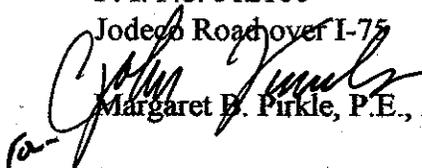


D.O.T. 66

**DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA**

INTERDEPARTMENT CORRESPONDENCE

FILE NH-IM-75-2(213) Henry County **OFFICE** Preconstruction
P. I. No. 312160
Jodeco Road over I-75 **DATE** November 30, 2004
FROM  Margaret B. Finkle, P.E., Assistant Director of Preconstruction
TO SEE DISTRIBUTION

SUBJECT PROJECT CONCEPT REPORT APPROVAL

Attached for your files is the approval for subject project.

MBP/cj

Attachment

DISTRIBUTION:

David Mulling
Harvey Keeper
Ken Thompson
Jamie Simpson
Michael Henry
Phillip Allen
Joe Palladi (file copy)
Paul Liles
Ben Buchan
Thomas Howell
BOARD MEMBER
FHWA



Memorandum

FHWA GA DIVISION

Subject: Concept comments NH-IM-75-2(213)

Date: 16 Nov 04

From: Transportation Engineer
Atlanta, Georgia

In Reply Refer To:
HTM-GA

To: Gus Shanine

David Painter of FHWA reviewed the concept report and offers the following comments on the bridge configuration. These are:

1. Adding up the lane widths under the bridge shows that the maximum span length is 121 feet rather than the 102 feet shown on the drawing.
2. We would prefer that GDOT use a full 10 foot shoulder rather than the 4 foot shoulder shown as the inside shoulder on the mainline.
3. We suggest installing a wall rather than slope paving at bents 1 and 5. This is in pursuit of the ultimate build out of this corridor which we have discussed and will continue to discuss with GDOT.

David Painter

Attachment: None

**DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA**

INTERDEPARTMENT CORRESPONDENCE

FILE NH-IM-75-2(213) Henry County **OFFICE** Preconstruction
P.I. No. 312160 **DATE** November 3, 2003

FROM  Margaret B. Pirkle, P.E., Assistant Director of Preconstruction

TO Paul V. Mullins, P.E., Chief Engineer

SUBJECT PROJECT CONCEPT REPORT

This project is the reconstruction and rehabilitation of the Jodeco Road/CR 824 interchange over I-75. The project includes improvements to Jodeco Road from 200'± west of McCullough Road to 600'± east of Meadowbrook Drive. The existing interchange is incurring operational problems due to growth in traffic caused by commercial developments, tractor-trailer trucks, and turning and through work trips. Currently, peak hour traffic conditions at the ramp intersections can cause excessive queuing to occur, extending back into the freeway. Existing and future intersection capacity analysis indicates that the ramp intersection operates at a level of service "F" without any improvements to the interchange. Jodeco Road consists of two, 12' lanes with rural unpaved shoulders on both sides. The posted speed limit is 45 MPH. Accident analysis for 1995 to 1997 data years average is four times higher than the statewide average for a facility of this type. I-75 consists of six lanes, three in each direction, and a posted speed limit of 55 MPH. Traffic estimates are as follows:

<u>ROUTE</u>	<u>2005 ADT</u>	<u>2025 ADT</u>
Jodeco Road	19,400	34,400
I-75	129,400	174,000

The proposed construction will widen Jodeco Road/CR 824 to provide three, 12' thru lanes and one 4' bike lane in each direction separated by a variable width raised median with curb and gutter and 5' sidewalk on both sides. Ten lanes will be required across the proposed bridge (3 westbound thru lane, 2 southbound left turn lanes, 2 northbound left turn lanes, and three eastbound thru lanes) with a 4' raised concrete median, 4' bike lanes, and 6' sidewalks on both sides. The ramps will be reconstructed/realigned and vary in width from 16' to 48' to accommodate turning lanes at the ramp termini. The existing bridge on Jodeco Road over I-75 will be replaced with a new 328' x 154.5' bridge with horizontal clearances that will not preclude future improvements to I-75. No changes are proposed for the existing I-75 roadway. Additional proposed improvements include relocating/realigning Feucht Road and Mount Olive Road, closing Octagon Road from Jodeco Road to Holloway Road, and closing a section of Meadowbrook Drive between Jodeco Road and Highland Drive in the southeast quadrant of the interchange. Traffic will be maintained via staged construction.

NH-IM-75-2(213) Henry
November 3, 2003

Environmental concerns include requiring a COE 404 Permit; a Categorical Exclusion is anticipated; possible UST sites impacted; a public information meeting has been held; time saving procedures are not appropriate.

The estimated costs for this project are:

	<u>PROPOSED</u>	<u>APPROVED</u>	<u>PROG DATE</u>	<u>LET DATE</u>
Construction (includes E&C and inflation)	\$19,605,000	\$19,605,000	2010	2010
Right-of-Way	\$17,053,000	\$17,053,000		
Utilities*	\$ 1,504,000	\$ 1,504,000		

*PMA sent 10-7-02 requesting Henry County do PE; DOT is to pay utilities 3-00.

The proposed project will improve traffic safety and operations in the I-75 interchange area and improve peak hour traffic flow. I recommend this project concept be approved.

MBP:JDQ/cj

Attachment

CONCUR Thomas L. Turner
Thomas L. Turner, P.E., Director of Preconstruction

APPROVE Robert M. Callan
For Robert M. Callan, Administrator, FHWA

APPROVE Paul V. Mullins
Paul V. Mullins, P.E., Chief Engineer

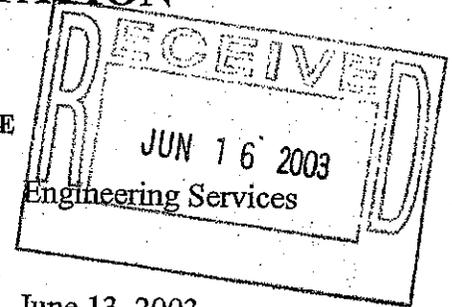
**DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA**

INTERDEPARTMENTAL CORRESPONDENCE

FILE: NH-IM-75-2(213) Henry
P.I. No.: 312160
I-75 @ Jodeco Road

OFFICE: Engineering Services

DATE: June 13, 2003



FROM: David Mulling, Project Review Engineer *REW*

TO: Meg Pirkle, Assistant Director of Pre-construction

SUBJECT: CONCEPT REPORT

We have reviewed the Concept Report submitted June 6, 2003 by the letter from James B. Buchan dated June 5, 2003, and have no additional comments:

The costs for the project are:

Construction	\$14,663,034
Inflation	\$3,159,975
E & C	\$1,782,301
Reimbursable Utilities	\$1,503,280
Right of Way	\$17,053,000

REW

c: Ben Buchan, attn: Jan Hilliard

SCORING RESULTS AS PER MOG 2440-2

Project Number: NH-IM-75-2(213)	County: Henry	PI No.: 312160
Report Date: 6/5/03	Concept By: DOT Office: Urban Design	
<input checked="" type="checkbox"/> Concept Stage	Consultant: Moreland Altobelli	
Project Type: Choose One From Each Column	<input checked="" type="checkbox"/> Major <input type="checkbox"/> Minor	<input type="checkbox"/> Urban <input checked="" type="checkbox"/> Rural
		<input type="checkbox"/> ATMS <input type="checkbox"/> Bridge Replacement <input type="checkbox"/> Building <input checked="" type="checkbox"/> Interchange Reconstruction <input type="checkbox"/> Intersection Improvement <input type="checkbox"/> Interstate <input type="checkbox"/> New Location <input type="checkbox"/> Widening & Reconstruction <input type="checkbox"/> Miscellaneous

FOCUS AREAS	SCORE	RESULTS
Presentation	100	
Judgement	100	
Environmental	100	
Right of Way	100	
Utility	100	
Constructability	100	
Schedule	100	

368

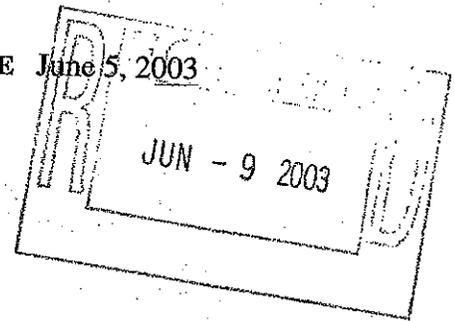
DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA

INTERDEPARTMENTAL CORRESPONDENCE

FILE NH-IM-75-2(213), Henry County
I-75 @ Jodeco Rd/CR 824 Bridge Replacement
for Safety Improvements
P.I. No. 312160

OFFICE Urban Design

DATE June 5, 2003



FROM *James B. Buchan*³⁸²
James B. Buchan, P.E., State Urban Design Engineer

TO Meg Pirkle, P.E., Assistant Director of Preconstruction

SUBJECT Project Concept Report Submittal for Approval

Transmitted herewith for your review and approval is the Project Concept Report for the proposed bridge replacement of the I-75 Interchange Jodeco Rd/CR 824 located in Henry County.

Please take the necessary steps to process this document through the Department's Project Development Process.

If you have any questions concerning this report, please contact Jan Hilliard or Nicoe Alexander at 404-656-5441.

JBB:JCH
Attachments

Distribution w/Attachments:

David Mulling, P.E.
Harvey Keepler
Carla Holmes, P.E.
Joseph P. Palladi, P.E.
Percy Middlebrooks, P.E.
Thomas B. Howell, Jr., P.E.
Paul Liles, P.E.

DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA

Office of Urban Design

PROJECT CONCEPT REPORT

Project Number: NH-IM-75-2 (213)

County: Henry

P. I. Number: 312160

Federal Route Number: I-75

State Route Number: 401

See Project location sketch on page 2.
I-75 @ Jodeco Road/C.R. 824 Interchange Improvement

Recommendation for approval:

Date of Report: May 30, 2003

DATE 6/5/03

DATE 6/5/03

Jan C. Hilliard

Project Manager

James B. Buchan

State Urban Design Engineer

The concept as presented herein and submitted for approval is consistent with that which is included in the Regional Transportation Program (RTP) and/or the State Transportation Improvement Program (STIP).

DATE _____

State Transportation Planning Administrator

DATE _____

State Financial Management Administrator

DATE _____

State Environmental/Location Engineer

DATE _____

State Traffic Safety and Design Engineer

DATE _____

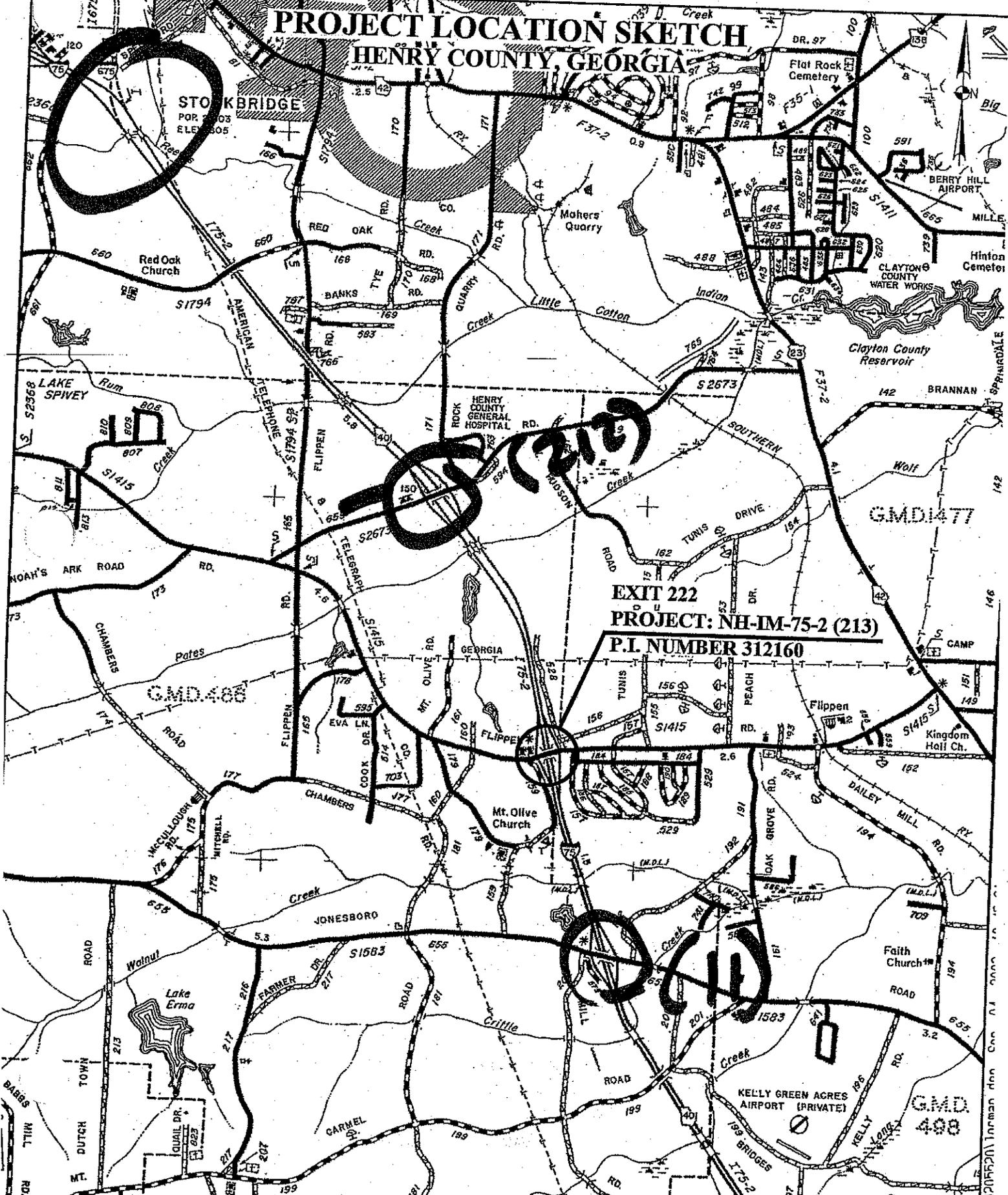
District Engineer

DATE _____

Project Review Engineer

DATE _____

PROJECT LOCATION SKETCH HENRY COUNTY, GEORGIA



EXIT 222
PROJECT: NH-IM-75-2 (213)
P.I. NUMBER 312160

STONE MOUNTAIN
POP. 1003
ELEV. 805

Red Oak Church

LAKE SPIVEY

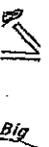
HENRY COUNTY
GENERAL HOSPITAL

GMD 488

GMD 477

S1415

GMD 408



4205520 Inrman Ann One One

Concept Report Page 3
Project Number: NH-IM-75-2 (213)
P.I. Number: 312160
County: Henry

Need and Purpose: The I-75 @ Jodeco Road/C.R. 824 Interchange in Henry County serves as a commuter route for residences to access I-75 from the suburban areas south of the City of Stockbridge. The existing ramps and the 2-lane bridge overpass do not provide sufficient left-turn and through capacity for the high peak hour turning movements experienced at this interchange. Currently, peak hour traffic conditions at the ramp intersections can cause excessive queuing to occur, extending back onto the freeway. Existing and future intersection capacity analysis indicates that the ramp intersection operates at level of service "F" without any improvements to the interchange. The Average Daily Traffic (ADT) of Jodeco Road is 19,400 vpd for the base year 2005 and 34,400 vpd for the design year 2025. These volumes exceed the capacity of a two-lane roadway, bridge, and related intersections.

In addition to the extreme traffic congestion, Jodeco Road has a high rate of traffic accidents. For the year 1995 and 1997 the accident rates are 4 times the statewide average accident rate for rural major collectors. The injury rates for these two years are 5 times the statewide average injury rate. A detailed accident summary is provided in the Appendix.

The proposed project is listed in the March 5, 2002 Atlanta Regional Commission's Transportation Improvement Program (TIP) and 2025 Regional Transportation Plan (RTP) and the Henry County Transportation Plan to upgrade the existing transportation infrastructure of Henry County.

The proposed project will provide for anticipated traffic growth along Jodeco Road as a result of future development. The recommended improvements will improve traffic flow, safety and accommodate all traffic operations within the interchange area.

Description of the proposed project: Project NH-IM-75-2 (213) proposes to improve the interchange of I-75 (S.R. 401) at Jodeco Road (C.R. 824), Exit-222 in Henry County. The project proposes to [REDACTED] roadway with a variable width raised median with left and right turn lanes at strategic locations. The Jodeco Road Bridge over I-75 will be replaced to accommodate the widening of the roadway. Four-foot bike lanes and 5-foot sidewalks on both sides are included along Jodeco Road with the bridge over I-75 having 6-foot sidewalks. The bike lanes and sidewalks are in accordance with the Henry County designation of Jodeco Road as a bike and pedestrian route corridor. The entrance and exit ramps would also be lengthen and widened for additional storage capacity. The project proposes to make improvements to Jodeco Road from 200 feet west of McCullough Road to 600 feet east of Meadowbrook Drive. Additional proposed improvements include the widening of the I-75 entrance and exit ramps and shoulders, relocating/realigning Feucht Road and Mt. Olive Road, closing Octagon Road from Jodeco Road to Holloway Road, and closing a section of Meadowbrook Drive between Jodeco Road and Highland Drive near the Hardees Restaurant in the southeast quadrant of the interchange. Since Octagon Road would be closed at Jodeco Road, and the existing park and ride lot could no longer be accessed from Jodeco Road, this project proposes to construct a new access to the park and ride lot on Holloway Road. Holloway Road from Patrick Henry Parkway to the park and ride lot would be paved as a part of this project. The total [REDACTED] approximately 1.0 mile along Jodeco Road.

Concept Report Page 4
 Project Number: NH-IM-75-2 (213)
 P.I. Number: 312160
 County: Henry

Is the project located in a Non-attainment area? Yes. The proposed project is listed in the March 28, 2000 RTP/TIP Conformity Determination Report, page 110 of Appendix I as HE-AR 216. Refer to the Need and Purpose for full project justification.

PDP Classification: Major project on existing location.

Federal Oversight: Full Oversight (X), Exempt(), State Funded(), or Other ()

Functional Classification: I-75/S.R. 401: Rural Interstate
 Jodeco Road (C.R. 824): Rural Major Collector

U. S. Route Number(s): N/A

State Route Number(s):
 I-75 ----- S.R. 401
 Jodeco Road ----- ST 1415

Traffic (AADT):

Roadway	Base Year 2005	Design Year 2025
Jodeco Road	19,400	34,400
I-75 Mainline	129,400	174,300

Existing Design Features:

Typical Section: Jodeco Road (CR 824) - Two 12-foot lanes, with rural unpaved shoulders on both sides.

I-75 On/Off-Ramps - One 16-foot lane with 1-foot paved inside shoulders, 10-foot outside shoulders and 4-foot grassed shoulders on both sides.

I-75 Mainline - Three 12-foot interstate lanes in each direction, separated by a 40-foot depressed grass median, which includes 10-foot paved inside shoulders. There are 10-foot paved shoulders and 2-foot grass shoulders on the outside of the interstate lanes.

	<u>Posted Speed</u>	<u>Max degree of curve</u>	<u>Max grade</u>
• Jodeco Road	45 mph	2°	4%
• I-75 Mainline	55 mph	tangent	2%
• I-75 Off-Ramps	35 mph	4°	5%

Width of right of way: Jodeco Road 80 ft.
 I-75 Mainline approx. 320 ft.

Concept Report Page 5
 Project Number: NH-IM-75-2 (213)
 P.I. Number: 312160
 County: Henry

Major Structures: Jodeco Road Bridge over I-75

<u>Structure I.D. No.</u>	151-0028-0
<u>Sufficiency Rating</u>	78.0
<u>Bridge Type</u>	Steel Girder
<u>Condition</u>	Good
<u>No. of spans</u>	4
<u>Length</u>	264'
<u>Maximum Span</u>	71'
<u>Deck Structure Width</u>	34.7'
<u>Roadway Width</u>	30.5'
<u>Minimum Vertical Clearance</u>	16'-4"
<u>Total Horizontal Clearance</u>	30.5'

Project Length: Jodeco Road 1.0 mile, from M.P. 4.2 - 5.2

Major Interchanges Adjacent to Project: Existing interchanges are located 1.0 mile south at Exit 221 (Jonesboro Road) and 2.0 miles north at Exit 224 (Eagles Landing Parkway/Hudson Bridge Road). The existing functionality of these interchanges will not be altered by this project.

Proposed Design Features:

Typical Section:

Jodeco Road (CR 824) - Six 12-foot urban lanes separated by a variable width raised median, with 4-foot bike lanes, 30" curb and gutter and 5-foot sidewalks on both sides. There are 16-foot shoulders on each side measured from the edge of pavement.

I-75 On/Off-Ramps - One 16-foot [redacted] with additional turn lanes at its intersection with Jodeco Road and 6-foot paved, 2-foot grass inside shoulders and 10-foot paved, 2-foot grass outside shoulders. There are 12-foot shoulders on each side measured from the edge of pavement.

I-75 Mainline - Typical section is unchanged under this project. However, bridge clearances will be set to allow for an additional [redacted] single occupancy lane and an HOV component as determined by the ongoing GDOT Planning Study. (See typical section)

CD?

Note: The maximum queue lengths were determined for all of the roadways proposed for improvement from the TRAF-CORSIM analysis contained in the Traffic Analysis Section attached. The maximum queue length was used in determining the required storage length of turn lanes on Jodeco Road, the I-75 ramps and all of the side streets within the project limits.

• Jodeco Road

Proposed Design Speed	45 mph		
Proposed Maximum grade	4%	Maximum grade allowable	8%
Proposed Maximum Degree of Curve	2°	Maximum allowable Degree of Curve	8°

- I-75 Ramps
 Proposed Design Speed 35 mph Ramp Proper; 55 mph at Freeway Diverge
 Proposed Maximum grade 5% Maximum grade allowable 6%
 Proposed Maximum Degree of Curve 4° Maximum allowable Degree of Curve 6°

- Feucht Drive
 Proposed Design Speed 25 mph
 Proposed Maximum grade 2% Maximum grade allowable 12%
 Proposed Maximum Degree of Curve 11.5° Maximum allowable Degree of Curve 25°

- Frontage Road
 Proposed Design Speed 35 mph
 Proposed Maximum grade 2% Maximum grade allowable 8%
 Proposed Maximum Degree of Curve Tangent Max. allowable Degree of Curve 16°

- Mt. Olive Road
 Proposed Design Speed 25 mph
 Proposed Maximum grade 2% Maximum grade allowable 12%
 Proposed Maximum Degree of Curve 19° Maximum allowable Degree of Curve 25°

- Patrick Henry Parkway
 Proposed Design Speed 35 mph
 Proposed Maximum grade 2% Maximum grade allowable 8%
 Proposed Maximum Degree of Curve 14° Maximum allowable Degree of Curve 16°

- Meadowbrook Drive
 Proposed Design Speed 25 mph
 Proposed Maximum grade 2% Maximum grade allowable 12%
 Proposed Maximum Degree of Curve 20° Maximum allowable Degree of Curve 25°

Right of Way:

- Width: Jodeco Road Variable 80 – 150 feet
 Feucht Drive Variable 60 – 80 feet
 Frontage Road 80 feet
 Mt. Olive Road 60 feet
 Patrick Henry Parkway 130 feet
 Meadowbrook Drive Variable 60 – 80 feet

- Easements: Temporary (x), Permanent (x), Utility (), Other ()
- Type of access control: Jodeco Road: Full (), Partial (), By Permit (X), Other ()
- Number of parcels impacted: 25 parcels
- Number of displacements: Three commercial properties.

Concept Report Page 7
 Project Number: NH-IM-75-2 (213)
 P.I. Number: 312160
 County: Henry

Proposed Major Structures: A new bridge over I-75 is proposed to accommodate the required Jodeco Road widening. It will replace the existing structure and consist of ten 12-foot lanes (three eastbound through lanes and two eastbound left-turn lanes; three westbound through lanes and two westbound left-turn lanes), with a 4-foot raised concrete median, 4-foot bike lanes and 6-foot sidewalks on both sides.

<u>Bridge Type</u>	Concrete Girder (Bulb Tee Beams)
<u>No. of spans</u>	4
<u>Length</u>	328'
<u>Maximum Span</u>	102'
<u>Deck Structure Width</u>	154'-5"
<u>Roadway Width</u>	140'
<u>Minimum Vertical Clearance</u>	17.0'
<u>Total Horizontal Clearance</u>	41'

* See attached typical section.

Traffic control during construction: Traffic will be maintained during construction and coordinated with design staging decisions.

Design Exceptions to controlling criteria anticipated:

	<u>UNDETERMINED</u>	<u>YES</u>	<u>NO</u>
HORIZONTAL ALIGNMENT:	<input type="radio"/>	<input type="radio"/>	<input checked="" type="radio"/>
ROADWAY WIDTH:	<input type="radio"/>	<input type="radio"/>	<input checked="" type="radio"/>
SHOULDER WIDTH:	<input type="radio"/>	<input type="radio"/>	<input checked="" type="radio"/>
VERTICAL GRADES:	<input type="radio"/>	<input type="radio"/>	<input checked="" type="radio"/>
CROSS SLOPES:	<input type="radio"/>	<input type="radio"/>	<input checked="" type="radio"/>
STOPPING SIGHT DISTANCE:	<input type="radio"/>	<input type="radio"/>	<input checked="" type="radio"/>
SUPERELEVATION RATES:	<input type="radio"/>	<input type="radio"/>	<input checked="" type="radio"/>
HORIZONTAL CLEARANCE:	<input type="radio"/>	<input type="radio"/>	<input checked="" type="radio"/>
SPEED DESIGN:	<input type="radio"/>	<input type="radio"/>	<input checked="" type="radio"/>
VERTICAL CLEARANCE:	<input type="radio"/>	<input type="radio"/>	<input checked="" type="radio"/>
BRIDGE WIDTH:	<input type="radio"/>	<input type="radio"/>	<input checked="" type="radio"/>
BRIDGE STRUCTURAL CAPACITY:	<input type="radio"/>	<input type="radio"/>	<input checked="" type="radio"/>

Environmental concerns: There is a wetland area near the road relocation of Feucht Drive, which may require a 404 permit. There are six UST's sites within the project area. There are no hazardous waste sites other than the UST's.

Level of environmental analysis: Categorical Exclusion

Are Time Savings Procedures appropriate? Yes (), No (X),

Concept Report Page 8
Project Number: NH-IM-75-2 (213)
P.I. Number: 312160
County: Henry

Project responsibilities:

- Design – *Henry County, GA*
- Right-of-Way Acquisition – *Georgia DOT*
- Relocation of Utilities – *Georgia DOT*
- Letting to contract – *Georgia DOT*
- Providing material pits – *Construction Contractor*
- Providing detours – *Construction Contractor*

Coordination: An initial concept team meeting was held December 13, 2001 and a final concept team meeting was held on October 17, 2002 at the Georgia DOT, Office of Urban Design. Minutes of these meetings are attached. A public information meeting was held on January 31, 2002. A synopsis of this meeting is attached. Georgia DOT will be responsible for funding reimbursable utility costs. Possible affected utilities include telephone, cable, power, gas, and water.

Other projects in the area:

1. Project STP-0000-00(561), P.I. No. 0000561 – Campground Road Extension from Jodeco Rd to SR 42 at Campground Road.
2. Project STP-0000-00(562), P.I. No. 0000562 – Hampton-Locust Grove Road from Strickland Road to I-75: Phase 1.
3. Project NH-IM-75-2(194), P.I. No. 311840 – Interchange reconstruction of I-75 at Jonesboro Road/S.R. 920.
Project STP-1583 (11), P.I. No. 331980 – SR 920/Jonesboro Road from I-75 to 1-way Pair / McDonough Road & Bridge.
Project NM-IM-75-2(212), P.I. No. 312150 – I-75 at Hudson Bridge Road / Eagles Landing Parkway.
Project STP-00TR(1), P.I. No. 363860 – Jodeco Road, expand Park & Ride lot and relocate Octagon Road.
7. Project MSL-0004-00 (431), P.I. No. 0004431 – Patrick Henry Parkway from end of existing road to Eagles Landing Parkway.

Scheduling – Responsible Parties' Estimate

- Time to complete the environmental process: 6 months.
- Time to complete preliminary construction plans: 13 months.
- Time to complete right-of-way plans: 3 months
- Time to complete final construction plans: 12 months.
- Time to complete the purchase of right-of-way: 12 months.

Other alternates considered:

1. No Build: No action would be taken to improve the existing interchange.
2. Alternate 1: The proposed project would be to widen and improve the I-75/Jodeco Road Interchange by widening the roadway to four lanes divided and adding turn-lanes. This alternate was eliminated from consideration because the traffic analysis indicated that it would not adequately handle the 2025 design year traffic volumes throughout the project length. A summary of the traffic analysis is contained within the Traffic Analysis Section attached to the concept report. The four-lane alternate would likely begin to have failing levels of service between 10 and 15 years after construction.
3. Side Street Alternates: Several side street relocation alternates were considered. The relocation of Mt. Olive Road originally tied into Jodeco Road directly. The relocation was modified to form a T-intersection with a proposed Frontage Road which is located approximately 1000 feet from the I-75 Southbound Ramps and planned to be built by others in the future.

The relocation of Feucht Drive was originally proposed to bend and form a T-intersection with the (old) Feucht Drive. It is now proposed that the relocated Feucht Drive be constructed into the (old) Feucht Drive with the (old) Feucht Drive tying into the relocated road, forming a T-intersection.

Meadowbrook Drive that is located approximately 300 feet east of the I-75 Northbound Ramp, was considered to be modified to a right-in, right-out intersection access to Jodeco Road. Under this alternate, motorists who wish to access the Interstate would have to turn right onto Jodeco Road, execute three lane changes and then U-turn at the intersection of Patrick Henry Parkway. This proposed side-street alternate was eliminated from consideration due to the potential for accidents and disruption of traffic flow on Jodeco Road. The proposed alternate closes the existing connection to Jodeco Road for Meadowbrook Drive traffic and provides a signalized improved intersection for motorists wishing to enter and exit onto Jodeco Road.

4. Recommended Alternate: The recommended alternate would be to widen and improve the I-75/Jodeco Road Interchange by widening the roadway to six lanes divided and adding turn lanes and configuring side streets as discussed above. This alternate would satisfy the project need and purpose by improving traffic flow and traffic safety and thus, reduce the traffic congestion at the existing diamond interchange.

Concept Report Page 10
Project Number: NH-IM-75-2 (213)
P.I. Number: 312160
County: Henry

Comments:

- The proposed project will be built as to not preclude the construction of HOV lanes on I-75 or additional single occupancy lanes.
- Henry County will determine the final alignment of the intersection of Mount Olive Road and a proposed frontage road southwest of the interchange during the environmental and preliminary design phases.

Attachments:

- Project Cost Estimate
- Typical sections
- Accident summaries
- Traffic Analysis & Traffic Flow Diagrams
- Bridge inventory
- Minutes of Initial Concept and Concept meetings
- Public Information Meeting Synopsis
- Conforming plan's network schematics showing thru lanes
- Concept Layout

Project Number: NH-IM-75-2 (213)

P.I. Number 312160

Henry County

**Detailed Construction Cost Estimate
I-75/Jodeco Road Interchange Improvements**

Major Structures

1. Bridge over I-75	38,676 SF @	\$75.00	\$2,900,700
		Subtotal	\$2,900,700

Grading and Earthwork

1. Unclassified Excavation	271,445 CY @	\$10.00	\$2,714,450
		Subtotal	\$2,714,450

Drainage

1. Longitudinal RCO (30" Average)	14,400 LF @	\$50.00	\$720,000
2. Catch Basin	96 ea @	\$2,000.00	\$192,000
3. Miscellaneous Drainage		Lump Sum	\$20,000
		Subtotal	\$932,000

Base & Paving

1. Graded Aggregate Base 12"	18,200 TN @	\$18.50	\$336,700
2. Graded Aggregate Base 10"	38,677 TN @	\$18.50	\$715,525
3. Asphalt Concrete 12.5 mm 165#/SY (1-1/2")	6,878 TN @	\$40.00	\$275,120
4. Asphalt Concrete 19.0 mm Superpave 220#/SY (2")	8,076 TN @	\$38.00	\$306,888
5. Asphalt Concrete 25 mm Superpave 330#/SY (3")	4,381 TN @	\$38.00	\$166,478
6. Asphalt Concrete 25 mm Superpave 440#/SY (4")	16,147 TN @	\$38.00	\$613,586
7. Bitum Tack Coat	9,320 GL @	\$1.50	\$13,980
8. Asphalt Concrete Leveling	2,185 TN @	\$36.00	\$78,660
		Subtotal	\$2,506,937

Concrete Work

1. Concrete P'vmt, Portland Cement	24,192 SY @	\$46.00	\$1,112,832
2. Concrete Median Paving	1,669 SY @	\$25.00	\$41,725
3. Concrete Parking Lot Paving	8,822 SY @	\$25.00	\$220,550
4. Driveway	1,358 SY @	\$30.00	\$40,740
5. Curb & Gutter	18,000 LF @	\$12.00	\$216,000
6. Sidewalk - 4"	10,550 SY @	\$25.00	\$263,750
		Subtotal	\$1,895,597

Signing, Striping, and Signals

1. Signs		Lump Sum	\$85,000
2. Striping	14,600 LF @	\$2.65	\$38,690
3. Signals	4 ea @	\$120,000.00	\$480,000
		Subtotal	\$603,690

Guardrail

1. Guardrail, Type T	900 LF @	\$38.40	\$34,560
2. Remove Existing	900 LF @	\$2.00	\$1,800
3. Bridge End Ties	4 ea @	\$525.00	\$2,100
4. Anchors TP 12	4 ea @	\$1,650.00	\$6,600
5. Anchors TP 1	4 ea @	\$500.00	\$2,000
		Subtotal	\$47,060

Traffic Control & Mobilization

1. Traffic Control		Lump Sum	\$500,000
2. Mobilization		Lump Sum	\$500,000
		Subtotal	\$1,000,000

Landscaping and Erosion Control

1. Clearing & Grubbing	20 ac @	\$10,000.00	\$200,000
2. Grassing	10 ac @	\$2,500.00	\$25,000
3. Erosion Control		Lump Sum	\$1,500,000
		Subtotal	\$1,725,000

Miscellaneous Items

1. Field Office TP 3	1 ea @	\$30,000.00	\$30,000
2. Remove Existing Pavement	6,900 SY @	\$4.00	\$27,600
3. Remove Existing Bridge	11,200 SY @	\$25.00	\$280,000
		Subtotal	\$337,600

Project Number: NH-IM-75-2 (213)

P. I. Number: 312160

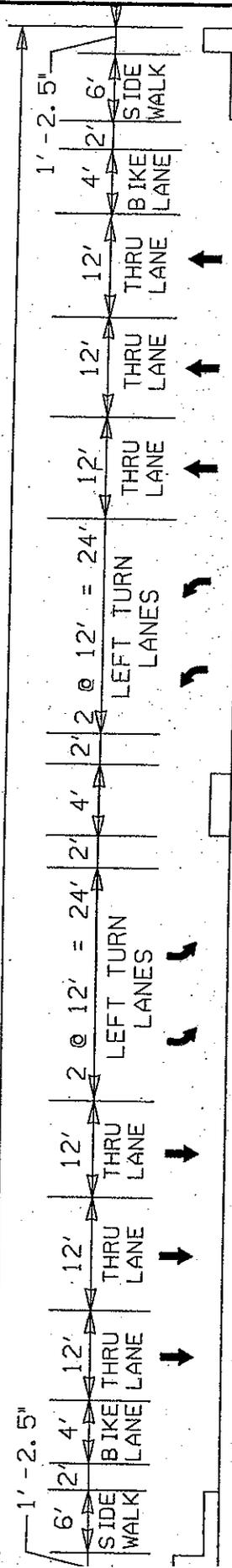
Henry County

COST ESTIMATE SUMMARY

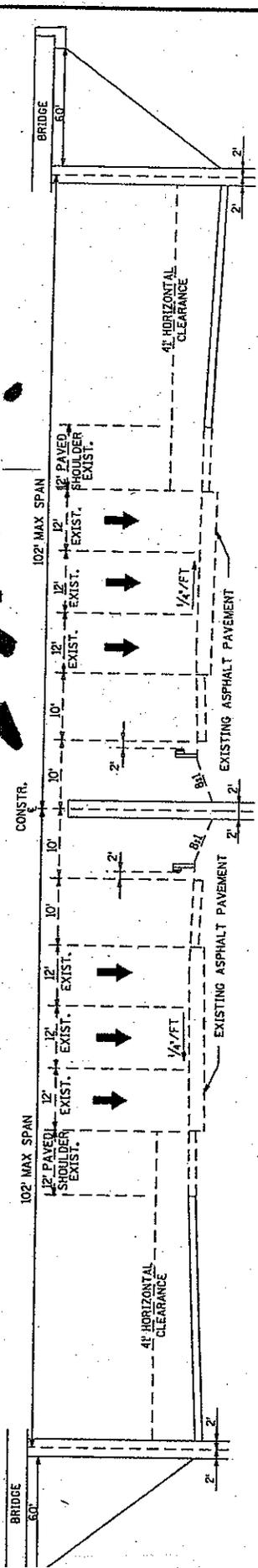
A.	Right-of-Way	\$17,053,000
B.	Utilities	\$1,503,280
	—Major Structures	\$2,900,700
	Grading and Earthwork	\$2,714,450
	Drainage	\$932,000
	Base and Paving	\$2,506,937
	Concrete Work	\$1,895,597
	Sign, Striping & Signals	\$603,690
	Guardrail	\$47,060
	Traffic Control & Mobilization	\$1,000,000
	Landscaping and Erosion Control	\$1,725,000
	Miscellaneous Items	<u>\$337,600</u>
C.	Construction Cost Subtotal	\$14,663,034
	—Four years of inflation @ 5%	\$3,159,975
	Engineering & Construction; 10%	<u>\$1,782,301</u>
	Total Construction Cost	\$19,605,310
	Total Project Cost	\$38,161,590

JODECO ROAD BRIDGE OVER I-75

154' - 5"



2 SPANS!

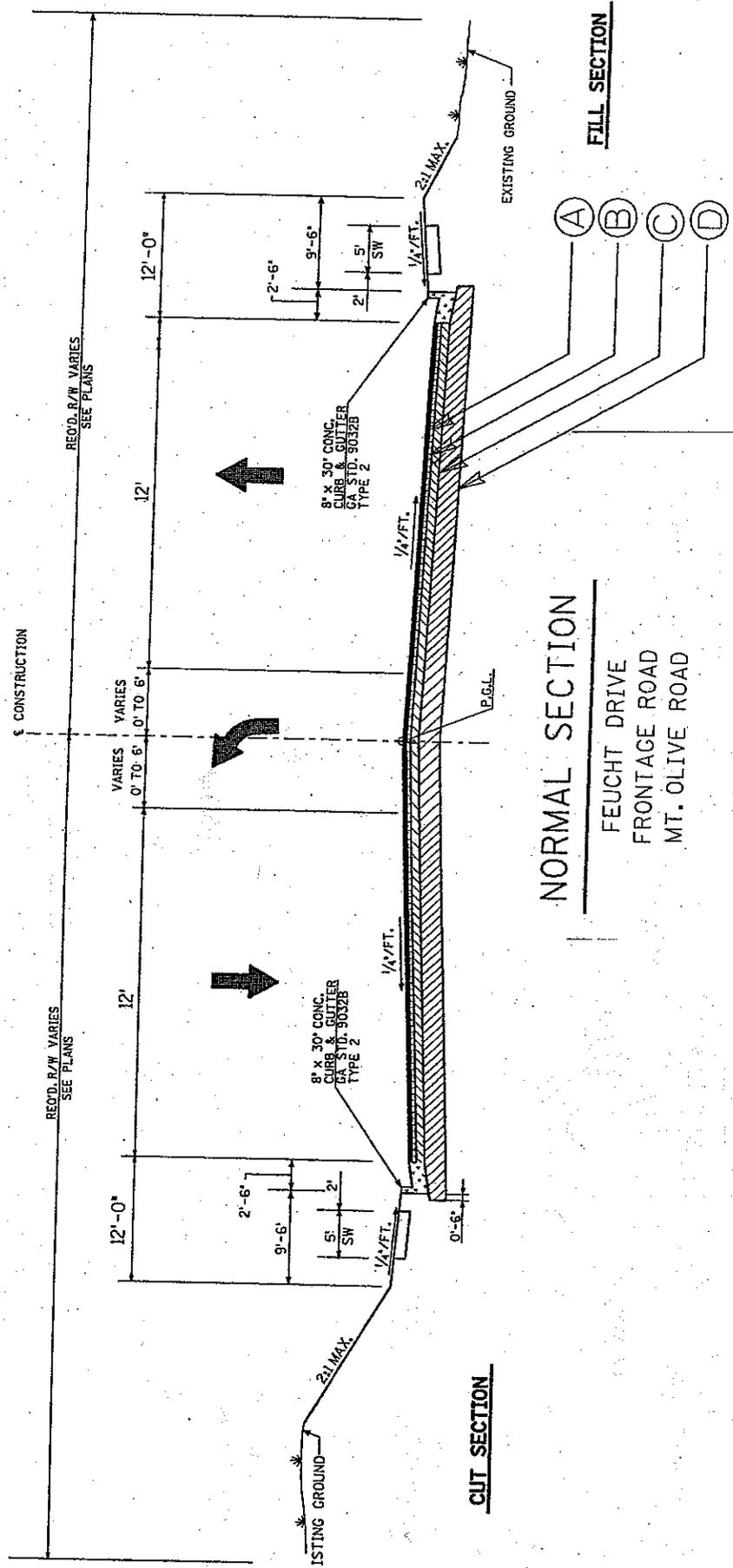


DATE	BY	CHKD	DATE

MA Michael A. Mabe 4211 Highway 88 in 2006 Marietta, Georgia 30067 (770) 429-2500		Department of Transportation State of Georgia	
I-75/JODECO ROAD INTERCHANGE TYPICAL SECTION JODECO ROAD BRIDGE OVER I-75		SHEET NO.	

205520\concept\typical.dgn Mar. 07, 2003 17:34:41

STATE	PROJECT NUMBER	DATE	SCALE



PROPOSED PAVEMENT FOR ESTIMATION PURPOSES ONLY

- Ⓐ ASPHALTIC CONC. 12.5mm SUPERPAVE (165 lbs/SY)
- Ⓑ ASPHALTIC CONC. 19mm SUPERPAVE (220 lbs/SY)
- Ⓒ ASPHALTIC CONC. 25mm SUPERPAVE (440 lbs/SY)
- Ⓓ GRADED AGGREGATE BASE (10")

NO.	REVISION	DATE	BY	REASON

PROJECT NO. _____ DATE _____

DESIGNED BY _____ CHECKED BY _____

DRAWN BY _____

APPROVED BY _____

DATE _____

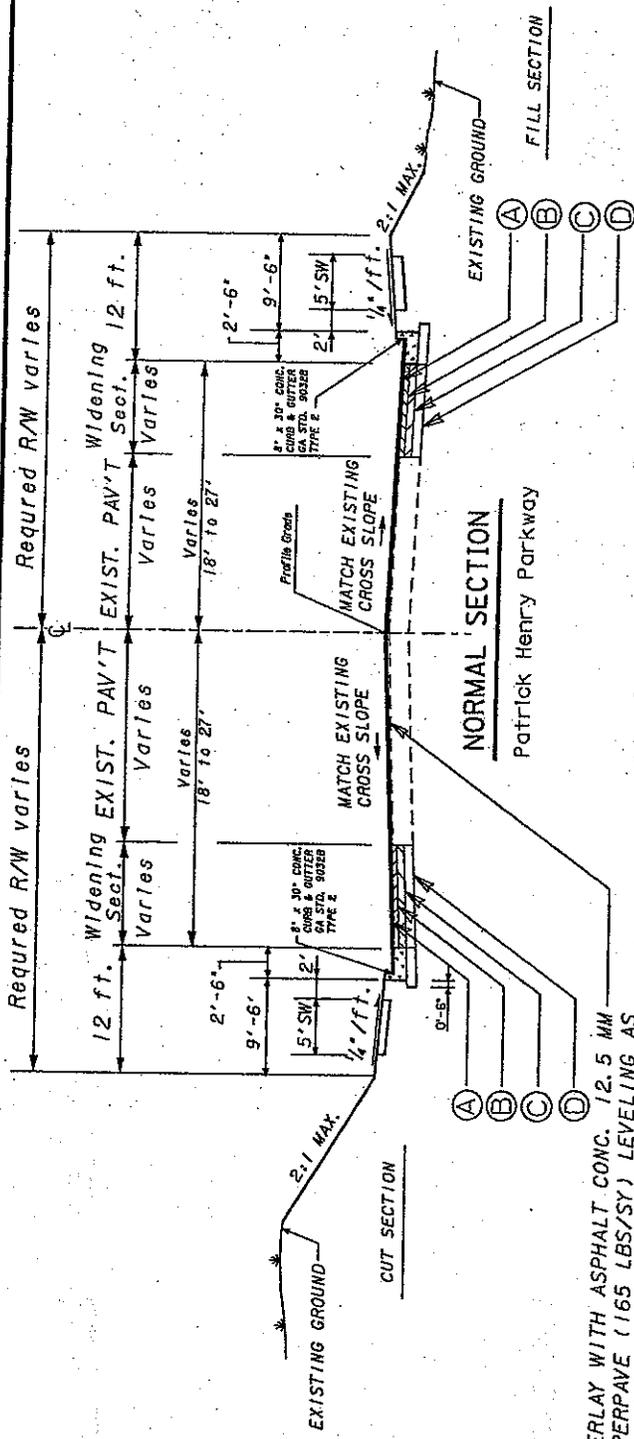
SCALE _____

PROJECT NAME: I-75/JODECO ROAD INTERCHANGE
TYPICAL SECTION
FEUCHT DRIVE, FRONTAGE ROAD AND MT. OLIVE ROAD

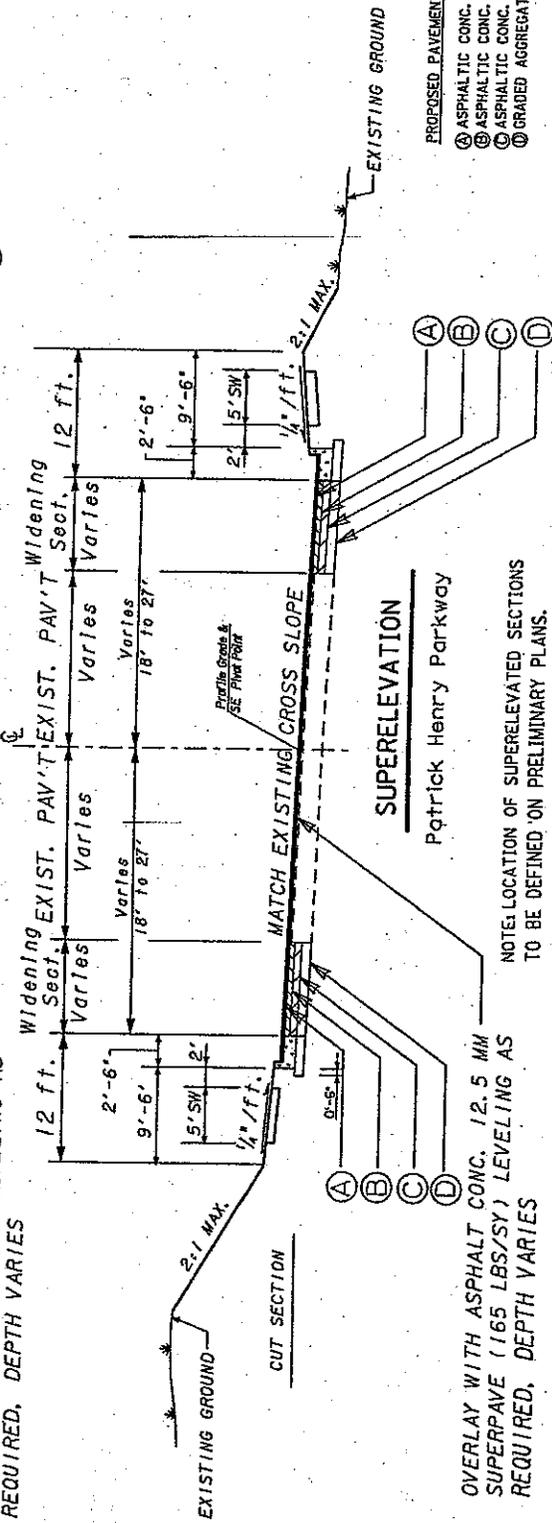
MA Moreland Alkhabli Associates, Inc.

concept\typsecmtolive.dgn Mar. 07, 2003 10:06:55

PROJECT NUMBER	DATE
PROJECT GENERAL	DATE
SHEET NUMBER	TOTAL SHEETS



OVERLAY WITH ASPHALT CONC. 12.5 MM SUPERPAVE (165 LBS/SY) LEVELING AS REQUIRED, DEPTH VARIES



OVERLAY WITH ASPHALT CONC. 12.5 MM SUPERPAVE (165 LBS/SY) LEVELING AS REQUIRED, DEPTH VARIES

- PROPOSED PAVEMENT FOR ESTIMATION PURPOSES ONLY
- Ⓐ ASPHALTIC CONC. 12.5mm SUPERPAVE (165 lbs/sy)
 - Ⓑ ASPHALTIC CONC. 19mm SUPERPAVE (220 lbs/sy)
 - Ⓒ ASPHALTIC CONC. 25mm SUPERPAVE (440 lbs/sy)
 - Ⓓ GRADED AGGREGATE BASE (00)

NOTE: LOCATION OF SUPERELEVATED SECTIONS TO BE DEFINED ON PRELIMINARY PLANS.

NOT TO SCALE

		I-75/JOECO ROAD INTERCHANGE TYPICAL SECTIONS PATRICK HENRY PARKWAY
REVISIONS NO. DESCRIPTION DATE BY	DATE TIME DRAWN BY CHECKED BY APPROVED BY	SHEET NO. TOTAL SHEETS

SUMMARY OF ACCIDENTS

Accident History of I-75/S.R. 401 at the interchange with C.R. 824 (Jodeco Road)

Year	AADT	No. Acc.	No. Inj.	No. Fat.	Calculated Acc. Rate	Statewide Acc. Rate	Calculated Inj. Rate	Statewide Inj. Rate	Calculated Fat. Rate	Statewide Fat. Rate
I-75/S.R. 401; from MP 14.05 to MP 14.61 (0.56 miles)										
1995	102,854	28	8	0	133	47	38	28	0	1.39
1996	102,854	33	13	0	157	50	62	29	0	1.56
1997	102,854	35	9	0	166	49	43	28	0	1.41
C.R. 824; from MP 4.50 to MP 4.65 (0.15 miles)										
1995	8,420	4	3	0	868	193	651	122	0	1.39
1996	8,973	1	0	0	204	201	0	122	0	1.56
1997	11,647	6	4	0	941	194	627	112	0	1.41

The information shown above was furnished by the GDOT Office of Traffic Operations and the Georgia Department of Public Safety, Accident Reporting Unit for the mile points indicated. Accident rates were calculated in units of number of accidents per 100 million vehicle-miles. Calculated accident and injury rates for accident data on the above segments of I-75/S.R. 401 and C.R. 824 were compared to the statewide averages for the corresponding roadway classes. These comparisons indicate that the accident rates for 1995 and 1996 on these segments were lower than the statewide averages and the injury rates were higher than the statewide averages.

The accidents have been further classified according to their type and location, and are presented in the following tables.

I-75/Jodeco Road Interchange Accident Inventory (includes accidents from both road segments)						
Road of Occurrence	Location / Intersecting Street	Total Accidents	Total Injuries	Accident Types		
				Sideswipe/Rear End	Angle	Other
1995 Accident Data						
I-75/S.R. 401	Mainline (not on ramp)	10	4	3	1	6
I-75/S.R. 401	Mainline (Ramp Entrance)	3	0	1	0	2
I-75/S.R. 401	Cross-street Intersection	15	4	8	6	1
C.R. 824	Mainline/Bridge Overpass	4	3	2	1	1
Totals		32	11	14	8	10
1996 Accident Data						
I-75/S.R. 401	Mainline (not on ramp)	14	4	5	0	9
I-75/S.R. 401	Mainline (Ramp Entrance)	4	2	2	0	2
I-75/S.R. 401	Middle of Ramp	2	1	1	0	1
I-75/S.R. 401	Cross-street Intersection	12	6	10	1	1
C.R. 824	Mainline/Bridge Overpass	1	0	1	0	0
Totals		33	13	19	1	13
1997 Accident Data						
I-75/S.R. 401	Mainline (not on ramp)	23	8	9	3	11
I-75/S.R. 401	Mainline (Ramp Entrance)	3	0	3	0	0
I-75/S.R. 401	Middle of Ramp	3	0	3	0	0
I-75/S.R. 401	Cross-street Intersection	6	1	4	2	0
C.R. 824	Mainline/Bridge Overpass	1	0	1	0	0
Totals		36	9	20	5	11

* Other = Struck Object, Overturned, Other Non-collision

TRAFFIC ANALYSIS & TRAFFIC FLOW DIAGRAMS

Future 2025 design year peak hour traffic conditions for ramp segments and intersections along Jodeco Road were analyzed using the Highway Capacity Software (HCS), version 4.1, and the TRAF-CORSIM software. Both software analyses result in a Level of Service (LOS) for the intersection; however, the TRAF-CORSIM model is a network oriented analysis tool that estimates the delay of each intersection approach as a function of the upstream and downstream traffic flow. TRAF-CORSIM models traffic queues from one intersection backing through an adjacent intersection. Therefore, extreme congestion at any one intersection may limit the traffic flow to downstream intersections. [REDACTED] This type of modeling is considered to more accurately depict actual intersection operations. The results of both analyses are provided below in Tables B-1 and B-2 for the future build condition.

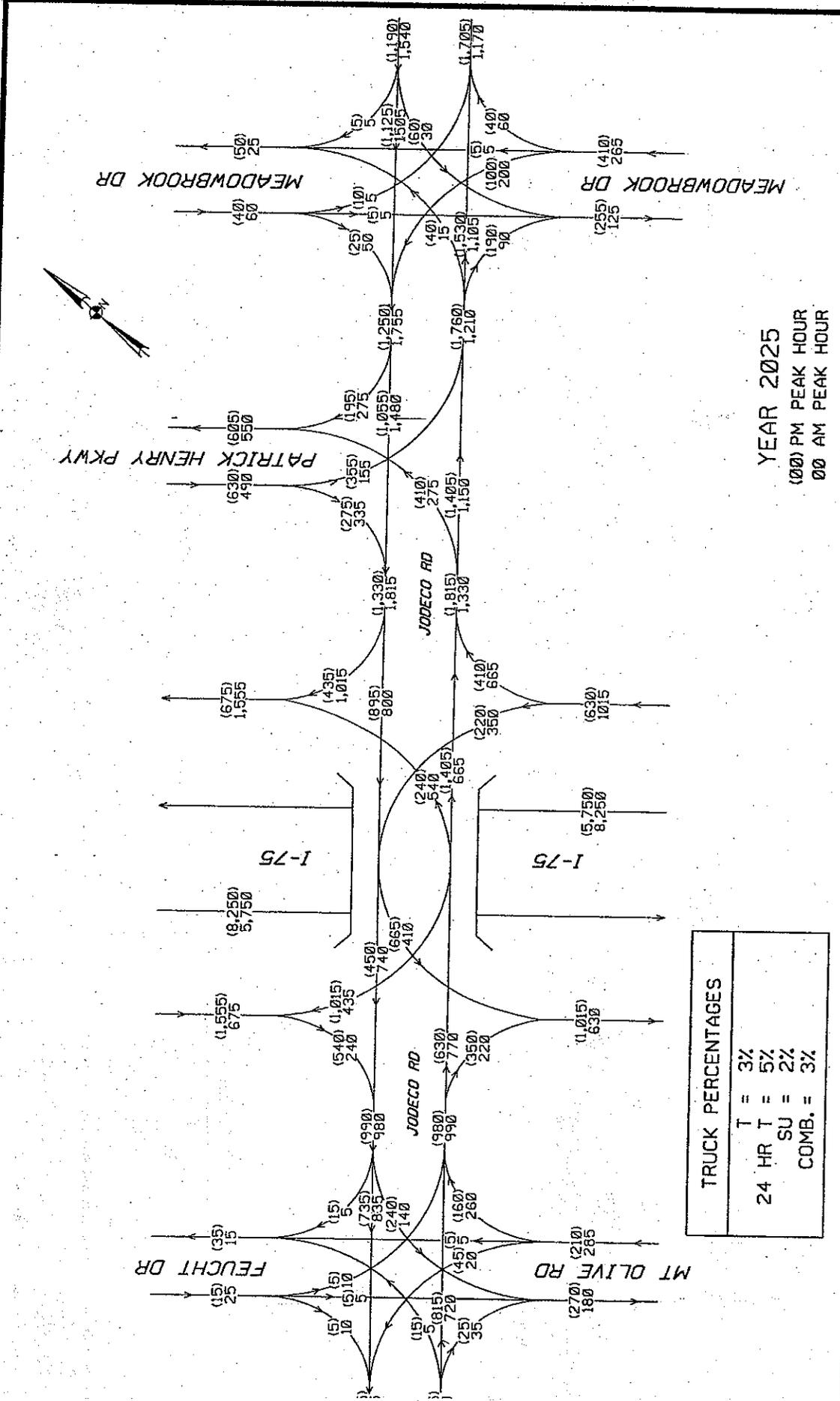
Table B-1 HCS Analysis Results

Intersection	AM		PM	
	Delay	LOS	Delay	LOS
Jodeco Rd at Mt. Olive Rd	17.8	B	19.4	B
Jodeco Rd at I-75 SB Off-Ramp	23.8	C	40.7	D
Jodeco Rd at I-75 NB Off-Ramp	40.0	D	19.7	B
Jodeco Rd at Patrick Henry Pkwy	24.5	C	28.2	C
Jodeco Rd at Meadowbrook Dr	49.8	D	33.2	C

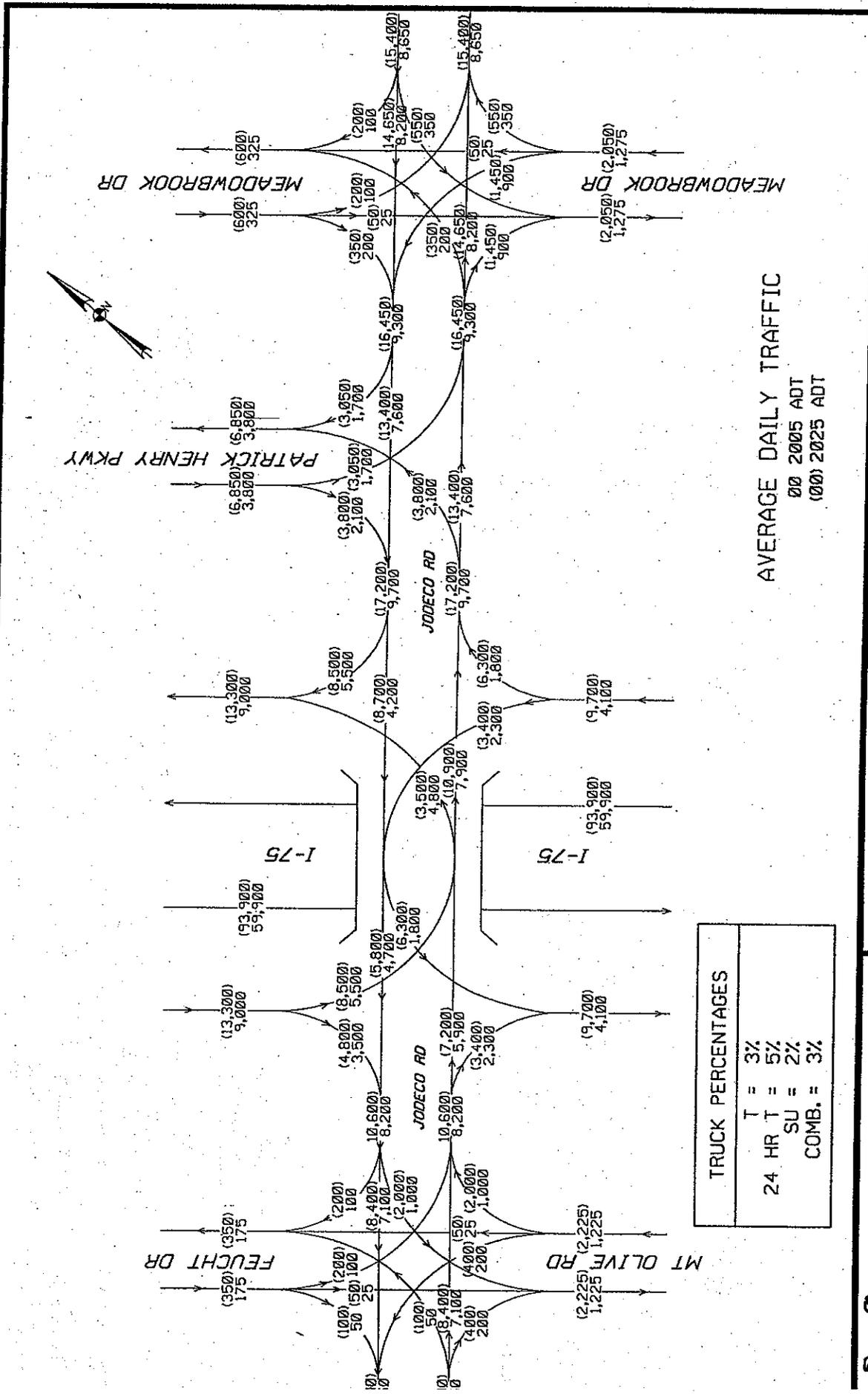
Table B-2 TRAF-CORSIM Analysis Results

Intersection	AM		PM	
	Delay	LOS	Delay	LOS
Jodeco Rd at Mt. Olive Rd	21.1	C	23.0	C
Jodeco Rd at I-75 SB Off-Ramp	33.1	C	40.5	D
Jodeco Rd at I-75 NB Off-Ramp	32.9	C	29.9	C
Jodeco Rd at Patrick Henry Pkwy	25.6	C	31.8	C
Jodeco Rd at Meadowbrook Dr	28.8	C	31.9	C

The results of the HCS analysis indicate that each intersection would independently operate at an acceptable level of service for both morning and evening peak time periods, with the exception of the Meadowbrook Road intersection. When analyzed within the context of a network of intersections, as modeled in TRAF-CORSIM, the results differ slightly. The I-75 NB On-ramp improves in both delay and level of service; however, the minor street intersections with Jodeco Road indicate noticeable increases in overall intersection delay and slightly lower levels of service.



YEAR 2025
 (00) PM PEAK HOUR
 (00) AM PEAK HOUR



SUMMARY OF TRAFFIC ANALYSIS OF NUMBER OF LANES REQUIRED

The Jodeco Road Interchange with I-75 was analyzed to determine the number of lanes necessary for adequate levels of service (LOS).

Highway Capacity Software (HCS) was used to determine the critical intersection that would ultimately determine the number of lanes required to maintain an acceptable level of service using the projected design year 2025 traffic volumes. The HCS analysis determined that the critical intersection along Jodeco Road is at its intersection with the I-75 northbound ramps. This intersection was evaluated with six different lane configurations. The results are reported below.

Summary of Results - Jodeco Road @ I-75 Northbound Ramps

3 - through lanes over bridge (C)	
1- westbound right turn lane with 400 vph right turn on red. (E)	LOS D
2 - northbound right turn lanes (E)	
3 - through lanes over bridge (C)	
1- westbound right turn lane with no right turns on red (F)	LOS F
2 - northbound right turn lanes (E)	
2 - through lanes on bridge (C)	
1- westbound right turn lane with 400 vph right turn on red (E)	LOS D
2 - northbound right turn lanes (E)	
2 - through lanes over bridge (C)	
1- westbound right turn lane with no right turns on red (F)	LOS F
2 - northbound right turn lanes (E)	
2 - through lanes on bridge (C)	
2 - westbound right turn lanes (D)	LOS D
2 - northbound right turn lanes (E)	
2 - through lanes on bridge (C)	
2 - westbound right turn lanes (D)	
1 - northbound right turn lane and 1- free flow right turn lane (D)	LOS C

The through traffic on Jodeco Road over the bridge is not a critical traffic movement. However, the auxiliary lanes (6-lane section) between I-75 and Patrick Henry Parkway are required to facilitate the right turn traffic movements from I-75 northbound to Jodeco Road eastbound and Jodeco Road westbound to I-75 northbound. Therefore, it is recommended that six lanes be provided due to the design hour traffic capacity requirements.

BRIDGE INVENTORY DATA LISTING GEORGIA DEPARTMENT OF TRANSPORTATION

Henry County

Structure ID: 151-0028-0
 Structure I.D. No.: 151-0028-0
 Bridge Information: 07

Preature Int.: 1-75
 Critical Bridge: 0
 Route Number Carried: CR00824
 Facility Carried: JODECO ROAD
 Location: 5 MI NW OF MCDONOUGH
 DOT District: 3
 Year Photo: 1998

Inspection Frequency: 24 Date: 12/30/1998
 Insp Freq: 0 00 Date: 0000
 Underwater Insp Freq: 0 00 Date: 0000
 Other Spc. Insp Freq: 0 00 Date: 0000

Structure Code: 00000
 Inventory Route (O/U): 1
 Type: 4
 Designator: 1
 Number: 01415
 Direction: 0

Altitude: 33-28.9
 Longitude: 84 -13.1
 Order Bridge: 000 %Shared: 00
 Number: 000000000000000

Defense Highway: 0
 Parallel Structure: N
 Direction of Traffic: 2
 Road Inventory Mile Post: 004.55
 Inspection Area: 03 Initials: DJM
 Structure I.D. No: 151-01415F-004.66E
 Structure I.D. No: 000-000000-000.000

- * 104 Highway System: 0
- * 26 Functional Classification: 07
- * 204 Federal Route Type: S No: 01415
- * 110 Truck Route: 0
- 206 School Bus Route: 1
- 217 Benchmark Elevation: 0.00
- 218 Datum: 0
- * 19 Bypass Length: 7
- * 20 Toll: 3
- * 21 Maintenance: 01
- * 22 Owner: 01
- * 31 Design Load: 5
- 37 Historical Significance: 5
- 205 Congressional District: 03
- * 27 Year Constructed: 1968
- 106 Year Reconstructed: 0000
- 33 Bridge Median: 0
- 34 Skew: 05
- 35 Structure Flared: 0
- 38 Navigation Control: N
- 213 Special Steel Design: 0
- 267 Type of Paint: 5

- * 42 Type Service On: 1 Under: 1
- 214 Movable Bridge: 00
- 203 Type Bridge: Z-O-M-O
- 259 Pile Encasement: 3
- * 43 Structure Type Main: 3 02
- 45 No. Spans Main: 004
- 44 Structure Type Appr: 0 0
- 46 No. Spans Appr: 0000
- 226 Bridge Curve Horz: 0 Vert: 1
- 111 Pier Protection: 0
- 107 Deck Structure Type: 1
- 108 Wearing Surface Type: 1 Membrane: 8 Protection: 8

SUFF. RATING: 78.0
 Signs & Attachments

- 223 Expansion Joint Type: 02
- 242 Deck Drains: 0
- 243 Parapet Location: 0 Height: 0 Width: 0
- 238 Curb: 0.8 1
- 239 Handrail: 1 1
- * 240 Median Barrier Rail: 0
- 241 Bridge Median Height: 0 Width: 0
- * 230 Guardrail Loc Dir Rear: 3 Fwrd: 3 Oppo Dir Rear: 0 Fwrd: 0
- 244 Approach Slab: 3
- 224 Retaining Wall: 0
- 233 Posted Speed Limit: 45
- 236 Warning Sign: 0
- 234 Delineator: 1
- 235 Hazard Boards: 0
- 237 Utilities Gas: 22 Water: 21 Electric: 00 Telephone: 22 Sewer: 00
- 247 Lighting Street: 0 Navigation: 0 Aerial: 0
- * 248 County Continuity No: 00

Cross Reference Report

Henry County

DOT MAINTENANCE

Fax: 4046577286

Oct 6 '99

10:53

P. 04

Structure ID: 151-0028-0

Structure I.D. No.: 151-0028-0-A
A Feature Int.: FAS 1415 JODECO ROAD
} Critical Bridge: 0
A Route Number Carried: SR00401
} Facility Carried: 1-75
Location: 5 MI NW OF MCDONOUGH

Inspection Frequency: 00 Date: 12/30/1998
Place Code: 00000

Inventory Route (O/U): 2
Type: 1
Designator: 1
Number: 00075
Direction: 0

Latitude: 33.28.9
Longitude: 33.13.1

Defense Highway: 1
Parallel Structure: N
Direction of Traffic: 2
Highway System: 1
Functional Classification: 01
Federal Route Type: 1 No: 075-2
Truck Route: 1
Bypass Length: 1
Toll: 3
Maintenance: 01
Owner: 01

Year Constructed: 1968

Type Service On: 1
Under: 1

Structure Type Main: 3 02

Structure I.D. No.: 151-00401D-222.23N
Referen I.D. No.:

Signs & Attachments

- * 240 Median Barrier Rail: 0
- * 230 Guardrail Loc Dir Rear: 6
Fwrd: 6
Oppo Dir Rear: 6
Fwrd: 6

Ratings

* 227 Collision Damage: 0

Measurements

- * 29 ADT: 107000 Year: 1998
- * 28 Lanes On: 02 Under: 06

- * 48 Max. Span Length: 0071
- * 49 Structure Length: 264

* 47 Tot. Horz. Cl: 56.6

Posting Data

- * 229 Shldr Width:
Rear Lt: 10.0 Type: 2 R: 10.0
Fwrd Lt: 10.0 Type: 2 R: 10.0
Pvment Width:
Rear: 36.0 Type: 2
Fwrd: 36.0 Type: 2
Intersection Rear: 1 Fwrd: 1

Vertical

- * 228 Min. Vert. Cl
Act. Oadm. Dir: 16' 09"
Oppo. Dir: 16' 04"
Posted Oadm. Dir: 00' 00"
Oppo. Dir: 00' 00"

* 10 Max Min Vert Cl: 16' 11" Dir: J

- * 208 Inspection Area: 03 Initials: DJM
- * 265 U/W Insp. Area: 0 Diver: ZZZ

* 248 County Continuity No: 00

Minutes of Initial Concept Team Meeting
December 13, 2001, 1:00 P.M., GDOT Urban Design Conference Room
I-75/Jodeco Road (C.R. 824) Interchange
Project Number: NH-IM-75-2 (213) P.I. Number: 312160
Henry County

A list of the attendees is attached.

Nicoe Alexander opened the meeting and had everyone introduce themselves. Jerry Brooks of MA presented the project and project layout and then Karla Poshedly discussed the Need and Purpose, traffic data and analysis and costs. Comments and questions on the project concept layout are listed below:

1. **Comment:** The right-of-way areas are unclear on the concept layout as to what parcels will and will not be affected.

Response: MAAI will verify actual right-of-way on the freeway and discuss with the GDOT Right-of-Way office to get a more accurate account.

2. **Comment:** A decision needs to be made as to whether a cul-de-sac or right turn bend needs to be shown at the intersection of Meadowbrook Drive. How will access be changed for properties around this intersection?

Response: After the property line information was placed on the concept layout plan, it was decided that a 25 mph bend with a cul-de-sac could be used at this location and that Meadowbrook Drive from Jodeco Road to Highland Drive could become a driveway to serve the Hardees' property. Access to the other properties at this intersection would not be affected by this modification.

3. **Comment:** Change Feucht Drive. Where does Feucht Drive go?

Response: Feucht Drive presently serves a restaurant. The vacant land around Feucht Drive is the property of one major landowner. Consequently, it was decided to show a new access drive that would bend and "T" into Feucht Drive. It was also stated that MAAI should tighten the horizontal curve and tie the dirt road away from the intersection.

3. **Comment:** Check the need for a traffic signal at Meadowbrook Drive. Build Meadowbrook intersection for a future signal.

Response: Traffic has been checked and initially, it does not appear that a traffic signal will be warranted the opening year. However, all preliminary design work would be done in anticipation of a traffic signal installation.

4. **Comment:** A Park & Ride lot cannot be closed and must be cleared with Office of Environment and Location.

Response: The concept layout was modified to have the driveway access to the Park & Ride lot moved from Jodeco Road to Holloway Road, so that the operation of the existing Park & Ride lot can be maintained.

5. **Comment:** Show the storage lane distances and queue lengths at the intersections (mainly ramp intersections with Jodeco).

Response: This information is contained within the traffic analysis section of the Concept Report and will be included in the Design Data Book that is developed during the Concept Validation Phase of the Project and is part of Preliminary Design work.

6. **Comment:** What is needed for the I-75 traffic lanes at this interchange.

Response: No improvements to I-75 or the entrance or exit gores are necessary with this project. The proposed new Jodeco Road Bridge over I-75 will be constructed to accommodate one additional general use lane and one HOV lane in both directions of I-75.

7. **Comment:** Will pedestrian detours be necessary during the construction of the new Jodeco Road Bridge over I-75?

Response: The construction staging of this project is planned to include maintaining pedestrian access over I-75 during construction.

8. **Comment:** There is a new pedestrian sidewalk design that includes handicap ramps that may require wider sidewalks.

Response: If the new pedestrian sidewalk design is approved by GDOT, then, it will be included in the preliminary design of this project.

9. **Comment:** Is the superelevation on or off the bridge?

Response: It is our intention to design the roadway so that there is no superelevation on the bridge.

10. **Comment:** Show Frontage Road options on plans for Public Information Meeting.

Response: The proposed frontage road in the southwest quadrant of the intersection will be shown with a note indicating that there is a proposed future extension that would tie into Jonesboro Road. Also, shown is a Mt. Olive Road Connector from the Proposed Frontage Road to the existing Mt. Olive Road that parallels I-75.

11. **Comment:** Henry County agrees on closing Meadowbrook Drive from Highland Drive to Jodeco Road and leaving only a driveway for the Hardee's property.

Response: The concept layout was modified accordingly with a 25 mph bend at the intersection of Meadowbrook Drive at Highland Drive.

-
12. **Comment:** Close Mt. Olive Road?

Response: Provide two alternatives for Mt. Olive Road and let the public give their input.

13. **Comment:** Henry County would like to have the intersection designed for a traffic signal at Meadowbrook Drive.

Response: The intersection of Jodeco Road at Meadowbrook Drive will be designed to accommodate a traffic signal.

14. **Comment:** Show options for relocated Park & Ride Lot. Look at Commercial/Park & Ride Lot at Mt. Olive Road.

Response: The concept studies for the proposed Park & Ride lot is beyond the scope of this Interchange Project. These studies will have to be considered in the concept phase of the proposed Park & Ride Project.

15. **Comment:** Include discussion in the concept report of the four-lane alternate for improving the Jodeco Road Interchange

Response: The four-lane alternate for improving Jodeco Road Interchange was added to the concept report under alternatives considered.

16. **Comment:** There were issues left out of the concept report: (1) the closing of the Park and Ride, (2) the signal at the I-75 Interchange, and (3) mention of a wetland.

Response: (1) The closing and reconstruction/relocation of the Park and Ride Lot is another project and is not part of this concept. This concept does not plan to close the

Park and Ride lot. There are impacts to the Park and Ride lot, which are described in the concept report as a relocation of the access driveway to the Park and Ride lot.

(2) The traffic signals at the I-75 interchange ramps are shown as existing on the concept layout and are analyzed in the *Traffic Analysis* section of the concept report.

(3) The wetland area near Feucht Drive is mentioned in the concept report under the title *Environmental Concerns*.

17. **Comment:** Are bike lanes required? Correct typical section in concept report that shows a bike path by adding 4 feet on both sides of the bridge and widening the shoulder.

Response: Bike lanes are now included in the concept of this road because Henry County has designated Jodeco Road as a bike route.

18. **Comment:** Urban Design stated that the point of failure for the four-lane bridge should be shown.

Response: A more in depth analysis of the four-lane bridge alternative is discussed and attached to the concept report. A statement referring to the point of failure is made in the *Alternatives Considered* section of the concept report

GEORGIA DEPARTMENT OF TRANSPORTATION

MEETING/CONFERENCE RECORD OF ATTENDEES

PURPOSE: Initial Concept Team Meeting
 LOCATION: conference room - GDOT
 DATE: 12/13/01 TIME: 1:00pm
 MODERATOR: Nicoe Alexander

NAME	ORGANIZATION	PHONE NO.	E-MAIL ADDRESS
1. <u>Joi Crawley</u>	<u>GA DOT - Urban</u>	<u>(4) 656-5441</u>	<u>joi.crawley@dot.ga.gov</u>
2. <u>Jim O'Neal</u>	<u>Henry County</u>	<u>7/954-2087</u>	
3. <u>JIM RISKER</u>	<u>Henry Co. - Member</u>	<u>770-954-2465</u>	
4. <u>Idanil McElroy</u>	<u>Henry Co. Chairman</u>		
	<u>Glenn Scarborough</u>	<u>MA</u>	
6. <u>Michelle Brawillette</u>		<u>GDOT/OEL</u>	
7. <u>Todd Hill</u>		<u>MA 770-263-5945</u>	
8. <u>JEFF SIMMONS</u>	<u>GDOT PLANNING</u>	<u>A-463-4376</u>	<u>jeff.simmons@dot.ga.gov</u>
9. <u>JERRY BROOKS</u>	<u>MAAI</u>	<u>770 898 7462</u>	<u>jbrooks@henry.ga.us</u>
10. <u>Joshua Grzegorzewski</u>	<u>FHWA</u>	<u>404-562-3655</u>	<u>joshua.grzegorzewski@fhwa.dot.gov</u>
11. <u>Nicoe Alexander</u>	<u>GDOT-URBAN</u>	<u>(4) 656-5441</u>	<u>nicoe.alexander@dot.state.ga.us</u>
12. <u>F. STONE</u>	<u>BELLSOUTH</u>	<u>404-761-5823</u>	<u>F.Stoneiii@bridge.bellsouth.com</u>
13. <u>Karl^o Poshedly</u>	<u>MA</u>	<u>770-263-5945</u>	<u>kposhedly@moreland-altobelli.com</u>
14. <u>GLENN BOWMAN</u>	<u>GDOT-URBAN</u>	<u>A-656-5441</u>	<u>GLENN.BOWMAN@DOT.STATE.GA.</u>
15. <u>GLENN A. WILLIAMS</u>	<u>GDOT UTILITIES</u>	<u>706 646 6549</u>	<u>GLENN.A.WILLIAMS@DOT.GA.</u>
16.			
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Minutes of Concept Team Meeting
October 17, 2002 10:00 A.M., GDOT Urban Design Conference Room
I-75/Jodeco Road (C.R. 824) Interchange
Project Number: NH-IM-75-2 (213) P.I. Number: 312160
Henry County

A list of the attendees is attached.

Mr. Joe Palladi opened the meeting and asked everyone to introduce themselves. Mr. Nicoe Alexander of Urban Design began the meeting by defining the project to be discussed and stated the project number and P.I. number. Ms. Karla Poshedly of Moreland Altobelli and Associates then presented the project need and purpose, design features of the project and project layout. Ms. Poshedly also summarized the public information meeting that was held on the project. She stated that there were complaints by businesses on the N.E. quadrant between the Interstate and Patrick Henry Parkway. The complaint was that the median would require patrons of these businesses who want to travel eastbound on Jodeco Road, to travel across the interstate bridge to make a U-turn. Another complaint by the businesses is that they are concerned with traffic circulation and parking problems on their site due to the construction and widening. Ms. Poshedly made the suggestion that alternate access may be considered as cost-to-cure issues, once right-of-way negotiations begin.

Other issues that Ms. Poshedly stated to open discussion about the project: 1) Should Patrick Henry Parkway be constructed to a 4-lane divided width from Jodeco Road to Holloway Road and then taper to two-lanes after the intersection of Holloway Road? 2) Hardees may be damaged enough by the widening and median that it may become a displacement property. 3) Construction staging of the bridge and roadway may require additional pavement in the N.E. quadrant of the project and further impact the existing Park and Ride lot and business next door.

Mr. Palladi responded to the first question. He said that extending the widening of Patrick Henry Parkway does not appear to be a part of the Need and Purpose of the interchange project. MA will have to verify this through appropriate traffic studies.

Henry County Commissioner Gary Freedman commented that if the Hardees is closed then maybe the Park & Ride Lot could be relocated to this corner. The disadvantages to such a location also were discussed.

Mr. Joe Palladi then made his comments and questions about the project:

- Does the project comply with the ARC model?
Ms. Poshedly confirmed that the project is in compliance with the ARC model.

- Is Jodeco Road on the ARC Bike Plan?
Ms. Poshedly stated that Jodeco Road is on the ARC bike route plan and that there are two long-range bike lane projects along Jodeco Road that will extend the bike lanes of this project.

- Do not put U-turn signs at interstate ramps for wrong way movement.

- Does the bridge over I-75 accommodate the SOV/HOV clear zone requirements of the interstate?
- Ms. Poshedly stated that the bridge over I-75 would be constructed to accommodate the SOV/HOV clear zone requirements. She stated that the typical section contained in the report reflects this.
- Does limited access affect Chevron?
Ms. Poshedly stated that the Chevron would not be impacted by the limited access as presently shown. If it is desired to extend the limited access further, then the limited access rights would have to be purchased from the existing Chevron station.
- Should we study adjusting the alignment of Jodeco Road to affect only one side or the other?
Ms. Poshedly stated that the concept proposes that the widening along Jodeco Road is to be predominantly on the north side. However, there is a small amount of right-of-way needed on the south side of Jodeco Road in the areas where the project is tying into existing Jodeco Road. The layout proposed was based on maintenance of traffic issues.
- Park & Ride lot will need to expand – maybe the whole quadrant could be used if some or all of the businesses on the NE side are acquired through the normal right-of-way process. The location of the Park & Ride lot would be best on the NE side because buses can make a right turn onto Jodeco Road and then turn right quickly onto the northbound I-75 On-Ramp. If the alignment could feasibly be moved to the north side of Jodeco Road, then there could be decreased damage to the south side.
- The project cannot be expanded to include further widening of Patrick Henry Parkway unless it is required to meet speed design tapers. Construction dollars can only be spent on side streets when it is necessary for the proper design of its intersection with Jodeco Road and directly related to the operations of the freeway interchange.

Henry County asked why there is no traffic signal at the Relocated Feucht Drive/Mount Olive Road intersection. Ms. Poshedly said that a traffic signal warrant study will be done at the intersection during the design phase and that if the traffic signal is warranted prior to the construction letting of the project then a traffic signal will be installed as part of this project. Mr. Palladi commented that all of the major intersections should be designed so as to accommodate future traffic signals.

Henry County Commissioner Gary Freeman commented that the KOA Campground located on Mount Olive Road is planning to expand and Henry County has asked the KOA Campground to pave a portion of Mount Olive Road that is along their frontage. Commissioner Freeman said that he thought the KOA Campground was located near the top of Mount Olive Road and that the way we have Mount Olive Road shown may impact their property. However, Mr. Jerry Brooks later pointed out that the KOA Campground is approximately a quarter of a mile south of Jodeco Road on the dirt portion of Mount Olive Road.

Commissioner Freeman also stated that citizens in the area would prefer if Mount Olive Road becomes the main frontage road and it tie directly with Jodeco Road. Ms. Poshedly stated that she

on the approaches of the left turn lanes. Mr. Palladi said such lanes would need to be discussed with FHWA on a case-by-case basis.

Henry County commented that the road labeled Chambers Road is called McCullough Road and the road labeled McCullough Road is called Plantation Boulevard. Ms. Poshedly said the labeling on the concept layout would be corrected. *(Field investigation revealed that the street name signs indicate Chambers Road and Plantation Boulevard. The concept layout was relabeled accordingly)*

Mr. Scott Zehngraff commented that he was glad that a left turn lane was provided at McCullough Road but the taper of the left turn lane appears to be too short. Ms. Poshedly said that there was not enough distance to drop two lanes before McCullough Road so a left turn lane was provided and then the lane was tapered. She said the taper lengths would be checked during Preliminary design. Mr. Palladi said to prove that you could not drop two lanes before McCullough Road because a left turn lane cannot be built as part of this project unless there is no other way to taper the lanes.

A representative of the Office of Environment and Location asked if there would be another PIM held on this project. Ms. Poshedly said she did not think there would be but a public hearing would likely be held during the Preliminary design phase. Mr. Palladi said that another PIM could be held if needed or wanted.

Mr. Kerry Gore of the GDOT District Utilities Office was asked to make his comments. He stated that Georgia Power has a pole line on the north side past the Park and Ride lot and then it crosses over Jodeco Road and runs along the south side. He said that there is a water line attached to the north side of the existing bridge and gas and BellSouth are attached to the south side of the existing bridge.

Additionally, Mr. Gore said he would like SUE (Subsurface Utilities Engineer) to review the preliminary utility plans. Henry County then commented that they would like to upgrade their existing 10" water line to 12" pipe. Mr. Gore reminded them that if their existing water lines are made of asbestos pipe, then there are environmental regulations that dictate the proper removal and disposal of such pipe. Henry County also commented that they have a 6" water pipe on Mount Olive Road that they plan to upgrade to 8".

A BellSouth representative commented that they were undecided as to whether they would upgrade their existing 6" duct run and have it attached to the new bridge or jack and bore under the interstate. BellSouth also stated that there is a 30' x 30' BellSouth easement on the side of Jodeco Road where the possible historic property is located. They said it would be cheaper if the project widening would stay off of their easement because the cabinet would cost approximately one million dollars to remove and relocate.

With no additional comments Mr. Palladi adjourned the meeting.

GEORGIA DEPARTMENT OF TRANSPORTATION

MEETING/CONFERENCE RECORD OF ATTENDEES

PURPOSE: CONCEPT TEAM MEETING - NH-1M-75-2(213) Henry Co.

LOCATION: URBAN DESIGN

P.I. NO. 312160

DATE: 10/17/02 TIME: 10:00 A.M.

MODERATOR: _____

NAME	ORGANIZATION	PHONE NO.	E-MAIL ADDRESS
1. JAN C. HILLIAR	GDOT-URBAN DESIGN	404-656-5441	Jan.Hilliara@dot.state.ga.us
2. STEPHANIE WELCH	FHWA-GADIVISION	404-562-3639	stephanie.welch@fhwa.dot.g
3. SCOTT ZEHNGRAFF	GDOT-OTS&D	9-635-8127	Scott.z@dot.g
4. EDDIE KING	BELLSOUTH	7-493-2006	Eddie.KING@BELLSOUTH.C
5. John Gregory	DOT	4-463-4375	John.Gregory@DOT.STATE.GA
6. ALEX STONE	MORELAND ATTORNEY	7-263-5945	astone@moreland-attbelle
7. GARY FREEDMAN	Henry County DOC	7-954-2400	
8. Jim O'Neil	Henry County DOT	7-954-2087	joneal@Co.Henry
9. Ron Wishon	GDOT-ENG. SVCS	4-651-7470	ron.wishon@dot.state.ga.us
10. Michael Murdoch	GDOT-ENV/LOC	4-699-4417	michael.murdoch@dot
11. A.J. THARSMITH	GDOT-OEL	(4)-699-4416	ANTHONY_THARSMITH@dot
12. Tom Queen	GDOT-Thomaston	(6) 646-6591	TOM.QUEEN@dot.state.ga
13. Joe Palladi	GDOT-Urban Design	404-656-5446	joe.palladi@dot.state
14. Allen Rapp	Henry Co. Water Auth	7-914-3688	Allen_Rapp@HCAUSA.G
15. Chris Camp	GDOT-Thomaston		
16. WALTER BOYD	FHWA	4-562-3651	walter.boyd@fhwa.dot.ga
17. Glenn Bowman	GDOT-Urban	4-656-5459	Glenn.Bowman@DOT-STA
18. Joshua Gregorzewski	FHWA-GA DIV.	4-562-3655	joshua.gregorzewski@fhwa.d
19. Marc MASTRONARDI	GDOT-AE	7228-7337	marc.mastronardi.d
20. KERRY GORE	GDOT-UTILITIES	706-6552	Kerry.gore@dot
21. GLENN A. WILLIAMS	GDOT-UTILITIES	7066466549	GLENN.A.WILLIAMS@DOT
22. J.T. RABUN	GDOT-OMR- Pavement Mngt	404-363-7582	JT.RABUN@dot.state.ga.us
23. JERRY BROOKS	MAAI	770-263-5945	jbrooks@moreland-attbelle.c
24. Nicole Alexander	GDOT-URBAN	(4) 656-5441	nicoalexander@dot.state.ga.us
25. Karta Poshedly	MAAI	770-263-5945	Kposhedly@moreland-att
26.			
27.			
28.			

STATE OF GEORGIA
DEPARTMENT OF TRANSPORTATION

INTERDEPARTMENT CORRESPONDENCE

FILE P. I. No. 312160 **OFFICE** Environmental/Location

DATE April 12, 2002

FROM Harvey D. Keepler, State Environmental/Location Engineer

TO **DISTRIBUTION BELOW**

SUBJECT: PUBLIC INFORMATION MEETING SYNOPSIS

PROJECT NUMBER: NH-IM-75-2(213)

PROJECT NAME: Interchange Improvements at I-75 and Jodeco Road

DATE: January 31, 2002

NUMBER IN ATTENDANCE: 82

FOR: 8

CONDITIONAL: 5

UNCOMMITTED: 5

AGAINST: 2

OFFICIALS IN ATTENDANCE: Gary Freedman, Henry County Commission
Ted Strickland, Stockbridge City Manager
Jim O'Neal, Henry County DOT
Leland Maddox, Henry County Commission

ADDITIONAL COMMENTS: Summary of written public comments for inclusion in the revised concept report as follows:

- Of major concern to local business owners is the acquisition of right-of-way and the proposed median along Jodeco Road, specifically on the east side of I-75, between Octogan Road and the second intersection with Meadow Church Road; left-turn access by westbound drivers to businesses located on the south side of Jodeco Road would require them to cross I-75, do a U-turn at the new intersection with Feucht Drive, then cross back over I-75 to then gain access
- Concerns were expressed about the flow of traffic and the overall level of service on Jodeco Road when the proposed 6-lane typical section tapers quickly into a two-lane section within less than half a mile on the east side of I-75, and within a third of a mile on the west side of I-75. Citizens anticipate

severe congestion to occur on Jodeco Road at the intersections of Tunis Road, Peach Road, Oak Grove Road and Dailey Mill Road as a result; recommend that traffic signals be installed at all intersections on Jodeco Road.

- Strong opposition by local residents to close the north end of Meadowbrook Drive to Jodeco Road. Citizens stated emergency vehicles would not have quick enough access, and cited anticipated drops in property values.
- Some comments suggested extending Jodeco Road improvements east to include Tunis Road, and converting Jodeco Road into a one-way pair (i.e. extend median the entire length of Jodeco Road)
- It was recommended that there be a dedicated right-turn lane from Patrick Henry Parkway onto Jodeco Road to handle the high right-turn vehicle demand.
- There were some comments regarding the scheduling of this project with respect to the Eagles Landing Parkway interchange project, and that the north interchange be constructed first. Also, it was desired that Patrick Henry Parkway be finished between the two roadways.
- There were additional concerns for areas outside the project limits that may be affected by the project, including school traffic to and from the existing Eagles Landing High School as well as the proposed elementary school to be constructed on Peach Road. Concerns were also raised about existing congestion conditions being exacerbated at the railroad grade crossing when a train comes through.

PREPARED BY:

TELEPHONE NO.:

cc: Frank L. Danchetz, P. E.
Thomas L. Turner, P. E.
David E. Studstill, P. E.
Harvey D. Keepler
District Engineer
Dist. Design Engineer
Immediate Supervisor

DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA

INTERDEPARTMENT CORRESPONDENCE

FILE NH-IM-75-2(213), Henry County
P. I. No. 312160

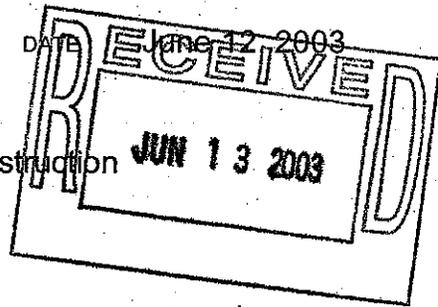
OFFICE Thomaston

FROM Thomas B. Howell, P.E., District Engineer

TO Margaret B. Pirkle, Assistant Director of Preconstruction

SUBJECT

PROJECT CONCEPT REPORT



We have reviewed the concept report on the above project and concur with the recommendation for approval.

Attached for your further handling is the concept report cover sheet which has been signed by the District Engineer.

DBM:PD

Attachment

xc: David Mulling

Harvey Keeper

**DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA**

Office of Urban Design

PROJECT CONCEPT REPORT

Project Number: NH-IM-75-2 (213)

County: Henry

P. I. Number: 312160

Federal Route Number: I-75

State Route Number: 401

*See Project location sketch on page 2.
I-75 @ Jodeco Road/C.R. 824 Interchange Improvement*

Recommendation for approval:

Date of Report: May 30, 2003

DATE 6/5/03

DATE 6/5/03

Jan C. Hilliard
Project Manager *JK*
James B. Puchan
State Urban Design Engineer

The concept as presented herein and submitted for approval is consistent with that which is included in the Regional Transportation Program (RTP) and/or the State Transportation Improvement Program (STIP).

DATE _____

State Transportation Planning Administrator

DATE _____

State Financial Management Administrator

DATE _____

State Environmental/Location Engineer

DATE _____

State Traffic Safety and Design Engineer

DATE 6/9/03

[Signature]
District Engineer

DATE _____

Project Review Engineer

DATE _____

**DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA**

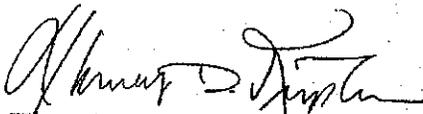
INTERDEPARTMENT CORRESPONDENCE

FILE: P.I. No. 312160

OFFICE: Environment/Location

DATE: June 18, 2003

FROM:

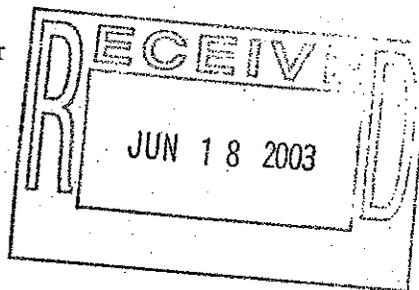

Harvey D. Keeper, State Environmental/Location Engineer

TO:

Meg Pirkle, P.E., Assistant Director of Preconstruction

SUBJECT:

PROJECT CONCEPT REPORT
NH-IM-75-2(213) HENRY COUNTY



The above subject concept report has been reviewed. One eligible historic resource should be listed on page 7. Page 7 - An EA should be anticipated instead of CE. Page 8 - At least 12 months should be given for the Environmental Process (instead of 6). "Potentially" eligible historic property located in NW Quadrant adjacent to project terminus. Since an EA is being prepared, a public hearing or opportunity for a hearing will have to be advertised.

If you have any questions, please contact me at (404) 699-4401.

HDK/rtt

Attachment

cc: David Mulling, P.E.
James B. Buchan

DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA

Office of Urban Design

PROJECT CONCEPT REPORT

Project Number: NH-IM-75-2 (213)

County: Henry

P. I. Number: 312160

Federal Route Number: I-75

State Route Number: 401

See Project location sketch on page 2.
I-75 @ Jodeco Road/C.R. 824 Interchange Improvement

Date of Report: May 30, 2003

Recommendation for approval:

DATE 6/5/03

DATE 6/5/03

Jan C. Hilliard
Project Manager ~~SR~~

James B. Buchan
State Urban Design Engineer

The concept as presented herein and submitted for approval is consistent with that which is included in the Regional Transportation Program (RTP) and/or the State Transportation Improvement Program (STIP).

DATE _____

DATE _____

DATE 6/18/03

DATE _____

DATE _____

DATE _____

* DATE _____

State Transportation Planning Administrator

James D. [Signature]
State Financial Management Administrator
State Environmental/Location Engineer

State Traffic Safety and Design Engineer

District Engineer

Project Review Engineer

State Bridge Design Engineer

**DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA**

Office of Urban Design

PROJECT CONCEPT REPORT

Project Number: NH-IM-75-2 (213)

County: Henry

P. I. Number: 312160

Federal Route Number: I-75

State Route Number: 401

*See Project location sketch on page 2:
I-75 @ Jodeco Road/C.R. 824 Interchange Improvement*

Recommendation for approval:

Date of Report: May 30, 2003

DATE 6/5/03

Jan C. Hilliard
Project Manager *JK*

DATE 6/5/03

James B. Puckan
State Urban Design Engineer

The concept as presented herein and submitted for approval is consistent with that which is included in the Regional Transportation Program (RTP) and/or the State Transportation Improvement Program (STIP).

DATE _____

State Transportation Planning Administrator

DATE _____

State Financial Management Administrator

DATE _____

State Environmental/Location Engineer

DATE _____

State Traffic Safety and Design Engineer

DATE _____

District Engineer

DATE _____

Project Review Engineer

* DATE 6/27/03

Paul V. Telle Jr.
State Bridge Design Engineer

DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA

Office of Urban Design

PROJECT CONCEPT REPORT

Project Number: NH-IM-75-2 (213)

County: Henry

P. I. Number: 312160

Federal Route Number: I-75

State Route Number: 401

See Project location sketch on page 2.
I-75 @ Jodeco Road/C.R. 824 Interchange Improvement

Recommendation for approval:

Date of Report: May 30, 2003

DATE 6/5/03

Jan C. Hilliard
Project Manager

DATE 6/5/03

James B. Buchan
State Urban Design Engineer

The concept as presented herein and submitted for approval is consistent with that which is included in the Regional Transportation Program (RTP) and/or the State Transportation Improvement Program (STIP).

DATE _____

State Transportation Planning Administrator

DATE 6/12/03

Ray Millh
State Financial Management Administrator

DATE _____

State Environmental/Location Engineer

DATE _____

State Traffic Safety and Design Engineer

DATE _____

District Engineer

DATE _____

Project Review Engineer

DATE _____

State Bridge Design Engineer

Department of Transportation
State of Georgia

INTERDEPARTMENTAL CORRESPONDENCE

NH-1M-75-2(213)
File: ~~BRST-074-2(40)~~ Henry County
P.I. No. 312160

Office: Traffic Safety & Design
Atlanta, Georgia
Date: June 25, 2003

PMA/ll
From: Phillip M. Allen, State Traffic Safety and Design Engineer

To: Meg Pirkle, Assistant Director of Preconstruction

Subject: Project Concept Report Review

We have reviewed the above referenced concept report for the proposed bridge replacement / interchange reconstruction at Interstate 75 @ Jodeco Road in Henry County.

The Office of Traffic Safety & Design finds this report satisfactory for approval because it will improve safety and traffic operations within this area.

PMA/sz

Attachment (signature page)

Cc: Harvey Kepler, Environment/Location Engineer
James Buchan, State Urban Design Engineer
Attention: Jan Hilliard
Thomas Howell, District Engineer – Thomaston
Attention: David Millen, District Preconstruction Engineer
David Mulling, Engineer Services, w/ attachment
Joe Palladi, State Transportation Planning Administrator
Paul Liles, State Bridge & Structural Design Engineer
Kathy Bailey, TMC
General Files
Office Files

DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA

Office of Urban Design

PROJECT CONCEPT REPORT

Project Number: NH-IM-75-2 (213)

County: Henry

P. I. Number: 312160

Federal Route Number: I-75

State Route Number: 401

See Project location sketch on page 2.
I-75 @ Jodeco Road/C.R. 824 Interchange Improvement

Recommendation for approval:

Date of Report: May 30, 2003

DATE 6/5/03

Jan C. Hilliard
Project Manager

DATE 6/5/03

James B. Buckner
State Urban Design Engineer

The concept as presented herein and submitted for approval is consistent with that which is included in the Regional Transportation Program (RTP) and/or the State Transportation Improvement Program (STIP).

DATE _____

State Transportation Planning Administrator

DATE _____

State Financial Management Administrator

DATE _____

State Environmental/Location Engineer

DATE 6-26-03

Phillip M. Allen
State Traffic Safety and Design Engineer

DATE _____

District Engineer

DATE _____

Project Review Engineer

DATE _____

State Urban Design Engineer

DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA

Office of Urban Design

PROJECT CONCEPT REPORT

Project Number: NH-IM-75-2 (213)

County: Henry

P. I. Number: 312160

Federal Route Number: I-75

State Route Number: 401

See Project location sketch on page 2.
I-75 @ Jodeco Road/C.R. 824 Interchange Improvement

Recommendation for approval:

Date of Report: May 30, 2003

DATE 6/5/03

DATE 6/5/03

Jan C. Hilliard

Project Manager

James B. Buchan

State Urban Design Engineer

The concept as presented herein and submitted for approval is consistent with that which is included in the Regional Transportation Program (RTP) and/or the State Transportation Improvement Program (STIP).

DATE 6/17/03

DATE _____

DATE 6/30/03

DATE 7/16/03

DATE _____

DATE _____

DATE _____

Joseph Pollard
State Transportation Planning Administrator

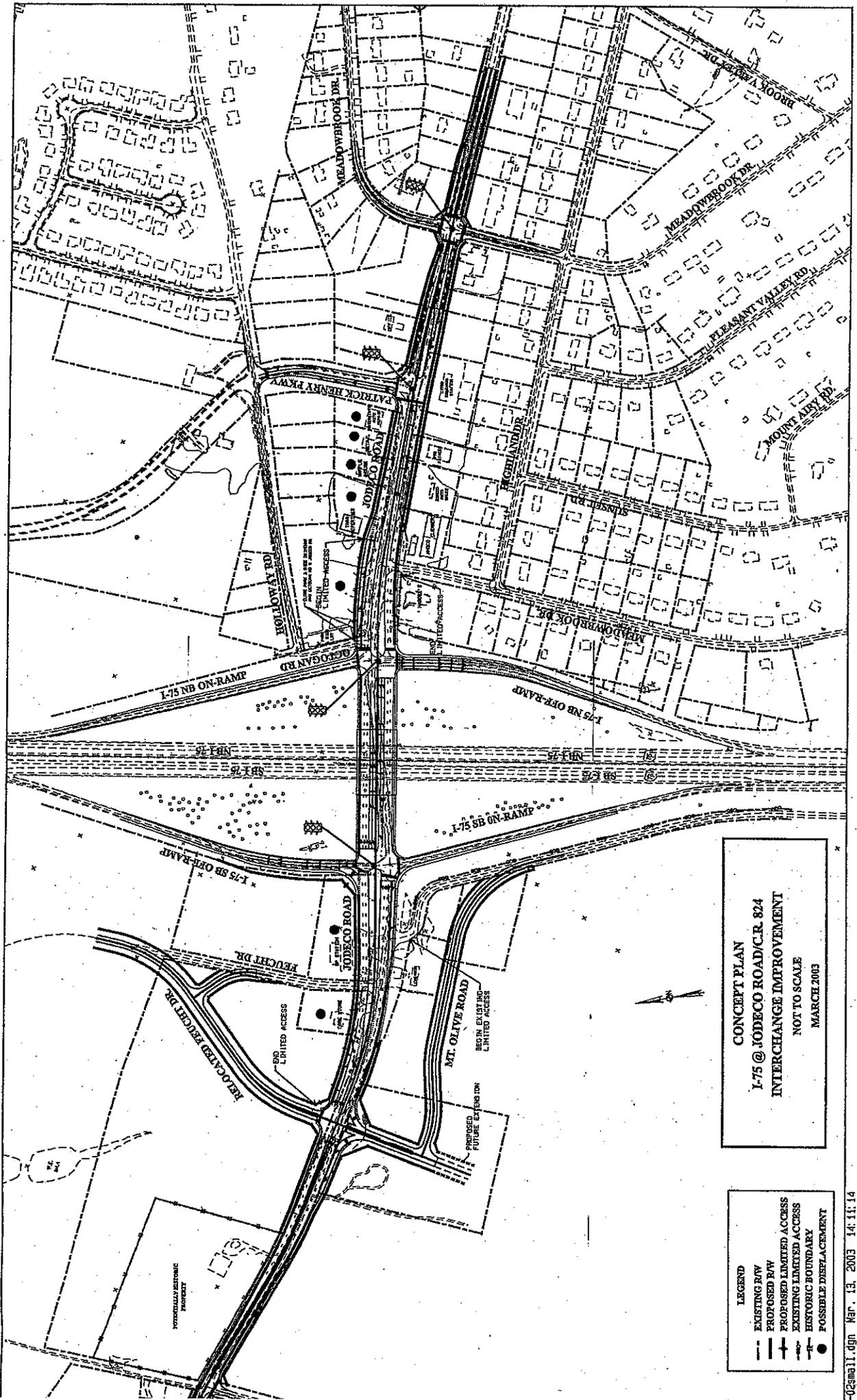
Ken Mitchell
State Financial Management Administrator

Armen D. Danyan
State Environmental/Location Engineer

Phillip M. Albrecht
State Traffic Safety and Design Engineer

District Engineer

Project Review Engineer



CONCEPT PLAN
I-75 @ JODECO ROAD/C.R. 824
INTERCHANGE IMPROVEMENT
 NOT TO SCALE
 MARCH 2003

- LEGEND**
- EXISTING R/W
 - - - PROPOSED R/W
 - + - PROPOSED LIMITED ACCESS
 - + - EXISTING LIMITED ACCESS
 - - - HISTORIC BOUNDARY
 - POSSIBLE DISPLACEMENT