



September 12, 2013

Mr. Rodney N. Barry, P.E.  
Division Administrator  
Federal Highway Administration  
Atlanta Federal Center  
61 Forsyth Street, S.W.  
Suite 17 T100  
Atlanta, Georgia 30303-3104

ATTN: Kelly Wade

RE: PI#(s): 250470, Columbia County, Old Petersburg Rd/Old Evans Fm Baston Rd to Washington Rd  
STP00-7063-00(01)

Dear Mr. Barry:

In accordance with 23 CFR 771.129(c), the subject proposed action has been reevaluated with respect to changes to the social, environmental, and economic effects. Enclosed are two copies of this Reevaluation. The proposed action currently has a GDOT Environmental Certification for Let schedule of August 1, 2013. This project is behind schedule and we appreciate your assistance and cooperation in prioritizing to help keep it on schedule.

The comments received 8/19/13 have been addressed in the document.

1. Please include a green sheet with all necessary special provisions attached in your next submittal.

**Included.**

2. Section VII – Please remove the discussion regarding culvert replacement at Reed Creek. All approved documents have shown a bridge at this stream, so this should not be considered a project change.

**Removed.**

3. Please confirm if the realignment of the Old Evans Road/Blue Ridge Rd/Old Petersburg Rd intersections was always proposed or if this is a project change. If it is not a change, then please rewrite Section A.2 as the beginning sentence states "There is a new intersection..." If this is a project change, please discuss under Section VII, include graphics depicting the details of the change, and confirm appropriate public and agency (schools/emergency responders) involvement that occurred.

**This is not a project change nor a new alignment as compared to the proposed project originally documented.**

**This was a language issue where the "new alignment" being described was in relation to the existing. The language has been clarified in the document.**

4. Page 3, Section X – We do not agree with how DGOT is documenting "involvement" in reevaluations. The reevaluation should clearly indicate what resources continue to have "involvement," so it is clear what resources are impacted. Examples of resources that were and continue to have "involvement" include relocations and noise among others.

**Those items did not have involvement under the last reevaluation which is how we have been reading approval and using the column. In this case meaning, if the last reevaluation did not represent a change in the area, it did not have involvement to put in that column for this reevaluation. There is a logic and rationale that can make sense of either method (how GDOT is currently using the column and how FHWA is proposing it**

*be used); however, if FHWA would like us to look at this column differently, we will need to make a process change and of course would be receptive to a specific conversation to that effect.*

5. As traffic studies have been updated since the previous approval, please include a section that discloses the updated traffic information.

**Updated.**

6. Page 1 of Attachment 1 - The second paragraph refers to a May 15, 2014 meeting, please revise.

**Revised.**

7. Please provide documentation of coordination with schools and emergency service providers regarding the proposed detours.

**Documentation provided in attachment 2.**

8. Please disclose detour length.

**Detour length discussed in Section A.2 of Attachment 1.**

9. Relocation potential:

- a. Please confirm if the 32 displacements are 32 out of the original 34 proposed displacements, or if some displacements were added and others were removed.
- b. What design refinements resulted in the reduction of displacements? Where are these design refinements documented?
- c. Please summarize type of displacements (residences, businesses, etc.)

**Displacements summarized. Changes explained.**

10. Table 1 – GDOT should know the limits of construction at this point; please revise the comment in ROW 2.

**Comment revised. Sign is not within construction limits.**

11. Please provide copies of dated newspaper advertisements and stories or other communications that were used to notify the public of the meeting.

**Copies of advertisements provided in Attachment 4.**

12. Page 8 of Attachment 1, Section C – Changes to project design and alignment are discussed here, with a reference to Section VII of the reevaluation; however, Section VII does not indicate a change in design and alignment.

**Revised.**

13. Page 10 of attachment 1 – In the 2004 report, impacts to Stream 1 was 55 linear feet not 39.

**Revised.**

14. Review page 12 for "Error!" and revise as appropriate.

**Revised.**

15. Ecology – comments on the August 2013 Ecology Addendum were submitted to GDOT Ecology and NEPA staff via email on 8/14/13. Please address those comments and ensure consistency among documents.

**Revised.**

16. Noise:

- a. Table 2 – the table should disclose impacted receptors, not receivers.
- b. Please explain how feasibility of barriers is determined.
- c. Please show the calculations used to determine reasonableness.

- d. Table 3 – Please disclose the receptor numbers and number of dwelling units for each barrier (see Table 8 of NIAA).
- e. Page 34 of Attachment 1:
  - i. Please update this page to reflect 6 comments received from the affected residents.
  - ii. It seems that the department is proposing to construct Barrier 1 because of the commitment that was made in the 2002 EA/FONSI, please disclose.
  - iii. Regarding Barrier 3 – it is unclear how this barrier meets reasonableness criteria documented in GDOT's approved Noise Policy (6 receptors total, of comments received, 2 supports, and 1 does not), please explain.

***All revised.***

- 17. SP150 – Traffic Control – Section C states "... 7:00 pm to Friday to 7:00 am Monday; it appears the first "to" should be removed.

***Removed.***

Based on the enclosed Reevaluation, it has been determined that the approved document remains valid and the proposed action can proceed to the next activity phase. Your concurrence in this determination is requested. Please provide approval or comments no later than Friday, September 13, 2013 so that the proposed action can proceed as scheduled. If you need further information, please contact Sean Diehl at (404) 631-1197.

Sincerely,



Glenn Bowman, P.E.  
State Environmental Administrator

cc (w/o attachment): George Brewer, GDOT Project Manager (via email)  
PDF for Project File; Hardcopy to General Files

**ENVIRONMENTAL COMMITMENTS TABLE** | Plan #: 250470 | County: Columbia | Date Updated: 09/12/2013 | Stage: Reevaluation for Let Certification  
 Transmittal Date for Plans Reviewed by OES: Final Plans

+Review  These commitments are feasible. (must be checked at all steps)  
 GDOT Project Manager: Stephanie Evans  
 PM Signature/Date: Stephanie Evans 7/19/2013

Plans incorporate the commitments. (must be checked to certify by listing)  
 Engineer of Record (EOR): JAN C. HILLIARD  
 EOR Signature/Date: JAN C. Hilliard 9/12/2013

Air/Noise: Le-Al-aa-aa-aa Arch 09/12/2013  
 Eco: 09/12/2013  
 NEPA: 09/12/2013

**A. Resources to be Delineated on the Plans and/or Listed in the Environmental Resource Impact Table (ERIT)**

Resource Name	Additional Information	Refer to	Name and Date of Report or Transmittal	Plan Sheet	Correctly Shown? ERIT
A-2 Perennial Stream 1 a (PS1a)	180 linear ft. impact	C-1, D-1, D-2	Ecology Addendum 08/09/2013	Yes	Yes
A-3 PS1a Buffer	Impacts exempted	C-1, D-4			
A-4 Perennial Stream 1 (PS1)	139 linear ft. impact	C-1, D-1, D-2			
A-5 PS1 Buffer	Impacts exempted	C-1, D-4			
A-6 Perennial Stream 2 (PS2, Red Creek)	No impacts	B-1, C-1, D-1			
A-7 PS2 Buffer (Red Creek)	Impacts exempt	C-1, D-4			
A-8 Intermittent Stream 2 A (IS2a)	43 linear ft. impact	C-1, D-1, D-2			
A-8 IS2a Buffer	Impacts exempted	C-1, D-4			
A-10 Intermittent Stream 2b (IS2b)	No impacts	C-1, D-1			
A-11 IS2b Buffer	Impacts except	C-1, D-4			
A-12 Perennial Stream 3 (PS 3) (adjacent to CSX RR)	No impacts	B-1, C-1, D-1			
A-13 PS3 Buffer (adjacent to CSX RR)	Impacts exempt	C-1, D-4			
A-14 Ephemeral Channel 2c (EC2c)	14 linear ft. impact	C-1, D-1, D-2			
A-15 Open Water 3a (OW3a)	No impacts	C-1, D-1			
A-16 OW3a Buffer	985 sq. ft. impact	C-1, D-1, D-3, D-4			
A-17 Wetland 4A	2,482 sq. ft. of impacts	C-1, D-4			
A-18 Perennial Stream 4 (PS4)	273 linear ft. of impacts	C-1, B-1, D-2			
A-19 PS4 Buffer	Impacts exempt	C-1, D-4			
A-20 Migratory Birds	See attached for details	B-1		Not req'd	

**B. Special Provisions (Attach all special provisions to the commitments table, if available)**

Special Provision	Purpose	Est. Cost	SP's Latest Date
B-1 SP 107.23G	Protection of Federally Protected Species (see attached)	negligible	08/08/2013
B-2 SP 150.11	Traffic Control	negligible	08/22/2013

**C. Plan Notes and Design Features (Description: For plan notes, provide exact wording in "quotes" and approximate location)**

Purpose	Description	Est. Cost	Correctly Shown?
C-1 Protect Environmental Resources	Include the following note in the ERIT for the affected resources: "The contractor shall ensure that no construction related activities other than those shown on the approved plans, including the use of easements, staging, construction, vehicular use, borrow or waste activities, sediment basins, and trailer placement occur within those areas"	N/A	Yes

**ENVIRONMENTAL COMMITMENTS TABLE**

PI#: 250470 | County: Columbia | Date Updated: 09/12/2013 | Stage: Reevaluation for Let Certification  
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		designated as ESAs."		
C-2	Noise Abatement	Two noise barriers are to be constructed. Barrier 1 is located just west of Blue Ridge Drive and Barrier 3 is located north of Old Petersburg Road at Lantern Lane.	\$349,600	Yes

**D. Necessary Permits, Buffer Variances and Mitigation Credits**

Permit, Variance, etc	Additional Information (permit details, number of credits needed, etc...)		Est. Cost	Acquired?
D-1 NW-14			negligible	No
D-2 Open Water, Wetland, and Stream Mitigation Credits		Impacts to open waters, wetlands, and streams will be mitigated by purchasing credits from an approved mitigation bank. 2,724.6 stream credits for 629 linear feet of stream impacts. 0.3 wetland credits for 0.06 acre of wetland and open water impacts	\$270,000	No
D-3 Notice of Intent (NOI) for NPDES		The Office of Bidding Administration and Construction Contractor will submit a NOI to the NPDES General Permit following award of the contract but prior to construction.	negligible	Will be acquired following letting

**E. Other Commitments or Requirements (Status: Pre- and Post. - Complete or Incomplete; During - Signature Req'd)**

Pre-, During, or Post	Commitment	Responsible party	Est. Cost	Status
E-1 Pre	All areas designated as ESA shall have orange fencing around property boundaries as shown on project plans.	Tennille/Design	N/A	Incomplete
E-2 Pre	Inspect culverts for migratory birds	Construction Contractor	negligible	Construction or Area Engineer signature required
E-3 Pre	Purchase of mitigation credits	OES	\$270,000	Incomplete
E-4 Pre	Survey for Georgia Aster - GDOT will coordinate with agencies such as FHWA and USFWS to determine eligible survey sites, conduct surveys, and relocate any identified species.	OES	negligible	Incomplete
E-5 During	A NPDES permit shall be required for this project. The permit shall be acquired by the construction contractor following award of the contract but prior to the start of construction.	Office of Bidding Administration / Construction Contractor	negligible	Construction or Area Engineer signature required
E-6 During	All areas designated as ESA shall have orange fencing.	Tennille/Design	N/A	Construction or Area Engineer signature required.
E-7 During	UST's and Hazardous Waste Sites: Any contaminated soil excavated during construction activities must be disposed of at a permitted lined municipal solid waste	Construction Contractor	N/A	Construction or Area Engineer signature

**ENVIRONMENTAL COMMITMENTS TABLE**

PI#: 250470 | County: Columbia | Date Updated: 09/12/2013 | Stage: Reevaluation for Let Certification  
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		landfill. All activities associated with UST sites will be in accordance with GDOT Standard Specifications, Provision 217. (with particular attention to, but not limited to the following parcels: 4, 14, 22, 29, 108, 111)		required.
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**Total Estimated Cost**    \$619,600

*If Project is Complete or Under Construction, Area or Construction Engineer affirms that all Special Provisions, Plan Notes and During Construction Commitments were or are being adhered to during the project's construction.*

Please Print Name and Title: \_\_\_\_\_ Signature: \_\_\_\_\_ Date: \_\_\_\_\_ Please provide an explanation if unable to sign.

**DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA**

**SPECIAL PROVISION**

**PROJECT: STP00-7063-00(001) COLUMBIA COUNTY  
P.I.# 250470**

**Section 107 – Legal Regulations and Responsibility to the Public**

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*Add the following to Subsection 107.23:*

**G. Protection of Federally Protected Species**

The following conditions are intended as a minimum to protect these species and its habitat during any activities that are in close proximity to the known location(s) of these species. When there is a conflict between the General Provisions and the Special Provisions, these Special Provisions will govern the work.

1. The Contractor shall advise all project personnel employed on this project about the potential presence and appearance of the federally protected barn swallow (*Hirundo rustica*), cliff swallow (*Petrochelidon pyrrhonota*), and eastern phoebe (*Sayornis phoebe*). All personnel shall be advised that there are civil and criminal penalties for harassing, harming, pursuing, hunting, shooting, wounding, killing, capturing, or collecting these species in knowing violation of the Migratory Bird Treaty Act of 1918. Pictures and habitat information will be provided to the Contractor at the preconstruction conference and shall be posted in a conspicuous location in the project field office until such time that Final Acceptance of the project is made.
2. Any construction activity that would disturb the underside of the bridge at STA 141+75 (Old Petersburg Road over Reed Creek/Stream 2) shall take place outside of the breeding and nesting season of phoebes and swallows, which begins April 1 and extends through August 31, unless exclusionary barriers are put in place to prevent birds from nesting. Any construction activity that would extend or replace box culverts at STA 206+50 (Stream 1) shall take place outside of the breeding and nesting season of phoebes and swallows, which begins April 1 and extends through August 31, unless exclusionary barriers are put in place to prevent birds from nesting. The following steps shall be followed if exclusionary barriers are to be used:
  - a. Exclusionary barriers shall be installed on bridges and culverts prior to March 1 or after August 31, but in no time in between this period.
  - b. The underside of the bridge or the inside of the box culvert shall be checked for nests prior to the placement of exclusionary barriers. If nests are present, the nest shall be checked to ensure that eggs or birds are not present. If the nests are found to be occupied by birds or eggs, the installation of exclusionary barriers shall be postponed until after August 31 when the breeding season is complete.
  - c. Prior to the installation of any exclusionary barriers, the project ecologist shall be notified by phone of the type of barrier and the proposed date of installation at (404) 631-1100.
  - d. For box culverts, exclusionary barriers may consist of overlapping strips of flexible plastic (also called "PVC Strip Doors" or "Strip Curtains") or an alternate material proposed by the Contractor and approved by the Project Engineer prior to installation.

- e. For bridges, exclusionary barriers may be netting made of plastic, canvas or other materials that are proposed by the Contractor and approved by the Project Engineer. The barriers shall cover the full length of the bridge to prevent the birds from accessing any existing nesting habitat.
  - f. If the exclusionary barrier fails to prevent nesting (i.e., birds are able to bypass barriers and build nests), postpone construction activities associated with the bridge until after August 31.
  - g. During construction activities, inspect exclusionary netting for holes or other defects that impair the netting's ability to exclude phoebes or swallows from inhabiting the bridge. Any holes or defects shall be repaired immediately.
3. In the event any incident occurs that causes harm to the barn swallow, cliff swallow, and eastern phoebe along the project corridor, the Contractor shall report the incident immediately to the
- a. Project Engineer
  - b. Glenn Bowman, State Environmental Administrator, Georgia Department of Transportation, Office of Environmental Services at (404) 631-1101.

All activity shall cease pending consultation by the Department with the U. S. Fish and Wildlife Service and the Federal Highway Administration.

4. The Contractor shall keep a log detailing any sightings or injury to barn swallows, cliff swallows, and eastern phoebes in or adjacent to the project until such time that Final Acceptance of the project is made. Following project completion, the log and a report summarizing any incidents involving these species shall be submitted by the Contractor to the
- a. Project Engineer
  - b. the State Environmental Administrator (Georgia Department of Transportation, Office of Environmental Services, 600 West Peachtree Street NW, Atlanta, Georgia 30308).

The GDOT Office of Environmental Services shall provide a copy of the report to the U.S. Fish and Wildlife Service and the Federal Highway Administration.

5. All costs pertaining to any requirement contained herein shall be included in the overall bid submitted unless such requirement is designated as a separate Pay Item in the Proposal.

## Eastern phoebe *Sayornis phoebe*

### Identification Tips:

Length: 5.75 inches

Dark bill

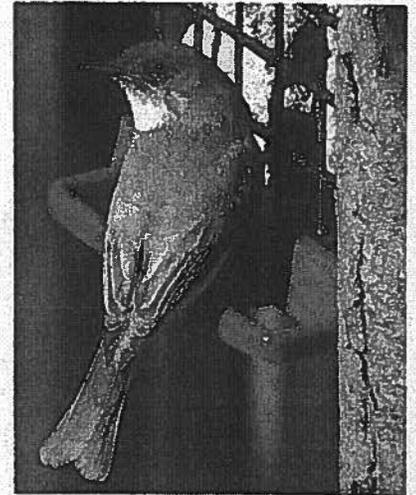
Pale throat and underparts – whitish in Spring, yellowish in Fall

Grayish – olive upperparts

Frequently wags tail

Juvenile has buffy wing bars

Often builds nest under bridges or around buildings near water



The Phoebe nest is a cup of mud and moss lined with grasses, hair, and feathers.



## Cliff swallow *Petrochelidon pyrrhonota*

### Identification Tips:

Length: 5-6 inches

Tiny bill

White forehead; dark chestnut throat

Dull steel-blue upperparts

Buff-white underparts

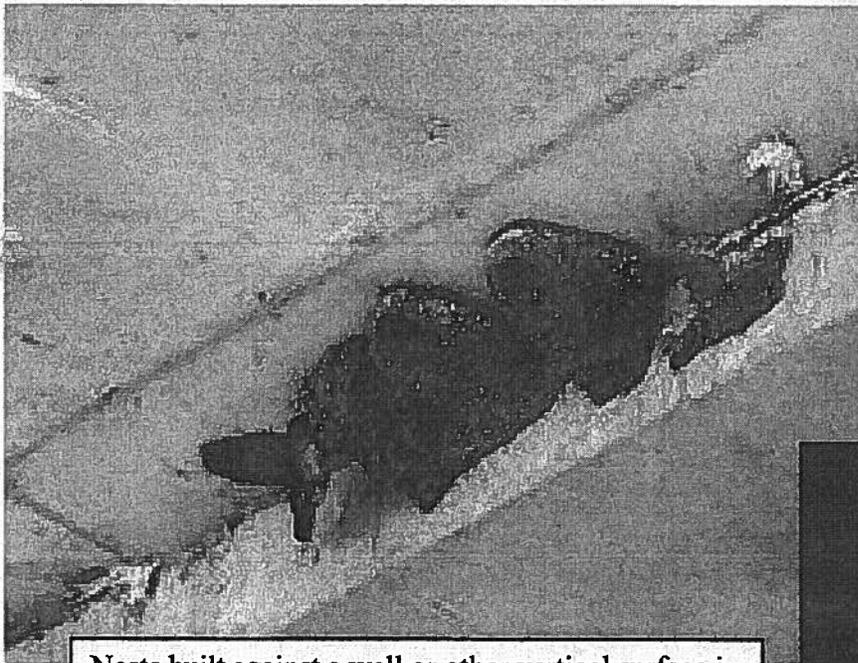
Stocky, square tail

Most often seen flying

Will nest communally in mud nests under bridges, in barns and caves, etc

### Similar species:

White forehead and tawny rump are distinctive in flight.



Nests built against a wall or other vertical surface in a gourd shape. Nests measure about 8 inches long, 6 inches wide and 4.5 inches high, with walls about 1/2 inch thick. The entrance, which is sometimes elongated into a tube is about 1.7 inches high and 2 inches high. Nests have a mud/grass shelf and cup lined with grass and feathers.



Juveniles are brown above, buff below, and have varying numbers of small white spots on their foreheads and throats

## Barn swallow *Hirundo rustica*

### Identification Tips:

Length: 6 inches

Tiny bill

Dark orange forehead and throat

Pale orange underparts

Dark, iridescent upperparts

Long, deeply forked tail

Juvenile similar to adult but paler underneath with a shorter tail

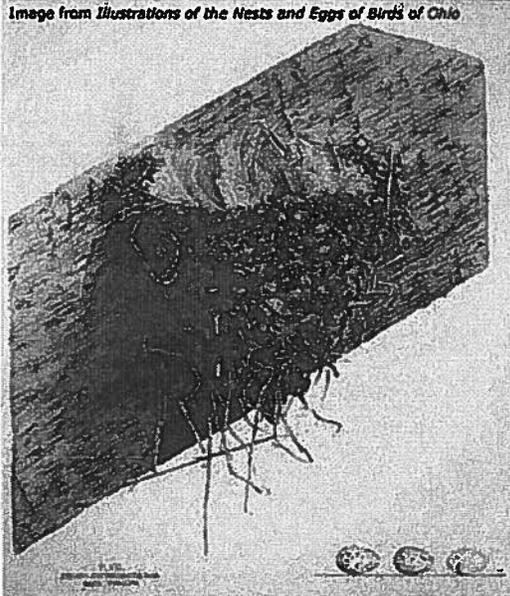
Most often seen flying

Will nest communally in mud nests under bridges, in barns and caves, etc

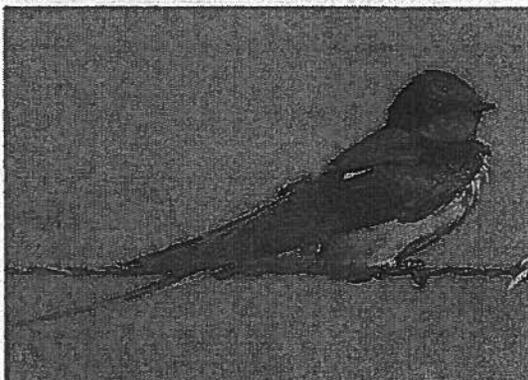


### Similar species:

The Barn Swallow can be told from all swallows by its deeply forked tail.



Nests built against a wall or other vertical surface in a half-cup, semicircular shape. Nests built on top of a beam or other horizontal surface form a complete cup about 3 inches across at the rim and 2 inches deep. Nests have a mud/grass shelf and cup lined with grass and feathers.



**DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA**

**SPECIAL PROVISION**

**PROJECT: STP00-7063-00(001)**

**COUNTY: COLUMBIA**

**P.I. NO: 250470-**

**SECTION 150 – TRAFFIC CONTROL**

*Add the following to Section 150:*

**150.11 SPECIAL CONDITIONS:**

- A. The contractor shall not install lane closures, flag or pace traffic, detour, or move equipment or material on the travel ways that interferes with traffic as follows:

1. Washington Rd (SR 104)  
Monday through Thursday: 7:00 am – 8:30 am & 3:30 pm - 7:00 pm  
Friday: 7:00 am – 8:30 am and 3:00 pm – 8:00 pm  
No weekend restrictions
2. Riverwatch Pkwy/Old Evans/ Petersburg Rd/ Blue Ridge Rd  
Monday through Friday: 7:00 am- 8:30 am & 3:30 pm-7:00 pm  
No weekend restrictions

Failure to comply with this directive will result in the assessment of Liquidated Damages in accordance with Special Provision 108.08.C.1

- B. Phase 1 construction shall be performed such that Columbia Industrial is closed no more than 30 calendar days. The contractor shall coordinate their work such that the closing of Columbia Industrial does not occur with any other road closures. The closing of Old Evans Road and Blue Ridge Road during this time is not permitted. The contractor shall submit their detour plan for review and approval 30 days prior to the closure. Portable Changeable Message Signs shall be incorporated into the TTC Detour plan to notify the public 14 days prior to the closure.

Failure to comply with this directive will result in the assessment of Liquidated Damages in accordance with Special Provision 108.08.C.2

- C. The intersection of Old Evans/Petersburg Rd/ Riverwatch Pkwy/Blue Ridge may be closed for one weekend beginning at 7:00 pm Friday to 7:00 am Monday for the tie in of the intersection. This work shall not interfere with any major events associated with Lakeside Middle/High Schools. The contractor shall submit a TTC detour plan that includes the date and time for the intersection closure to the engineer at least 30 days prior to the closure. Portable Changeable Message Signs shall be incorporated into the TTC Detour plan to notify the public 14 days prior to the closure.

# DEPARTMENT OF TRANSPORTATION STATE OF GEORGIA

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## ENVIRONMENTAL REEVALUATION

### I. GENERAL INFORMATION

**Project No.** STP00-7063-00(001)  
**P.I. No.** 250470  
**County** COLUMBIA  
**STIP/TIP No.** RC07-000017  
**Funded Years** Right-of-Way: 2002, 2004, 2005, 2007; Construction: 2014  
**Funding Codes** Right-of-Way: Q23, L230S, Q24; Construction: LY10S, LY20S, TIA, M240  
**Project Name** Old Petersburg Rd/Old Evans Fm Baston Rd to Washington Rd  
**Project Limits** The proposed project begins in a commercial area on Washington Road, branches off on new location, and continues through residential area along Old Evans Road to Old Petersburg Road and ends at Baston Road.

### II. DESCRIPTION OF PROJECT IN ORIGINAL ENVIRONMENTAL DOCUMENT:

#### A. Existing Facility

Old Petersburg and Old Evans Road are two lane (one in each direction) urban roads with both curb and gutter, and urban shoulders.

#### B. Proposed Project

The project consists of the widening and improving of Old Petersburg Road and Old Evans Road. The proposed concept would consist of a four-lane (two lanes in each direction) roadway with bike lanes and a 20-foot raised median on a minimum of 150-feet of right-of-way. The roadway would have urban shoulders including curb and gutter, and sidewalks. The widening of Old Petersburg Road would start on existing location beginning approximately 1,400 feet west of the intersection of Baston Road and extending to the intersection with Old Evans Road. From this point, the project would follow Old Evans Road in a northwesterly direction on existing location to Columbia Industrial Boulevard and then extend westward on new location to tie into Washington Road at the intersection with Town Center Drive and Washington Road. A new bridge would be constructed to grade separate the roadway over the CSX Railroad. The project length would be approximately 2.92 miles.

#### C. Changes Documented in Previous Reevaluations

An Environmental Reevaluation was completed on 04/12/2004 discussing impacts to Streams 1, 2, 3, and 4. Avoidance and minimization procedures as well as necessary mitigation were detailed. The need for an Individual Section 404 Permit was anticipated.

**III. TYPE OF ENVIRONMENTAL DOCUMENT: EA/FONSI**

Actions Requiring Concurrences Prior to Environmental Document or Reevaluation Approval	YES	N/A	If Yes, Date of Concurrence
Section 106/Assessment of Effects	<input checked="" type="checkbox"/>	<input type="checkbox"/>	4/30/1999
Section 106/Memorandum of Agreement	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Individual Section 4(f) Evaluation	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
<i>De Minimis</i> Acknowledgment/Requirements	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Protected Species/No Effect	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
Protected Species/Section 7 Consultation with USFWS	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Protected Species/Section 7 Consultation with NMFS	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Essential Fish Habitat Coordination with NMFS	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
FWCA/USFWS Coordination for Longitudinal Stream Encroachments, Existing Culvert Extensions (+100 feet), or New Culvert Construction	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
PM <sub>2.5</sub> Interagency Concurrence	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
USCG Navigable Water Determination	<input type="checkbox"/>	<input checked="" type="checkbox"/>	

**IV. FHWA DOCUMENT APPROVAL DATE: 6/3/2002**

**V. DATE(S) OF PRIOR REEVALUATION(S): 4/12/2004**

**VI. HAS PROJECT, PROJECT LIMITS, OR ROW/EASEMENTS CHANGED SINCE THE LAST APPROVAL: Yes**

**VII. DESCRIPTION OF PROJECT, PROJECT LIMITS, OR ROW CHANGES AND WHY CHANGES WERE MADE:**

The project description has changed to reflect a more accurate measurement of the project length due to the availability of plan level details: "The project length would be approximately 3.10 miles."  
 The construction limits at Sydney Street intersection have been reduced resulting in a decrease in required ROW. After an in depth geometric review, it was determined that the profile could tie-in to the existing terrain at the radius of return. This essentially eliminated the previous design which extended the proposed profile approximately 240 ft.  
 The construction limits at Briarwood Drive intersection have been reduced resulting in a decrease in required ROW. After an in depth geometric review, it was determined that the profile could tie-in to the existing terrain at Station range 20+00 to 21+70 (previously 20+00 to 22+14.02), shortening the limit of construction 44 ft.  
 The property owner of parcel 150 requested a change in easement from temporary to permanent. ROW office approved and communicated change to design. The result is an increase in permanent ROW of 0.093 acre and a decrease in temporary easement of the same 0.093 acre.

**VIII. HAVE THE ENVIRONMENTAL STUDIES BEEN UPDATED SINCE THE LAST PROJECT APPROVAL, AND IF SO, WHY: Yes, because there have been changes to the project based on changes made during ROW negotiations and final design**

**IX. HAVE THERE BEEN ANY CHANGES TO OR ADDITIONAL RESOURCES IDENTIFIED WITHIN THE AFFECTED ENVIRONMENT: Yes. See Att. 1 for the history of changes.**

**X. REVIEW OF EFFECTS**

"Yes" or "No" denotes whether effects to environmental resources have changed as a result of project changes or changes in the effected environment.

A. SOCIAL ENVIRONMENT	INVOLVEMENT UNDER PREVIOUS APPROVAL?		HAVE EFFECTS CHANGED SINCE LAST REEVALUATION?		REMARKS OR REFERENCE TO ATTACHMENT
	YES	NO	YES	NO	
1. Land Use Changes	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
2. Community Impacts	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	1,2
3. Relocation Potential	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	1,2
4. Churches and Institutions	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	1,4
5. Parks/Recreation Areas/Wildlife Refuges	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	1
6. Title VI/E.O. 12898	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
7. Public Controversy Potential	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
8. Public Involvement	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	1,2,4
9. Economic Impacts	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
10. Other	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	1

B. CULTURAL ENVIRONMENT	INVOLVEMENT UNDER PREVIOUS APPROVAL?		HAVE EFFECTS CHANGED SINCE LAST REEVALUATION?		REMARKS OR REFERENCE TO ATTACHMENT
	YES	NO	YES	NO	
1. Historic Sites	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
2. Archaeological Resources	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
3. Other	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	1,2

C. NATURAL ENVIRONMENT	INVOLVEMENT UNDER PREVIOUS APPROVAL?		HAVE EFFECTS CHANGED SINCE LAST REEVALUATION?		REMARKS OR REFERENCE TO ATTACHMENT
	YES	NO	YES	NO	
1. Waters of the U.S./State Waters	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	1,2,3
2. Water Quality/303(d) List	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
3. Wild and Scenic Rivers	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
4. Essential Fish Habitat	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
5. Farmland	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
6. Floodplains	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
7. Protected Species	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	1,2,3
8. Invasive Species	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	1,2
9. Wildlife and Habitat	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	1,2
10. Other	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	1

D. PHYSICAL ENVIRONMENT	INVOLVEMENT UNDER PREVIOUS APPROVAL?		HAVE EFFECTS CHANGED SINCE LAST REEVALUATION?		REMARKS OR REFERENCE TO ATTACHMENT
	YES	NO	YES	NO	
1. Noise	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	1,2,3
2. Climate Change	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	1
3. Air	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	1,2,3
4. Energy/Mineral Resources	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
5. Construction/Utilities	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	1
6. USTs	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	1,2
7. Hazardous Waste Sites	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	1,2
8. Other	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	1

E. PERMITS/VARIANCES/ COMMITMENTS REQUIRED	INVOLVEMENT UNDER PREVIOUS APPROVAL?		HAVE EFFECTS CHANGED SINCE LAST REEVALUATION?		REMARKS OR REFERENCE TO ATTACHMENT
	YES	NO	YES	NO	
1. U.S. Coast Guard Permit	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	1
2. Forest Service/Corps Land	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
3. CWA Section 404 Permit	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	1,2,3
4. Tennessee Valley Authority Permit	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
5. Buffer Variance	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	1,2,3
6. Coastal Zone Management Coordination	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
7. NPDES	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	1
8. Cemetery Permit	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
9. Other Permits	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
10. Other Commitments	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	See Green Sheet

F. SECTION 4(f) APPLICABILITY	INVOLVEMENT UNDER PREVIOUS APPROVAL?		HAVE EFFECTS CHANGED SINCE LAST REEVALUATION?		REMARKS OR REFERENCE TO ATTACHMENT
	YES	NO	YES	NO	
1. <i>De Minimis</i>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
2. Programmatic	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
3. Individual	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
4. Section 6(f) Applicability	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
5. Other	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	1

**XI. NEED FOR PUBLIC INVOLVEMENT:**

- A Public Information Open House was held on 05/14/2013
- There have been no changes in the project design or environmental effects that would require a Public Information Open House.

**XII. FINDINGS/CONCLUSIONS**

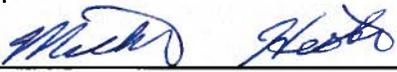
- Based on the analysis contained in this reevaluation, it has been determined that the changes in project design and/or environmental effects would not significantly alter the conclusions reached in the approved environmental document and/or previous reevaluations.
- There have been no changes in the design/ROW of this project nor have there been changes in project effects or the affected environment. Therefore, the conclusions reached in the approved environmental document and/or previous reevaluations remain valid.

PREPARED BY:

  
\_\_\_\_\_  
Sean Diehl  
Nepa Analyst  
Georgia Department of Transportation

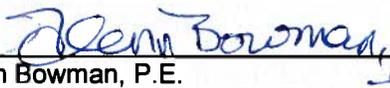
9/12/13  
\_\_\_\_\_  
Date

REVIEWED BY:

  
\_\_\_\_\_  
Michael Hester  
NEPA Team Leader  
Georgia Department of Transportation

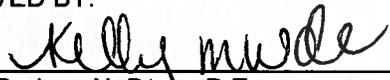
9/12/13  
\_\_\_\_\_  
Date

CONCURRED BY:

  
\_\_\_\_\_  
Glenn Bowman, P.E.  
State Environmental Administrator  
Georgia Department of Transportation

Sept 12, 2013  
\_\_\_\_\_  
Date

APPROVED BY:

  
\_\_\_\_\_  
for Rodney N. Barry, P.E.  
Division Administrator  
Federal Highway Administration

9/13/13  
\_\_\_\_\_  
Date

Purpose for Reevaluation: Change for Let October 2013

Attachments appear in the following order:

1. Effects Evaluation
2. Correspondence
3. Report Coordination
4. Public Information Open House Materials

**DEPARTMENT OF TRANSPORTATION**

**STATE OF GEORGIA**

**Project STP00-7063-00(01)**

**Columbia County**

**PI No. 250470**

**Reevaluation**

**Attachment 1**

**EFFECTS EVALUATION**

**ENVIRONMENTAL REEVALUATION**  
**ATTACHMENT (1): EFFECTS EVALUATION**  
**PROJECT NO. STP00-7063-00(001)**  
Columbia **COUNTY**  
**PI NO. 250470**

**A. SOCIAL ENVIRONMENT**

**A.2. Community Impacts**

Efforts to minimize community impacts in terms of road closures and detours were carefully planned in construction phasing allowing GDOT to maintain traffic on all roads for most of the construction process. However, brief road closures (no more than 30 days at Columbia Industrial Blvd. and one weekend at Old Evans Road and Blue Ridge Road) will be necessary to complete elements of construction phases.

The road closures and detour (see detour map on page 3, Figure 1) were presented to the public in a Public Information Open House (PIOH) on May 15, 2013 (see section A.8 Public Involvement on page 6 below). Specifics and restrictions of the road closures and detour are detailed in Special Provision 150.11 (Attachment 2) and are summarized below.

No lane closures, flag or traffic pacing, or movement of equipment or material on the travel ways can interfere with morning and evening weekday commuting hours listed (see SP 150.11, Attachment 2). The special provision also stipulates the use of Portable Changeable Message Signs to notify the public 14 days prior to any closure.

Phase I construction shall be performed such that Columbia Industrial Blvd. will need to be closed. The road closure will not occur for more than 30 calendar days and will not occur with any other road closures (the closing of Old Evans Road and Blue Ridge Road during this time is not permitted).

The detour would direct traffic around the intersection via Washington Road, Evans to Locks Road, Blue Ridge Drive and North Belair Road. Local traffic will be able to enter and exit; however, normal routes of travel maybe altered depending on destination resulting in small increases in travel distances and times for the weekend of the detour. Thru traffic traveling Old Evans Road between Blue Ridge Drive and North Belair Road would experience a detour length of approximately 2.6 miles. Local traffic would experience detours ranging from 1.1 mile to 3.9 miles depending on point of origin in the neighborhood and destination. 3.9 miles represents the

maximum detour length possible, traveling from one side of the road closure on Old Evans Road to the other side on Old Evans Road via the detour route and is not expected to be typical.

The intersection of Old Evans/Petersburg/Riverwatch Pkwy/Blue Ridge may be closed for one weekend for the tie in of the intersection. The work shall only occur on non-event weekends as described in detail in the special provision to include major events associated with the local schools, the week of the Masters Golf Tournament, and the Columbia County Christmas Parade.

This represents an unavoidable, yet minor, disturbance to the neighborhoods adjacent to the proposed closures. Given the short distance of detour, short proposed time interval of closures, level of public communication, and proactive planning the detour is not expected to impact or disrupt emergency services (see Attachment 2) or public school routes (see Attachment 2). The effects to the community are therefore anticipated to be minor, being those of slight inconvenience in travel route.

### **A.3. Relocation Potential**

Since 2001, GDOT has executed 30 relocations: 18 owner occupied residences, 6 tenant occupied residences, 3 businesses, and 3 non-resident occupied owners. The non-resident occupied owners included two estates of deceased property owners and 1 landlord of a tenant occupied residence. The 2002 EA/FONSI summarized 34 relocations. The reduction in relocations are a function of tenant occupation at time of purchase and changes in occupation type from the original EA/FONSI to time of GDOT purchase and relocation and are outlined below.

Several of the tenant occupied residences and businesses were not occupied at the time of purchase, so there were no relocations involved. Also, a few of the owner occupied residence relocations became non-resident occupant (NRO) relocations: 2 due to the death of occupants (the relocations being that of the belongings of the deceased on behalf of the estate executor) and 1 the result of a property becoming a tenant occupied residence (a tenant resident relocation) with a NRO going to the owner. It also seems a couple of the previously listed owner occupied residences had since become tenant properties and became tenant relocations.

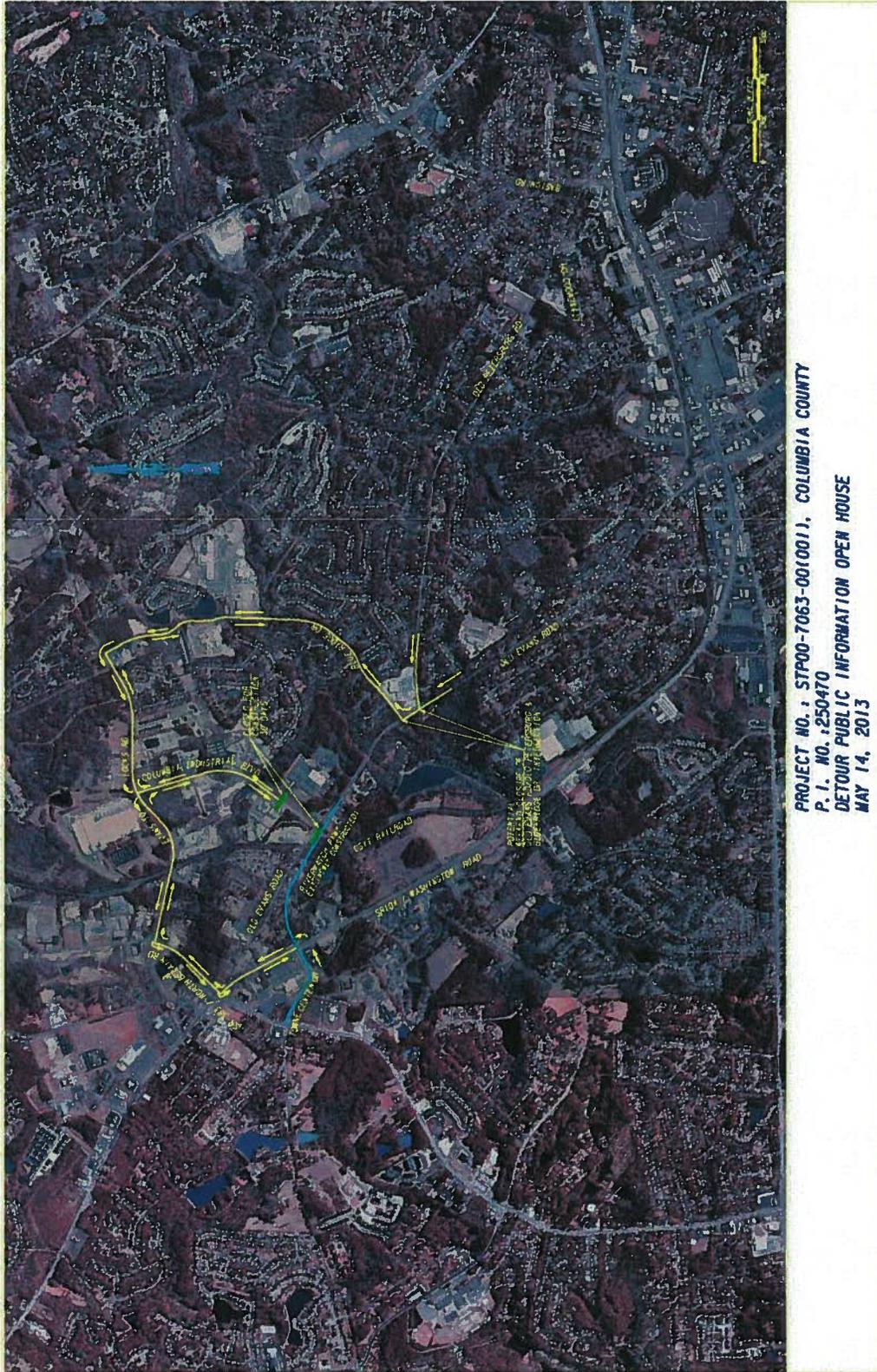


Figure 1

**A.4. Churches and Institutions**

The original 2002 EAFONSI detailed five churches and one school. Following is a discussion of the changes in potential effects to those churches and school as well as effects to newly identified churches in the proposed project area.

The following table (Table 1 Churches and Institutions) lists churches and institutions to which parking and use of facility would not be adversely affected. All driveways will be extended to reach proposed road alignments. Access to each facility would be changed due to the implementation of the 20-ft. median, causing the entrance to be right-in, right-out only. However, the addition of bike lanes and sidewalks would make cycling and walking a safe and viable option for travel for the local community visiting these churches and institutions.

**Table 1 Churches and Institutions**

Name	Address	Required ROW (sq. ft.)	Temporary Easement (sq. ft.)	Permanent Easement (sq. ft.)	Comment
First Church of Christ Scientist Augusta Georgia	467 Old Evans Road	none	2,697.8	None	Design note: "stay off trees"
Islamic Society of Augusta Inc.	465 Old Evans Road	9,848.85	None	20,618.58	None
Bible Fellowship Church	3701 Old Petersburg Road	10,298.17	None	6,773.44	None
Martinez Baptist Church	3632 Lynnwood Drive	2,639.35	1,717.49	None	None
Central Church of Christ	3650 Old Petersburg Road	4,151.46	None	6,803.01	Design refinements have changed the ROW from the front of the grounds from 13 ft. to a tapering width of 18 ft. to 8 ft. By using header curbs, all original parking is preserved.
West Augusta Primitive Baptist Church	3545 Old Petersburg Road	17,117.63	None	6,439.2	Original ROW in front of property ranged from 13 ft. to 25 ft. ROW has increased to range from 20 ft. to 30 ft. No impact is still anticipated despite the slight increase.

Name	Address	Required ROW (sq. ft.)	Temporary Easement (sq. ft.)	Permanent Easement (sq. ft.)	Comment
Cathedral of Praise	3670 Old Petersburg Road	1,804.06	5,972.19	None	None
Prince of Peace Lutheran Church	3707 Old Petersburg Road	7,329.02	9,839.04	None	Design refinements have reduced required ROW from approximately 25 ft. to approximately 21 ft.
The First Baptist Church of Evan	515 N Belair Road	3,674.31	13,836.5	None	The need for removal of a building was discussed in the 2002 EA/FONSI on the property; however, the building is no longer present. GDOT did not acquire nor remove the building; therefore, it seems the church had other reasons for its removal in the elapsed time.
Keystone Prep School	3765 Old Petersburg Road	12,527.24	None	4,334.21	Design refinements have reduced the required ROW in the front of the school from approximately 25 ft. to approximately 11 ft.

**Mosaic Methodist Church 478 Columbia Industrial Blvd.**

A concern was voiced at the Public Information Open House held on May 14, 2013 (see section A.8 below, Attachment 4) about possible short term impacts to the Mosaic Methodist Church located on Columbia Industrial Boulevard during the detour. There is no anticipated disruption to access facility's access to Evans to Locks Road<sup>1</sup>. There is also no anticipated communication hurdles to members regarding the detour given the, the proposed early signage indicating the detour, typical organized church methods of communication to congregations (such as pre-service announcements, service bulletins, church newsletters), and their church website with a link to a church calendar and an alternating image announcements banner on the homepage. Since this church is located approximately 0.2 mile north of the project limits of

---

<sup>1</sup> Evans to Locks Road is a substantial east/west traffic artery to the north of the proposed project and temporary road closures.

construction and access will be maintained by major road routes, primarily Evans to Locks Road, no other short term or long term impacts are expected.

**In Focus Church 562 Old Evans Road**

This property is located outside of the limits of construction, therefore no ROW is required. The proposed intersection closure at Old Evans Road and Columbia Industrial Blvd. will pose an inconvenience to members, requiring use of detours, but will not prevent access. Additionally, Old Evans Road south of the church will dead end as the proposed project will tie Old Evans Road into Washington Road south of the property. Members accustomed to accessing the facility via Old Evans Road from the south will instead travel north on the proposed Riverwatch Parkway to Washington Road and then travel north on Washington Road to access Old Evans Road to the east of the church. This does not represent an adverse change in access, rather the improved function of the proposed road, in the form of bike lanes, sidewalks, and vehicle flow, is anticipated to benefit members.

**A.5. Parks/Recreation Areas/Wildlife Refuges**

**Kelly Park 3683 Old Petersburg Road**

This facility is owned by a private organization, the Community Club of Martinez, Inc., and is used for little league baseball. Approximately 10,015.21 sq. ft. of ROW and 8,780.98 sq. ft. of permanent easement for construction are required of the property. Parking and use of the facility would not be adversely affected as the parking and baseball fields are located at the far rear of the property and the ROW and easements are road side. Access to the facility would be changed at the entrance due to the implementation of the 20-ft. median, causing the entrance to be right in, right out only; however, the addition of bike lanes and sidewalks would make walking and cycling to the park another safe and viable option for nearby community members.

**A.8. Public Involvement**

A Public Information Open House was held on May 15, 2014 to discuss noise walls and a detour. The public was given an opportunity to view maps illustrating the detour and locations of noise walls for comment. The details of the noise walls and relevant public involvement are discussed in section D.1. below. The details of the detour were previously discussed in section A.2. A synopsis of the event is recorded in a synopsis memo (see Public Information Open House Synopsis Memo in Attachment 4) and comments regarding the detour are discussed in this section.

The meeting was well attended with 78 attendees. Response was positive overall with no negative responses received. 11 comments were documented, 9 by comment card and 2 by Court Reporter. Of the 9 comment cards received, 8 are "for" the proposed detour and 1 "conditional." Neither of the Court Reported comments discussed the detour. Comments were collectively addressed in a response letter and sent to attendees who left addresses (see PIOH Response Letter in Attachment 4).

All comments expressed that their questions were answered by GDOT personnel and that they understood the detour after having attended the meeting. Further, 6 of the comments specifically articulated, in their own words, how well they thought the open house was operated and the information presented.

Three concerns arose as a result of the open house. One concern was voiced by two individuals about the short term impact to the Mosaic Methodist Church located on Columbia Industrial Boulevard, presumably during the detour. Access to the church will be maintained via Evans to Lock Road and no impact is anticipated. See section A.4. Churches and Institutions on page 4 for discussion.

Another comment raised a concern with regard to the maintenance of acquired ROW properties and the observed frequency of accidents, particularly at the corner of Old Petersburg Road and McCormick Road as well as the corner of Old Evans Road and Petersburg Road. "There have been numerous accidents and it has become a dumping site." GDOT is not aware of other complaints regarding the maintenance of the mentioned properties. Columbia County is responsible for maintenance of the existing roadway and the purchased properties. They have been informed of this concern via copy of the response letter mentioned above (see Attachment 4). The concern over intersection safety was shared by another comment and is discussed below.

The third concern is for the traffic and safety at the intersection of Old Petersburg Road and McCormick Road. The comment expressed a desire for traffic lights at the intersection. The proposed project will widen Old Petersburg Road from 2 lanes to 4 lanes with a raised median in this location which will provide additional capacity and contribute to the safety of the intersection.

**A.10. Other**

The 2002 EA/FONSI discussed land use changes, economic impacts, and Title VI (sectioned under Environmental Justice). Neither the changes to the project discussed in section VII of this Environmental Reevaluation, nor conditions in the social environment represent significant changes to the previous determinations of the 2002 EA/FONSI and do not require further discussion.

**B. CULTURAL ENVIRONMENT**

**B.3. Other**

The 2002 EA/FONSI discussed archaeological and historic consequences. Neither the changes to the project discussed in section VII of this Environmental Reevaluation, nor conditions in the cultural environment represent significant changes to the previous determinations of the 2002 EA/FONSI and do not require further discussion (see Attachment 2).

**C. NATURAL ENVIRONMENT**

The August 2013 Ecology addendum (see 2013 Ecology Addendum Executive Summary in Attachment 2) details the results of an updated ecology study. A field assessment was conducted in 2000 and an Ecology Report was completed in April 2004. Field surveys were conducted in September 2010 and March 2013. Following is a discussion of the findings in the addendum.

By use of bridges for crossings of Perennial Stream 2 (PS2) and Perennial Stream 3 (PS3), complete avoidance of impacts within these features would be achieved. Additionally, by use of orange barrier fencing (OBF), avoidance of impacts to Intermittent Stream 2b (IS2b) would also be achieved.

**C.1. Waters of the U.S./State Waters**

The September 2010 and the March 2013 ecology surveys resulted in the identification of six new jurisdictional features within the project corridor totaling five perennial streams, two intermittent streams, one ephemeral channel, one open water, and one wetland.

The 2004 report listed approximately 730 linear feet of stream impacts. Due to project modifications and identification of additional features, permanent impacts to jurisdictional streams will total 629 linear feet and 0.06 acre of wetland.

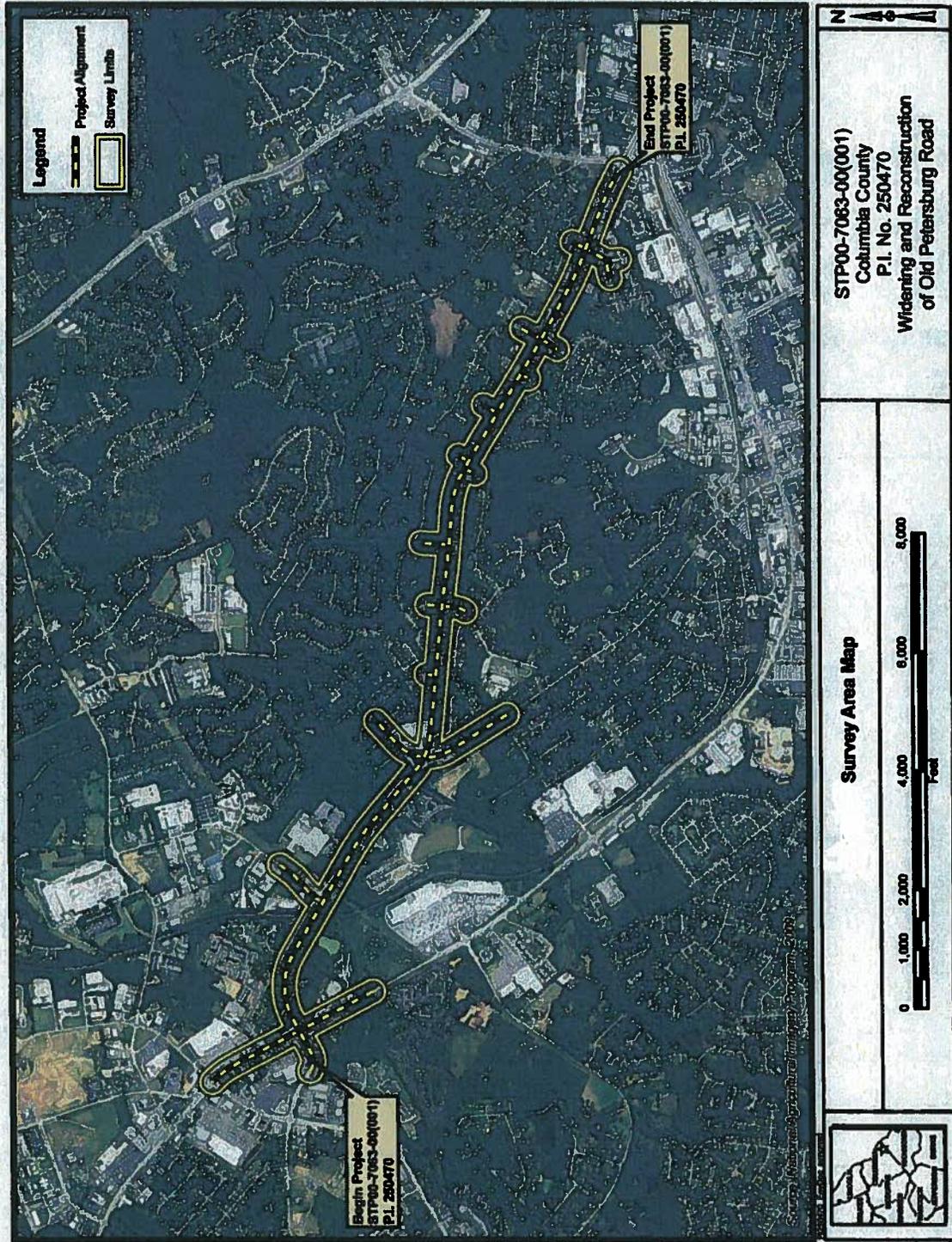


Figure 2

**Perennial Stream 1 – 139 linear feet (0.05 acre) of permanent impacts**

Description and Characteristics: Perennial Stream 1 (PS1, see Figure 3 on page 11) is a somewhat impaired perennial stream approximately one mile west of Baston Road. This feature flows through a double concrete box culvert under Old Petersburg Road.

Impacts, Avoidance and Mitigation: PS1 was addressed in the 2004 report with approximately 55 linear feet of anticipated impact by a culvert extension. It is now anticipated that approximately 139 linear feet of impacts would result from the proposed project since the existing eight-foot by eight-foot box culvert is being replaced with an eight-foot by six-foot triple box culvert rather than being a culvert extension as discussed in the 2004 EA/FONSI. Fish and Wildlife Coordination Act (FWCA) consultation is required for Stream PS-1. The U.S. Fish and Wildlife Service (USFWS) provided concurrence under FWCA on 09/06/2013. (see Attachment 2).

This stream is perpendicular to the existing roadway; therefore shifting the project alignment in any direction would not result in further minimization of impacts to this feature. Impacts are minimized by replacing the existing culvert in the same location.

**Perennial Stream 1a – 160 linear feet (0.009 acre) of permanent impacts**

Description and Characteristics: Perennial Stream 1a (PS1a, see Figure 3 on page 11) is a newly identified, somewhat impaired, perennial tributary to PS1. PS1a is a warm water stream located south of Old Petersburg Road and west of PS1. The PS1a and PS1 confluence is approximately 80 feet south of an existing box culvert under Old Petersburg Road.

Impacts, Avoidance and Mitigation: A portion of this stream will be relocated through a new 42-inch culvert under a residential driveway resulting in approximately 160 linear feet of permanent fill impacts. Fish and Wildlife Coordination Act (FWCA) consultation is required for Stream PS-1a. The U.S. Fish and Wildlife Service (USFWS) provided concurrence under FWCA on 9/06/2013. (see Attachment 2).

Due to the proximity of this feature to the project centerline, as well as the alignment of the feature, complete avoidance to impacts would require a shift of the project alignment to the northeast. Along the northern side of the existing roadway, several single family homes and cross roads are located, impeding an alignment shift without impacting family homes. This feature is crossed by a driveway that is required to provide access to a single-family home.

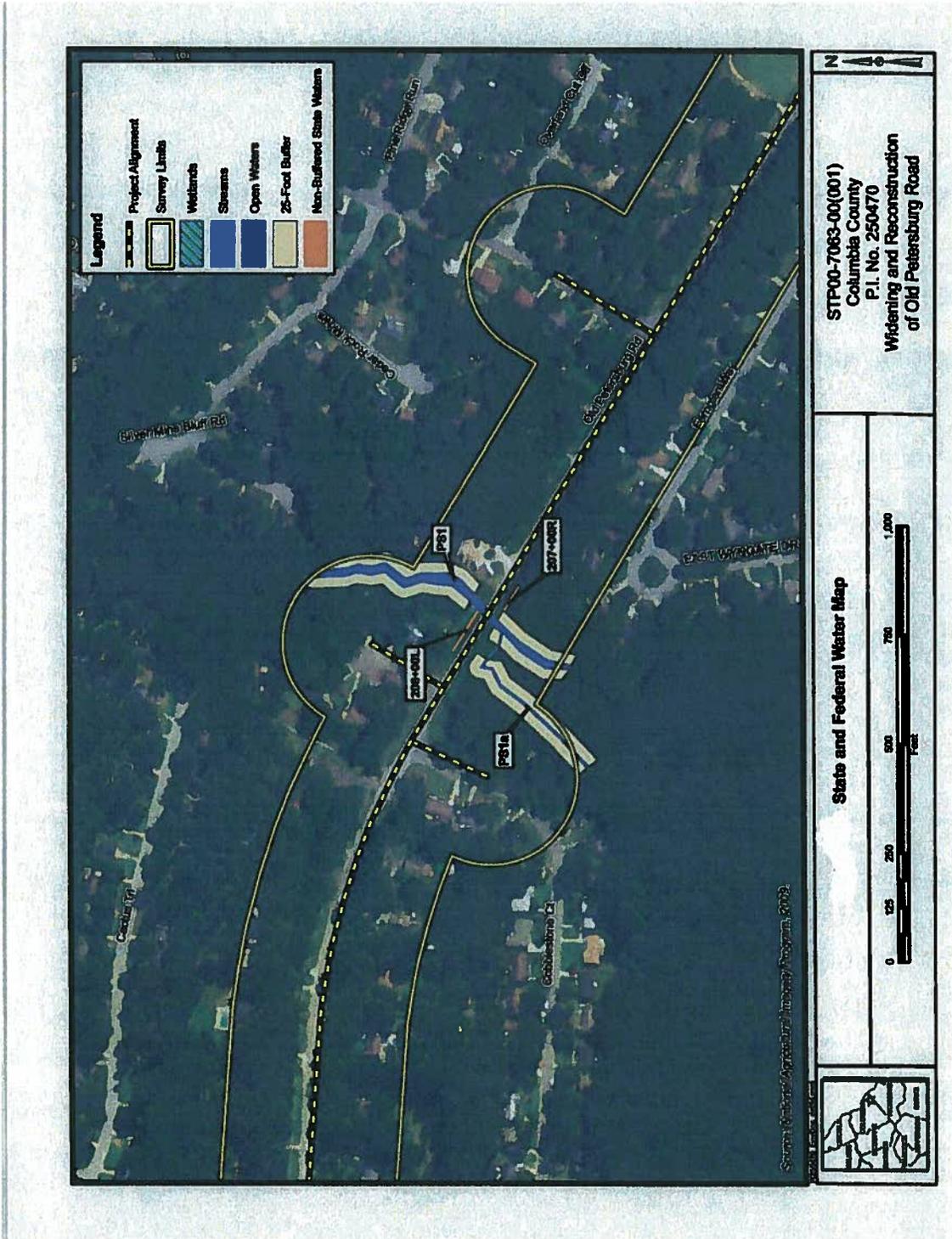


Figure 3

**Perennial Stream 2 (PS2, Reed Creek) - No Impact**

PS 2 (see Figure 4 on page 13) will be crossed by a clear span bridge and would not be impacted as a result of the proposed project. Perennial Stream 2 (PS2, Reed Creek) was addressed in the 2004 report and no change is represented from the 2004 reevaluation.

**Intermittent Stream 2a – 43 linear feet (0.003 acre) of permanent impacts**

Description and Characteristics: Intermittent Stream 2a (IS2a, see Figure 4 on page 13) is a newly identified, somewhat impaired intermittent stream. The upper limit of IS2a is located at a culvert under Blue Ridge Road, approximately 130 feet north of the intersection with Old Evans Road.

Impacts, Avoidance and Mitigation: Approximately 17 linear feet (0.001 acre) of permanent impacts associated with the extension of a culvert and 26 linear feet (0.002 acre) of permanent impacts from riprap will result from the proposed project. Impacts proposed are associated with the extension of an existing culvert, necessary to accommodate the required project ROW. This feature lies within a reconstruction section of the alignment within the intersection of Blue Ridge Drive and Petersburg Road. Modification to the intersection alignment would impact several residential and commercial establishments deeming this modification unfeasible.

**Intermittent Stream 2b – No Impact**

Intermittent Stream 2b (IS2b, see Figure 4 on page 13) is a newly identified stream; somewhat impaired, warm water, and intermittent. IS2b originates at a culvert within an apartment complex, south of Old Evans Road and exits the culvert into a large plunge pool. No impacts to this feature are anticipated as a result of project implementation.

**Ephemeral Channel 2c – 14 linear feet (0.0006 acre) of permanent impacts**

Description and Characteristics: Ephemeral Channel 2c (EC2c, see Figure 4 and Figure 5 on pages 13 and 14) is a newly identified ephemeral channel. This feature exhibits moderate channel definition and bed and bank profile; however, no groundwater contribution or baseflow was observed at the time of the survey. No flow was observed within the channel during the ecology surveys, but small pockets of water were observed throughout the channel, which would be attributed to the large rain event (over 2 inches) in the previous 48 hours.

Impacts, Avoidance and Mitigation: Approximately 14 linear feet (0.0006 acre) of impacts are proposed resulting from a culvert extension. Impacts proposed are associated with the extension of an existing culvert, necessary to accommodate the required project ROW. Shifting the project alignment south would result in increased impacts to PS2 and IS2b. Shifting the project alignment to the north would increase impacts to EC2c.

**Perennial Stream 3 – (PS 3) No Impacts**

PS 3 (see Figure 5 on page 14) will be crossed by a clear span bridge and would not be impacted as a result of the proposed project. PS 3 was addressed in the 2004 report and no change is represented from the 2004 reevaluation.

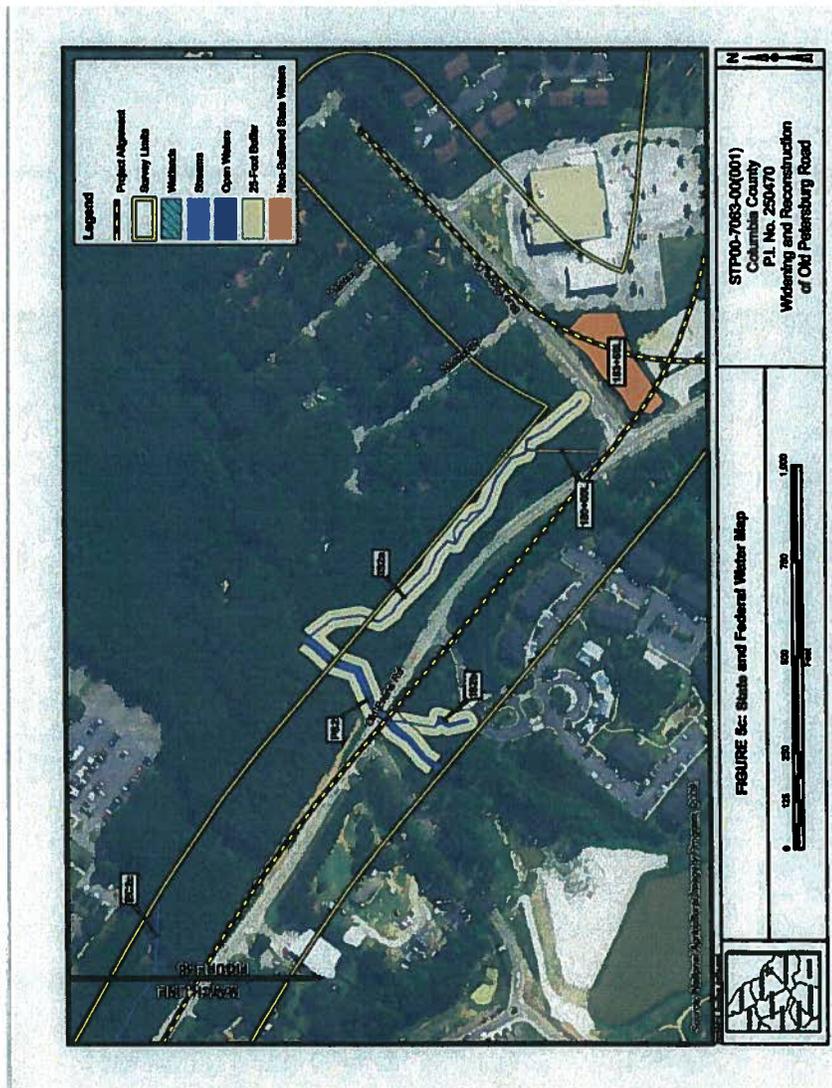


Figure 4



Figure 5

### **Open Water 3a – No Impacts**

Open Water 3a (OW3a, see Figure 6 on page 16) is a newly identified feature. This feature is a man-made, approximately 2.4-acres pond located south of Old Evans Road within a residential area, with an apparent recreational use. No impacts to this feature are anticipated as a result of the proposed project.

### **Perennial Stream 4 (PS 4) – 273 linear feet of permanent impacts**

Description and Characteristics: PS 4 (see Figure 6 on page 16) is a warm water, somewhat impaired, perennial stream located south of Old Evans Road.

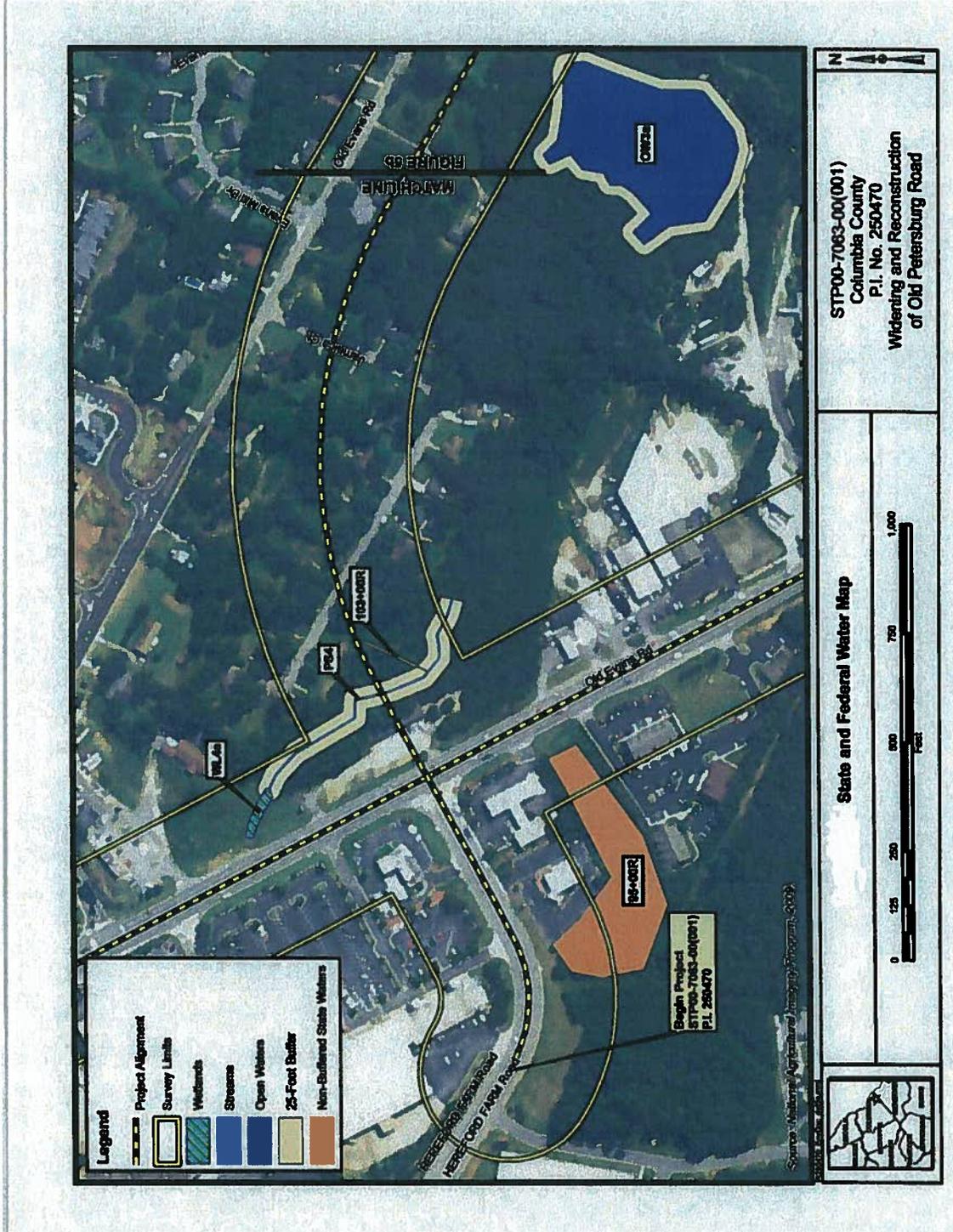
Impacts, Avoidance and Mitigation: Perennial Stream 4 (PS4) was addressed in the 2004 report which stated that approximately 675 linear feet of impacts to PS4 would occur (measured from required ROW to required ROW). Fish and Wildlife Coordination Act (FWCA) consultation is required for Stream PS-4. Concurrence was received on February 22, 2007 for 675 feet of impacts to this feature.

Based on revised impact calculations, approximately 247 linear feet (0.04 acre) of permanent impacts associated with a new culvert and approximately 26 linear feet (0.004 acre) of permanent impacts associated with riprap installation are proposed. This stream is perpendicular to the proposed project and any shifts would not result in impact minimization. The U.S. Fish and Wildlife Service (USFWS) provided concurrence under FWCA on 09/06/2013 (see Attachment 2).

### **Wetland 4a – 0.06 acre of permanent impacts**

Description and Characteristics: Wetland 4a (WL4a, Figure 6 on page 16) is a Class 4, palustrine emergent wetland approximately 0.06 acre in size located approximately southeast of the intersection of Washington Road and Old Evans Road. WL4a is located at the headwaters of PS4, within the footprint of an abandoned agricultural pond.

Impacts, Avoidance and Mitigation: Approximately 0.05 acre of WL4a will be permanently filled and approximately 0.01 acre of this feature will be permanently cleared as a result of project implementation. Due to the proximity of this feature to the existing roadway, and the fact that a large commercial area is located west of the road alignment, shifting the alignment west to avoid this feature would be unfeasible. Impacts to this feature have been reduced to the fullest extent.



### C.7. Protected Species

The April 2004 Ecology Report addressed federally protected species within Columbia County. Species reported included bald eagle (*Haliaeetus leucocephalus*), mat-forming quillwort (*Isoetes tegetiformans*), Michaux's sumac (*Rhus michauxii*), pool sprite (*Amphianthus pusillus*), relict trillium (*Trillium reliquum*) and Georgia aster (*Symphyotrichum georgianum*). The 2004 report stated the project as proposed would have no effect on any protected species. No suitable habitat was identified for any of the mentioned protected species within the survey corridor.

Since the 2004 report, the bald eagle has been delisted from the ESA; however, it remains protected under the Bald and Golden Eagle Protection Act (BGEPA). Additionally, the Michaux's sumac was listed in the USFWS county list; however, this species is not included in the USFWS IPaC database or GDNR protected species lists for Columbia County, so it was not included in the August 2013 Ecology Report. No other changes have occurred to the listing status or effect determinations for the remaining species. Several species were not addressed in the 2004 report, which has been since listed in IPaC for Columbia County.

#### *Red-cockaded woodpecker – Federal and State Endangered*

The red-cockaded woodpecker (RCW) is a federally and state endangered species with a historical range that included much of the southeastern US in mature pine forests. Because of the loss of habitat, the RCWs range is generally confined to the Coastal Plain in Georgia with only one population center known to occur in the Piedmont physiographic province. Nest and roost cavities are excavated in old living pines. Trees selected for cavities are usually infected with red heart fungus, which softens the heartwood, making excavation easier. RCWs prefer pine or pine/hardwood stands that are thirty years old or older for foraging.

No RCW individuals were observed within or adjacent to the project survey corridor during the 2010 and 2013 surveys. No mature longleaf pine stands with low understory vegetation or mature longleaf pine-hardwood stands greater than 30 years in age exist within the project corridor. The GDNR did not identify occurrences of this species within a three-mile radius of the project site. Implementation of the project would have no effect on the red-cockaded woodpecker or its habitat.

*Pool Sprite – Federal and State Threatened*

The federally and state threatened pool sprite is typically found in shallow, flat-bottomed depression on granite outcrops with thin, gravelly soils and seasonal inundation. The life cycle of the pool sprite is three to four weeks in suitable conditions. During dry years, the pool sprite may lay dormant until wetter weather occurs.

The GDNR did not identify known occurrences of this species within three miles of the study area in the 2010 early coordination letter. No granite outcrops were observed within the study area during field surveys. The project as proposed would have no effect on the federally threatened pool sprite.

*Relict trillium – Federal Endangered and State Endangered*

Relict trillium is a perennial herb that dies after the fruit matures. This species is found in the Piedmont region, in ravines or on alluvial terraces. Relict trillium succeeds in undisturbed hardwoods that are mature and moist.

The GDNR did not identify known occurrences of this species within three miles of the study area in the 2010 early coordination letter. The proposed project area does not contain ravines or alluvial terraces. Further, the mixed pine and hardwood habitat type is fragmented and surrounded by the commercial and residential development, bisected by roads, and within disturbed and developed areas. Due to the disturbance of the hardwood habitats and the absence of ravines and alluvial terraces, no suitable habitat was identified. The project would have no effect on relict trillium.

*Mat-Forming quillwort- Federally Endangered and State Endangered*

The mat-forming quillwort is known to be found in the Piedmont region, in shallow, flat-bottomed depressions on granite outcrops, where water collects after a rain. The depressions are less than one foot in depth, are entirely rock-rimmed. They may be dry for much of the summer. This plant is evident only when en masse as a greenish mat of quill-like leaves.

The GDNR did not identify known occurrences of this species within three miles of the study area in the 2010 early coordination letter. No granite outcrops were observed in the study area. The project would have no effect on the mat-forming quillwort.

*Shortnose sturgeon – Federal Endangered and State Endangered*

The shortnose sturgeon inhabits large coastal rivers, though it occasionally enters the Atlantic Ocean. They prefer the near shore marine, estuarine, and riverine habitat of large river systems. Spawning typically occurs in freshwater, mid-channel areas of river bends over coarse substrates, such as rock and gravel. The Savannah, Ogeechee, and Altamaha rivers in Georgia have known populations of shortnose sturgeon.

Three perennial streams (PS1, PS1a, and PS4) are proposed for impacts associated with project implementation. Of the three streams, two have been previously impacted by existing culverts (PS1 and PS1a) and one stream is proposed for new impacts. PS4 is approximately three feet wide and 2.5 feet deep. This stream does not constitute near shore marine, estuarine, or riverine habitat of large river systems, nor is it a major tributary to the Savannah River. Due to the distance from the main stem of the Savannah River and the size of the stream, implementation of the project would have no effect on the shortnose sturgeon.

*Georgia Aster – Federal Candidate and State Threatened*

Georgia aster is a perennial herb, which forms colonies by underground stems. The range of the Georgia aster encompasses four states including Georgia, Alabama, North Carolina, and South Carolina. In Georgia, it has been located in 22 counties, mainly in the Piedmont ecoregion. Preferred habitat for the Georgia aster includes edges and openings in rocky, upland oak-hickory-pine forests, and ROW areas throughout these dry habitats. Habitat is often along utility ROW areas and other openings where current land management mimics natural disturbance regimes. The primary controlling factor appears to be the availability of light. Early successional habitat that is not dominated by woody or invasive species may be considered suitable habitat.

The GDNR did not identify known occurrences of this species within three miles of the study area in the 2010 early coordination letter. The areas where light availability is prevalent along the project corridor (utility and transportation ROW, residential lawns, open field, and commercial/residential landscape areas) as well as the edges of the mixed pine and hardwood habitat type are regularly disturbed by mowing. Due to the regular disturbance of the areas with sufficient light availability to support this species, GDOT made a recommendation that the project would have no effect on the Georgia aster.

On 8/29/2013, FHWA (see Attachment 2) made the comment during their review of the ecology report that they did not agree with the GDOT determination of no suitable habitat and requested a field survey for the species be completed. Upon consultation with USFWS on 9/12/2013 (see Attachment 2), the USFWS suggested that the areas of suitability could be narrowed down via soil typing (see yellow area of Figure 7 Soil Geology of Project Area on page 20). Using this soil typing information, careful attention to other necessary habitat characteristics and further photographic evidence to be obtained from district personnel in the field, GDOT will coordinate survey limits in consultation with USFWS and conduct surveys of specifically identified areas in the project corridor.

As part of GDOT's commitment to conservation, any identified species will be relocated to a suitable location in coordination with an appropriate cooperating organization such as the Georgia Native Plant Society, the Georgia Master Gardener's Association, and the UGA Cooperative Extension Office in Columbia County.

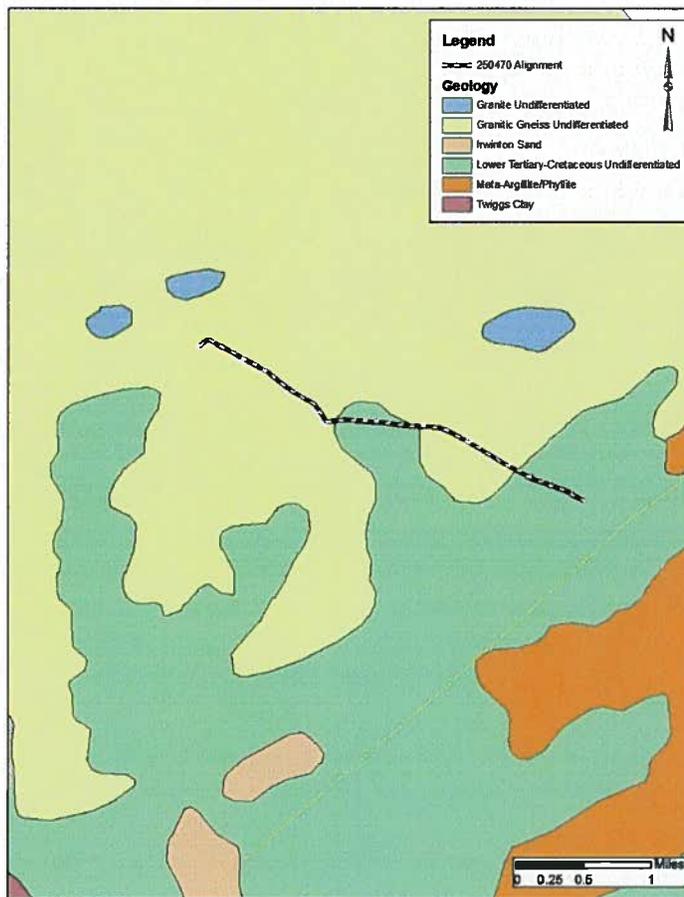


Figure 7 Soil Geology of Project Area

### *Bald and Golden Eagle*

The Bald and Golden Eagle Protection Act (BGEPA) of 1940 provides for the protection of the bald eagle and the golden eagle (*Aquila chrysaetos*) by prohibiting, except under certain specified conditions, the taking, possession and commerce of such birds.

The bald eagle was addressed in the 2004 report as a federally protected species and stated that the project would have no effect on the bald eagle. Since the 2004 report, the bald eagle has been delisted and is under the protection of the BGEPA. Per the GDNR Natural Heritage Program database, the nearest bald eagle nest is approximately 9.9 miles south of the project, within the Hephzibah quadrangle of Richmond County. The project as proposed would have no effect and no "take" on the bald eagle.

### **C.8. Invasive Species**

In 2004, only 16 priority invasive plant species were listed in the invasive species lists. The list of priority invasive plants has since been expanded. The 2004 ecology report identified four invasive species within the project corridor: Chinese privet (*Ligustrum sinense*), Japanese honeysuckle (*Lonicera japonica*), kudzu (*Pueraria montana*), and Chinese wisteria (*Wisteria sinensis*).

During the 2010 and 2013 ecology surveys, four additional invasive species were identified within the project corridor: autumn olive (*Elaeagnus umbellata*), chinaberry (*Melia azedarach*), English ivy (*Hedera helix*), and multiflora rose (*Rosa multiflora*). The additional invasive species were located at the edge of existing ROW corridors and in riparian areas.

During the construction process, GDOT would take measures during the project construction to prevent or minimize the spread of these species, as appropriate for the time of year. These measures would include the removal and disposal of vegetative parts in the soil that may reproduce by root raking prior to moving the soil, burning on-site any such parts and above-ground parts that may bear fruit, controlling or eradicating infestations prior to construction, and cleaning of vehicles and other equipment prior to leaving the site. The measures used would be those which are appropriate for the particular species and the specific site conditions that exist within the project as described in the *Georgia Standard Specifications Section 201, Clearing and Grubbing of Right-of-Way*.

### **C.9. Wildlife and Habitat**

#### *Migratory Birds*

The Migratory Bird Treaty Act (MBTA) and the Executive Order on the Responsibility of Federal Agencies to Protect Migratory Birds (EO 13186), requires the protection of migratory birds and their habitats. GDOT surveys under bridges and large culverts that would be impacted as part of a proposed project. If birds such as the barn swallow (*Hirundo rustica*) are observed nesting under the bridge or culvert, demolition or reconstruction of that structure will be scheduled to take place at a time when the nests are not being used.

The 2004 report addressed migratory birds and stated that though foraging habitat may occur within the forested areas, fragmentation would render these areas unsuitable for migratory birds. Per the 2004 report, the entire alignment is located in highly fragmented habitat and would only extend the pre-existing edge effect. The fragmentation is extensive enough that species which are sensitive to edge effects would not find suitable habitat along the proposed alignment.

During the 2010 and 2013 ecology surveys, no migratory bird nesting habitat was observed; however, foraging habitat was observed within the forested areas of the project corridor. Though approximately 20 acres of clearing are proposed, the vegetation community has been previously fragmented by development and transportation ROW. None of the culverts or bridges within the project area were found to contain migratory bird nests. Because the bridge over PS2 will be replaced and vegetative clearing would occur, Special Provisions 107.23(G) would be implemented to protect migratory birds during project construction (see Attachment 2).

#### *Bats*

GDOT surveys under bridges and large culverts that would be reconstructed or removed as part of a proposed project. If bats are observed roosting under the bridge or culvert, or evidence of bat utilization of the area is present (e.g. the presence of guano under the structure), Special Provisions would be implemented to protect bats while construction is taking place.

The culverts within the project study areas were surveyed on March 25, 2013, for bats. Though roosting habitat may exist within the culverts and forested areas adjacent to the project corridor, no individuals or evidence of the presence of bats (i.e. odors or guano staining) were identified. Because clearing is proposed within forested areas, the project is expected to have no significant adverse effect on bat species.

### *Critical Habitat*

Critical habitat, as defined in the Endangered Species Act of 1973, is a term for habitat given special protection for the benefit of a listed species. Critical habitat was not addressed in the 2004 report. The USFWS Critical Habitat Portal indicates that no critical habitat is designated for Columbia County and none of the species listed within Columbia County have designated critical habitat. The project would have no effect on critical habitat.

### *Parcel 76 – 2001 Public Hearing Comment of Protected Species by Homeowner*

A comment was received during the Public Hearing held August 30, 2001 from a resident claiming to have planted threatened and endangered species on their property. An ecology field survey conducted at the time discovered the Callaway Ginger (*Hexastylis shuttleworthii* var. *harperi*) and the Pink Lady Slipper (*Cypripedium acaule*). These plants at the time were listed as state “unusual” species, but not federally listed; with no further action or mitigation necessary. Since then, the relocation of the resident has been completed and ROW has been acquired. Also, Pink Lady Slipper The property was re-surveyed on 9/11/2013 for all protected species, with a focus, based on the information from the previous homeowner, Callaway Ginger (*Hexastylis shuttleworthii* var. *harperi*) and the Pink Lady Slipper (*Cypripedium acaule*). No state or federally listed species or their potential habitat were found on the parcel.

Specifically in regards to the Pink Lady Slipper, habitat within the site does not exhibit the pine component typically associated with known populations. Canopy pines were limited to less than five trees with the majority being on the eastern portion of the property. The eastern portion of the property has a dense understory/vine component that covers almost 100-percent of the ground. While the understory is more open on the western portion of the property, the site does not exhibit typical ladyslipper habitat. The former resident may have planted pink ladyslipper on-site and, while specimens also may have survived under close habitat management, the site in its current state does not provide suitable habitat for this species.

Callaway ginger, which is no longer state-listed, was previously reported from the site. During the field survey, a species of *Hexastylis* (wild ginger) was observed along the western side of Stream 4. No flowering specimens were observed (the flowering period is April - June for most species of *Hexastylis*). The observed specimens were in clumps and did not exhibit a spreading/creeping growth habit. Rather the leaves originated from a central root system. The majority of the plants were located on the western side of the stream; however, several clumps were located on the eastern side of the stream. Each observed clump was marked with a 36-

inch wire pin flag with orange polka dots over a white field (see attached photos). A total of 127 clumps were marked (122 west of Stream 4 and 5 east of Stream 4. Based on the surveyor's prior experience with Callaway ginger and other species of *Hexastylis*, it is our opinion that the species within the survey area is not Callaway ginger. While gingers can be difficult to identify without flowers, they can be separated into the general groups based on leaf appearance. Callaway ginger leaves are rounded to heart-shaped (cordate), 3 to 7 centimeters (cm) long, strongly mottled, and arise from shallow-creeping rhizomes. Due to the creeping nature of the rhizomes, the plant can form mats. The leaves of the on-site specimens are consistently arrow-like and do not have the cordate or rounded leaves nor the strong mottling typical of Callaway ginger. The mature leaves are consistently 7 to 10 (cm) in length. None of the plants observed had leaves arising from creeping rhizomes. Rather, each plant was a distinct clump arising from a central, well-developed root system. Though not flowering, the observed plants did not have any of the predominant characteristics of Callaway ginger. Without flowers, it is not possible to identify the plants to species. Likely candidates are *H. arifolia* and *H. speciosa*. *H. speciosa* does not naturally occur in Georgia; however, the former resident noted that she had planted the specimens. The planted specimens could have been transplanted natives or a purchased variety. The *Hexastylis* on-site do not appear to be *H. shuttleworthii* var. *harperi* (Callaway ginger). Regardless of exact determination, no species of *Hexastylis* are currently listed as state-protected in Georgia.

#### **C.10. Other**

The 2002 EA/FONSI discussed farmland, floodplains, and water quality. Neither the changes to the project discussed in section VII of this Environmental Reevaluation, nor conditions in the natural environment represent significant changes to the previous determinations of the 2002 EA/FONSI and do not require further discussion.

### **D. PHYSICAL ENVIRONMENT**

#### **D.1. Noise**

##### **Evaluation of Noise**

Georgia DOT completed a Noise Impact Assessment for the proposed project, available in December 1999 and discussed in the 2002 EA/FONSI. A Noise Impact Assessment Addendum was completed in August 2012 with the purpose to comply with the "Highway Traffic Noise Policy and Guidance" which was issued in July 2010 (revised January 2011) by the Federal Highway Administration (FHWA) and with the July 2011 Georgia Department of

Transportation (GDOT) noise policy and also update the noise model from STAMINA to FHWA Traffic Noise Model (TNM), version 2.5 with updated traffic information. The assessment was conducted in accordance with the procedures outlined in FHWA Regulation 23 CFR 772. Version 2.5 of FHWA's Traffic Noise Model (TNM) software was used to calculate existing and future traffic noise levels for the proposed project. A second Noise Impact Assessment Addendum was conducted and made available June 26, 2013 with the purpose to update the noise barrier discussion based on final design.

The following assessment documents the results of the noise analyses completed for the proposed project, in order to:

- Provide baseline noise levels that will be used in determining the projects' impact.
- Predict the effects that the proposed projects will have on the noise environment.
- Identify noise sensitive sites where noise impacts are likely to occur.
- Determine the feasibility of noise abatement measures that will eliminate or reduce expected noise impacts and satisfy the requirements of Title 23 of the Code of Federal Regulations Part 772 (23 CFR 772).
- Design the most cost effective noise wall height that will eliminate or reduce noise impacts associated with the proposed US 441/SR 24 roadway widening where both reasonable and feasible.

#### What is Noise?

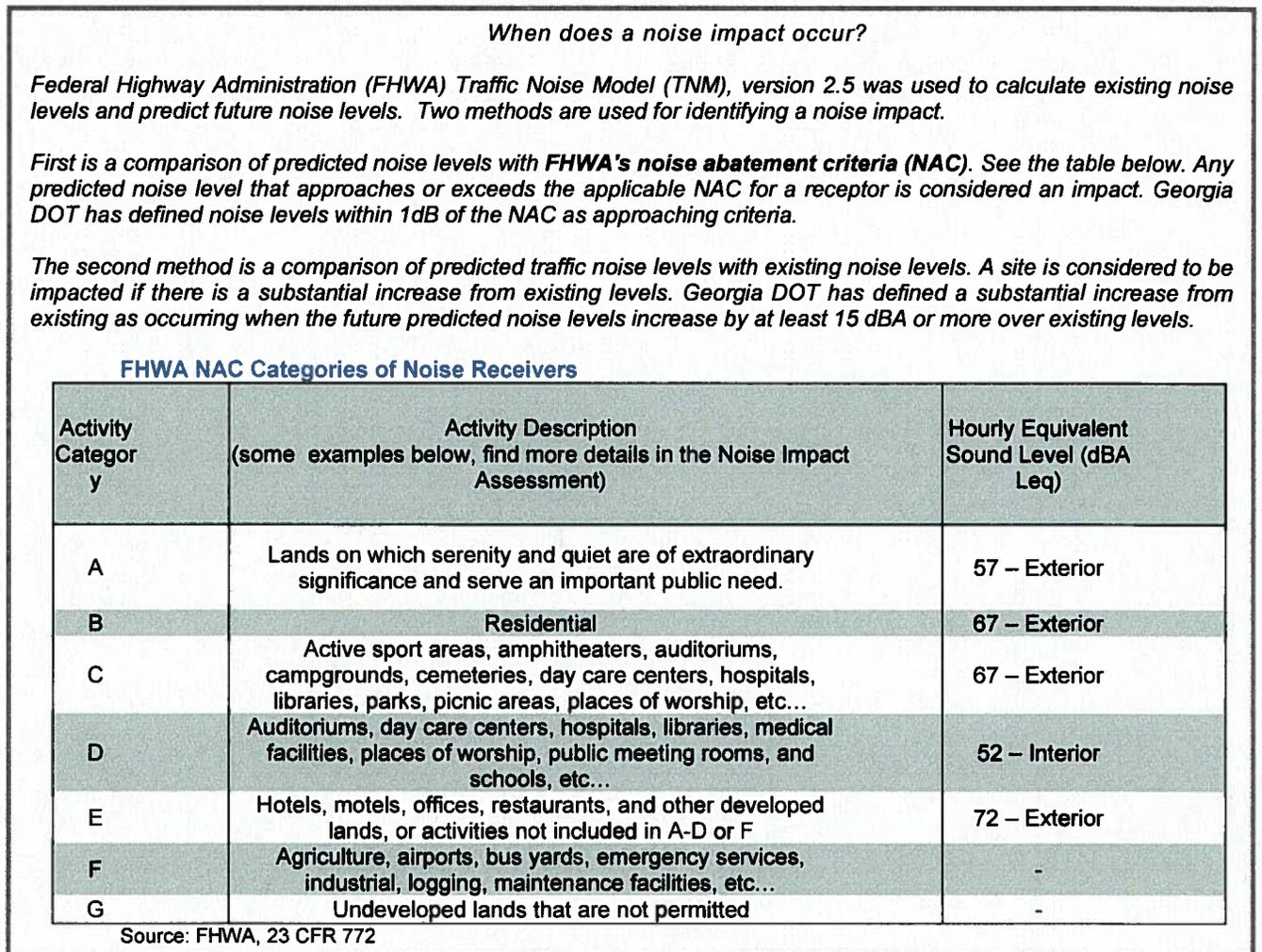
*Noise is "any sound that is undesired or interferes with a person's hearing of something". Traffic noise is associated with loud or persistent noises from cars and trucks. Engines, mufflers, and tire contact with the roadway generate the majority of traffic noise.*

*Noise is measured in units of sound pressure defined as decibels (dB), which are given an A-weighting to reflect sound levels within the range of a normal human ear. Also, because traffic sound levels are never consistent due to the changing number, type and speed of the vehicles, a single value is used to represent the average or equivalent sound level and is expressed as "Leq." Measurements of traffic noise appear as units of dBA Leq.*

**Assessment of Noise**

There were a total of 551 receivers used to model the noise impacts along the project corridor. The receivers represent 475 residential dwellings, 9 churches, 2 schools, 17 offices, 8 restaurants, 27 retail sites, 3 manufacturing site, 1 utility, and 9 recreational sites. Results from the model for existing noise levels in this area range from 45.1 to 70.1 dBA Leq.

**Project Impact on Noise**



**Figure 8**

The future no build year (2035) conditions shows the impacts at 30 residential receptor and 1 office land use receptor since they exceed the noise level criteria in their respective activity category. All these impacted receptors in the no-build condition are impacted on the basis of

approaching or exceeding the criteria set forth in 23 CFR 772 and not due to a substantial increase criteria.

The future build year (2035) condition shows impacts at 97 receptors since they exceeded the noise level criterion of 66 dBA Leq for residential land use, 1 receptor since it exceeded the noise level criterion of 71 dBA Leq for office land use, 2 receptors since they exceeded the noise level criterion of 71 dBA Leq for restaurant land use, and 1 receptor since it exceeded the noise level criteria of 66 dBA Leq for recreational land use. Additionally, receptors 36 and 90 were also impacted due to a substantial increase as the predicted future traffic noise levels would "substantially exceed" the existing noise levels. Table 1: Impacted Noise Receptors shows impacted receptors for the future build condition (2035). Additionally, refer to Table 1: Impacted Noise Receptors to view the receptor numbers that would be impacted in the existing and future conditions for the Old Petersburg Road/Old Evans Road project.

**Table 1: Impacted Noise Receptors**

Receptor	Units	Land Use Category	Existing in dBA Leq (2010)	Design Year (2035)	
				Build in dBA Leq	No Build in dBA Leq
9	1	Restaurant	67.1	71.1	69.9
11	1	Restaurant	68.4	71.5	70.2
36	1	Residential	58.1	74.3	59.8
72	1	Residential	60.9	71.8	62.8
90	1	Residential	55.9	70.9	57.9
91	1	Residential	56.0	70.2	58.0
95	1	Residential	61.5	68.7	63.6
104	1	Residential	64.6	68.0	67.2
126	1	Residential	65.5	68.2	67.8
127	1	Residential	64.4	69.8	66.5
128	1	Residential	63.6	70.2	65.7
129	1	Residential	63.4	70.8	65.6
130	1	Residential	62.6	70.3	64.8
131	1	Residential	63.4	70.8	65.5
143	1	Residential	62.2	68.2	64.4
144	1	Residential	62.3	68.4	64.4
145	1	Residential	62.5	67.9	64.6
146	1	Residential	62.5	67.9	64.6
168	1	Residential	63.1	70.7	65.2
194	1	Residential	63.2	70.8	65.3
195	1	Residential	64.1	71.5	66.2

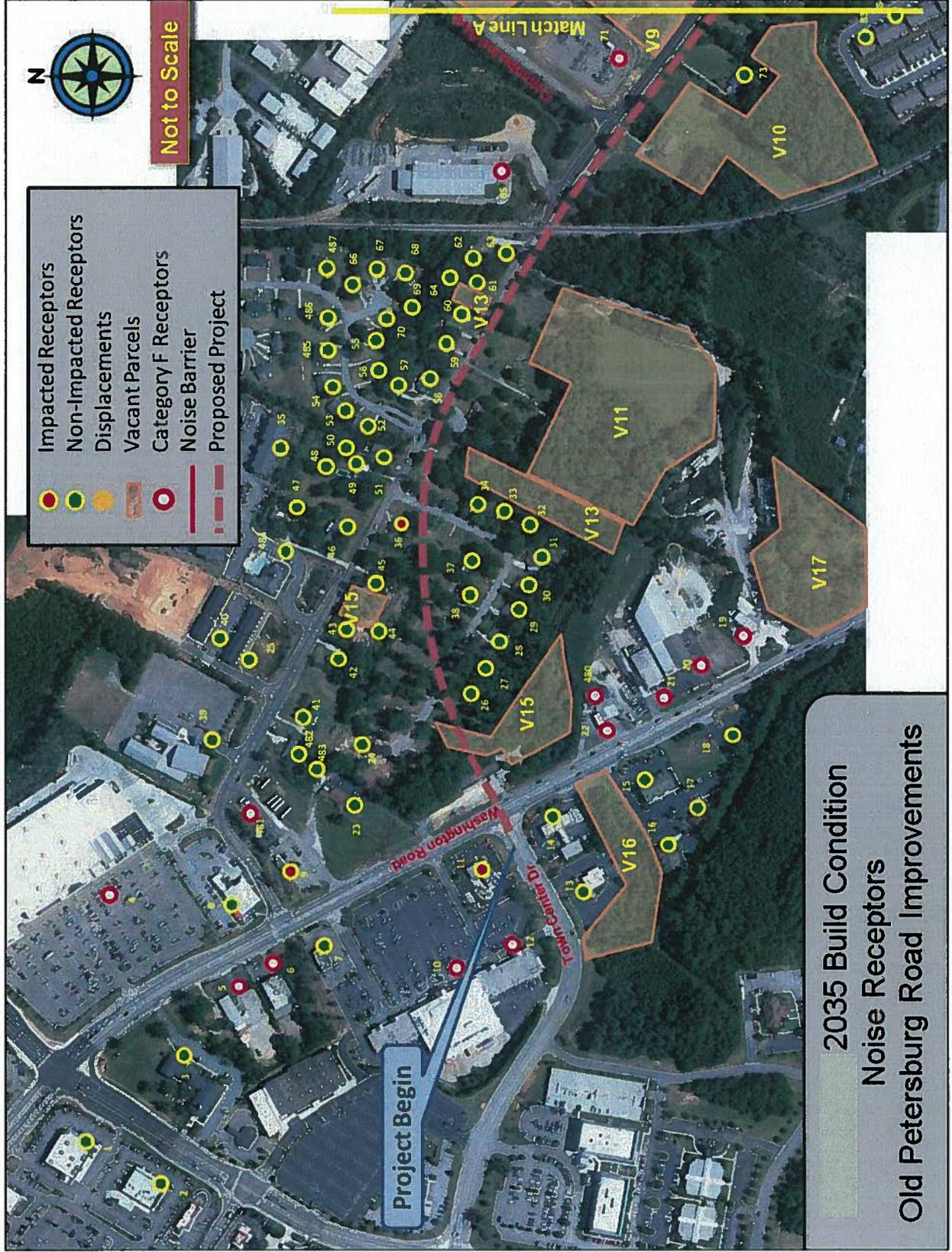
Receptor	Units	Land Use Category	Existing in dBA Leq (2010)	Design Year (2035)	
				Build in dBA Leq	No Build in dBA Leq
196	1	Residential	64.0	71.4	66.1
197	1	Residential	64.5	71.8	66.6
198	1	Residential	64.3	71.6	66.4
199	1	Residential	64.2	71.4	66.3
200	1	Residential	64.2	71.2	66.3
201	1	Residential	61.8	68.3	63.9
202	1	Residential	61.3	66.3	63.4
210	1	Residential	61.2	67.5	64.2
211	1	Residential	60.3	66.0	62.7
214	1	Residential	65.4	67.4	69.6
215	1	Residential	63.9	65.9	68.4
216	1	Residential	65.3	67.3	69.4
217	1	Residential	64.4	66.5	68.3
218	1	Residential	64.3	66.4	68.4
219	1	Residential	63.3	65.3	66.7
220	1	Residential	62.3	64.3	66.0
221	1	Residential	64.0	66.1	67.4
222	1	Residential	62.1	64.3	67.4
243	1	Residential	62.6	67.8	65.1
244	1	Residential	61.6	66.7	63.8
246	1	Residential	63.4	68.3	65.7
249	1	Residential	62.2	67.9	64.5
250	1	Residential	61.9	67.9	64.2
251	1	Residential	62.5	68.2	64.9
257	1	Residential	60.5	66.4	62.9
258	1	Residential	61.5	68.0	63.8
259	1	Residential	61.6	68.2	64.0
263	1	Residential	61.6	67.4	63.9
266	1	Residential	57.9	66.1	60.2
269	1	Residential	60.9	66.7	63.3
270	1	Residential	60.4	66.2	62.7
272	1	Residential	59.1	66.2	61.4
275	1	Residential	59.6	66.2	61.9
293	1	Residential	60.5	66.6	62.8
294	1	Residential	61.4	67.5	63.7
295	1	Residential	63.4	70.4	65.6
312	1	Residential	60.2	67.1	62.4
313	1	Residential	62.6	68.5	64.8

Receptor	Units	Land Use Category	Existing in dBA Leq (2010)	Design Year (2035)	
				Build in dBA Leq	No Build in dBA Leq
322	1	Residential	61.9	69.5	64.0
323	1	Residential	62.2	69.8	64.4
330	1	Residential	62.8	70.4	64.9
331	1	Residential	62.1	70.3	64.3
339	1	Church	61.0	69.2	63.2
340	1	Residential	59.6	68.2	61.9
341	1	Residential	61.7	70.0	63.9
346	1	Residential	60.3	67.9	62.4
347	1	Residential	62.5	68.9	64.5
348	1	Residential	60.8	67.0	62.4
349	1	Residential	62.6	68.7	64.7
350	1	Residential	63.0	65.2	68.8
351	1	Residential	63.0	65.2	68.7
352	1	Residential	62.2	68.1	64.4
353	1	Residential	64.8	69.3	67.1
354	1	Residential	62.5	68.3	64.8
355	1	Residential	64.2	69.1	66.5
356	1	Residential	59.9	66.2	62.2
362	1	Residential	64.0	69.2	66.3
382	1	Residential	64.5	69.1	67.0
383	1	Residential	66.6	69.7	69.1
384	1	Residential	66.0	69.6	68.5
390	1	Residential	61.7	66.6	64.1
391	1	Residential	61.8	67.1	64.5
397	1	Residential	63.0	68.3	65.4
400	1	Residential	60.7	69.4	63.2
406	1	Residential	61.0	69.6	63.5
407	1	Residential	63.0	71.3	65.4
410	1	Church	62.1	67.7	64.4
412	1	Church	60.7	66.0	63.2
413	1	Church	61.6	68.8	64.1
418	1	Residential	60.5	67.6	64.5
424	1	Residential	60.1	67.2	62.6
425	1	Residential	64.8	70.7	67.2
427	1	Residential	63.5	69.6	66.0
442	1	Residential	62.1	69.6	64.5
443	1	Residential	67.6	71.6	69.7
444	4	Residential	62.1	69.6	64.5

Receptor	Units	Land Use Category	Existing in dBA Leq (2010)	Design Year (2035)	
				Build in dBA Leq	No Build in dBA Leq
467	1	Residential	65.3	68.8	67.3
468	1	Residential	65.2	68.7	67.2
469	1	Residential	67.3	70.8	69.2
477	1	Office	70.1	73.3	72.1
529	1	Residential	65.1	69.9	67.2
536	1	Residential	62.7	68.4	64.9
537	1	Recreational (Park)	60.0	68.5	62.4
	107	impacted receptors			

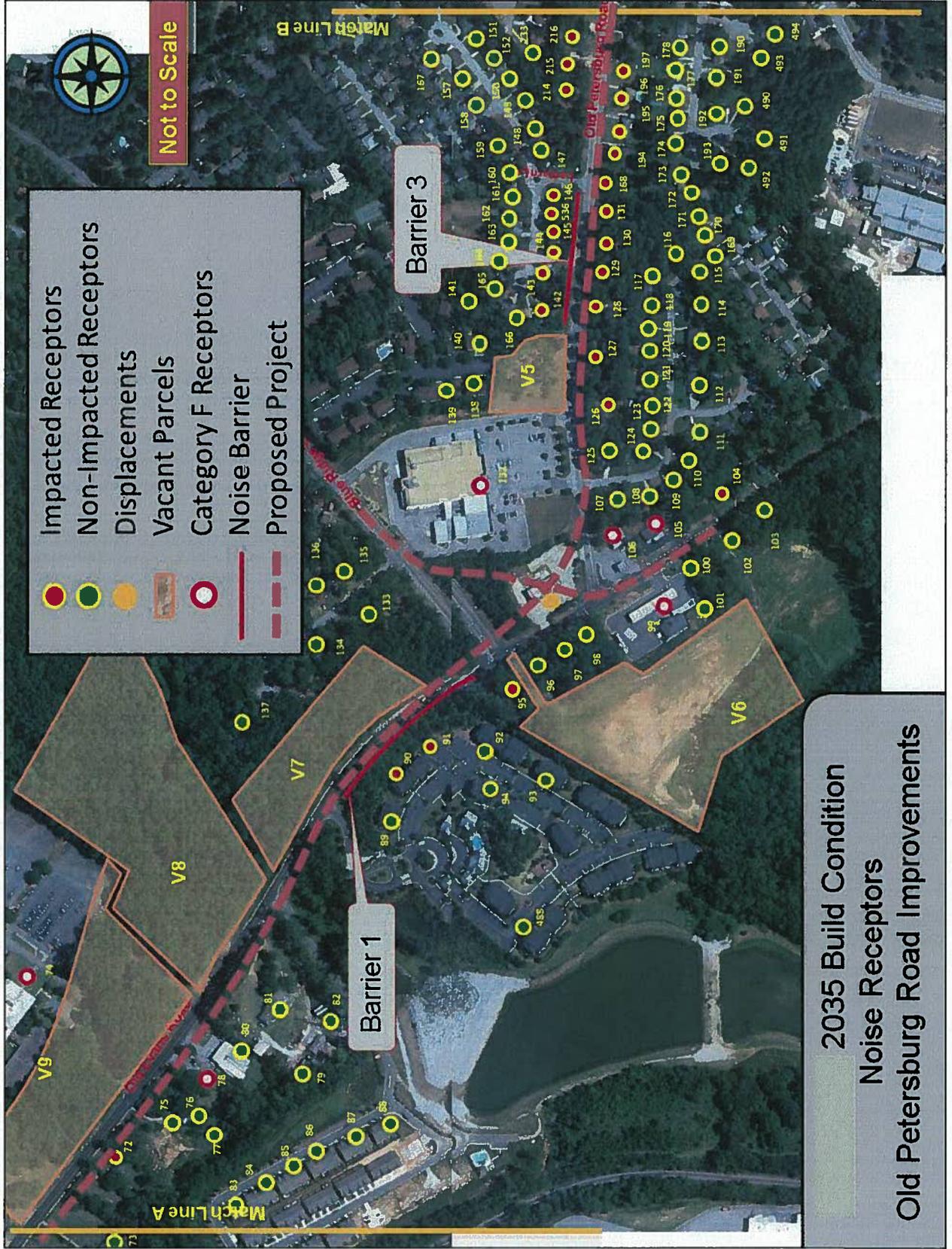
Source: Noise Impact Assessment dated 07/11/2012

The approaching NAC level for all impacted receptors is 66 dBA Leq.



2035 Build Condition  
 Noise Receptors  
 Old Petersburg Road Improvements

Figure 9



2035 Build Condition  
 Noise Receptors  
 Old Petersburg Road Improvements

Figure 10

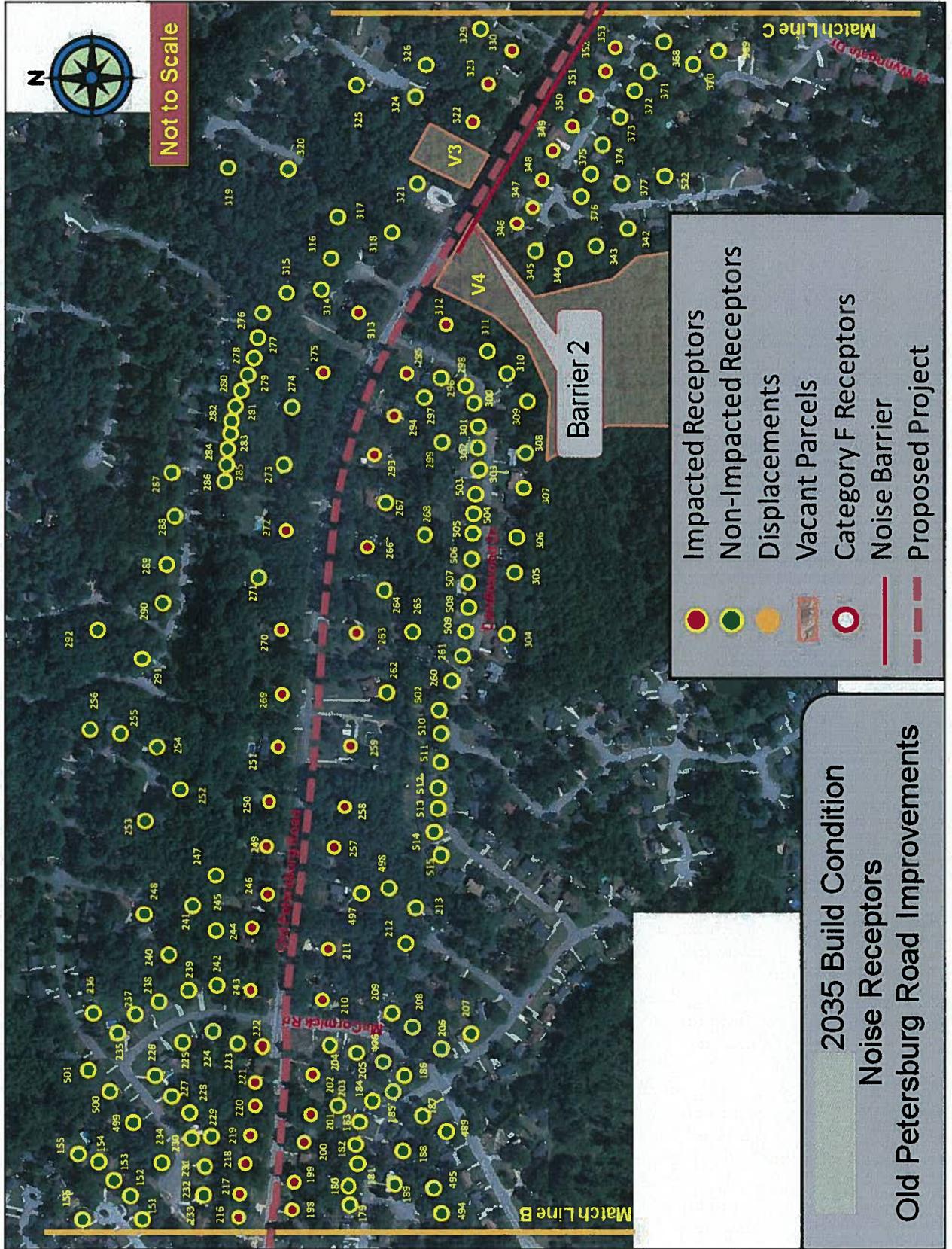


Figure 11

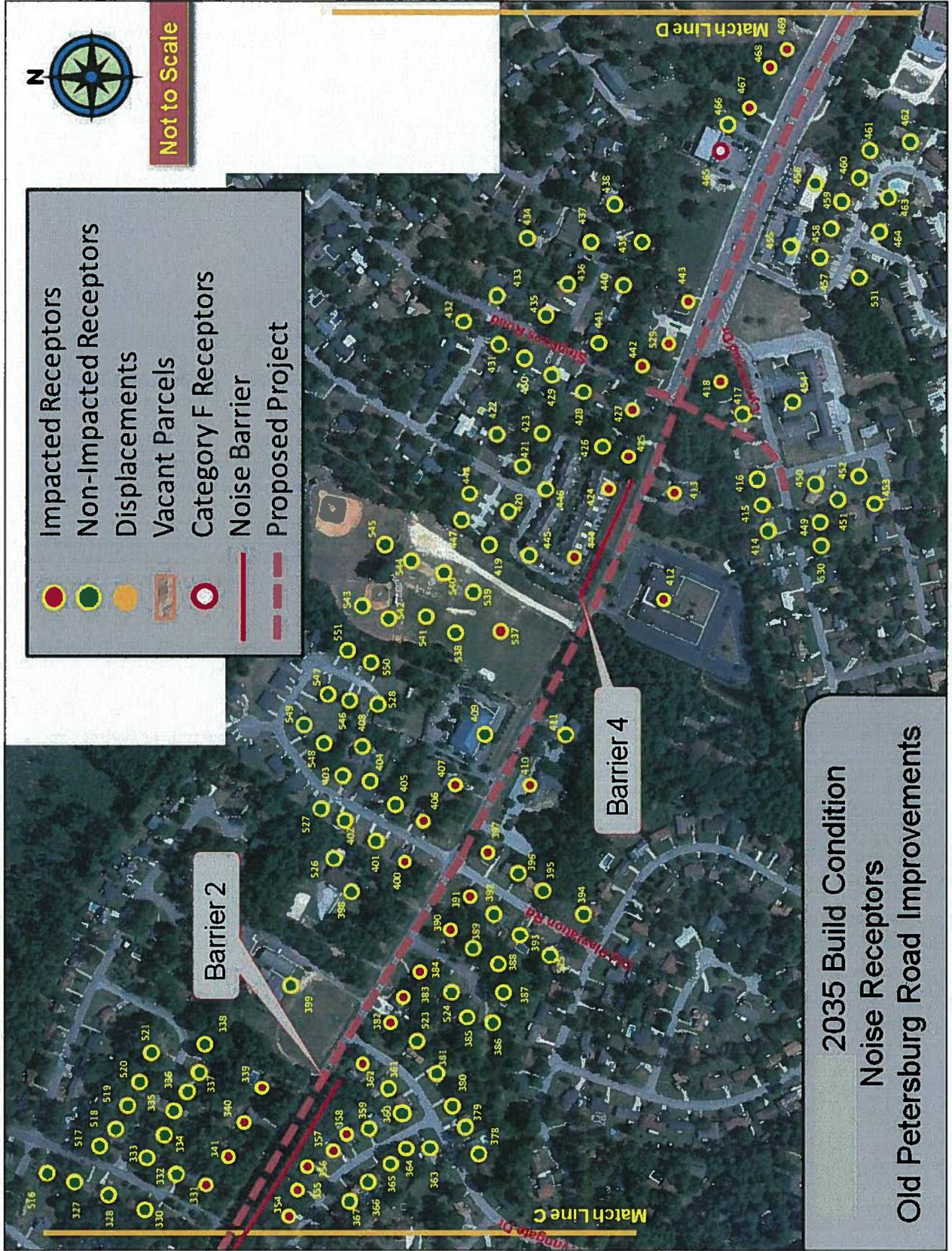


Figure 12

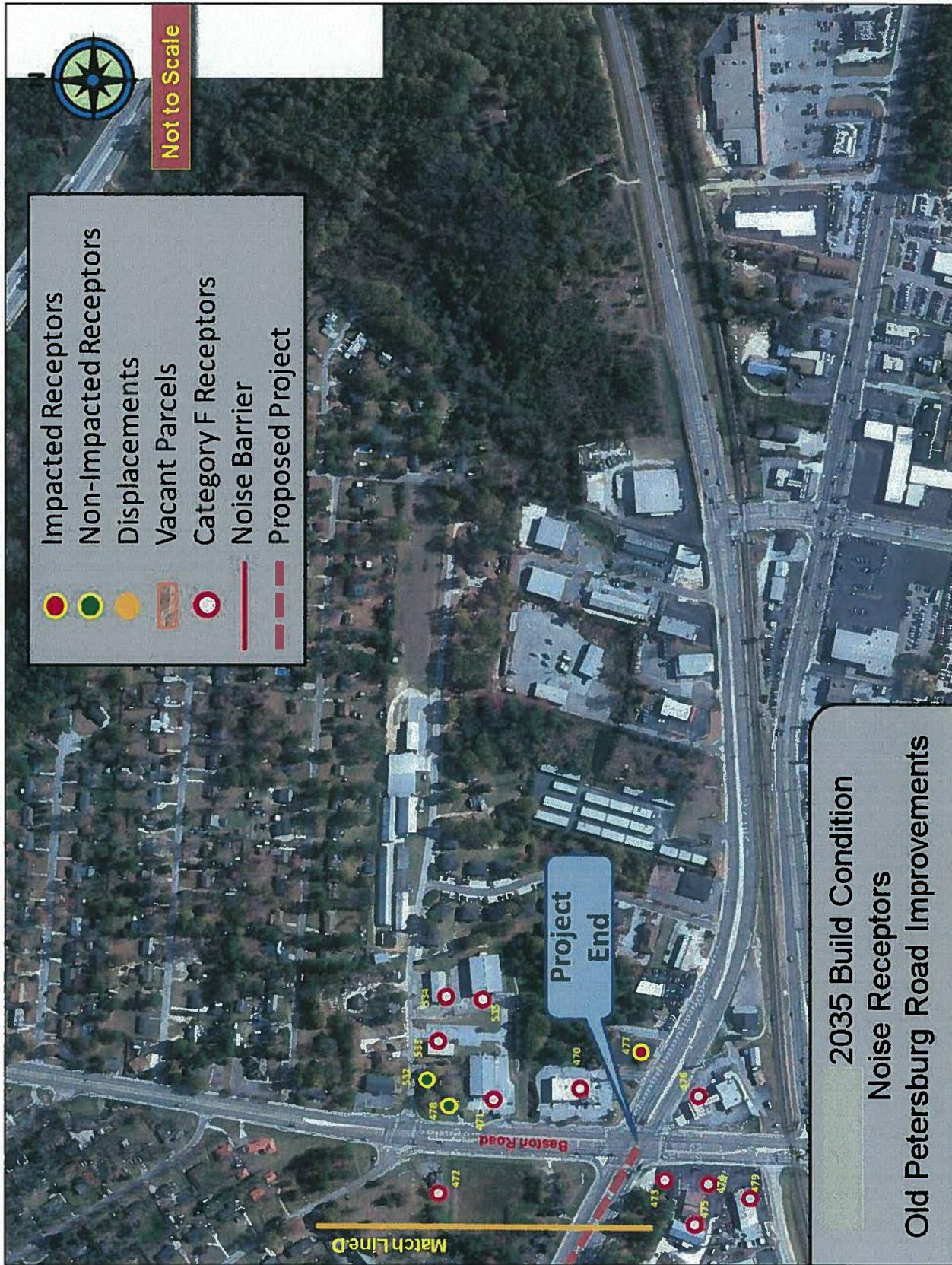


Figure 13

## **Noise Abatement Feasibility and Reasonableness**

### Feasibility

- *Noise reduction:* a calculated noise reduction of at least 5 dBA must be achievable for a minimum of one impacted receptor. Each noise receptor which receives a 5 dBA reduction (whether classified as impacted or not) is considered to be a benefited receptor.
- *Constructability:* a noise abatement measure must be able to be constructed using reliable and common engineering practices.
- *Safety and Maintainability:* an exterior noise abatement measure should conform to the AASHTO Green Book and Roadside Design Guide and should be accessible to maintenance personnel and not prevent access to other highway appurtenances (e.g., drainage structures). The maximum barrier height that can feasibly be maintained is 30 feet.
- *Access:* an abatement measure must allow sufficient access to adjacent properties.

### Reasonableness

Noise abatement was considered for the impacted receptor sites and evaluated for reasonableness based on the following criteria:

- *Noise Reduction:* at least one benefited receptor must receive a minimum noise level reduction of 7 dB(A) – i.e., the noise reduction design goal.
- *Cost Effectiveness:* Using a \$20 per square foot cost for the required noise barrier, the total cost must not exceed a \$55,000 average allowance per benefited receptor.
- *Property Owners and Residents:* The decision to provide abatement will be made in collaboration with property owners and tenants of a benefited receptor. A noise barrier will only be constructed if at a minimum 50% plus one of the respondents vote in favor of noise abatement.

The first two criteria must be satisfied before contacting property owners and residents. Meeting the first criteria listed in the table below (Table 2 Noise Barrier Evaluation Summary Table). Though multiple options may have been considered for a barrier to consider cost, the most cost effective are expressed in table 4 while the options, if any, are discussed in each section following the table specifically detailing the barrier and decision to build or not build.

### **Noise Abatement Measures Considered**

Using a \$20 per square foot cost for the required noise barrier, the estimated cost, must not exceed a \$55,000 average allowance per benefited receptor (dwelling unit), reasonable cost. Table 3 shows the calculations for each barrier.

Based on determinations of reasonableness, three noise barriers (noise barriers 1, 3, and 4) were proposed for this project. Figures 9 on page 31 through 13 on page 35 show the modeled barriers for the proposed project.

Barrier 1: For the previously approved barrier 1 discussed in the original 2002 EA/FONSI, information such as decibel reduction, reasonable cost, and estimated cost are updated as per new noise policy and also with updated traffic information and is provided in the Table 2 Noise Barrier Evaluation Summary Table above for information purposes only. Since this barrier was presented to the public as part of the original EA/FONSI, the current decision to construct is based on the rules regarding barriers and the positive feedback received. However, an effort to engage current residents for response was undertaken.

A surveys/invitation to a public information meeting on May 6, 2013 was sent regarding barrier 1. A detour/noise meeting was held on May 14, 2013. Unfortunately no comments about the noise walls were received at that time. A second mailing went out on June 26, 2013, with stamped envelopes enclosed, in an attempt to determine if the citizens desire the walls (see example letter in Attachment 3). Three comments were received in favor of the wall and none opposed (see Noise Wall Memo in Attachment 2). The barrier would be constructed as part of the proposed project.

Barrier 2: Barrier 2 is located south of Old Petersburg Road between Candlewood Drive and N Wynngate Drive. This barrier targets impacted receptor 346, 347, 348, 349, 350, 351, 352, 353, 354, 355, and 356 as well as non-impacted receptors 357, 358, and 359. Analysis for this barrier was done with a noise barrier height of 15 feet. This barrier would cost an estimated \$378,300 and its reasonable cost is \$330,000. As the estimated cost of the barrier is higher than the reasonable cost of the barrier, this barrier is determined to be not reasonable and cost effective (see Table 2 Noise Barrier Evaluation Summary Table). Further this barrier was also analyzed with two different barrier heights of 19 feet and 15 feet, but the estimated cost of this barrier far exceeded the example detailed above and shown in Table 3 making it even less reasonable and cost effective.

**Table 2 Noise Barrier Evaluation Summary Table**

Barrier	Dwelling Units	Barrier Dimensions	List of Feasible Receivers	Decibel Reduction	Reasonable Cost (\$)	Estimated Cost (\$)	Reasonable Yes/No
1	12	550'x20'= 11,000 sq.ft.	89	5.8	\$55,000x36= \$1,980,000	\$20x11,000= \$220,000	Yes
	12		90	7.3			
	12		91	6.0			
	24		92	1.5			
	12		93	1.0			
	12		94	2.1			
	24		488	1.0			
2	1	1261'x15'= 18,915 sq.ft.	346	2.5	\$55,000x6= \$330,000	\$20x18,915= \$378,300	No
	1		347	5.3			
	1		348	3.1			
	1		349	7.1			
	1		350	6.1			
	1		351	5.2			
	1		352	3.2			
	1		353	7.5			
	1		354	4.3			
	1		355	7.6			
	1		356	2.2			
	1		357	2.4			
	1		358	0.9			
1	359	0.2					
3	1	432'x15'= 6,480 sq.ft.	142	5.5	\$55,000x6= \$330,000	\$20x6,480= \$129,600	Yes
	1		143	8.5			
	1		144	9.5			
	1		145	9.8			
	1		146	5.5			
	1		536	8.7			
4	3	499'x15'= 7,485 sq.ft.	424	8.1	\$55x7= \$385,000	\$20x7,485= \$149,700	Yes
	4		444	7.8			
	1		445	3.6			
	1		446	4.0			

**Barrier 3:** Barrier 3 is located north of Old Petersburg Road at Lantern Lane. This barrier targets impacted receptors 142, 143, 144, 145, 146, and 536. The barrier would cost an estimated \$129,600 and its reasonable cost is \$330,000. As the reasonable cost is significantly higher than the estimated cost of the barrier, this barrier is determined to be reasonable and cost effective (see Table 2 Noise Barrier Evaluation Summary Table).

A surveys/invitation to a public information meeting on May 6, 2013 was sent regarding barrier 3. Unfortunately no comments about the noise walls were received. A second mailing

went out on June 26, 2013, with stamped envelopes enclosed, in an attempt to determine if the citizens desire these walls (See Example Letter in Attachment 2). Two comments were received in favor of the barrier and one opposed with a request for more information. Upon receipt of more information and clarification, an email in favor of the barrier was received from the previously opposing citizen (see Noise Wall Memo in Attachment 2). The barrier would be constructed as part of the proposed project.

Barrier 4: Barrier 4 is located north of Old Petersburg Road at Coach Lane between Kelly Park and Stephens Road. This barrier targets impacted receptor 424 (3 units), 444 (4 units) as well as non-impacted receptors 445 and 446. This barrier would cost an estimated \$149,700 and its reasonable cost is \$385,000. As the reasonable cost is significantly higher than the estimated cost of the barrier, this barrier was determined to be reasonable and cost effective.

During the final field plan review it was determined that barrier 4 located north of Old Petersburg Road at Coach Lane between Kelly Park and Stephens Road is not feasible due to a limited amount of ROW between the road and a detention pond (the detention pond is located in-between the roadway and the residents). If a barrier were constructed within the clear zone (between the road and the detention pond) it would have to be shielded with a roadside barrier; however, this is also not feasible due to inadequate space for maintenance. Therefore, this barrier is not feasible to construct.

#### **Affect of project construction on noise**

Georgia DOT recognizes that minimizing construction noise is important; however, in the absence of standardized federal criteria for assessing construction noise impacts related to transportation projects (FHWA Construction Noise Handbook, 2006), it is necessary to primarily rely on the standards and requirements developed by local governments to determine the criteria to which contractors must adhere.

In Georgia, contractors on all highway construction projects are required to adhere to GDOT Standard Specification Section 107.01 – Laws to Be Observed, which states in part, “The Contractor shall at all times observe and comply with all such laws, ordinances, codes, regulations, orders and decrees...” unless the necessary variance is obtained.

In order to further minimize construction noise, GDOT’s Office of Environmental Services will give the Project Manager and the design team the noise sensitive receptor information as

early as possible during project development. This information would be used for the incorporation of construction noise control strategies in the project layout and design.

#### **Indirect or cumulative impacts to noise**

There are no known adverse indirect or cumulative effects related to noise that would result from the proposed projects. Overtime, some types of development (residential or commercial) may occur as a result of the proposed projects and could combine with some past residential, commercial, industrial and institutional development to increase noise effects within the project corridor. While these effects are expected, they are not expected to be significant or adverse.

#### **D.2. Climate Change**

The issue of global climate change is an important national and global concern that is being addressed in several ways by the federal government. The Transportation section is the second largest source of total greenhouse gas emissions (GHG) in the U.S. and the largest source of CO<sub>2</sub> emissions – the predominant GHG. In 2004, the transportation sector was responsible for 31% of all U.S. CO<sub>2</sub> emissions. The principal anthropogenic (human-made) source of carbon emissions is the combustion of fossil fuels, which account for approximately 80 percent of anthropogenic emissions of carbon worldwide. Almost all (98%) of transportation-sector emissions result from the consumption of petroleum products such as motor gasoline, diesel fuel, jet fuel, and residual fuel.

To date, no national standards have been established regarding greenhouse gases, nor has the US Environmental Protection Agency (USEPA) established criteria or thresholds for GHG emissions. On April 2, 2007, the Supreme Court issued a decision in *Massachusetts et al v. Environmental Protection Agency et al* that the USEPA does have authority under the Clean Air Act to establish motor vehicle emissions standards for CO<sub>2</sub> emissions. The USEPA is currently determining the implications to national policies and programs as a result of the Supreme Court decision. However, the Court's decision did not have any direct implications on requirements for developing transportation projects.

Recognizing these concerns, the Federal Highway Administration (FHWA) is working with other modal administrations through the Department of Transportation Center for Climate Change and Environmental Forecasting to develop strategies to reduce transportation's

contribution to greenhouse gases – particularly CO<sub>2</sub> emissions – and to assess the risks to transportation systems and services from climate changes.

Because climate change is a global issue and the emissions changes due to project alternatives are very small compared to global totals, GHG emissions were not calculated for the alternatives considered. The FHWA does not believe it is informative at this point to consider GHG gas emissions in a project level NEPA document. The climate impacts of CO<sub>2</sub> emissions are global in nature. Further, due to the interactions between elements of the transportation system as a whole, emissions analyses would be less informative than ones conducted at regional, state, or national levels. Because of these concerns, CO<sub>2</sub> emissions cannot be usefully calculated in this document in the same way that other vehicle emissions are addressed. As more information emerges and as policies and legal requirements evolve, approaches to climate change at both the project and policy level will be reviewed and updated.

### **D.3. Air**

The proposed project was evaluated for its consistency with state and federal air quality goals, including CO, Ozone, PM 2.5, and MSAT's in the Air Quality Impact Assessment Addendum dated 7/11/2012 (see attachments 2 and 3). Results indicated that the project is consistent with the SIP for the attainment of clean air quality in Georgia and is in compliance with both state and federal air quality standards.

**Ozone:** This project is in an area where the SIP does not contain any transportation control measures. Therefore, the conformity procedures of the Final Conformity Guidance do not apply. The limits of the project evaluated in this assessment are identified in the State Transportation Improvement Program (STIP) by project number 250470.

**Carbon Monoxide (CO):** The proposed project was evaluated for the potential to result in increased CO concentrations in the project are signalized intersections of Towne Center Drive at Washington Road and Blue Ridge Drive at Old Petersburg Road/Old Evans Road. These are the only signalized intersections within the study area with proposed improvements as part of this project.

Modeling of the two intersections revealed the highest concentration of CO potentially occurring at the intersection of Blue Ridge Drive at Old Petersburg/Old Evans Road. The design year (2035) PM predicted one-hour concentration at this intersection is 8.9 ppm. This value is lower than the NAAQS maximum one-hour average time of 35 ppm. Because the models are

associated with the peak-hour traffic flow and because the highest one-hour concentration does not exceed the eight-hour standard, it is deemed unnecessary to analyze the eight-hour average concentrations.

**PM 2.5:** This project is contained within a PM 2.5 attainment area; therefore, an assessment is not required.

**MSAT:** A qualitative analysis provides a basis for identifying and comparing the potential differences among MSAY emissions, if any, from the various alternatives.

For each alternative, the amount of MSAT's emitted would be proportional to the vehicle miles traveled, or VMT, assuming the other variables such as fleet mix are the same for each alternative. Table 3 presents the vehicle miles of travel summary of the existing, no-build and build alternatives.

**Table 3: Vehicle Miles of Travel (VMT) Summary for Old Petersburg Road/Old Evans Road**

Old Petersburg Road/old Evans Road	Existing Conditions (2011) – Project length (2.95 miles)	No-Build Conditions (2035) – Project length (2.95 miles)	Build Conditions (2035) – Project length (2.55 miles)
AADT	19,400	31,200	34,800
VMT	57,230	92,040	88,740

Note: VMT is calculated by multiplying the AADT by the improved facility length in miles.

The MSAT emissions for the preferred action alternative are less than the no-build alternative along the project corridor. This is primarily due to realigning the Old Evans Road from existing location to a new location. This resulted in reduction of project length and also VMT reduction for the build condition. According to EPS's MOBILE 6.2 model, emissions of all of the priority MSAT except for diesel particulate matter decrease as speed increases. The extent to which these speed-related emissions decreases will offset VMT-related emissions increases cannot be reliably projected due to the inherent deficiencies of technical models. However, regardless of the alternative chose, emissions will likely be lower than present levels in the design year as a result of EPA's national control programs, which are projected to reduce annual MSAT emissions by 72 percent between 1999 and 2050. Local conditions may differ from these national projections in terms of fleet mix and turnover, VMT growth rates, and local control measures. However, the magnitude of the EPA-projected reductions is so great (even

after accounting for VMT growth) that MSAT emissions in the study area are likely to be lower in the future in nearly all cases.

However, the magnitude and duration of these potential increases compared to the No-Build alternative cannot be reliably quantified due to incomplete or unavailable information in forecasting project-specific MSAT health impacts. In sum, when a highway is widened, the localized level of MSAT emissions for the build alternative could be higher relative to the no-build alternative, but this could be offset due to increases in speeds and reductions in congestion (which are associated with lower MSAT emissions). Also, MSAT will be lower in other locations when traffic shifts away from them.

However, on a regional basis, EPA's vehicle and fuel regulations, couple with fleet turnover, will over time cause substantial reductions that, in most cases, will cause region-wide MSAT levels to be significantly lower than today.

#### **D.5. Construction/Utilities**

The 2002 EA/FONSI discussed construction/utilities. Neither the changes to the project discussed in section VII of this Environmental Reevaluation, nor conditions in the natural environment represent significant changes to the previous determinations of the 2002 EA/FONSI and do not require further discussion.

#### **D.6. USTs**

A subsurface investigation was conducted to determine whether contamination from UST's or hazardous waste is present within the required right-of-way (ROW) and results were summarized in a memo from the Office of Materials and Research dated November 12, 2003 (see Attachment 3). This memo was not discussed in the 2004 Environmental Reevaluation (4/12/04) and will be discussed here (addressing the UST sites investigated) and in section D.7 on page 44 (addressing the Hazardous Waste Site investigated).

Five sites were investigated for UST contamination for Parcel Numbers 4, 14, 29, 108, and 111. Contamination was not encountered at Parcel Numbers 4, 14, 29 and 108. The UST's will not be acquired at Parcel Numbers 4 and 14<sup>2</sup>.

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<sup>2</sup> See next note.

The UST's have been removed from Parcel Numbers 29 and 108. Parcel 29 is now the site of a Lowe's Home Improvement store. According to a 1992 UST investigation report (see Attachment 3<sup>3</sup>), the UST system has been removed from parcel 108.

Contamination from UST's was not encountered at Parcel Number 111. S & ME Inc. removed the UST's in 2001 (see Attachment 3) and filed a properly certified UST Closure Report on November 6, 2008 with Georgia Department of Natural Resources Environmental Protection Division (EPD) Underground Storage Tank Management Program. EPD concurred with a letter of "no further action required" on December 10, 2008 (see Attachment 3).

Any contaminated soil excavated during construction activities at Parcels 4, 14, 22, 29, 108, and 111 must be disposed of at a permitted lined municipal solid waste landfill. All activities associated with UST sites will be in accordance with GDOT Standard Specifications, Provision 217.

#### **D.7. Hazardous Waste Sites**

A subsurface investigation was conducted to determine whether contamination from UST's or hazardous waste is present within the required right-of-way (ROW) and results were summarized in a memo from the Office of Materials and Research dated November 12, 2003 (see Attachment 3). This memo was not discussed in the 2004 Environmental Reevaluation (4/12/04) and will be discussed here (addressing the Hazardous Waste Site investigated) and in section D.6 on page 43 (addressing the UST sites investigated).

Parcel Number 22 was investigated for possible soil contamination from hazardous waste. No contamination was encountered within required ROW at this site. However, any contaminated soil excavated during construction activities at Parcel 22 must be disposed of at a permitted lined municipal solid waste landfill. All activities associated with Hazardous Waste Sites will be in accordance with GDOT Standard Specifications, Provision 107.22.

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<sup>3</sup> Note the attachment references Parcel 108 as previously being labeled in 1992 as parcel 14. This is not the same Parcel 14 currently labeled in plans and discussed in the previous paragraph.

## **D.8. Other**

### Traffic Data

The predicted traffic volume for year 2011 is 19,400 volume per day (vpd). Traffic is predicted to be even greater in design year 2015 at 31,200 vpd and design year 2036 at 34,800. The proposed project would accommodate the flow of traffic in the near and distant future.

## **E. PERMITS/VARIANCES/COMMITMENTS REQUIRED**

### **E.1. U.S. Coast Guard Permit**

According to the 2002 EA/FONSI a U.S. Coast Guard Permit is not required for this project because no waters under Coast Guard jurisdiction are involved. Neither the changes to the project discussed in section VII of this Environmental Reevaluation, nor conditions in the physical environment have changed this determination.

### **E.3. CWA Section 404 Permit**

The 2004 Ecology Report identified 730 linear feet of stream impact. The project required an Individual Permit (IP) due to one stream impact of 675 linear feet. Based on updated designs, approximately 629 linear feet of stream (total) and 0.06 acre of wetland will be impacted as a result of project implementation. The project impacts would be covered under Nationwide Permit (NWP) 14 (Linear Transportation Projects). A pre-construction notification (PCN) will be required for this project.

Stream impacts exceeding 100 linear feet and wetland impacts exceeding 0.10 acre are to be mitigated per the USACE, Savannah District, *Standard Operating Procedure for Compensatory Mitigation Requirements for Adverse Impacts to Wetlands, Open Waters and Streams*, dated March 2004. The project as proposed would require 2,724.6 stream mitigation credits and 0.30 wetland mitigation credit from a USACE-approved mitigation bank servicing the Middle Savannah River Watershed.

### **E.5. Buffer Variance**

Open Water 3a (OW3a) Buffer: A small portion of the buffer of OW3a will be impacted by construction activities outside of exemption areas. A buffer variance will be obtained under criteria A from the Georgia Environmental Protection Division for these impacts prior to construction and no stream credits will be required.

#### **E.7. National Pollutant Discharge Elimination System**

The NPDES was created by the federal Clean Water Act to control water pollution by regulating the discharge of pollutants to surface waters. In Georgia, any ground disturbing activities that exceed one acre are covered under the State's NPDES permit. Ground disturbing activities exceeding one acre would occur for the proposed project. Therefore, a Notice of Intent (NOI) to the NPDES General Permit will be submitted prior to construction.

##### *State Waters*

State Waters are defined by the Official Code of Georgia 12-7-1 and protected by the Georgia Erosion and Sedimentation Control Act of 1975. In compliance with the NPDES permit under Section 402 of the Clean Water Act, any encroachment within the designated 25-foot or 50-foot buffer of a State Water will be described, and the need for a variance will be indicated.

Ten non-jurisdictional, non-buffered state waters (NBSW) were identified along the project corridor. These features are not federally jurisdictional, nor do they require a 25-foot state buffer.

#### **F. SECTION 4(f) APPLICABILITY**

##### **F.5. Other**

According to the 2002 EA/FONSI no Section 4(f) applicable sites were within the project corridor. Neither the changes to the project discussed in section VII of this Environmental Reevaluation, nor conditions in the project corridor have changed this determination.

**DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA**

**Project STP00-7063-00(01)**

**Columbia County**

**PI No. 250470**

**Reevaluation**

**Attachment 2**

**CORRESPONDENCE**

**AUGUSTA REGIONAL TRANSPORTATION STUDY  
FY 2013 - 2016 TRANSPORTATION IMPROVEMENT PROGRAM**

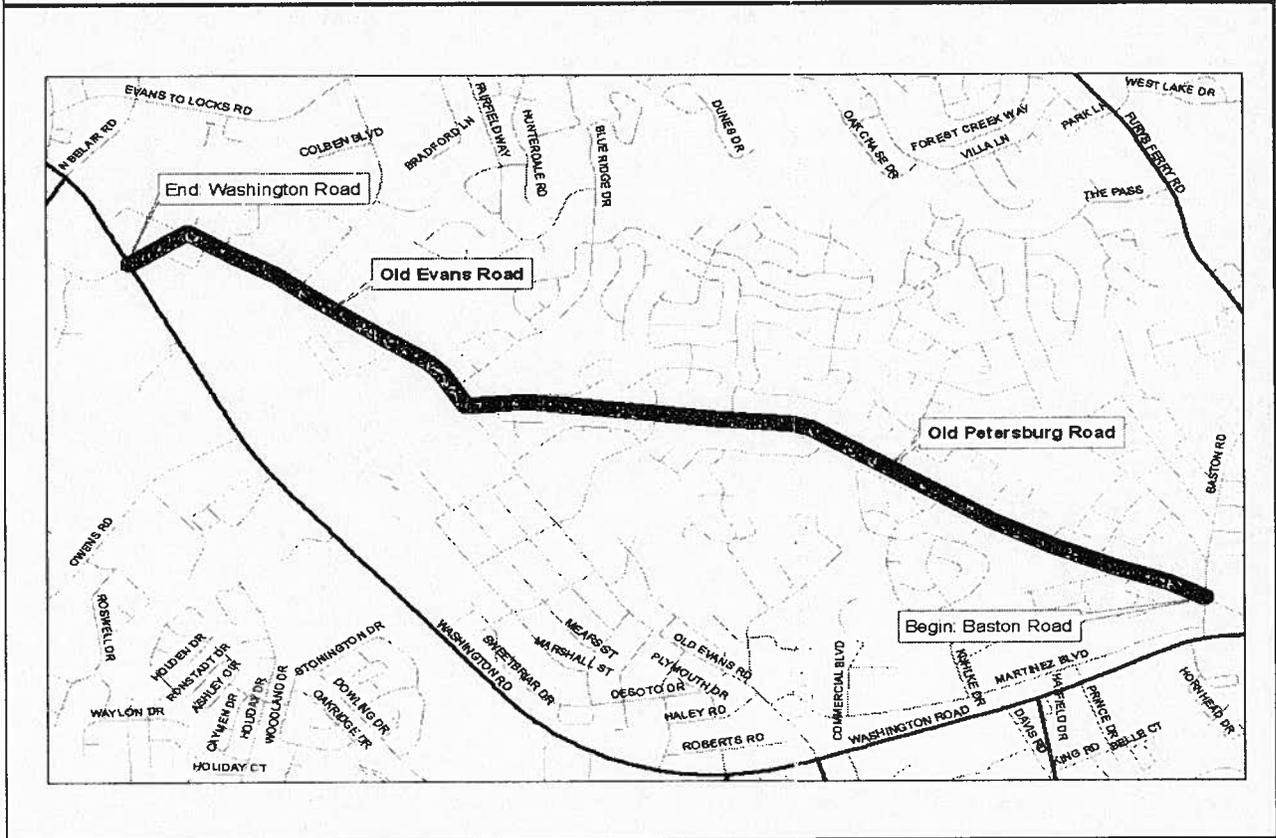
<b>PROJECT NAME:</b>	OLD PETERSBURG ROAD		<b>CST. YEAR:</b>	FY 2014	
<b>PROJECT DESCRIPTION:</b>	WIDEN TO FOUR THRU LANES WITH TURN LANES AS NEEDED, FROM OLD EVANS ROAD TO BASTON ROAD. EXTEND FROM OLD EVANS ROAD TO WASHINGTON ROAD.		<b>P.I. NOS:</b>	250470 / 25R470 / 0008833	
			<b>TIP #:</b>	STP-2	
			<b>COUNTY:</b>	COLUMBIA	
<b>LENGTH (MI):</b>	2.55	<b># OF LANES-EXISTING:</b>	0 / 2	<b>PLANNED:</b>	4
<b>TRAFFIC VOLUMES (ADT):</b>	(2006)	10,450	(2035)		23,380
<b>LOCAL RD. #</b>	CR 145	<b>ST./U.S.#</b>		<b>PROJECT #</b>	STP-7063-(1)
<b>PURPOSE AND NEED:</b>	Capacity, Operational, & Connectivity				

**COMMENTS / REMARKS:** The ARTS Bike Plan recommends consideration of bike and pedestrian facilities in design  
Funding split between sources - LY20S - \$1,124,874.95 / LY10S - \$3,599,601.25 / L230 - \$ 18,619,234

PROJECT PHASE	\$ SOURCE	AUTHORIZED	FY 2013	FY 2014	FY 2015	FY 2016	LRP TIER 2	LRP TIER 3	TOTAL
PRELIMINARY ENGINEERING	Authorized	\$2,906,290							\$2,906,290
RIGHT-OF-WAY	Authorized	\$26,430,410							\$26,430,410
UTILITIES	Federal/State								\$0
CONSTRUCTION	Federal/State			\$4,724,476	\$35,202,360				\$39,926,836
<b>PROJECT COST</b>		<b>\$29,336,700</b>	<b>\$0</b>	<b>\$4,724,476</b>	<b>\$35,202,360</b>	<b>\$0</b>	<b>\$0</b>	<b>\$0</b>	<b>\$69,263,536</b>
FEDERAL COST		\$23,469,360	\$0	\$3,779,381	\$28,161,888	\$0	\$0	\$0	\$55,410,829
STATE COST		\$5,867,340	\$0	\$944,895	\$7,040,472	\$0	\$0	\$0	\$13,852,707
LOCAL COST		\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0

<b>DOT DISTRICT:</b>	2	<b>CONGRESSIONAL DIST.:</b>	10th	<b>RC:</b>	CSRA
<b>FUNDING FOR PI#1</b>	L230S	<b>FUNDING FOR PI#:</b>	LS230	<b>FUNDING FOR PI#:</b>	

**PROJECT LOCATION**



**DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA**

**SPECIAL PROVISION**

**PROJECT: STP00-7063-00(001)  
COUNTY: COLUMBIA  
P.I. NO: 250470-**

**SECTION 150 – TRAFFIC CONTROL**

*Add the following to Section 150:*

**150.11 SPECIAL CONDITIONS:**

- A. The contractor shall not install lane closures, flag or pace traffic, detour, or move equipment or material on the travel ways that interferes with traffic as follows:
1. Washington Rd (SR 104)  
Monday through Thursday: 7:00 am – 8:30 am & 3:30 pm - 7:00 pm  
Friday: 7:00 am – 8:30 am and 3:00 pm – 8:00 pm  
No weekend restrictions
  2. Riverwatch Pkwy/Old Evans/ Petersburg Rd/ Blue Ridge Rd  
Monday through Friday: 7:00 am- 8:30 am & 3:30 pm-7:00 pm  
No weekend restrictions

Failure to comply with this directive will result in the assessment of Liquidated Damages in accordance with Special Provision 108.08.C.1

- B. Phase 1 construction shall be performed such that Columbia Industrial is closed no more than 30 calendar days. The contractor shall coordinate their work such that the closing of Columbia Industrial does not occur with any other road closures. The closing of Old Evans Road and Blue Ridge Road during this time is not permitted. The contractor shall submit their detour plan for review and approval 30 days prior to the closure. Portable Changeable Message Signs shall be incorporated into the TTC Detour plan to notify the public 14 days prior to the closure.

Failure to comply with this directive will result in the assessment of Liquidated Damages in accordance with Special Provision 108.08.C.2

- C. The intersection of Old Evans/Petersburg Rd/ Riverwatch Pkwy/Blue Ridge may be closed for one weekend beginning at 7:00 pm Friday to 7:00 am Monday for the tie in of the intersection. This work shall not interfere with any major events associated with Lakeside Middle/High Schools. The contractor shall submit a TTC detour plan that includes the date and time for the intersection closure to the engineer at least 30 days prior to the closure. Portable Changeable Message Signs shall be incorporated into the TTC Detour plan to notify the public 14 days prior to the closure.

Failure to re-open the intersection within the time allowed for the closure will result in assessments of Liquidated Damages in accordance with Special Provision 108.08 C.3

- D. Beginning the Friday before the Masters Golf Tournament, which begins the Monday of the first full week in April and extending to the Monday after the Masters, the contractor shall cease all operations except for maintaining Traffic Control and Erosion Control. During this period, the contractor shall provide a clean and neat appearance throughout the project, including, but not limited to trash pick-up, mowing, dressing, grassing and mulching.

Failure to cease operations during the Masters Golf Tournament will result in assessments of Liquidated Damages in accordance with Special Provision 108.08.C.4

- E. Beginning Friday night of the Columbia County Christmas Parade Weekend and extending to Monday morning, the contractor shall cease all operation, except for Traffic Control and Erosion Control, on Washington Road. During this period, the contractor shall provide a clean and neat appearance throughout this section of the project, including, but not limited to trash pick-up, mowing, dressing, grassing and mulching.

Failure to cease operations during the Columbia County Christmas Parade will result in assessments of Liquidated Damages in accordance with Special Provision 108.08.C.5





**Emergency Medical Service**

P.O. Box 14848 \* Augusta, Georgia 30919 \* (706) 434-4000 \* Fax: (706) 396-2100

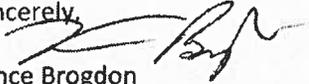
August 20, 2013

Pam,

Gold Cross EMS is aware of the detour on Old Evans Road. I have read the SP 150 enclosed.

Thank you for the information.

Sincerely,

  
Vince Brogdon

CEO

## Diehl, Sean

---

**From:** Diehl, Sean  
**Sent:** Friday, September 06, 2013 2:30 PM  
**To:** Hester, Michael  
**Subject:** FW: Detour for Riverwatch

Sean Diehl - NEPA Planner  
Office of Environmental Services  
Georgia Department of Transportation  
600 W. Peachtree Street, 16th Floor  
Atlanta, GA 30308  
(404) 631-1197



Please consider the environment - do you really need to print this email?

"Our nation's roadsides should be maintained as if they were our nation's front yards." John Kartesz - *Roadside Use of Native Plants*

---

**From:** Brewer, George  
**Sent:** Friday, September 06, 2013 1:59 PM  
**To:** Diehl, Sean  
**Subject:** FW: Detour for Riverwatch

See below.

---

**From:** Schlachter, Matt [<mailto:MSCHLACHTER@columbiacountyga.gov>]  
**Sent:** Friday, September 06, 2013 1:59 PM  
**To:** Brewer, George  
**Subject:** Fwd: Detour for Riverwatch

Their response.

Sent from my iPhone

Matt Schlachter, PE  
Director  
Columbia County Board of Commissioners  
Construction & Maintenance Division  
Mailing: P O Box 498 · Evans, GA 30809  
Physical: 630 Ronald Reagan Dr., Building B, 2nd Floor  
Phone: (706) 868-3356 · Fax: (706) 312-7172

Begin forwarded message:

**From:** "Beatty, Tim" <[tbeatty@ccboe.net](mailto:tbeatty@ccboe.net)>  
**Date:** September 6, 2013, 12:52:30 PM EDT  
**To:** "Schlachter, Matt" <[MSCHLACHTER@columbiacountyga.gov](mailto:MSCHLACHTER@columbiacountyga.gov)>  
**Subject:** RE: Detour for Riverwatch

We will adapt.

Sincerely,

Tim Beatty

---

**From:** Schlachter, Matt [<mailto:MSCHLACHTER@columbiacountyga.gov>]  
**Sent:** Thursday, September 05, 2013 3:33 PM  
**To:** Beatty, Tim  
**Subject:** RE: Detour for Riverwatch

Do you think you will have a response for GDOT by tomorrow afternoon? They are trying to turn the project over to the construction department and wanted to have your comments included in the file.

**Matt Schlachter, PE**  
**Director**  
Columbia County Board of Commissioners  
Engineering Services Division  
*Mailing:* P O Box 498 • Evans, GA 30809  
*Physical:* 630 Ronald Reagan Dr., Building A  
Phone: (706) 868-3356 • Fax: (706) 312-7172  
E-mail: [mschlachter@columbiacountyga.gov](mailto:mschlachter@columbiacountyga.gov)  
[www.columbiacountyga.gov](http://www.columbiacountyga.gov)

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**From:** Beatty, Tim [<mailto:tbeatty@ccboe.net>]  
**Sent:** Thursday, September 05, 2013 2:19 PM  
**To:** Schlachter, Matt  
**Subject:** RE: Detour for Riverwatch

Got it. Thank you.

Sincerely,

Tim Beatty

---

**From:** Schlachter, Matt [<mailto:MSCHLACHTER@columbiacountyga.gov>]  
**Sent:** Tuesday, September 03, 2013 4:10 PM  
**To:** Beatty, Tim  
**Subject:** Detour for Riverwatch

Did the maps ever make it to you? I sent them in separate emails last week. DOT is trying to close out the project so that it can go to bid next month.

**Matt Schlachter, PE**  
**Director**  
Columbia County Board of Commissioners  
Engineering Services Division  
*Mailing:* P O Box 498 • Evans, GA 30809  
*Physical:* 630 Ronald Reagan Dr., Building A

## Diehl, Sean

---

**From:** Lawrence, Charles W.  
**Sent:** Tuesday, March 15, 2011 9:16 AM  
**To:** Rish, Laura  
**Subject:** RE: Columbia 250470 Old Petersburg Road - Revised Studies Request

Good Morning Laura,

As the changes to the project do not increase the project footprint and do not otherwise affect history, the reevaluation is clear for history. A copy of this email will be saved to the history file.

Thanks,

Charles

Charles W Lawrence  
Transportation Planner Historian

Georgia Department of Transportation  
Office of Environmental Services  
600 West Peachtree Street, NW, 16th Floor  
Atlanta, Georgia 30308

404.631.1072  
[cwlawrence@dot.ga.gov](mailto:cwlawrence@dot.ga.gov)

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**From:** Rish, Laura  
**Sent:** Tuesday, March 15, 2011 8:46 AM  
**To:** Phillips, Amber; Pomfret, Jim; Meyers, Sharilyn; Lawrence, Charles W.  
**Cc:** Chamblin, Douglas; Lawrence, Sandy; Hester, Michael  
**Subject:** Columbia 250470 Old Petersburg Road - Revised Studies Request

Good Morning,

I've added the signed and Word copies of the Revised Studies Request for this project to OEL Work Products. I have 3 sets of plans that I will pass out to Amber, Jim, and Charles. Sharilyn, I believe you've been coordinating with design and Ecological Solutions to get the QCQA for ecology done, so these changes should be covered under that report that's underway.

Amber – Otis is currently having traffic updated, but he has not gotten back to me with an ETA. I figure that you wouldn't want to start it until the new noise policy is approved anyway.

Note our schedule: Michael (or whoever will be reassigned to this project) will need your studies and any concurrences/coordination completed by September 30. This will allow a reevaluation to go to FHWA in October. All preconstruction green sheet commitments, permits, etc, will need to be complete by the end of December for the early January 2012 certification for a March 2012 Let.

Please let Michael (or me for the next few weeks) know if you have any questions as you move forward.

*Laura B. Rish*

NEPA/Environmental Analyst

GDOT Environmental Services

600 West Peachtree Street, 16th Floor

Atlanta, GA 30308

Phone: (404) 631-1415

Fax: (404) 631-1916

[LRish@dot.ga.gov](mailto:LRish@dot.ga.gov)

**Baughman, Pamela**

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**From:** Baughman, Pamela  
**Sent:** Monday, November 19, 2012 8:49 AM  
**To:** 'lhesler@astribe.com'; 'joseph.blanchard@astribe.com'; 'aberryhill@mcn-nsn.gov'; 'espain@muscogeenation-nsn.gov'; 'elagrone@mcnnc.com'; 'tyahola@mcnnc.com'; 'Samuel S. Alexander'; 'jsharp@mcnnc.com'; 'rdeere@mcnnc.com'; 'rthrower@pci-nsn.gov'; 'bradleymueller@semtribe.com'; 'elliottYork@semtribe.com'; 'alisonswing@semtribe.com'  
**Cc:** 'Kelly.Wade@fhwa.dot.gov'  
**Subject:** 250470\_Columbia\_Addendum Archaeological Report  
**Attachments:** 250470\_ARCH\_IHSR\_2012.11.19.pdf

Please find attached a Georgia Department of Transportation In-House Archaeological Survey Report. On behalf of the Federal Highway Administration Georgia Division, in keeping with a government-to-government relationship and in compliance with 36CFR800, we are requesting any comments you may have on this Section 106 archaeological letter. Responses to this letter regarding tribal concerns should be addressed to the attention of Mr. Jim Pomfret, the Department's American Indian liaison.

Thanks,  
Pam

***Pamela J. Baughman***  
*Archaeologist*  
*Georgia Department of Transportation*  
*Office of Environmental Services*  
*One Georgia Center*  
*600 W. Peachtree Street NW, 16th floor*  
*Atlanta, GA 30308*  
*(404) 631-1198*  
***pbaughman@dot.ga.gov***

## **Baughman, Pamela**

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**From:** Baughman, Pamela  
**Sent:** Monday, November 19, 2012 9:24 AM  
**To:** 'lhesler@astribe.com'; 'joseph.blanchard@astribe.com'; 'aberryhill@mcn-nsn.gov'; 'espain@muscogeenation-nsn.gov'; 'elagrone@mcnnc.com'; 'tyahola@mcnnc.com'; 'Samuel S. Alexander'; 'jsharp@mcnnc.com'; 'rdeere@mcnnc.com'; 'rthrower@pci-nsn.gov'; 'bradleymueller@semtribe.com'; 'elliottYork@semtribe.com'; 'alisonswing@semtribe.com'  
**Cc:** 'Kelly.Wade@fhwa.dot.gov'; Pomfret, Jim  
**Subject:** 250470\_Columbia\_Section 106 Documentation  
**Attachments:** 250470\_Schoeneberg\_1999.pdf; 250470\_Entorf\_1992.pdf; 250470\_ARCH\_Report\_2011.08.17.pdf

On behalf of the Federal Highway Administration Georgia Division (FHWA), in keeping with a government-to-government relationship, I am forwarding to you Section 106 documentation for the above-mentioned project, PI 250470, Columbia County. The Georgia Department of Transportation (GDOT) is currently in the process of updating older project files in order to assure coordination with American Indian tribal governments.

Please find attached one copy of the two archaeological reports written during previous investigations in 1992 and 1999. In 1992 the FHWA/GDOT did not initiate tribal consultation on this project, as there was no consultation protocol in place until 2001. The enclosed information was prepared for use in compliance with Section 106 of the National Historic Preservation Act of 1966 and subsequent amendments.

In addition to these older reports, you have also been previously sent two more reevaluations of this project, both reported by Pamela Baughman, in 2011 and more recently in 2012. The 2011 report is also attached here for your information; the 2012 report was forwarded to you in an earlier email. The FHWA and GDOT look forward to continued consultation with you on this project.

Please review the enclosed Section 106 documentation and provide comments or concerns you may have with regard to the information contained therein. Your continued consultation in this project is appreciated. If you have any questions concerning the enclosed material or need additional information please contact the GDOT Tribal Liaison, Jim Pomfret at 404.631.1256 or [jpomfret@dot.ga.gov](mailto:jpomfret@dot.ga.gov).

Thanks,  
Pam

***Pamela J. Baughman***  
*Archaeologist*  
*Georgia Department of Transportation*  
*Office of Environmental Services*  
*One Georgia Center*  
*600 W. Peachtree Street NW, 16th floor*  
*Atlanta, GA 30308*  
*(404) 631-1198*  
***[pbaughman@dot.ga.gov](mailto:pbaughman@dot.ga.gov)***



*Absentee Shawnee Tribe of Oklahoma*  
*2025 S. Gordon Cooper*  
*Shawnee, Oklahoma 74801-9381*  
*(405) 275-4030 Fax: 405-878-4533*

Cultural/Historic  
Preservation Department

January 11, 2013

Jim Pomfret  
Archaeology Team Leader  
Georgia Department of Transportation  
Office of Environmental Services  
600 West Peachtree Street, NW  
Atlanta, GA 30308

RE: PI No. 250470, Columbia Co., GA

Dear Jim:

We have received the request to comment on the above referenced project. After review of the provided documents and the data we have on file in our office, we show that there are no historic properties within the delineated project location and, therefore, there will be no effect on historic properties.

Due to the interest we have in the area, however, we would like to be kept informed of any archaeological discoveries of significance and/or of any inadvertent discoveries of human remains. As always, please inform our office of any inadvertent discoveries of human remains within 48 hours and if there are any change in the project location. Any inadvertent discovery of human remains should remain in situ until the appropriate organizations are consulted with.

If you have any questions or comments, please feel free to contact me at the information listed below. You may also contact Joseph H. Blanchard, Cultural Preservation Director/Tribal Historic Preservation Office at 405-275-4030 ex.203 or by email at [joseph.blanchard@astribe.com](mailto:joseph.blanchard@astribe.com).

Sincerely,

*Liana Staci Hesler*

**Liana Staci Hesler**  
**THPO Specialist/Tribal Historic Preservation Office**  
**Absentee Shawnee Tribe of Oklahoma**  
**2025 S. Gordon Cooper Drive**  
**Shawnee, Oklahoma 74804**  
**(405) 275-4030 ext. 199**

**DEPARTMENT OF TRANSPORTATION**

**STATE OF GEORGIA**

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**INTERDEPARTMENT CORRESPONDENCE**

**FILE** P.I. No. 250470-

**OFFICE** Environmental Services

**DATE** August 17, 2011

**FROM**  Pamela J. Baughman, Archaeologist

**TO** Mike Hester, NEPA Team Leader

**SUBJECT** Archaeological Reevaluation of Project STP00-7063-00(001), P.I. #250470,  
Columbia County

The following is being furnished to you for inclusion as written, in the appropriate environmental document:

"In compliance with Section 106 of the National Historic Preservation Act of 1966 and amendments thereto, Project STP00-7063-00(001), P.I. #250470, Columbia County, has been surveyed with respect to archaeological resources, especially those on or eligible for inclusion in the National Register of Historic Places (NRHP). The purpose of the survey was to locate, identify and evaluate the significance of any archaeological resources within the proposed project's area of potential environmental effect.

This project consists of the widening and improvement of Old Petersburg Road and Old Evans Road. The proposed concept includes a four-lane (two lanes in each direction) roadway with bike lanes and a 20-foot raised median on a minimum of 150 feet of right-of-way. The roadway would also have urban shoulders, including curb and gutter, and sidewalks. The widening of Old Petersburg Road would start on existing location approximately 1,400 feet west of the intersection of Baston Road and extend to the intersection with Old Evans Road. From this point, the project would follow Old Evans Road in a northwesterly direction on existing location to Columbia Industrial Boulevard and then extend westward on new location to tie into Washington Road at the intersection with Towne Center Drive. A new bridge would be constructed to grade separate the roadway over the CSX Railroad. This project was surveyed for archaeological resources in 1992 by Robert Entorf and in 1999 by Erica Schoeneberg. During each survey, one archaeological site was located; however, these sites, 9CB143 and 9CB523, were both found to be ineligible for the NRHP. At this time, the project has been revised to include the following changes: Project limits have been reduced along Sydney Street and Briarwood Drive, approximately 0.093 acre of temporary easement has been changed to permanent easement, and the existing culvert over Reed Creek is being replaced with a bridge. An archaeological survey consistent with these project revisions was performed within the project corridor.

An archaeological survey (Level I) was conducted in accordance with "GDOT/FHWA Cultural Resource Survey Guidelines" developed by the GDOT Staff Archeologists in consultation with DNR Historic Preservation Division Staff and concurred in by the Federal Highway Administration and State Historic Preservation Officer. These guidelines provide general survey

boundaries and methodological approaches to archaeological surveys based on the type/scope of work of proposed highway projects and are followed during the initial identification of archaeological resources.

By agreement, since no archaeological resources were located within the project's area of potential effect, no signed concurrence form the State Historic Preservation Officer is required. It is concluded, therefore, that the project will have no effect upon archaeological resources on or eligible for inclusion in the NRHP provided that the project conforms to that described above.

cc: Dr. David Crass, Director and Deputy SHPO, Muscogee (Creek) Nation, Poarch Band of Creek Indians, Seminole Tribe of Florida, Mr. Rodney C. Barry, Federal Highway Administration (Attn: Kelly Wade)

# GDOT Archaeological Assessment In-House Survey Report

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GDOT Project: Old Petersburg Rd/Old Evans Rd from Baston Rd to Washington Rd

Project No.: STP00-7063-00(001)

PI No.: 250470-

County: Columbia

GA SHPO HP No.:

---

Principal Investigator: Pamela Baughman

Crew Members: N/A

Date of Survey: N/A

**Results:**

- Negative Findings       Ineligible or Unknown (Noncontributing) Resource(s) Identified  
(Statement of Eligibility Required)
- No Potential to Cause Effect to Archaeological Resources  
(no ground disturbing activities—field survey not required)

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## Project Location and Area of Potential Effect (APE)

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USGS Quadrangle(s): Evans

**Project Description:** This project consists of the widening and improvement of Old Petersburg Road and Old Evans Road. The proposed concept includes a four-lane (two lanes in each direction) roadway with bike lanes and a 20-foot raised median on approximately 110-150' of proposed right-of-way. The roadway would also have urban shoulders, including curb and gutter, and sidewalks. The widening of Old Petersburg Road would start on existing location approximately 1,400 feet west of the intersection of Baston Road and extend to the intersection with Old Evans Road. From this point, the project would follow Old Evans Road in a northwesterly direction on existing location to Columbia Industrial Boulevard and then extend westward on new location to tie into Washington Road at the intersection with Towne Center Drive. A new bridge would be constructed to grade separate the roadway over the CSX Railroad. The total length of the project is 3.13 miles.

**Area of Potential Effect:** The APE is limited to the proposed right-of-way of the project as well as any required easements. The 100-ft. expanded survey corridor (ESC) was not examined for this project. The project has been revised since it was originally proposed. This report serves to document the changes in the project description and recognize that the project APE has changed in the following ways: 1) Areas of right-of-way have been added or easements changed to right-of-way at 4 locations in order to

changes in the project corridor and state that these areas have been adequately reviewed for archaeological resources.

### **Survey Results**

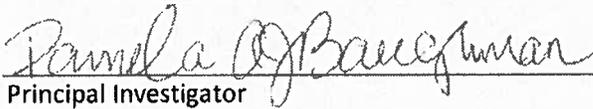
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No Archaeological Resources Identified

*By agreement, because no archaeological resources were located within the project's area of potential effect, no signed concurrence from the State Historic Preservation Office is required.*

### **Signature of Principal Investigator**

---

  
Principal Investigator

11.19.12  
Date

### **Attachment Checklist (check all that apply)**

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- 1. Project Location Map
- 2. USGS Topographic Map with Survey Information
- 3. Photograph(s)
- 5. Statement of Eligibility
- 6. State Site Form
- 7. References
- 8. Other: 1-km GNAHRGIS search information

### **Distribution List:**

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Dr. David Crass, DNR-HPD, Division Director and Deputy SHPO  
Mr. Rodney N. Barry, P.E., FHWA (Attn: Kelly Wade)  
Absentee Shawnee Tribe, Muscogee (Creek) Nation, Muscogee (Creek) National Council, Poarch  
Band of Creek Indians, Seminole Tribe of Florida  
Michael Hester, NEPA Manager

**Addendum to the Ecology Report  
August 2013**

**Columbia County  
P.I. No. 250470**

**Widening and Reconstruction of Old Petersburg Road**

**Prepared by:  
Ecological Solutions, Inc.  
630 Colonial Park Drive, Suite 200  
Roswell, GA 30075**

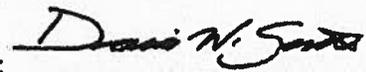
**Prepared for:  
Georgia Department of Transportation  
Office of Environmental Services  
600 W. Peachtree Street NW  
Atlanta, GA 30308**

Report Author: \_\_\_\_\_



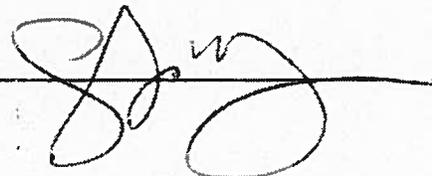
Senior Ecologist

Consultant Reviewer: \_\_\_\_\_



Senior Ecologist, Vice President

GDOT Reviewer: \_\_\_\_\_



Addendum to the Ecology Report Overview  
Columbia County  
PI # 250470

<b>Impacts to Federally Jurisdictional Resources</b>		
<b>Resource Type</b>	<b>Length of Impact (feet)</b>	<b>Area of Impact (acres)</b>
Perennial Stream	572	0.103
Intermittent Stream	43	0.003
Ephemeral Stream	14	0.0006
<b>TOTAL</b>	<b>629</b>	<b>0.1066</b>
Wetland	NA	0.06
Open Water	0	0
<b>TOTAL</b>	<b>0</b>	<b>0.06</b>

<b>Present in the Project Area</b>	
Invasive Species	Yes
Bald Eagle Nest, Habitat	No
Critical Habitat	No
Essential Fish Habitat	No
Bat Roosting Habitat	Yes
Migratory Bird Habitat	No

<b>Agency Coordination</b>	
FWCA	Yes (PS1, PS1a, PS4)
Section 7	No
Buffer Variance	Yes (OW3a)
Buffer Mitigation	No
404 Permit	Yes (NWP14)
404 Permit Mitigation	Yes (2,724.6 stream credits and 0.30 wetland credit)

Federal and State Protected Species							
Species Name	Common Name	Federal Rank	State Rank	Habitat Present	Species Present	Special Provision	Biological Determination
<i>Picoides borealis</i>	Red-cockaded woodpecker	E	E	N	N	NR	NE
<i>Amphiantus pusillus</i>	pool sprite	T	T	N	N	NR	NE
<i>Isoetes tegetiformans</i>	mat-forming quillwort	E	E	N	N	NR	NE
<i>Symphyotrichum georgianum</i>	Georgia aster	C	T	N	N	NR	NE
<i>Trillium reliquum</i>	relict trillium	E	E	N	N	NR	NE

T = Threatened; E = Endangered; C = Candidate; NE = No Effect; NR = Not Required

## Executive Summary

Georgia Department of Transportation (GDOT) project STP00-7063-00(001), PI 250470, proposes the widening and reconstruction of Old Petersburg Road, in Columbia County, Georgia. The project corridor is located approximately 0.2 mile north of the City of Evans. Approximate project length is 3.2 miles. A field assessment was conducted in 2000 and an Ecology Report was completed in April 2004. Since the 2004 Report, the project alignment has been revised. Because of changes to the project alignment and design, subsequent field surveys were conducted in September 2010 and March 2013. This addendum addresses changes to project design and survey findings.

**Habitats and Land Use Areas** – The April 2004 Report addressed habitats and land uses for the project corridor. Per the 2004 Report, approximately 80 percent of the project corridor consists of residential and commercial areas and the remaining 20 percent consists of fragmented woods and riparian corridor. Additional to the listed habitat and land uses, the September 2010 and March 2013 field surveys identified transportation ROW, open field, utility ROW, and Waters of the U.S.

**Invasive Species** – The 2004 ecology report identified four invasive species within the project corridor: Chinese privet (*Ligustrum sinense*), Japanese honeysuckle (*Lonicera japonica*), kudzu (*Pueraria montana*), and wisteria (*Wisteria sinensis*). During the 2010 and 2013 ecology surveys, four additional invasive species were identified within the project corridor: autumn olive (*Elaeagnus umbellata*), chinaberry (*Melia azedarach*), English ivy (*Hedera helix*), and multiflora rose (*Rosa multiflora*). Measures during construction to control the spread of invasive species are identified in this report.

**Federally Threatened, Endangered, and Candidate Species** – The April 2004 report addressed federally protected species within Columbia County. Species reported included bald eagle (*Haliaeetus leucocephalus*), mat-forming quillwort (*Isoetes tegetiformans*), Michaux's sumac (*Rhus michauxii*), pool sprite (*Amphianthus pusillus*), relict trillium (*Trillium reliquum*) and Georgia aster (*Symphyotrichum georgianum*). The 2004 report stated the project as proposed would have no effect on any protected species. Since the 2004 report, the bald eagle has been delisted; however, it remains protected under the Bald and Golden Eagle Protection Act (BGEPA). No changes have occurred to the listing status or effect determinations for the remaining species. One species [red-cockaded woodpecker (*Picoides borealis*)] was not addressed in the 2004 report, which has been since listed in IPaC for Columbia County. No suitable habitat for this species was observed within the survey corridor. The project as proposed would have no effect on the red-cockaded woodpecker.

**Bald and Golden Eagle Protection Act (BGEPA)** – The bald eagle was addressed in the 2004 report as a federally protected species. The report stated that the project would have no effect on this species. Since the 2004 report, the bald eagle has been delisted and is under the protection of the BGEPA. Per the GDNR Natural Heritage Program database, the nearest bald eagle nest is approximately 9.9 miles south of the project, within the Hephzibah quadrangle of Richmond County. No American bald eagle or golden eagle (*Aquila chrysaetos*) individuals, nests, or foraging, nesting, or roosting habitats were identified within the project area during the 2010 or 2013 field surveys. Due to the lack of bald or golden eagles or their respective habitat within the project corridor, project implementation would have no effect on bald or golden eagles.

**Bats** – Bats were not addressed in the 2004 report. The culverts within the project study areas were surveyed on March 25, 2013, for bats. Roosting habitat may exist within the culverts and forested areas adjacent to the project corridor; however, no individuals or evidence of the presence of bats (i.e. odors or guano staining) were identified. Because clearing is proposed within forested areas, the project is expected to have no significant adverse effect on bat species.

**Critical Habitat** – Critical habitat was not addressed in the 2004 report. The USFWS Critical Habitat Portal indicates that no critical habitat is designated for Columbia County and none of the species listed within Columbia County have designated critical habitat. The project would have no effect on critical habitat.

**Essential Fish Habitat (EFH)** – EFH was not addressed in the 2004 report. The proposed project does not occur in a county that has EFH; therefore, the project would have no impacts to EFH.

**Migratory Birds** – Per the 2004 report, the entire alignment is located in highly fragmented habitats. The fragmentation is such that migratory bird species sensitive to edge effects would not find suitable habitat along the proposed alignment. The culverts and bridges within the study area may support nesting migratory birds; however, no nests were observed during the March 2013 field survey. Because the bridge over PS2 will be replaced, Special Provisions 107.23(G) would be implemented to protect migratory birds prior to and during project construction (Appendix A).

**Jurisdictional Waters of the U.S.** – Field studies were conducted in 2000 and findings were summarized in the 2004 report. Per the 2004 report, three perennial streams (Stream 1, Stream 2 and Stream 4) and one intermittent stream (Stream 3) were identified. Field studies conducted September 2010 and March 2013 identified five perennial streams (PS1, PS1a, PS2, PS3, and PS4), two intermittent streams (IS2a and IS2b), one ephemeral channel (EC2c), one open water (OW3a), and one wetland (WL4a) within the survey area. Approximately 629 linear feet of stream and 0.06 acre of wetland will be impacted as a result of project implementation. The project impacts are covered under Nationwide Permit (NWP) 14 (Linear Transportation Projects). A pre-construction notification (PCN) will be required for this project. Proposed impacts would require the purchase of compensatory mitigation credits from a USACE-approved mitigation bank servicing the project area. Credit requirements total 2,724.6 stream mitigation credits and 0.30 wetland mitigation credits.

**State Protected Species** – Per correspondence from GDNR dated August 30, 2010, no known occurrences of state-listed species have been recorded within a three-mile radius of the project study areas. The project would have no effect on state-protected species.

**State Waters and Buffers** – A total of 18 state waters, of which eight are state waters requiring a buffer were identified within the survey corridor during the field surveys. A stream buffer variance will be required for impacts proposed to OW3a buffer.

DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA

SPECIAL PROVISION

PROJECT: STP00-7063-00(001) COLUMBIA COUNTY  
P.I.# 250470

Section 107 – Legal Regulations and Responsibility to the Public

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*Add the following to Subsection 107.23:*

**G. Protection of Federally Protected Species**

The following conditions are intended as a minimum to protect these species and its habitat during any activities that are in close proximity to the known location(s) of these species. When there is a conflict between the General Provisions and the Special Provisions, these Special Provisions will govern the work.

1. The Contractor shall advise all project personnel employed on this project about the potential presence and appearance of the federally protected barn swallow (*Hirundo rustica*), cliff swallow (*Petrochelidon pyrrhonota*), and eastern phoebe (*Sayornis phoebe*). All personnel shall be advised that there are civil and criminal penalties for harassing, harming, pursuing, hunting, shooting, wounding, killing, capturing, or collecting these species in knowing violation of the Migratory Bird Treaty Act of 1918. Pictures and habitat information will be provided to the Contractor at the preconstruction conference and shall be posted in a conspicuous location in the project field office until such time that Final Acceptance of the project is made.
2. Any construction activity that would disturb the underside of the bridge at STA 141+75 (Old Petersburg Road over Reed Creek/Stream 2) shall take place outside of the breeding and nesting season of phoebes and swallows, which begins April 1 and extends through August 31, unless exclusionary barriers are put in place to prevent birds from nesting. Any construction activity that would extend or replace box culverts at STA 206+50 (Stream 1) shall take place outside of the breeding and nesting season of phoebes and swallows, which begins April 1 and extends through August 31, unless exclusionary barriers are put in place to prevent birds from nesting. The following steps shall be followed if exclusionary barriers are to be used:
  - a. Exclusionary barriers shall be installed on bridges and culverts prior to March 1 or after August 31, but in no time in between this period.
  - b. The underside of the bridge or the inside of the box culvert shall be checked for nests prior to the placement of exclusionary barriers. If nests are present, the nest shall be checked to ensure that eggs or birds are not present. If the nests are found to be occupied by birds or eggs, the installation of exclusionary barriers shall be postponed until after August 31 when the breeding season is complete.
  - c. Prior to the installation of any exclusionary barriers, the project ecologist shall be notified by phone of the type of barrier and the proposed date of installation at (404) 631-1100.
  - d. For box culverts, exclusionary barriers may consist of overlapping strips of flexible plastic (also called "PVC Strip Doors" or "Strip Curtains") or an alternate material proposed by the Contractor and approved by the Project Engineer prior to installation.

- e. For bridges, exclusionary barriers may be netting made of plastic, canvas or other materials that are proposed by the Contractor and approved by the Project Engineer. The barriers shall cover the full length of the bridge to prevent the birds from accessing any existing nesting habitat.
  - f. If the exclusionary barrier fails to prevent nesting (i.e., birds are able to bypass barriers and build nests), postpone construction activities associated with the bridge until after August 31.
  - g. During construction activities, inspect exclusionary netting for holes or other defects that impair the netting's ability to exclude phoebes or swallows from inhabiting the bridge. Any holes or defects shall be repaired immediately.
3. In the event any incident occurs that causes harm to the barn swallow, cliff swallow, and eastern phoebe along the project corridor, the Contractor shall report the incident immediately to the
- a. Project Engineer
  - b. Glenn Bowman, State Environmental Administrator, Georgia Department of Transportation, Office of Environmental Services at (404) 631-1101.

All activity shall cease pending consultation by the Department with the U. S. Fish and Wildlife Service and the Federal Highway Administration.

4. The Contractor shall keep a log detailing any sightings or injury to barn swallows, cliff swallows, and eastern phoebes in or adjacent to the project until such time that Final Acceptance of the project is made. Following project completion, the log and a report summarizing any incidents involving these species shall be submitted by the Contractor to the
- a. Project Engineer
  - b. the State Environmental Administrator (Georgia Department of Transportation, Office of Environmental Services, 600 West Peachtree Street NW, Atlanta, Georgia 30308).

The GDOT Office of Environmental Services shall provide a copy of the report to the U.S. Fish and Wildlife Service and the Federal Highway Administration.

5. All costs pertaining to any requirement contained herein shall be included in the overall bid submitted unless such requirement is designated as a separate Pay Item in the Proposal.

# Barn swallow *Hirundo rustica*

## Identification Tips:

Length: 6 inches

Tiny bill

Dark orange forehead and throat

Pale orange underparts

Dark, iridescent upperparts

Long, deeply forked tail

Juvenile similar to adult but paler underneath with a shorter tail

Most often seen flying

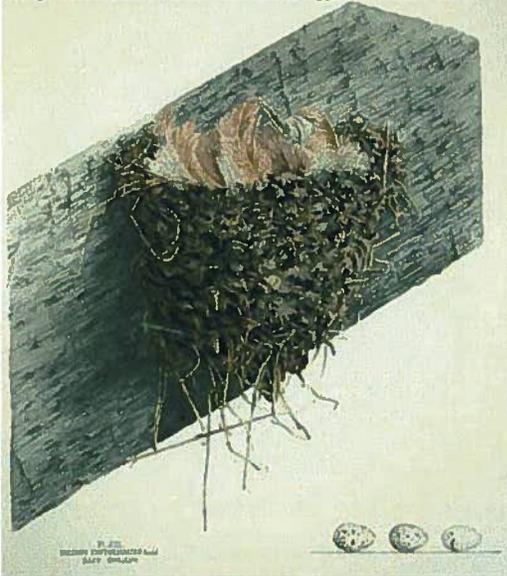
Will nest communally in mud nests under bridges, in barns and caves, etc



## Similar species:

The Barn Swallow can be told from all swallows by its deeply forked tail.

Image from *Illustrations of the Nests and Eggs of Birds of Ohio*



Nests built against a wall or other vertical surface in a half-cup, semicircular shape. Nests built on top of a beam or other horizontal surface form a complete cup about 3 inches across at the rim and 2 inches deep. Nests have a mud/grass shelf and cup lined with grass and feathers.



## Eastern phoebe *Sayornis phoebe*

### Identification Tips:

Length: 5.75 inches

Dark bill

Pale throat and underparts – whitish in Spring, yellowish in Fall

Grayish – olive upperparts

Frequently wags tail

Juvenile has buffy wing bars

Often builds nest under bridges or around buildings near water



The Phoebe nest is a cup of mud and moss lined with grasses, hair, and feathers.





## United States Department of the Interior

Fish and Wildlife Service  
105 Westpark Drive, Suite D  
Athens, Georgia 30606

SEP 06 2013

West Georgia Sub Office  
P.O. Box 52560  
Ft. Benning, Georgia 31995-2560

Coastal Sub Office  
4980 Wildlife Drive  
Townsend, Georgia 31331

Mr. Rodney Barry, P.E.  
Division Administrator  
Federal Highway Administration, Georgia Division  
61 Forsyth Street, SW  
Suite 17T100  
Atlanta, Georgia 30303  
ATTN: Ms. Kelly Wade

RE: GDOT P.I. No. 250470, USFWS Log# 04EG1000-2013-CPA-0848

Dear Mr. Barry:

Thank you for your August 29, 2013, electronic mail regarding Georgia Department of Transportation (GDOT) project STP00-7063-00(001). We submit the following comments under provisions of the Fish and Wildlife Coordination Act (FWCA) (48 Stat. 401, as amended; 16 U.S.C. 661 *et. seq.*).

GDOT proposes to widen Old Petersburg Road in Columbia County, Georgia. As a consequence of project implementation, 615 linear of stream channel would realize impacts, as detailed in GDOT's August 9, 2013 consultation package and revisions from August 29, 2013.

GDOT investigated options to minimize and avoid impacts to aquatic resources. Further avoidance of resources was not practicable due to design constraints and potential residential displacements. GDOT would mitigate for impacts with the acquisition of credits from an approved mitigation bank.

We concur with your determination that impacts to streams along the project corridor are unavoidable and necessary to implement the proposed project. GDOT's mitigation proposal satisfies your agency's responsibilities under FWCA. No additional compensation is necessary.

If you have any questions or require further information, please contact staff biologist Pete Pattavina, at 706-613-9493, ext. 236.

Sincerely,

Robin B. Goodloe  
Acting Field Supervisor

cc: Doug Chamblin, GDOT  
file

**From:** [Katy.Allen@dot.gov](mailto:Katy.Allen@dot.gov) [<mailto:Katy.Allen@dot.gov>]  
**Sent:** Thursday, September 12, 2013 2:45 PM  
**To:** Chamblin, Douglas; Meyers, Sharilyn  
**Cc:** Bowman, Glenn; D'Avino, Gail; Hester, Michael; [Pete.Pattavina@fws.gov](mailto:Pete.Pattavina@fws.gov); [Kelly.Wade@dot.gov](mailto:Kelly.Wade@dot.gov)  
**Subject:** FW: FW: STP00-7063-00(001), Columbia, 250470 - 2013 Ecology Addendum/request for FWCA

Based on the information below, we can not reach a conclusion that that a survey should not be performed. Please plan to perform a survey at an appropriate time in October for the Georgia aster.

*Katy Allen, P.E.*

Environmental Team Leader  
Federal Highway Administration, Georgia Division  
61 Forsyth St, SW - Suite 17T100  
Atlanta, GA 30303

Phone: 404-562-3657 Fax: 404-562-3703

**Please consider the environment before printing this message!**

**From:** Pattavina, Pete [[mailto:pete\\_pattavina@fws.gov](mailto:pete_pattavina@fws.gov)]  
**Sent:** Thursday, September 12, 2013 10:48 AM  
**To:** Allen, Katy (FHWA)  
**Cc:** Tom Patrick; Chamblin, Douglas; Meyers, Sharilyn; Bowman, Glenn; D'Avino, Gail; [mhester@dot.ga.gov](mailto:mhester@dot.ga.gov)  
**Subject:** Re: FW: STP00-7063-00(001), Columbia, 250470 - 2013 Ecology Addendum/request for FWCA

Hi, Katy. Looking at the photos GDOT provided, it is very difficult to determine that habitat is not present in some of the images pictured. I'm not sure if they bring us any closer to a decision as to whether or not a survey should be completed for aster. Old Petersburg Road is about 4 miles east of the type locality for the species, a very old record, and I don't see any other records of the species nearby, if that means much of anything. I will say that Old Petersburg Road lies over a different geologic unit than where we have the record to the east, so that might make the area less likely to support the species. Where the type locality is known, it is over Meta-Argillite/Phyllite, and it is my understanding that ultramafic areas could be found as blocks within Phyllite, perhaps more suitable for Georgia aster and the plant communities it is found within in the Piedmont, most times. The Old Petersburg Road area is over Granite Gneiss in the Piedmont, but much of the roadway is over sandy-mud soils from Lower Tertiary-Cretaceous where it is in the Coastal Plain, an area where we wouldn't typically think of GA aster being found. Over those Coastal Plain soils, I would say that we are outside of the species' range, so that might cut down the corridor suitability some--and it might be worth looking at a smaller portion of the roadway in terms of habitat suitability. I've attached a quick graphic showing the boundaries of the Granite Gneiss in the Piedmont (highlighted in blue). Everything north of the blue line is Piedmont and everything south of the blue line is Coastal Plain geology.

Photo 1: This is one of images where I would be comfortable saying that there is no potential habitat. The right-of-way is too maintained, two roads running parallel to one another, meaning

there is a lot of historical ground disturbance, and I wouldn't expect the species to be able to compete in the wet ditch area.

Photo 2: Again, this right-of-way appears too disturbed to support the species.

Photo 3: It is difficult to say if habitat would be present in this photo. It is certainly possible, depending upon the historic forest management. It looks fairly open in the understory, but it doesn't look like great habitat, since it looks like a water oak pictured, and perhaps sweet gum and perhaps small tulip poplars in the midstory. Sometimes GA aster can hangout in the understory and it is only visible a number of seasons after a timber harvest.

Photo 4: Habitat appears unlikely. I've seen the species in a thin border of privet, but what the woodland looks pretty moist from the photos, perhaps not conducive for GA aster and the competition with other species.

Photo 5: I've seen GA aster in areas like this, rights-of-way with exposed soils. I've also seen it on the edge of a cemetery in some pretty poor woods in Gwinnett, also growing along a stop sign near Athens, in a recently constructed subdivision. In the picture, the railroad right-of-way is recently cut, but typically they are not mowed as often as roads. Again, not great habitat for the species, and if it were to be found in this area, probably only a couple orphaned clumps. Not good habitat and I would be surprised if GA aster could be here, but it is possible.

Photo 6: Powerlines are great areas for GA aster. The picture is difficult to comment on, because it looks like it was taken in an area that is maintained in a different manner in the foreground than in the background, where there is a lot of broom sedge. In the background, it could be great habitat, the foreground, not so much. Powerlines like the one pictured are sometimes maintained by adjacent landowners as well as the power company.

On Wed, Sep 11, 2013 at 10:10 AM, <[Katy.Allen@dot.gov](mailto:Katy.Allen@dot.gov)> wrote:

Tom, I have a favor to ask: Can you provide FHWA some technical assistance in deciding whether or not GDOT should survey for the GA Aster in October on this project in Augusta?

From the photos, some of the areas seem perhaps not to be suitable habitat, but along the ditches and in the utility ROW I am not so sure and would appreciate your expertise, if you are willing to provide.

GDOT plans to let this project in October. However, that does not necessarily preclude requiring a survey, if warranted, before they would award a contract. On the other hand, I don't want to waste taxpayer dollars if to you and USFWS, as technical experts, are of the opinion that there is no suitable habitat present.

If there is any additional information that would help, I will coordinate with GDOT to provide. I don't know if these are the only photos, or just representative photos. This project has been around for over 10 years, so there may be some additional photographs at GDOT.

Regards,

Katy Allen, P.E.  
Environmental Team Leader  
Federal Highway Administration, Georgia Division  
61 Forsyth St, SW - Suite 17T100  
Atlanta, GA 30303  
Phone: 404-562-3657 Fax: 404-562-3703  
Please consider the environment before printing this message!

From: Meyers, Sharilyn [mailto:[SMeyers@dot.ga.gov](mailto:SMeyers@dot.ga.gov)]  
Sent: Tuesday, September 10, 2013 1:50 PM  
To: Wade, Kelly (FHWA); Allen, Katy (FHWA); 'Pattavina, Pete ([pete\\_pattavina@fws.gov](mailto:pete_pattavina@fws.gov))'  
Cc: Chamblin, Douglas; Hester, Michael; Diehl, Sean; D'Avino, Gail; Bowman, Glenn  
Subject: RE: STP00-7063-00(001), Columbia, 250470 - 2013 Ecology Addendum/request for FWCA  
Importance: High

Good Afternoon Kelly, Katie, and Pete,

Our consultant has provided some additional information in support of their no effect determination for the GA aster along with a photo log. Please let us know if you concur with their response or if you would for GDOT to perform the additional survey for the species.

"During our surveys of the project corridor, we identified several areas that fit the general description of potential habitat for the Georgia aster. The potential habitat of this species can be worded so generally that it mistakenly includes all roadside ROW. Suitable habitat is described as edges and openings in rocky, upland oak-hickory-pine forests, and ROW areas through these dry habitats. The most suitable habitat is often along utility ROW areas and other openings where current land management mimics natural disturbance regimes. The primary controlling factor appears to be the availability of light and early successional habitat that is not dominated by woody or invasive species may be considered suitable habitat.

Our surveys did identify roadside ROW and areas where light availability is prevalent along the project corridor (utility and transportation ROW, residential lawns, open field, and commercial/residential landscaped areas); however, these areas are regularly disturbed by mowing and experience more frequent disturbance than highway ROW that may be more suitable habitat for Georgia aster. The mowing and other disturbance in these areas occurred much more frequently than natural disturbance regimes (typically fire). In addition, these areas are not adjacent to the preferred forested communities and do not resemble dry, prairie like conditions preferred by Georgia aster.

These results were the same as those found during the original ecology survey in 2004. For this reason, we determined that the project would have no effect on the federally candidate Georgia aster."

Thank you,

Sharilyn Meyers  
Ecology Team Leader  
Office of Environmental Services  
600 W. Peachtree Street, 16th Floor  
Atlanta, GA 30308  
(404) 631-1594 phone  
(404) 631-1916 fax

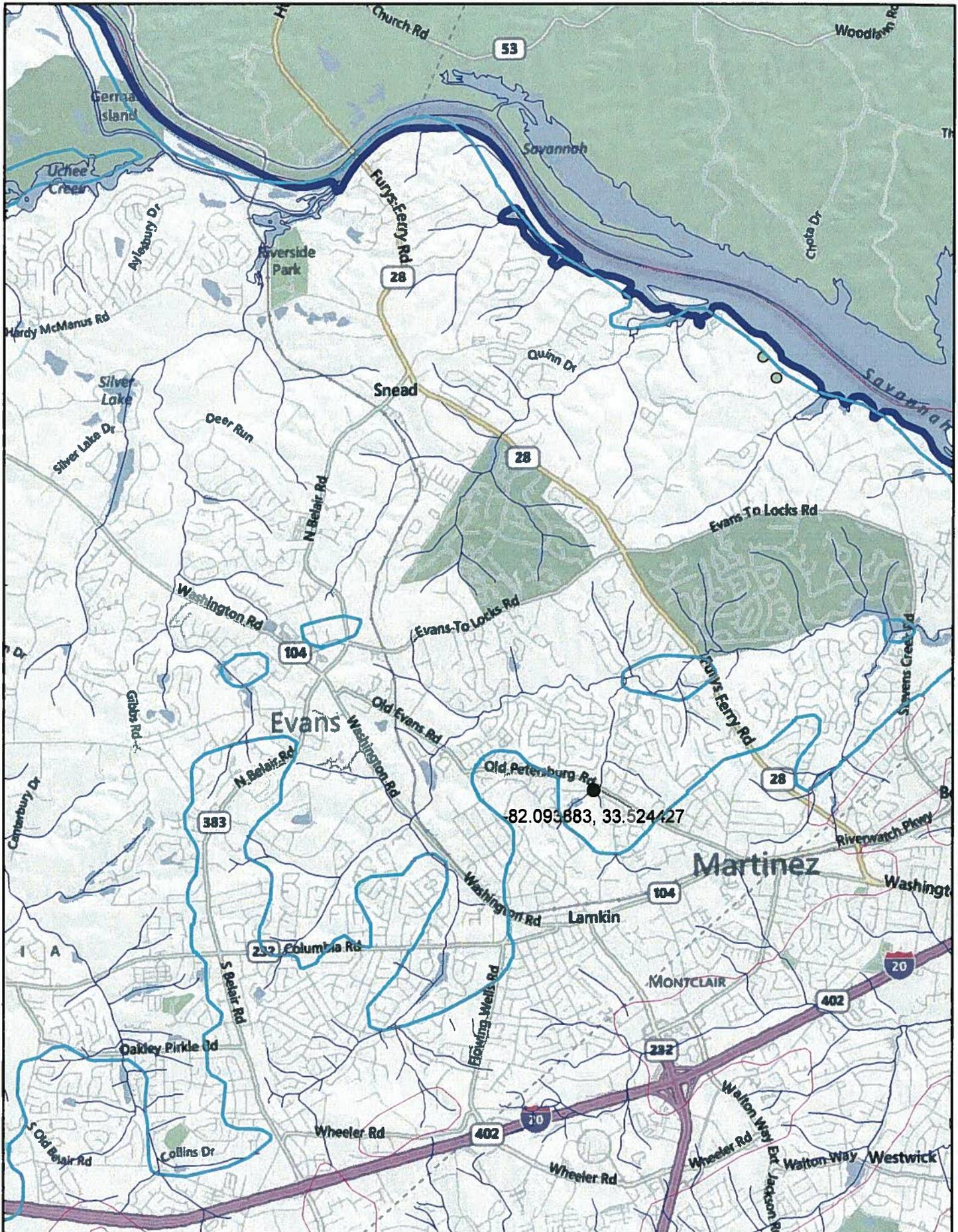
From: [Kelly.Wade@dot.gov](mailto:Kelly.Wade@dot.gov)<<mailto:Kelly.Wade@dot.gov>> [<mailto:Kelly.Wade@dot.gov>]  
Sent: Thursday, August 29, 2013 12:52 PM  
To: Meyers, Sharilyn; [pete\\_pattavina@fws.gov](mailto:pete_pattavina@fws.gov)<[mailto:pete\\_pattavina@fws.gov](mailto:pete_pattavina@fws.gov)>  
Cc: Chamblin, Douglas; Hester, Michael; Diehl, Sean;  
[Katy.Allen@dot.gov](mailto:Katy.Allen@dot.gov)<<mailto:Katy.Allen@dot.gov>>  
Subject: RE: STP00-7063-00(001), Columbia, 250470 - 2013 Ecology Addendum/request for FWCA

Hi Sharilyn,

Based on your letter dated 8/21/13 suitable habitat exists for the Georgia Aster, therefore is necessary to perform a survey for individuals prior to construction. I will go ahead and initiate FWCA and make effect determinations for the other species, however GDOT will need to commit to perform the survey for GA Aster during this year's survey season. The commitment should also state that if individuals are observed, GDOT will coordinate with FHWA, FWS and DNR to determine the appropriate course of action.

Thanks,

Kelly Wade  
Environmental Specialist  
Federal Highway Administration  
61 Forsyth Street, SW  
Suite 17T100  
Atlanta, GA 30303



**DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA**

---

**INTERDEPARTMENTAL CORRESPONDENCE**

<b>FILE</b>	PI# 250470	<b>OFFICE</b>	Environmental Services
<b>FROM</b>	Mark Ballard, Ecologist Ecological Solutions, Inc.	<b>DATE</b>	September 12, 2013
<b>TO</b>	File		
<b>SUBJECT</b>	Georgia Department of Transportation (GDOT) Project STP00-7063-00(001), Columbia County, PI # 250470; Description of potential habitat for Callaway ginger ( <i>Hexastylis shuttleworthii</i> var. <i>harperi</i> ) and pink ladyslipper ( <i>Cypripedium acaule</i> )		

The purpose of this memo is to address concerns regarding the potential presence of Callaway ginger (formerly state-listed as unusual) and pink ladyslipper (state-listed unusual) within a parcel along the referenced project. These concerns were identified in an August 30, 2001, comment received from a resident (Linda McFarland) along the project corridor. The property address is 128 Jamaica Ct, Evans, GA 30809. Ms. McFarland commented:

*I have federally endangered wild plants growing in my backyard. My biggest concern is getting those plants moved - many are only above ground in the spring. I have also spent a great deal of money developing the native garden area - I would like this reflected in the monetary value of my property and in the time and expense in relocating the plants especially the endangered ones.*

Subsequently, the Georgia Department of Transportation (GDOT) conducted a site visit at the property in 2002 and determined that the plant species were the state-listed Callaway ginger and pink ladyslipper and that no federally protected species were located on the property.

The project is scheduled for let in October 2013 and a follow-up survey for the referenced property was conducted on September 11, 2013. The subject parcel is approximately 1 acre and is located off of Jamaica Ct. Prior to the field survey, available resources were reviewed for federal and state protected species listed for Columbia County. Pink ladyslipper is not listed for Columbia County and Callaway ginger is no longer state-listed. The site visit determined that the residential structure has been removed. The majority of the site is wooded with the exception of an approximately 40-foot by 70-foot grassed area. A perennial stream (Stream 4) is located within the property.

The portion of the property east of Stream 4 is heavily vegetated with a very thick understory and groundcover. Dominant canopy species are sweetgum (*Liquidambar styraciflua*) and loblolly pine (*Pinus taeda*). Sweetgum provides the majority of the tree cover. The understory is dominated by Chinese privet (*Ligustrum sinense*), Carolina cherry (*Prunus caroliniana*), and Japanese privet (*Ligustrum japonicum*). The majority of the eastern portion of the property had a very dense coverage of Chinese wisteria (*Wisteria sinensis*) and Japanese honeysuckle (*Lonicera japonica*). Combined, these two species shaded or covered the majority of the ground. Other dominant vines included poison-ivy (*Toxicodendron radicans*) and Virginia creeper (*Parthenocissus quinquefolia*). Remnant ornamental species including hollies and monkey-grass (*Liriope* sp.) were also observed near the location of the former home.

The portion of the property west of Stream 4 consists of a small terrace and an adjacent wooded slope that extends to the maintained right-of-way along Washington Road. Dominant canopy species included sweetgum, water oak (*Quercus nigra*), and oaks (*Quercus* spp.). Understory species included Carolina cherry and American elm (*Ulmus americana*). Compared to the eastern side of the stream, the understory was relatively open with significantly less Chinese privet. While Chinese wisteria was not observed, the groundcover had extensive coverage of Japanese honeysuckle and poison-ivy. The understory and groundcover within the 25-foot buffer of the stream appeared to be more open than the adjacent slope. There is potential that this area was maintained by the prior resident given the lack of privet. While the understory was open, the canopy was dense which limited sunlight availability. Refer to the attached photos for representative on-site conditions.

### Pink ladyslipper (*Cypripedium acaule*) - State-listed Unusual

Pink ladyslipper, also called moccasin flower, is a perennial orchid which has a single, showy pink flower topping a solitary, pubescent stalk. The large, distinctive basal leaves are 8 to 30 centimeters (cm) long and 2.5 to 15 inches wide. The leaves are bright green with sticky hairs and raised, parallel veins on the upper surface and grayish underneath. The flower stalk can grow up to 0.6 meter tall. The flower is composed of a pink to magenta, pouch-like petal and two reddish brown or green, twisted petals on either side of the pouch. Rarely, the pouch is white. Topping the flower structure is a green sepal which curves over the top of the petals. The pink ladyslipper produces an oval capsule fruit (May to July) about 4 cm long, containing many thousands of tiny seeds.

When in flower, no other Georgia plant can be mistaken for pink ladyslipper. Two other Georgia orchids, lily-leaved tway-blade (*Liparis liliifolia*) and showy orchis (*Galearis spectabilis*), also produce paired basal leaves, however, the leaves of these species are smooth and glossy without raised, parallel veins.

Preferred habitat for the pink ladyslipper includes upland pine and mixed pine-hardwood forests. This species requires highly acidic soils and will tolerate a range of shade and moisture. Pink ladyslipper prefers at least partial shade, well-drained slopes, and an understory that is not densely vegetated. In mountainous areas, this species can be found near edges of rhododendron thickets and in bogs. Seeds of this species will only germinate if they land on soil containing a specific fungus that provides nutrients for germination and plant growth.

The pink ladyslipper can be found throughout eastern United States, the upper Midwest, and adjacent provinces of Canada. In Georgia, it is known to occur in almost all northern counties. Many populations are known to occur on U. S. National Forest property in northern Georgia. This species thrives in areas with periodic burning that helps maintain the community's pine component.

As noted, pink ladyslipper is not listed for Columbia County. Habitat within the site does not exhibit the pine component typically associated with known populations of pink ladyslipper. Canopy pines were limited to less than five trees with the majority being on the eastern portion of the property. The eastern portion of the property has a dense understory/vine component that covers almost 100-percent of the ground. While the understory is more open on the western portion of the property, the site does not exhibit typical pink ladyslipper habitat. The former resident may have planted pink ladyslipper on-site. While the specimens may have survived under close habitat management, the site in its current state does not provide suitable habitat for this species.

### Callaway ginger (*Hexastylis shuttleworthii* var *harperi*) - Not Listed

Callaway ginger was previously reported from the site. At the time of the original comment, this species was listed as unusual. Based on review of Georgia Department of Natural Resources Wildlife Resources Division information ([http://georgiawildlife.com/sites/default/files/uploads/wildlife/nongame/text/html/protected\\_species/Plants.html](http://georgiawildlife.com/sites/default/files/uploads/wildlife/nongame/text/html/protected_species/Plants.html)), the species is no longer state-listed.

Callaway ginger, a perennial herb, is a member of the Aristolochiaceae family (Dutchmans Pipe family). Leaves are thick and evergreen with strong mottling or variegation. The leaves are round to cordate (heart shaped) and 3 to 7 cm in length. When crushed, the leaves emit a strong ginger odor. The plant spreads via rhizomes that send up new leaves. This spreading growth habit results in a dense, mat-like growth with known populations covering several square meters.

Flowers are solitary and originate from the base of the plant and can be obscured by leaf litter. The flowers are brownish-red and resemble urns or jugs causing some to refer to gingers as little brown jugs. Flowers are critical in the identification of gingers. *Hexastylis shuttleworthii* and its varieties have the largest flower, reaching 2 to 3 cm in length, of any *Hexastylis*.

This species is known from the Coastal Plain of Alabama and Georgia Piedmont. A well-established population has been documented at Calloway Gardens in Georgia and is commonly cultivated within the gardens. Preferred Coastal Plain habitat includes mesic hammocks and the toe of slope along forest slopes and associated floodplains. Piedmont habitat consists of saturated, peaty soils adjacent to forested wetlands.

During the field survey, a species of *Hexastylis* was observed along the western side of Stream 4. No flowering specimens were observed (the flowering period is April - June for most species of *Hexastylis*). The observed specimens were in clumps and did not exhibit a spreading/creeping growth habit. Rather the leaves originated from a central root system. The majority of the plants were located on the western side of the stream; however, several clumps were located on the eastern side of the stream. Each observed clump was marked with a 36-inch wire pin flag with orange polka dots over a white field (see attached photos). A total of 127 clumps were marked (122 west of Stream 4 and 5 east of Stream 4).

Based on the surveyor's prior experience with Callaway ginger and other species of *Hexastylis*, it is our opinion that the species within the survey area is not Callaway ginger. While gingers can be difficult to identify without flowers, they can be separated into the general groups based on leaf appearance. As discussed above, Callaway ginger leaves are rounded to cordate, 3 to 7 cm long, strongly mottled, and arise from shallow-creeping rhizomes. Due to the creeping nature of the rhizomes, the plant can form mats.

The leaves of the on-site specimens are consistently arrow-like and do not have the cordate or rounded leaves nor the strong mottling typical of Callaway ginger. The mature leaves are consistently 7 to 10 cm in length. None of the plants observed had leaves arising from creeping rhizomes. Rather, each plant was a distinct clump arising from a central, well-developed root system. Though not flowering, the observed plants did not have any of the predominant characteristics of Callaway ginger. Without flowers, it is not possible to identify the plants to species. Likely candidates are *H. arifolia* and *H. speciosa*. *H. speciosa* does not naturally occur in Georgia; however, the former resident noted that she had planted the specimens. The planted specimens could have been transplanted natives or a purchased variety. The *Hexastylis* on-site do not appear to be *H. shuttleworthii* var. *harperi* (Callaway ginger). Regardless of exact determination, no species of *Hexastylis* are currently listed as state-protected in Georgia.

In conclusion, the site does not provide habitat for pink ladyslipper and the *Hexastylis* on-site does not appear to be Callaway ginger. No species of *Hexastylis* are currently state-listed. Construction of the project should have no effect on these two species.

MB

Attachment

cc: George Brewer, GDOT Project Manager  
Sean Diehl, GDOT NEPA Analyst



Photograph 1 - Subject property as viewed from Jamaica Ct



Photograph 2 - Dense understory with Japanese privet



Photograph 3 - Chinese privet and Chinese wisteria forming dense cover



Photograph 4 - Residential debris within survey area

50506-124\_EnvDoc\_Columbia\_470\_EnvDocMemo.FH11



### Representative Photographs

STP00-7063-00(001)  
Columbia County  
P.I. No. 250470  
Widening and Reconstruction  
of Old Petersburg Road



Photograph 5 - Looking toward Jamaica Ct from survey area



Photograph 6 - Stream 4



Photograph 7 - Typical conditions west side of Stream 4



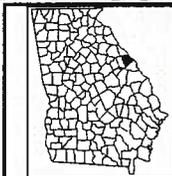
Photograph 8 - Typical habit of on-site *Hexastylis*

50508-124\_EnvDoc\_Columbia\_470\_EnvDocMemo.FH11

	<p><b>Representative Photographs</b></p>	<p>STP00-7063-00(001) Columbia County P.I. No. 250470 Widening and Reconstruction of Old Petersburg Road</p>
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Photograph 9 - Pin flags marking identified *Hexastylis* clumps



**Representative Photographs**

STP00-7063-00(001)  
Columbia County  
P.I. No. 250470  
Widening and Reconstruction  
of Old Petersburg Road

**DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA**

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**INTERDEPARTMENT CORRESPONDENCE**

<b>FILE</b>	P.I. No. 250470	<b>OFFICE</b>	Environmental Services
		<b>DATE</b>	August 5, 2013
<b>FROM</b>	Amber L. Phillips 		
<b>TO</b>	File		
<b>SUBJECT</b>	STP00-7063-00(001), Columbia County - Noise Memo		

There have been no changes to the project; however, since the completion of the June 26, 2013 Noise Addendum, public involvement has wrapped up for Barrier 1, located South of Old Evans Road West of Old Evans Road and Blue Ridge Drive and Barrier 3, located north of Old Petersburg Road and Lantern Lane. The Department received three comments in favor of constructing Barrier 1 (at an apartment complex) and three comments in favor of constructing Barrier 3. No one opposed the barriers. Therefore, Barrier construction is proposed as part of the proposed project. (See Attached for comments)

ALP

Cc:

General Files

Noise Impact Assessment Addendum II PI # 250470  
STP00-7063-00(001), Columbia County  
June 27, 2013

Old Petersburg Road/Old Evans Road from Baston Road to Washington road

**Executive Summary**

This addendum was completed to update the noise barrier discussion based on final design. Two barriers are currently proposed for construction and undergoing public involvement to determine reasonableness.

Approved By: 

Date: 6-27-2013

**Noise Impact Assessment Addendum:**

P.I. No. 250470

GDOT Project STP00-7063-00(001)

Columbia County, Georgia

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**Date:**

July 19, 2012

**Brief project name:**Old Petersburg Road/Old Evans Road from  
Baston Road to Washington Road**MPO Plan name:**Augusta Regional Transportation Study (ARTS)  
2030 Long Range Transportation Plan (LRTP)**TIP identification number:**

STP-2

**Existing Year:**

2011

**Build Year :**

2035

**LOS:****Existing (2011)**

LOS C

**Build (2035)**

LOS C

**Executive Summary**

The purpose of this report is to comply with the "Highway Traffic Noise Policy and Guidance" which was issued in July 2010 (revised January 2011) by the Federal Highway Administration (FHWA) and with the July 2011 Georgia Department of Transportation (GDOT) noise policy and also update the noise model from STAMINA to FHWA Traffic Noise Model (TNM), version 2.5 with updated traffic information. Original noise study was completed in December 1999.

For the Old Petersburg Road/Old Evans Road project, the existing (2011) condition showed four impacted receptors (4 residential units). The no-build (2035) condition showed sound-level impacts on thirty receptors

(29 residential units, 1 office unit). The build (2035) condition showed sound-level impacts on one hundred and five receptors (101 residential units, 1 office unit, 2 restaurant units, and 1 recreational unit). Furthermore two residential units in the build conditions were also impacted due to a substantial increase criteria as the predicted future traffic noise levels would "substantially exceed" the existing noise levels.

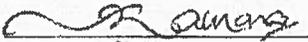
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**Statement of Likelihood:**

Based on the studies completed to date, 105 impacted receptors have been identified and it has been determined that noise abatement is likely, but not guaranteed, at three locations described as follows: barriers are proposed south of Old Evans Road west of Old Evans Road and Blue Ridge Drive intersection., north of Old Petersburg Road at Lantern Lane and also north of Old Petersburg Road at Coach Lane between Kelly Park and Stephens Road. Noise abatement at these locations is based upon noise analyses and design criteria. The final decision on the installation of any abatement measure(s) will be made upon the completion of the public involvement process.

**Prepared By:**

Consultant Preparer: Ramana Vadarevu

Signature:  Date: 7/19/2012

Consultant Reviewer: Pat Smeeton

Signature:  Date: \_\_\_\_\_

Approved By: 

Date: 8-28-2012

**Georgia Department of Transportation**  
**Citizen Survey for Proposed Noise Wall 1**

*Please print responses.*

Name: Richard Scott

Address: 473 Old Evans Road, Apt 1012, Martinez, GA 30907

Do you support the proposed noise wall?       YES     NO

Comments:

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I would like to be contacted regarding the proposed project and/or noise barrier. The best number to contact me at is: \_\_\_\_\_ between the hours of \_\_\_\_\_.

Thank you for your input.  
Please return this comment card in the provided pre-addressed, pre-stamped envelope.

**Georgia Department of Transportation**  
**Citizen Survey for Proposed Noise Wall 1**

*Please print responses*

Name: Kellon Hanes

Address: 473 Old Evans Rd Marietta Ga  
30907 Apt 1023

Do you support the proposed noise wall?  YES  NO

Comments:  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

I would like to be contacted regarding the proposed project and/or noise barrier. The best number to contact me at is: \_\_\_\_\_ between the hours of \_\_\_\_\_.

Thank you for your input.  
Please return this comment card in the provided pre-addressed, pre-stamped envelope.

**Georgia Department of Transportation  
Citizen Survey for Proposed Noise Wall 1**

*Please print responses.*

Name: Ollie Steates

Address: 473 Old Evans Rd Apt. 1015 Martinez, GA 30907

Do you support the proposed noise wall?  YES  NO

Comments: I think the noise barrier is absolutely  
necessary. My apartment backs up to Old Evans Rd  
and it is already pretty noisy.

*I would like to be contacted regarding the proposed project and/or noise barrier. The best number to contact me at is: \_\_\_\_\_ between the hours of \_\_\_\_\_.*

Thank you for your input.  
Please return this comment card in the provided pre-addressed, pre-stamped envelope.

**Phillips, Amber**

---

**From:** Mark stagich <stagichm@hotmail.com>  
**Sent:** Monday, July 15, 2013 2:27 PM  
**To:** Phillips, Amber  
**Subject:** RE: Candlestick Way proposed noise wall

Amber Phillips,

Thank you for emailing me the correct location of the sound barrier. I am in favor of the barrier.

Thanks,

Mark Stagich

> From: [aphillips@dot.ga.gov](mailto:aphillips@dot.ga.gov)  
> To: [stagichm@hotmail.com](mailto:stagichm@hotmail.com)  
> Date: Thu, 11 Jul 2013 16:35:49 -0400  
> Subject: Candlestick Way proposed noise wall  
>  
> Mr. Stagich,  
>  
> Please find attached the plan sheet showing the proposed sound barrier in the location of your neighborhood. Let me know if you need any additional information or have any questions. I look forward to hearing from you about your desires for the wall.  
>  
> Thanks,  
>  
>  
> Amber L. Phillips  
> Georgia Department of Transportation  
> Office of Environmental Services  
> One GA Center  
> 600 West Peachtree Street  
> Floor 16  
> Atlanta, GA 30308  
> Phone: 404-631-1117  
> Fax: 404-631-1916  
>  
>  
>  
>  
> -----Original Message-----  
> From: [go1606@gdot.ad.local](mailto:go1606@gdot.ad.local) [<mailto:go1606@gdot.ad.local>]  
> Sent: Thursday, July 11, 2013 11:10 AM  
> To: Phillips, Amber

> Subject: Scanned from a Xerox multifunction device

>

>

>

> Please open the attached document. It was scanned and sent to you using a Xerox multifunction device.

>

> Attachment File Type: pdf

>

> multifunction device Location: 16th Floor, 600 West Peachtree Street, Atlanta, GA 30308

> Device Name: go1606

>

>

> For more information on Xerox products and solutions, please visit <http://www.xerox.com>

>

> Georgia DOT provides funding and technical assistance to support construction and maintenance of the State's harbors in Savannah and Brunswick, which contributes an annual economic value of \$18.5 billion and supports more than 350,000 jobs. The Department is a local sponsor for the U.S. Army Corps of Engineers that constructs and maintains these harbors.

>

> Visit us at <http://www.dot.ga.gov>; or follow us on <http://www.facebook.com/GeorgiaDOT> and <http://twitter.com/gadepoftrans>

>

**Georgia Department of Transportation  
Citizen Survey for Proposed Noise Wall 3**

Please print responses.

Name: MARK STAGICH

Address: 362 CANDLESTICK WAY MARTINEZ, GA. 30907

Do you support the proposed noise wall?       YES  NO

Comments:

ACCORDING TO THE CONDITIONS OF THE SALE, I WAS  
GIVEN A TEMPORARY EASEMENT TO REPLACE MY FENCE  
TO IT'S ORIGINAL LOCATION. THE PROPOSED SITE FOR THIS  
WALL WILL NOT ALLOW FOR THIS.

I would like to be contacted regarding the proposed project and/or noise barrier. The best number to contact me at is: 706-339-6046 between the hours of 9am & 5pm.

Thank you for your input.  
Please return this comment card in the provided pre-addressed, pre-stamped envelope.

**Georgia Department of Transportation**  
**Citizen Survey for Proposed Noise Wall 3**

*Please print responses.*

Name: Jerome Osborne, Executor of the Estate of Gay Osborne

Address: P.O. Box 4114 Marietta GA 30947

Do you support the proposed noise wall?

YES  NO

Comments:

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I would like to be contacted regarding the proposed project and/or noise barrier. The best number to contact me at is: \_\_\_\_\_ between the hours of \_\_\_\_\_.

Thank you for your input.  
Please return this comment card in the provided pre-addressed, pre-stamped envelope.

PROJECT: STP-7063(1)  
COUNTY: Columbia  
P.I. #: 250470

DATE: August 4, 1992

NAME OF BUSINESS: Fast Fare

ADDRESS: 444 Old Evans Road, Martinez, Georgia 30907

OWNER: Fast Fare Inc.

PARCEL #: 14 ← NOW CALLED PAR. 108

UST SYSTEM TO BE ACQUIRED: No

BORING NUMBER(S): #1 & #2

BORING LOCATION/STATION #: #1 - 40 ft. from exist. C/L Old Evans Rd., 75  
ft. from exist. C/L Old Petersburg Rd.  
#2 - 40 ft. from exist. C/L Old Evans Rd., 137  
ft. from exist C/L Old Petersburg Rd.

DEPTH OF BORING(S): #1 & #2 = 17 ft.

DEPTH GROUNDWATER ENCOUNTERED: #1 & #2 = Dry

SOIL TYPE: Brown Silty Sand

INSTRUMENTATION USED: Combustible Gas Analyzer (CGA) and Sensidyne Detector  
Tubes (SDT)

CONTAMINATION ENCOUNTERED: No

LEVEL OF CONTAMINATION:

BORING(S)	#1	#2
CGA (%)	0	0
SDT (ppm gas)	0	0

ADDITIONAL COMMENTS: UST system has been removed from this site. Pump  
island is still in place.

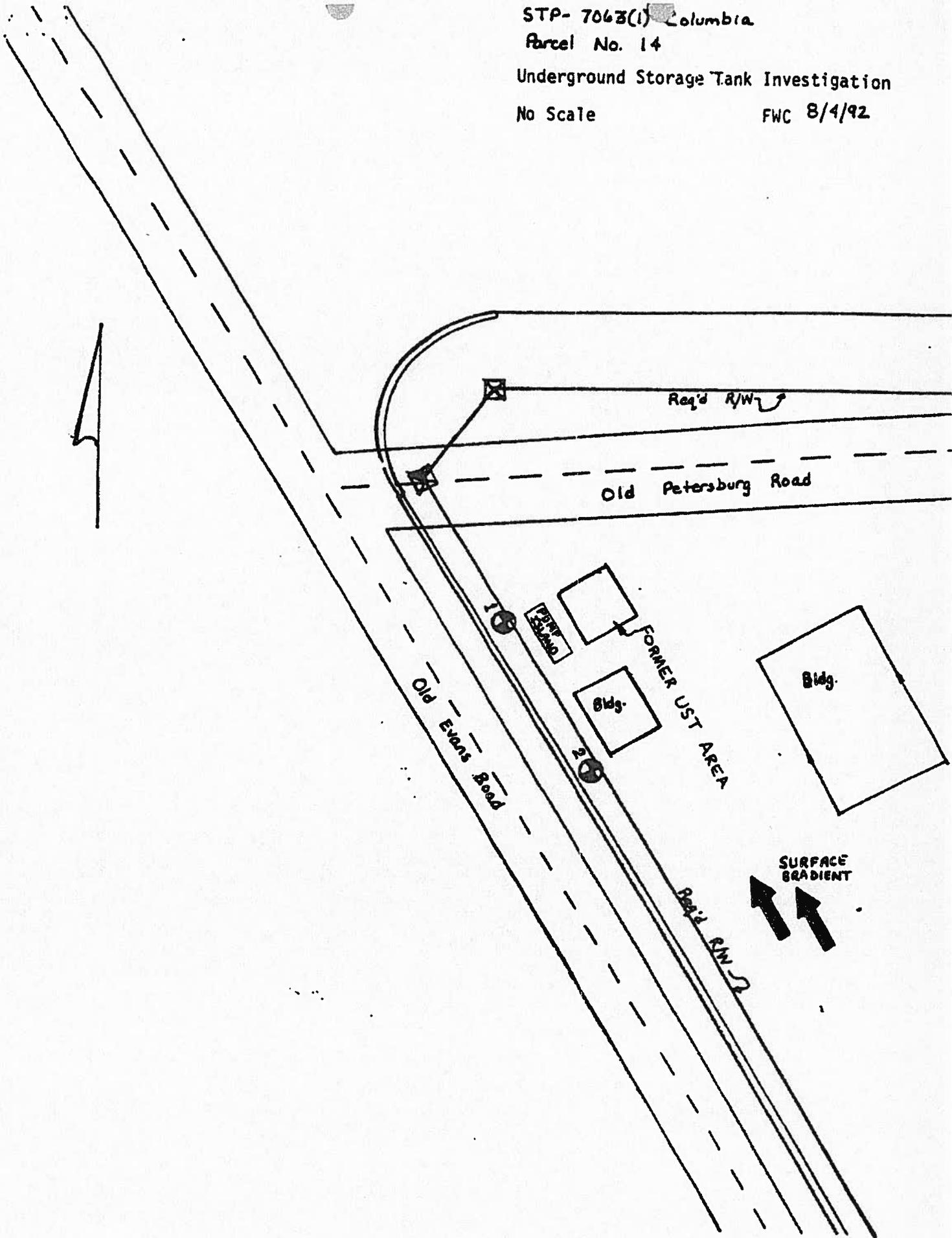
STP- 7063(1) Columbia

Parcel No. 14

Underground Storage Tank Investigation

No Scale

FWC 8/4/92



**DEPARTMENT OF TRANSPORTATION**

**STATE OF GEORGIA**

**INTERDEPARTMENT CORRESPONDENCE**

**FILE** STP-7063(1)COLUMBIA **OFFICE** Materials & Research  
PI 250470 Forest Park, Georgia  
*28 Nov* **DATE** November 12, 2003

**FROM** Georgene M. Geary, P.E., State Materials and Research Engineer

**TO** Michael L. Thomas, P.E., District Engineer, Tennille  
Attn: Phillip Scarborough

**SUBJECT** **RESULTS OF UST/HAZARDOUS INVESTIGATION**

The attached information details the results of the subsurface investigation conducted for the subject project to determine whether contamination from USTs or hazardous waste is present within the required right-of-way. Six sites were investigated, Parcel Nos. 4, 14, 22, 29, 108 and 111.

Contamination from USTs was not encountered at Parcel Nos. 4, 14, 29 and 108. The USTs will not be acquired at Parcel Nos. 4 and 14. The USTs have been removed from Parcel Nos. 29 and 108. Right-of-way acquisition may proceed.

Contamination from USTs was not encountered at Parcel No. 111. Right-of-way acquisition will include the UST system. Right-of-way acquisition may proceed. The Office of Right-of-Way, acting as the authorized agent for the property owner, should remove the UST system from Parcel No. 111 utilizing qualified consultants.

Parcel No. 22 was investigated for possible soil contamination from hazardous waste. No contamination was encountered within the required right-of-way at this site. Please refer to the attached information sheet and laboratory test reports. Since contamination was not encountered, it appears that right-of-way acquisition may proceed.

Michael L. Thomas  
Page Two  
November 12, 2003

If you have any questions, they may be addressed to Thomas Scruggs or  
Claire Durham at 404-363-7546 of this office.

GMG/TES/CCD

Attachment

c: Tom Turner, Director of Preconstruction  
Terry McCollister w/attachment (attn: Mack Cravey), Office of Right-  
of-Way  
Harvey Keeper w/attachment, Office of Environment and Location  
Joe Wheeler w/attachment, Office of Consultant Design  
District Right-of-Way Office w/attachment, Tennille

# Georgia Department of Natural Resources

Environmental Protection Division  
Underground Storage Tank Management Program  
4244 International Parkway, Suite 104, Atlanta, Georgia 30354  
Noel Holcomb, Commissioner  
Carol A. Couch, Ph.D., Director  
404/362-2687

December 10, 2008

Mr. Larry Hunter  
Circle K Stores, Inc.  
2440 Whitehall Park Dr., Suite 800  
Charlotte, NC 28273

**SUBJECT: UST Closure Report**  
**No Further Action Required:**  
Circle K Store #5345  
446 Old Evans Road  
Martinez, Columbia County, GA  
Facility ID: 9036006\*4

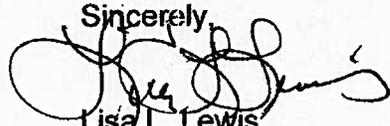
Dear Mr. Hunter:

The Georgia Underground Storage Tank Management Program (USTMP) has received your consultant's letter, dated November 6, 2008, that forwarded a properly certified UST Closure Report. The report was prepared by S&ME, Inc.

Based on current requirements of the Georgia Underground Storage Tank Act, the Georgia Rules for Underground Storage Tank Management (GUST Rules) and the data submitted, the USTMP has determined that **no further action is required for the referenced release.**

However, further corrective action may be required if mandated through more stringent State or Federal statutory or regulatory changes. Additional measures may also be required if existing or future drinking water systems or surface water bodies within two miles of the site are impacted by any dissolved contamination resulting from this release, or if previously unidentified soil contamination, dissolved contamination or free product are identified as originating from this site.

If you have any technical questions, please contact Ronald J. Wallace at (404) 362-2589.

Sincerely,  
  
Lisa L. Lewis  
Unit Coordinator  
Corrective Action Unit II

RJW:  
s:\and\anddocs\ust\cau2\ronaldw\pending8\9036006-34  
cc: Jesse L. Keeffe, Jr., P.G., S&ME, Inc.  
Ronald J. Wallace, GA EPD  
File (CA): Columbia, 9036006



November 6, 2008

Circle K Stores, Inc.  
2440 Whitehall Park Drive, Suite 800  
Charlotte, North Carolina 28273

Attention: Mr. Larry Hunter

Reference: **USTMP Closure Report**  
Circle K Store #5345  
446 Old Evans Road  
Columbia County  
Martinez, Georgia  
S&ME Project No. 1614-08-335

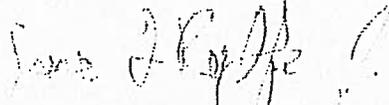
Dear Mr. Hunter:

S&ME, Inc. has completed the USTMP Closure Report for the referenced property. The attached report presents the findings of S&ME's USTMP Closure which was performed in general accordance with the Underground Storage Tank (UST) Closure Guidance, GUST-9 [Revised Nov 2006] and as authorized by your acceptance of S&ME Proposal No. 1614-6209-08, dated August 22, 2008 and our Agreement for Services.

S&ME appreciates the opportunity to provide this USTMP Closure Report for the project. Please contact us at your convenience if there are questions regarding the information contained in this report.

Sincerely,

S&ME, Inc.

  
Jesse L. Keefe, Jr., P.G.  
Project Geologist

  
Thomas Behnke, P.G.  
Environmental Manager

**Georgia Department of Natural Resources**

Environmental Protection Division

Land Protection Branch

4244 International Parkway, Suite 104, Atlanta, Georgia 30354

Lonice C. Barrett, Commissioner

Harold F. Reheis, Director

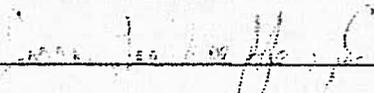
(404)362-2537

**USTMP CLOSURE REPORT FORM**

If a boring or monitor well was extended to groundwater, the Professional Engineer (PE) or Professional Geologist (PG), registered in the State of Georgia, that supervised the work must complete and sign the following statement:

"I have supervised and directed the installation of the boring or monitor well and the interpretation of groundwater data, in accordance with the Water Well Standards Act, the Professional Engineer Act and the Professional Geologist Act. This report complies with the standards of the USTMP Act, Rules, and guidelines and other applicable state and federal environmental regulations. The information presented herein is true and accurate."

Name (print) Jesse Lee Keefe Jr.

Signature  Date 11/26/01

PG/PE Certification Expiration Date 12/31/2008

  
Georgia Stamp or Seal

# USTMP CLOSURE REPORT FORM

Facility ID #: 9036006

Complete this form and provide documentation to substantiate information as outlined in the Underground Storage Tank (UST) Closure Guidance Document (GUST-9). Use a separate form for each tank excavation.

**I. GENERAL**

A. UST OWNER Company Name (if applicable): Circle K Stores, Inc.  
 Mailing Address: 2440 Whitehall Park Drive, Suite 800 City: Charlotte State: NC Zip: 28273  
 Owner's Name (printed): Mr. Larry Hunter Phone: (704) 583-5727

I hereby certify that the information in this Closure Report and in all the attachments is true, accurate, and complete, and the Closure Report satisfies all criteria and requirements of Rule 391-3-15-.09 of the Georgia Rules for Underground Storage Tank Management.

Signature (of owner listed under "Name" above): [Signature] Date: 1/14/08

**B. REMOVAL CONTRACTOR (Prime Contractor/Prime consultant)**

Company: S&ME, Inc.  
 Mailing Address: 134 Suber Road City: Columbia State: SC Zip: 29210

Name of Company Representative (printed): Tom Behnke, P.G. Phone: (803) 561-9024

I hereby certify that I have performed or supervised the work detailed in this report, and have examined and am familiar with the information submitted in this and all attached documents. The submitted information is, to the best of knowledge, true, accurate, complete, and in accordance with the Georgia Rules for Underground Storage Tank Management, revised February, 1995.

Signature (of same contractor listed under "Name"): [Signature] Date: 1/19/08

C. UST Site Facility Name: Circle K Store #5345 County: Columbia Fac. I.D. #: 9036006

Street Address: 446 Old Evans Road City: Martinez State: GA Zip: 30907

**II. TANKS AND PIPING CLOSURE DATA**

**A. LIST USTs THAT HAVE BEEN CLOSED (Use the same tank ID # as on the 7530-1):**

TANK ID#	<u>1</u>	<u>2</u>	<u>3</u>	<u>4</u>
Product	<u>Gasoline</u>	<u>Gasoline</u>	<u>Gasoline</u>	<u>Diesel</u>
Size (gals)	<u>12,000</u>	<u>12,000</u>	<u>12,000</u>	<u>12,000</u>
How Closed	<u>RG</u>	<u>RG</u>	<u>RG</u>	<u>RG</u>
Date Last Used	<u>09/01/2008</u>	<u>09/01/2008</u>	<u>09/01/2008</u>	<u>09/01/2008</u>
Date Closed	<u>09/16-24/2008</u>	<u>09/16-24/2008</u>	<u>09/16-24/2008</u>	<u>09/16-24/2008</u>

(Date removed from ground or filled in-place)

**LIST ANY USTs STILL IN USE AT THE FACILITY (Use same tank ID # as on 7530-1):**

TANK ID#	<u>N/A</u>	_____	_____	_____	_____
Product	_____	_____	_____	_____	_____
Size (gals)	_____	_____	_____	_____	_____

B. PIPING: How was Piping closed?  Removed.  Emptied, capped, left in place.  
 Emptied filled with inert material.

If only piping was closed, give date: \_\_\_\_\_ (month, day, and year)

**III. SAMPLING AND ANALYTICAL**

**A. Soil/Groundwater Sampling:** The quantity of samples taken should be in accordance with USTMP closure guideline (GUST-9) requirements and all samples must be collected in accordance with current EPA-approved sampling procedures.

**B. Regulated Substance Released:** Whenever free product is encountered and/or analytical results indicate that BTEX, PAH, or TPH contamination is present in the soil and/or groundwater, a release must be reported to EPD via telephone or fax by the next business day explaining what has been found and what steps were taken to eliminate any hazardous conditions and prevent the spread of contamination. Indicate here what substance, if any, was released:

None  Gasoline  Diesel    Kerosene    Used Oil    Other (Name):  
Date release reported to EPD: Spoke with Kelly Adams of GADEP on 09/18/2008

**C. Laboratory Analytical Methods Used (check all that were used):**

5035-8021B    5035-8015  5035-8260  8100    Other 8270

If Method 5035 was used to sample, which method was used to collect and contain the samples?  
Encore™    Syringe/corer and field-preserved in 40 ml vial

**IV. TANK EXCAVATION SAMPLES (see Section V. of this form for piping trench samples)**

<u>Size (capacity in gallons) of UST</u>	<u># of samples required per UST</u>
<1,050	1
1,050 - 12,500	2
> or equal to 12,501	2 per UST + 1 per additional 10,000 gals

(Collect 1 sample per UST if a groundwater sample was collected within 2 feet of the excavation.)

**A.** Based on the total number of USTs closed as reported on this form, the total number of tank excavation samples taken for this site was: eight (8)

**B.** If over-excavation is performed, take one confirmation sample every 30 linear feet along the base of the sides (within 1 ft of the bottom of the excavation) and one sample per 200 sq ft along the bottom of the excavated area.

- 1) Was over-excavation performed? Yes  No
- 2) If "yes", what was the area of the excavation in square feet? 1,050
- 3) Enter total number of over-excavation samples for this site here: Thirteen (13) samples were submitted for laboratory analysis (seven (7) over-excavation samples, four (4) sidewall samples, and two (2) product piping locations). A total of (22) samples were obtained for screening with an OVA-FID (that include the thirteen over-excavation samples submitted for laboratory analysis).

- C. Site-Specific Hydrogeology:** 1.) Was Groundwater encountered? \_\_\_ Yes X No  
 2.) If encountered, at what depth: N/A feet  
 3.) If Table B Threshold Levels are being used, how far is the nearest drinking water well or point of withdrawal for drinking water? N/A ft.

*\*Please note that this well located 4,000 ft from the former UST location is City Well #10 and is hydraulically separated from the surficial aquifer. City Well #10 is screened in the Floridan Aquifer.*

**D. Groundwater conditions:** If more than one foot of groundwater covers more than 50% of the base of the excavation, a groundwater sample may be taken in lieu of soil samples from the base of the excavation. One soil sample per UST must still be collected at the fill-pipe end of each UST along the sidewalls at the soil-water interface.

Enter total number of soil-water interface samples for this site here: N/A

**V. PIPING SYSTEM EXCAVATION SAMPLES**

**A. PIPING TRENCH**

Distance from UST to nearest dispenser island: Less Than 25 ft \* 25 feet or more  
# of samples required for each trench: 0\* 1 sample per 25 feet\*\*

What was the distance from the USTs along each piping trench to the nearest dispenser island?

Unknown (feet) (if more than one trench)

How many confirmation samples were collected from each piping trench?

1 (piping trench 1)

4 (piping trench 2)

**B. DISPENSER ISLAND**

Number of dispenser islands \* Length of each Dispenser Island (ft) / 25(ft) = # of Samples  
 (Rounded up to nearest whole number)

How many dispenser islands were present in the closed system(s)? 2

How long was each dispenser island (ft)? 32

How many dispenser samples were collected? 4

\* Although no piping trench samples are required if the piping length is <25 ft., dispenser samples are required. Exception: If the dispenser is directly above the tank excavation, no piping samples and no dispenser samples would be required.

\*\* This includes all fittings (couplings, elbows, flex hoses, etc.) between the tank and the dispenser island. Do not count fittings at the tank excavation and the islands. For straight piping runs, estimate 20 ft between couplings.

**VI. EXCAVATED SOIL**

**A. Sampling:**

How many cubic yards of material was excavated? <500 cubic yards but placed back into excavation.  
 Based on one sample per 200 cubic yards of excavated soil or fraction thereof, the total number of excavated soil samples: three (3)

## VII. CLOSURE SUMMARY

### A. CONCLUSIONS

\_\_\_\_\_ Soil or groundwater contamination exists in excess of the levels specified in the above situations and this closure report is being submitted within a certified CAP-Part A.

\_\_\_\_\_ Soil or groundwater contamination exists in excess of the levels specified in the above situations and this closure report is being submitted without a certified CAP-Part A.

X Clean Closure, No Further Action Required because analytical results indicate the condition marked below:

\_\_\_ BTEX and TPH are below detection limits (BDL) in the soil.

\_\_\_ BTEX and PAHs are BDL in the soil and TPH (and BTEX) is vertically delineated to BDL above the groundwater table.

\_\_\_ BTEX and PAHs are above detection limits in soil but below Table A Threshold Levels, and TPH, PAHs, and BTEX in soil is vertically delineated to BDL above the groundwater table.

X BTEX and PAHs are above detection limits but below Table B Threshold Levels, a water supply survey indicates there are no potential receptors within the applicable radii, and BTEX, PAHs, and TPH in soil is vertically delineated to BDL above the groundwater table.

\_\_\_ BTEX and PAHs are less than Table A Threshold Levels and BTEX, PAHs or TPH is not vertically delineated to BDL above the groundwater table because groundwater is encountered in the boring or the excavation, and the water sample does not contain BTEX or PAHs above Federal or State MCLs.

\_\_\_ BTEX and PAHs are less than Table B Threshold Levels and BTEX, PAHs, or TPH is not vertically delineated to BDL above the groundwater table because groundwater is encountered in the boring or excavation, and the water sample does not contain BTEX or PAHs above In-stream Water Quality Standards, and the water supply survey indicates that there are no water supplies within the applicable radii.

**B. SITE MAP (Attach to report):** The map must be to scale OR, as a minimum, distances between the tank pit area, piping trenches, dispenser islands, sewer, water, utility lines (or other preferential pathways), road and main buildings must be accurately indicated on the map. These listed features must be depicted on the map in order to accurately interpret the data. The map must also include a north (N) directional arrow. Tank ID's must correspond to EPA Form 7530-1 and sample locations, sample identification numbers and depths must also be shown. Sample numbers must correspond to attached laboratory analytical data. Although not mandatory, photos may be attached to help clarify the UST system layout.

**DEPARTMENT OF TRANSPORTATION**

**STATE OF GEORGIA**

**Project STP00-7063-00(01)**

**Columbia County**

**PI No. 250470**

**Reevaluation**

**Attachment 3**

**REPORT COORDINATION**

Keith Golden, P.E., Commissioner



DEPARTMENT OF TRANSPORTATION

One Georgia Center, 600 West Peachtree Street, NW  
Atlanta, Georgia 30308  
Telephone: (404) 631-1000

September 11, 2012

Mr. Rodney N. Barry, P.E.  
Division Administrator  
Federal Highway Administration  
Atlanta Federal Center  
61 Forsyth Street, S.W.  
Suite 17 T100  
Atlanta, Georgia 30303-3104

ATTN: Kelly Wade

Dear Mr. Barry:

Re: Project STP00-7063-00(001), Columbia County, P.I. No. 250470 - Old Petersburg  
Road/Old Evans Road from Baston Road to Washington Road

Please find enclosed the noise addendum for the above noted project. It is being sent to you for your information and files.

Should you need further information, please contact Amber Phillips at (404) 631-1117.

Sincerely,

A handwritten signature in cursive script that reads "Glenn Bowman".

Glenn Bowman, P.E.  
State Environmental Administrator

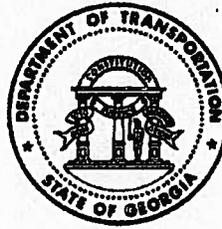
GB/zc  
Enclosures

cc:

General File (letter, report)  
Project File (electronic)  
Reading File (letter only)

**Keith Golden, P.E., Commissioner**

---



**GEORGIA DEPARTMENT OF TRANSPORTATION**

One Georgia Center, 600 West Peachtree Street, NW  
Atlanta, Georgia 30308  
Telephone: (404) 631-1000

July 3, 2013

Mr. Rodney N. Barry, P.E.  
Division Administrator  
Federal Highway Administration  
Atlanta Federal Center  
61 Forsyth Street, S.W., Suite 17 T100  
Atlanta, Georgia 30303-3104  
ATTN: Kelly Wade

Re: Project STP00-7063-00(001), Columbia County, P.I. No 250470: Old Petersburg Road/Old Evans Road from Baston Road to Washington Road

Dear Mr. Barry:

Please find enclosed the Noise Assessment Addendum II for the above noted project. It is being sent to you for your information and files. Should you need further information, please contact Amber Phillips at 404-631-1117 or Soli Shakshuki at 404-631-1093.

Sincerely,

A handwritten signature in black ink that reads "Glenn Bowman" followed by a stylized flourish.

Glenn Bowman, P.E.  
State Environmental Administrator

GB/AP  
Enclosures

cc: General File (with attachment)

Keith Golden, P.E., Commissioner



GEORGIA DEPARTMENT OF TRANSPORTATION

One Georgia Center, 600 West Peachtree Street, NW  
Atlanta, Georgia 30308  
Telephone: (404) 631-1000

August 9, 2013

Mr. Rodney N. Barry, P.E., Division Administrator  
Federal Highway Administration  
Suite 17T100  
61 Forsyth Street, S.W.  
Atlanta, Georgia 30303-3104  
ATTN: Kelly Wade

Re: Ecology Addendum Transmittal and Request for Coordination under the Fish and Wildlife Coordination Act (FWCA) for GDOT Project STP00-7063-00(001), Columbia County, P.I. No. 250470; Widening and Reconstruction of Old Petersburg Road

Dear Mr. Barry:

The purpose of this letter is to transmit the Addendum to the Ecology Report for the above referenced project and to request coordination under FWCA. Georgia Department of Transportation (GDOT) project STP00-7063-00(001), PI 250470, proposes to widen and reconstruct Old Petersburg Road in Columbia County, Georgia. The project corridor is located approximately 0.2 mile north of the City of Evans. Approximate project length is 3.2 miles. A field assessment was conducted in 2000 and an Ecology Report was completed in April 2004. Since the 2004 Report, the project alignment has been revised. Because of changes to the project alignment and design, subsequent field surveys were conducted in September 2010 and March 2013. This addendum addresses changes to project design and survey findings.

The anticipated impacts associated with the proposed project are as follows:

- Approximately 0.06 acre of permanent wetland impacts, requiring the purchase of 0.30 wetland credits. No wetlands were identified in the April 2004 Ecology Report within the project corridor;
- Approximately 629 linear feet of permanent stream impacts, requiring the purchase of 2,724.6 stream credits. The 2004 Ecology Report documented 730 feet of stream impact, requiring 3,622 stream credits;
- No effect to the federally protected bald eagle (*Haliaeetus leucocephalus*), mat-forming quillwort (*Isoetes tegetiformans*), Michaux's sumac (*Rhus michauxii*), pool sprite (*Amphianthus pusillus*), relict trillium (*Trillium reliquum*) and Georgia aster (*Symphotrichum georgianum*) and red-cockaded woodpecker (*Picoides borealis*);
- No effect on critical habitat;
- No impacts to essential fish habitat;
- Special Provisions 107.23G will be implemented; therefore, no impacts to migratory birds;
- No effect on state protected species;
- No significant adverse effect on bats; and
- Non-exempt impacts to state protected buffers are proposed, requiring a state stream buffer variance for OW3a.

The Department wishes to reinstate coordination under FWCA for three features that are described in the attached August 2013 Addendum. Consultation under FWCA was previously completed in 2007 for 675 feet of impact to Stream 4. Impacts to these features are described below:

Perennial Stream 1 – 139 linear feet (0.05 acre) of permanent impacts

PS1 is a somewhat impaired perennial stream approximately one mile west of Baston Road. This feature flows through a double concrete box culvert under Old Petersburg Road. The proximity to the existing roadway and location within an urban setting may affect water quality by increase in water temperature and roadway runoff. Approximately 90 linear feet (0.03 acre) of permanent impacts associated with the extension of a culvert and 49 linear feet (0.02 acre) of permanent impacts from riprap will result from the proposed project. This stream is perpendicular to the existing roadway; therefore, shifting the project alignment in any direction would not result in further minimization of impacts to this feature. An existing culvert is also in place; therefore, using a less impacting measure such as bridge or open bottom culvert would not be feasible.

Mr. Rodney N. Barry, P.E.  
PI 250470  
August 9, 2013  
Page 2

Perennial Stream 1a – 160 linear feet (0.009 acre) of permanent impacts

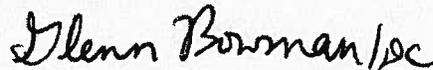
Perennial Stream 1a (PS1a) was not previously discussed in the April 2004 report. This newly identified resource is a somewhat impaired, perennial tributary to PS1. PS1a is a warm water stream located south of Old Petersburg Road and west of PS1. Approximately 160 linear feet of permanent impacts associated with filling a portion of the channel and routing the stream through a new 42-inch culvert will result from the installation of a residential driveway associated with the proposed project. Due to the proximity of this feature to the project centerline, as well as the alignment of the feature, complete avoidance to impacts would require a shift of the project alignment to the northeast. Along the northern side of the existing roadway, several single-family homes and cross roads are located, impeding an alignment shift without impacting family homes.

Perennial Stream 4 – 273 linear feet of permanent impacts

This stream is a warm water, somewhat impaired, perennial stream located south of Old Evans Road. Approximately 247 linear feet (0.04 acre) of permanent impacts associated with a new culvert and approximately 26 linear feet (0.004 acre) of permanent impacts associated with riprap installation are proposed. Concurrence under FWCA was previously received on February 22, 2007, for 675 feet of fill and relocation impacts to this feature. This feature is located within a section of the project along new alignment. The project design has changed to only install a culvert under the new location alignment instead of fully rerouting the stream. In order to minimize impacts to the fullest extent, riprap is only being used upstream of the proposed culvert for required stabilization purposes. No riprap is proposed downstream of the proposed culvert in order to minimize impacts to this feature.

The Department requests your concurrence that all reasonable avoidance and minimization alternatives have been explored and that you initiate coordination with the U.S. Fish and Wildlife Service under FWCA. The Department also requests concurrence with the "no effect" determinations for listed species. If you should have any questions or need additional information, please contact Sharilyn Meyers (404-631-1594) or Doug Chamblin (404-631-1447) of the GDOT Office of Environmental Services.

Sincerely,



Glenn Bowman, P.E.  
State Environmental Administrator

cc: George Brewer, GDOT Project Manager  
Sean Diehl, GDOT NEPA Analyst  
Eugene Hopkins, GDOT ECB  
Lisa Westberry, GDOT Mitigation  
Catherine Samay, GAEPD  
Danielle Floyd, GAEPD  
Pete Pattavina, USFWS

GB/HDC/ds  
Attachment

**DEPARTMENT OF TRANSPORTATION**

**STATE OF GEORGIA**

**Project STP00-7063-00(01)**

**Columbia County**

**PI No. 250470**

**Reevaluation**

**Attachment 4**

**Public Information Open House Materials**

Keith Golden, P.E., Commissioner



GEORGIA DEPARTMENT OF TRANSPORTATION

One Georgia Center, 600 West Peachtree Street, NW  
Atlanta, Georgia 30308  
Telephone: (404) 631-1000

May 14, 2013

Thank you for attending the detour open house for PROJECT NUMBER STP00-7063-00(001), P.I. No. 250470, the proposed Riverwatch Parkway Extension (Old Petersburg Road/Old Evans Road from Baston Road to Washington Road). In this handout package you will find a project description with the proposed detour route, detour/project location map and comment card.

As you enter the room, you will notice displays of the proposed detour route. Georgia Department of Transportation (GDOT) representatives, who can be identified by the nametags they are wearing, are available to discuss the detour route and answer your questions. Please take this opportunity to discuss the proposed detour route with a GDOT representative. There will be no formal presentation.

A court reporter will be available for those persons who would like to make a verbal statement about the project. You may also complete a comment card and deposit it into the box provided here, or send in written comments about the detour route until May 24, 2013. Written comments should be sent to Mr. Glenn Bowman, P.E., State Environmental Administrator, Georgia Department of Transportation, 600 West Peachtree Street NW, 16<sup>th</sup> Floor, Atlanta, GA 30308. Comments can also be made via the web at [www.dot.ga.gov](http://www.dot.ga.gov). Click on Public Outreach from the Information Center dropdown menu at the top right side of the page. All comments will be made a part of the project record. We hope you will take advantage of one of these opportunities to let GDOT know your view of the proposal.

The displays and plans will be available for review for ten days after the detour open house at the Georgia Department of Transportation Augusta Area Engineer's Office located at 4260 Frontage Road in Augusta, Georgia. A copy of all comments received will be available for public review at this same location and at the Georgia Department of Transportation, Office of Environmental Services, 600 West Peachtree Street NW, 16<sup>th</sup> Floor, Atlanta, GA 30308, as soon as compilation is completed.

Again, thank you for attending this detour open house and for giving us your comments. If you should have any questions or need additional information, feel free to contact the project manager George Brewer at (478) 538-8604 or Michael Hester at (404) 631-1255 of the Office of Environmental Services.

Sincerely,

Handwritten signature of Glenn Bowman in black ink.

Glenn Bowman, P.E.  
State Environmental Administrator

GB/mh

Attachments

## **PROJECT AND DETOUR DESCRIPTION**

**Project STP00-7063-00(001) consists of the widening and improving of Old Petersburg Road and Old Evans Road. The proposed concept would consist of a four-lane (two lanes in each direction) roadway with bike lanes and a 20-foot raised median on a minimum of 150 feet of right-of-way. The roadway would have urban shoulders including curb and gutter, and sidewalks.**

**The widening of Old Petersburg Road would start on existing location beginning approximately 1,400 feet west of the intersection of Baston Road and extending to the intersection with Old Evans Road. From this point, the project would follow Old Evans Road in a northwesterly direction on existing location to Columbia Industrial Boulevard and then extend westward on new location to tie into Washington Road at the intersection with Towne Center Drive and Washington Road. Two new bridges would be constructed. The first to grade separate the roadway over the CSX Railroad and the second is to replace an existing culvert located at Reed Creek. The project length would be approximately 3.10 miles.**

**During construction, Columbia Industrial Boulevard will be closed at its intersection with Old Evans Road to through traffic for approximately one month. Traffic will be detoured using Blue Ridge Drive, Evans to Locks Road, and North Belair Road.**







cc: Russell McMurry, P.E  
Jimmy Smith  
Vonda Everett  
George Brewer



August 9, 2013

«AddressBlock»

Re: Project STP00-7063-00(001), Columbia County, P.I. No. 250470, Riverwatch Parkway extension from Baston Road to Washington Road via Old Petersburg Road and Old Evans Road. – Responses to Open House Comments

«GreetingLine»

Thank you for your comments concerning the proposed project referenced above. We appreciate your participation and all of the input that was received as a result of the May 14, 2013 Public Information Open House (PIOH). Every written comment received and verbal comment given to the court reporter at the PIOH will be made part of the official record of the project. On behalf of the Georgia Department of Transportation, please accept our apologies for the delay in sending this response.

A total of 78 people attended the PIOH. Of the 11 respondents who formally commented, 9 were in support of the project, 0 were opposed, 1 were uncommitted, and 1 expressed conditional support.

The attendees of the PIOH and those persons sending in comments afterwards raised the following questions and concerns. The Georgia Department of Transportation (GDOT) has prepared this one response letter that addresses all comments received so that everyone can be aware of the concerns raised and the responses given. Please find the comments summarized below (*in italics*) followed by our response.

- *“Detour will be a temporary inconvenience for Mosaic United Methodist Church.”*

We recognize the inconveniences detours can place on a community and consequently utilize combinations of design techniques and construction scheduling to minimize the need, distances, and time frame of detours. The road closures and detours have been carefully stipulated in a special provision to the design plans to coordinate with important community events and communicate the road closures and detours in advance to the public, in part via portable changeable message signs along the project corridor. Extensive care has been taken to shorten and minimize the impacts of the detours associated with the proposed project to the community.

- *“Purchased properties not being maintained, specifically at intersections of Old Petersburg Road and McCormick Road as well as Old Evans Road and Old Petersburg Road.”*

We are not aware of any other reports of problems regarding the properties purchased for the proposed project. Columbia County is responsible for maintenance of the existing roadway and the purchased properties. They will be copied on this letter and informed of this concern.

- *“Traffic and safety at the intersection of Old Petersburg Road and McCormick Road.”*

The proposed project will widen Old Petersburg Road from 2 lanes to 4 lanes with a raised median in this location which will provide additional capacity and contribute to the safety of the intersection.

Again, thank you for your comments concerning this project. Should you have any further questions, comments, or concerns, please call the project manager, George Brewer, at (478)538-8604 or the environmental analyst, Sean Diehl, at (404) 631-1197.

Sincerely,

Glenn Bowman, P.E.  
State Environmental Administrator

GB/sd

cc: George Brewer, GDOT Project Manager

# Georgia Department of Transportation

## Detour Open House Comment Card

PROJECT NUMBER STP00-7063-00(001), Columbia County, P.I. No. 250470

May 14, 2013

Please print responses.

Name DON BARTUES  
Address 298 W. WYNGATE DR.  
MARTINEZ GA. 30907

Do you support the proposed detour?  For  Against  Conditional  Uncommitted

Comments BEST CONDITIONS. WOULD ALLOW GOOD  
BALANCE BETWEEN ECONOMY & CONSUMER.

How did you hear about this meeting?  Radio  Newspaper  Signs  Word of Mouth  
 Other N/A.

Was the location of the meeting convenient for you to attend?  Yes  No

If no, please suggest a general location that is more convenient to your community.

N/A.

Was the time of the meeting convenient for you to attend?  Yes  No

If no, please suggest a time frame that is more convenient for you. N/A.

Were your questions answered by GDOT personnel?  Yes  No

Do you understand the detour after attending this meeting?  Yes  No

Please share your suggestions on improving the way GDOT conducts public meetings.

NO SUGGESTIONS, A CLERK WITH LTR PAGE W/CAPABLE  
PEOPLE TO LISTEN AND RESPOND. THANK YOU.

Mail To:

Mr. Glenn Bowman, P.E., State Environmental Administrator  
Georgia Department of Transportation  
600 West Peachtree Street, NW - 16<sup>th</sup> Floor



# Georgia Department of Transportation

## Detour Open House Comment Card



PROJECT NUMBER STP00-7063-00(001), Columbia County, P.I. No. 250470

May 14, 2013

Please print responses.

Name Carolyn Moore, Pastor, Mosaic UMC

Address 682 Low Meadow Dr Evans 31801

(church address- 478 Columbia Industrial Blvd.)

Do you support the proposed detour?  For  Against  Conditional  Uncommitted

Comments We support community development, and appreciate all the advance information we can get, especially as the construction date nears. Education and communication are helpful for those businesses + organizations located on Columbia Industrial Blvd.

How did you hear about this meeting?  Radio  Newspaper  Signs  Word of Mouth  Other \_\_\_\_\_

Was the location of the meeting convenient for you to attend?  Yes  No

If no, please suggest a general location that is more convenient to your community.

Was the time of the meeting convenient for you to attend?  Yes  No

If no, please suggest a time frame that is more convenient for you. \_\_\_\_\_

Were your questions answered by GDOT personnel?  Yes  No

Do you understand the detour after attending this meeting?  Yes  No

Please share your suggestions on improving the way GDOT conducts public meetings.

Appreciate the good spirit in the room.

**Mail To:**

Mr. Glenn Bowman, P.E., State Environmental Administrator  
Georgia Department of Transportation  
600 West Peachtree Street NW - 16<sup>th</sup> Floor

# Georgia Department of Transportation

## Detour Open House Comment Card

PROJECT NUMBER STP00-7063-00(001), Columbia County, P.I. No. 250470

May 14, 2013

Please print responses.

Name Jean B Carr (Mosaic UMC)

Address Bill Parker Street  
MARTINEZ, GA 30907

Do you support the proposed detour?  For  Against  Conditional  Uncommitted

Comments \_\_\_\_\_

This proposal is painful for Mosaic Church "short-term"  
but will be a healthy alternative when finished.  
Added traffic light at Columbia and BLVD <sup>old Evans Road</sup> plus fly over  
for the railroad track is an added bonus  
(helps with congestion making a turn)

How did you hear about this meeting?  Radio  Newspaper  Signs  Word of Mouth

Other It was not promoted far enough in advance for folks to plan to attend

Was the location of the meeting convenient for you to attend?  Yes  No } m.

If no, please suggest a general location that is more convenient to your community.

Was the time of the meeting convenient for you to attend?  Yes  No

If no, please suggest a time frame that is more convenient for you. \_\_\_\_\_

Were your questions answered by GDOT personnel?  Yes  No

Do you understand the detour after attending this meeting?  Yes  No

Please share your suggestions on improving the way GDOT conducts public meetings.

The maps & personnel together told the planned proposal perfectly

### Mail To:

Mr. Glenn Bowman, P.E., State Environmental Administrator  
Georgia Department of Transportation  
600 West Peachtree Street, NW - 16<sup>th</sup> Floor

# Georgia Department of Transportation

## Detour Open House Comment Card

PROJECT NUMBER STP00-7063-00(001), Columbia County, P.I. No. 250470

May 14, 2013

Please print responses.

Name Arthur Apple  
Address 6457 ROUSSEAU CREEK RD.  
THOMSON, GA 30824-5034

Do you support the proposed detour?  For  Against  Conditional  Uncommitted

Comments The detour will be an inconvenience to Mosca U.M.S.  
for the 30 days of construction, but should benefit  
the Church after the construction with the addition  
of Traffic lights and four lanes on old Evans road where  
Industrial Blvd. runs into it.

How did you hear about this meeting?  Radio  Newspaper  Signs  Word of Mouth

Other There was a short notice of the meet

Was the location of the meeting convenient for you to attend?  Yes  No

If no, please suggest a general location that is more convenient to your community.

Was the time of the meeting convenient for you to attend?  Yes  No

If no, please suggest a time frame that is more convenient for you. Maybe 6-6:30 To allow  
rush

Were your questions answered by GDOT personnel?  Yes  No

Do you understand the detour after attending this meeting?  Yes  No

Please share your suggestions on improving the way GDOT conducts public meetings.

I was impressed with this type of meeting, thinking it would  
be a sit down, lecture type meeting, Every one that explains  
The detour was very knowledgeable.

Mail To:

Mr. Glenn Bowman, P.E., State Environmental Administrator  
Georgia Department of Transportation  
600 West Peachtree Street, NW - 16<sup>th</sup> Floor

# Georgia Department of Transportation

## Detour Open House Comment Card

PROJECT NUMBER STP00-7063-00(001), Columbia County, P.I. No. 250470

May 14, 2013

Please print responses.

Name Ed Matthews  
Address 3745 Roscommon North  
MARTINEZ, GA 30909

Do you support the proposed detour?  For  Against  Conditional  Uncommitted

Comments

Much needed population growth has surpassed  
the roads in this area. Glad to hear that  
sidewalks are included. Wish lights for the  
road could be added

How did you hear about this meeting?  Radio  Newspaper  Signs  Word of Mouth  
 Other TV

Was the location of the meeting convenient for you to attend?  Yes  No

If no, please suggest a general location that is more convenient to your community.

Was the time of the meeting convenient for you to attend?  Yes  No

If no, please suggest a time frame that is more convenient for you.

Were your questions answered by GDOT personnel?  Yes  No

Do you understand the detour after attending this meeting?  Yes  No

Please share your suggestions on improving the way GDOT conducts public meetings.

Fine as they are

Mail To:

Mr. Glenn Bowman, P.E., State Environmental Administrator  
Georgia Department of Transportation  
600 West Peachtree Street, NW - 16<sup>th</sup> Floor

Georgia Department of Transportation

Detour Open House Comment Card

PROJECT NUMBER STP00-7063-00(001), Columbia County, P.I. No. 250470

May 14, 2013

Please print responses.

Name MICHAEL BLISS

Address 444 PHEASANT RUN DR  
EVANS, GA 30809

Do you support the proposed detour?  For  Against  Conditional  Uncommitted

Comments THIS HAS BEEN NEEDED FOR QUITE SOME TIME. DETOUR IS WELL PLANNED

How did you hear about this meeting?  Radio  Newspaper  Signs  Word of Mouth  
 Other

Was the location of the meeting convenient for you to attend?  Yes  No

If no, please suggest a general location that is more convenient to your community.

Was the time of the meeting convenient for you to attend?  Yes  No

If no, please suggest a time frame that is more convenient for you.

Were your questions answered by GDOT personnel?  Yes  No

Do you understand the detour after attending this meeting?  Yes  No

Please share your suggestions on improving the way GDOT conducts public meetings.

Mail To:

Mr. Glenn Bowman, P.E., State Environmental Administrator  
Georgia Department of Transportation  
600 West Peachtree Street, NW - 16<sup>th</sup> Floor

Georgia Department of Transportation

Detour Open House Comment Card

PROJECT NUMBER STP00-7063-00(001), Columbia County, P.I. No. 250470

May 14, 2013

Please print responses.

Name Doris Williams
Address 444 Pheasant Run Dr
Evans, GA 30809

Do you support the proposed detour? [X] For [ ] Against [ ] Conditional [ ] Uncommitted

Comments

How did you hear about this meeting? [ ] Radio [X] Newspaper [ ] Signs [ ] Word of Mouth [ ] Other

Was the location of the meeting convenient for you to attend? [X] Yes [ ] No

If no, please suggest a general location that is more convenient to your community.

Was the time of the meeting convenient for you to attend? [X] Yes [ ] No

If no, please suggest a time frame that is more convenient for you.

Were your questions answered by GDOT personnel? [X] Yes [ ] No

Do you understand the detour after attending this meeting? [X] Yes [ ] No

Please share your suggestions on improving the way GDOT conducts public meetings.

Mail To:

Mr. Glenn Bowman, P.E., State Environmental Administrator
Georgia Department of Transportation
600 West Peachtree Street, NW - 16th Floor

# Georgia Department of Transportation

## Detour Open House Comment Card

PROJECT NUMBER STP00-7063-00(001), Columbia County, P.I. No. 250470

May 14, 2013

Please print responses.

Name Pat Finney  
Address 347 McCormick Rd  
\_\_\_\_\_  
\_\_\_\_\_

Do you support the proposed detour?  For  Against  Conditional  Uncommitted

Comments My immediate concern is for the  
purchased properties that are not being  
maintained. Especially the corner of old  
Petersburg and McCormick. There have been  
numerous accidents and it has become a  
dumping site. The corner of old Evans and  
Petersburg as well. ~~has not~~  
\_\_\_\_\_

How did you hear about this meeting?  Radio  Newspaper  Signs  Word of Mouth  
 Other \_\_\_\_\_

Was the location of the meeting convenient for you to attend?  Yes  No

If no, please suggest a general location that is more convenient to your community.  
\_\_\_\_\_

Was the time of the meeting convenient for you to attend?  Yes  No

If no, please suggest a time frame that is more convenient for you. \_\_\_\_\_

Were your questions answered by GDOT personnel?  Yes  No

Do you understand the detour after attending this meeting?  Yes  No

Please share your suggestions on improving the way GDOT conducts public meetings.  
\_\_\_\_\_  
\_\_\_\_\_

### Mail To:

Mr. Glenn Bowman, P.E., State Environmental Administrator  
Georgia Department of Transportation  
600 West Peachtree Street, NW - 16<sup>th</sup> Floor

Georgia Department of Transportation

Detour Open House Comment Card

PROJECT NUMBER STP00-7063-00(001), Columbia County, P.I. No. 250470

May 14, 2013

Please print responses.

Name Buffie & Frank Schmidt

Address 4027 Old Petersburg Rd  
Macon GA 30907

Do you support the proposed detour?  For  Against  Conditional  Uncommitted

Comments \_\_\_\_\_

I like the 30 day limit!

would be nice if there was  
some payment to business  
owners if they miss the  
30 day limit.

How did you hear about this meeting?  Radio  Newspaper  Signs  Word of Mouth

Other TV News

Was the location of the meeting convenient for you to attend?  Yes  No

If no, please suggest a general location that is more convenient to your community.

Was the time of the meeting convenient for you to attend?  Yes  No

If no, please suggest a time frame that is more convenient for you.

Were your questions answered by GDOT personnel?  Yes  No

Do you understand the detour after attending this meeting?  Yes  No

Please share your suggestions on improving the way GDOT conducts public meetings.

more advance notice perhaps announcement  
in water bill flyers a month previous

Mail To:

Mr. Glenn Bowman, P.E., State Environmental Administrator  
Georgia Department of Transportation  
600 West Peachtree Street, NW - 16<sup>th</sup> Floor

(78)

DETOUR MEETING  
 PUBLIC/CITY OFFICIALS SIGN-IN SHEET  
 PROJECT: STP00-7063-00(001), PI. NO. 250470-  
 COLUMBIA COUNTY  
 MAY 14, 2013

GRAVETOWN

Please print

Name	Affiliation	Address	Phone No.
Tammy Shepherd <del>Fatih Alcega</del>	Col. County Chamber	1000 Business Blvd Evans GA 31717 and Petersburg Rd	706-651-0018 706-855-7740
Trey Allen	C.C.BoC		706.631.8924
HAMMERS MGR	WL-Roadie	4108 S Gray St. S.A.	716-133-4736
Paul DeCamp	Augusta MPO	525 Telfair, Augusta	(706) 821-1796
FRANK NEAL	City of Grovesmith	105 Old W. Washington	(706) 596-2093
Matt Schlachter	Col. County		706-868-3356
<del>Jeffery School</del>			

**DETOUR PUBLIC INFORMATION OPEN HOUSE**  
**Recorded Statements on 05/14/2013**

1 RECORDED STATEMENTS

2 PUBLIC OPEN HOUSE

3 MS. GERALDINE ARENSMAN: I live at 4038 Old  
4 Petersburg Road, which is at the corner of Old  
5 Petersburg and McCormick. South Columbia Elementary  
6 School is on McCormick Drive. We have a heavy problem  
7 at school times from Old Evans and from Old Petersburg,  
8 but there's not going to be any traffic lights, and  
9 there is a lot of traffic. I don't know how much is  
10 enough traffic that we need traffic lights, but we have  
11 numerous accidents. It's going to be more traffic  
12 coming from Riverwatch, and I think for safety of  
13 people, we need to have traffic lights.

14 MS. TAMMY SHEPHERD: The Chamber supported the  
15 TSPLOTS, the Transportation Investment Act, and we are  
16 excited about the eight projects and look forward to  
17 the Riverwatch Parkway being the number one project  
18 completed, and anything we can do to assist DOT and the  
19 County and helping with that, we'd be happy to do so.

20 [End of comments; Public Meeting concludes at 7:00  
21 p.m.]

22 //

23 //

24 //

25 //