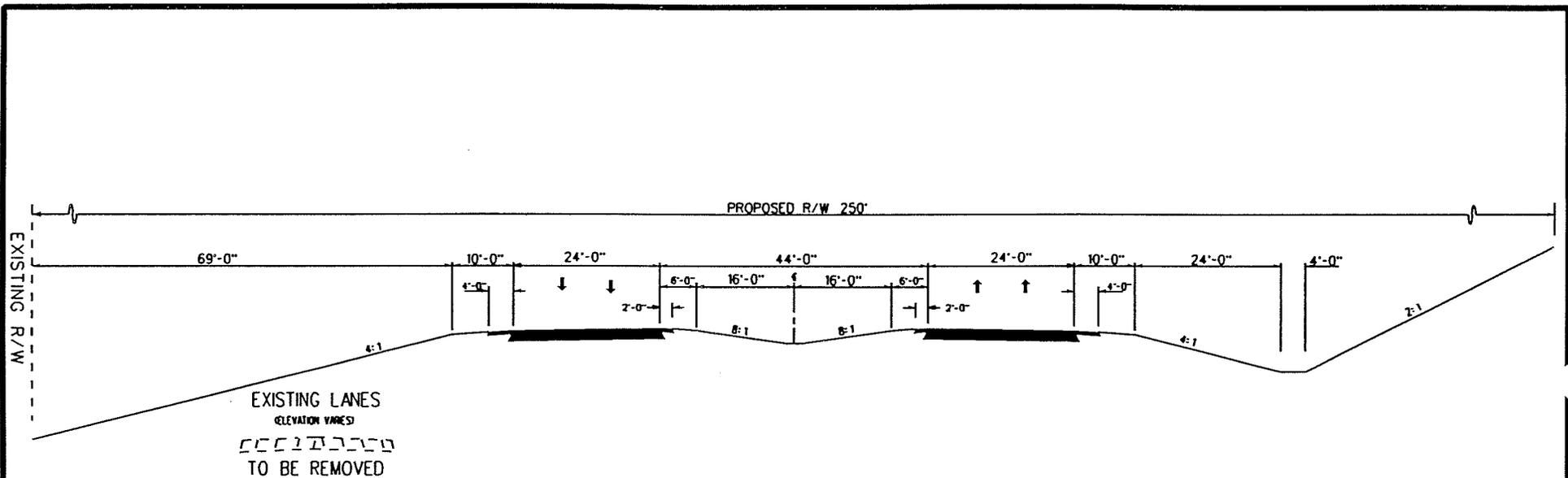
C. MAJOR STRUCTURES	\$ 0	
D. GRADING AND DRAINAGE	\$ 109,000	
E. BASE AND PAVING	\$ 138,000	
F. LUMP ITEMS	\$ 59,000	
G. MISCELLANEOUS	\$ 35,000	
H. SPECIAL FEATURES	<u>\$ 0</u>	
 SUBTOTAL CONSTRUCTION COST	 \$ 341,000	 \$ 1,137,000
E. & C. (10%)	\$ 34,000	
INFLATION 2 yr(s) @ 5% per year	<u>\$ 39,000</u>	
 TOTAL CONSTRUCTION COST	 \$ 414,000	 \$ 1,380,000
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GRAND TOTAL CONSTRUCTION COST	\$ 414,000	\$ 1,380,000

URBAN WIDENING-2 TO 4 LANES WITH 20' RAISED MEDIAN WIDEN ON ONE SIDE (48' PAV'T)



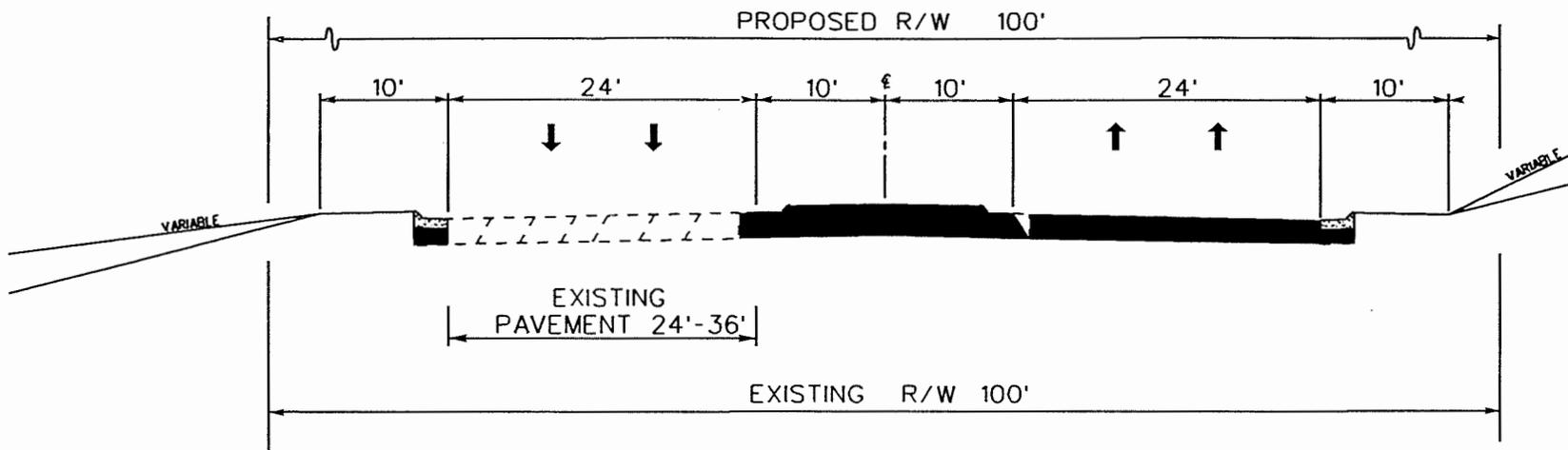
**TYPICAL CROSS SECTION**  
**SR 31 IMPROVEMENTS**  
**EDS-441(27) COFFEE CO**  
**FROM 1 MILE NORTH OF BROXTON CITY LIMITS**  
**TO 2,100 FEET NORTH OF CR 336/BRO SMITH ROAD**

NOT TO SCALE



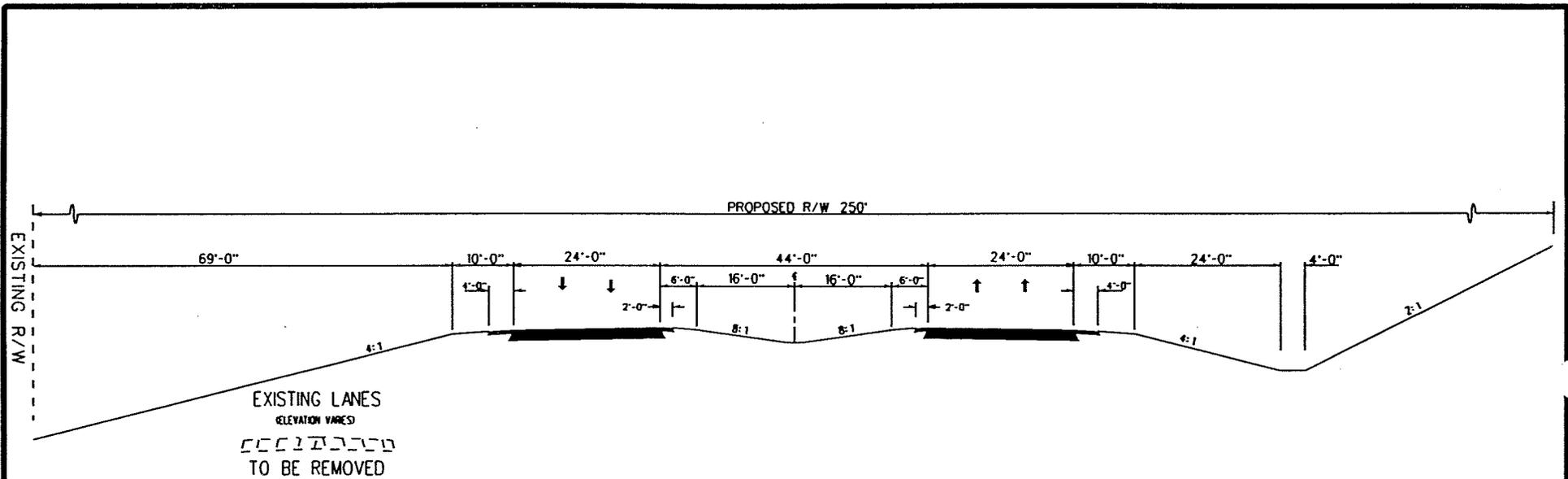
**TYPICAL CROSS SECTION**  
**SR 31 IMPROVEMENTS**  
**EDS-441(27) COFFEE CO**  
**FROM 2,100 FEET NORTH OF CR 336/BRO SMITH ROAD**  
**TO 1,100 FEET NORTH OF CR 118/LINDSEY MERRIT ROAD**

NOT TO SCALE



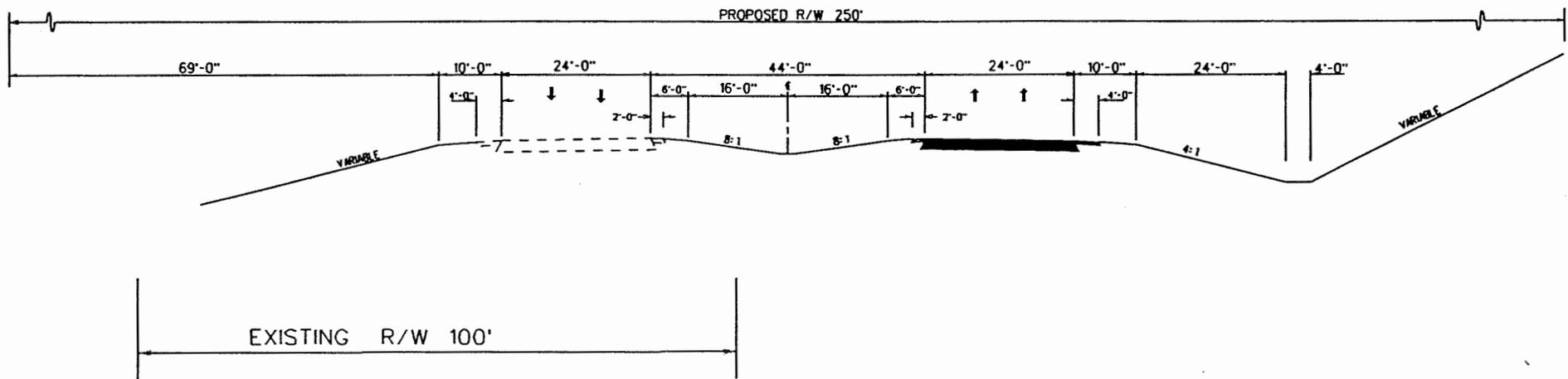
**TYPICAL CROSS SECTION**  
**SR 31 IMPROVEMENTS**  
**EDS 441(27) COFFEE COUNTY**  
**FROM 1,100 FEET NORTH OF CR 118/LINDSEY MERRIT ROAD**  
**TO 800 FEET NORTH OF CR 124/EARNEST PRIDGEN ROAD**

NOT TO SCALE



**TYPICAL CROSS SECTION**  
**SR 31 IMPROVEMENTS**  
**EDS-441(27) COFFEE CO**  
**FROM 800 FEET NORTH OF CR 124/EARNEST PRIDGEN ROAD.**  
**TO JUST SOUTH CR 498/SAPPS SILL ROAD**

NOT TO SCALE



**TYPICAL CROSS SECTION**  
**SR 31 IMPROVEMENTS**  
**EDS-441(27) COFFEE CO**  
**FROM JUST SOUTH OF CR 498/SAPPS SILL ROAD**  
**TO JUST NORTH OF MILL CREEK BRIDGE**

NOT TO SCALE

PROJECT NUMBER/COUNTY: EDS-441(27) COFFEE

P. I. NUMBER: 42/910

This project is identified as a component in the Governor's Road Improvement Program (GRIP) and funding is anticipated to be with state funds only. Therefore, this project is not identified in the State Transportation Improvement Program.

*Nesta E. Kiser*  
STATE TRANSPORTATION PLANNING ADMINISTRATOR

DATE: 12/3/99