

**DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA**

**OFFICE OF DESIGN POLICY & SUPPORT
INTERDEPARTMENTAL CORRESPONDENCE**

FILE P.I. # 0012875 **OFFICE** Design Policy & Support
DeKalb County
GDOT District 7 - Metro Atlanta **DATE** March 4, 2016
CR 3329 & CR 5156 – Georgetown
Streetscape, Median & Turn-lane Improvements

FROM *Kim Phillips*
for Brent Story, State Design Policy Engineer

TO SEE DISTRIBUTION

SUBJECT APPROVED CONCEPT REPORT

Attached is the approved Concept Report for the above subject project.

Attachment

DISTRIBUTION:

Hiral Patel, Director of Engineering
Joe Carpenter, Director of P3/Program Delivery
Genetha Rice-Singleton, Assistant Director of P3/Program Delivery
Albert Shelby, State Program Delivery Engineer
Darryl VanMeter, State Innovative Delivery Engineer
Bobby Hilliard, Program Control Administrator
Cindy VanDyke, State Transportation Planning Administrator
Eric Duff, State Environmental Administrator
Bill DuVall, State Bridge Engineer
Andrew Heath, State Traffic Engineer
Angela Robinson, Financial Management Administrator
Lisa Myers, State Project Review Engineer
Charles "Chuck" Hasty, State Materials Engineer
Lee Upkins, State Utilities Engineer
Richard Cobb, Statewide Location Bureau Chief
Kathy Zahul, District Engineer
Scott Lee, District Preconstruction Engineer
Nicholas Fields, District Utilities Engineer
Carleton Fisher, Project Manager
BOARD MEMBER - 6th Congressional District

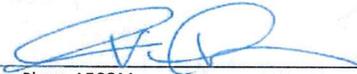
**DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA
PROJECT CONCEPT REPORT**

Project Type: Bicycle/Ped Facility P.I. Number: 0012875
 GDOT District: 7 County: DeKalb
 Federal Route Number: N/A State Route Number: N/A
 Project Number: N/A

** UPDATED for current costs. KLP*

The CR 3329 & CR 5156 – Georgetown Area Multimodal Improvements project will provide multimodal improvements to Chamblee Dunwoody Road, in DeKalb County, Georgia. Bike and pedestrian facilities will be added within the project limits. This project is not a vehicular capacity improvement project.

Submitted for approval:


 Sean Pharr, AECOM 11.23.2015
Date

M. G. O. T.
 Local Government Sponsor, City of Dunwoody 11/23/15
Date

Albert V. Shelby III
 State Program Delivery Engineer 12.1.15
Date

Elaine Armater
 GDOT Project Manager, Elaine Armater 12.1.15
Date

Recommendation for approval:

Program Control Administrator Date
** Hiral Patel / KLP* 12-17-15

State Environmental Administrator Date
** Ken Werho / KLP* 12-10-15

For State Traffic Engineer Date
** Lisa Myer / KLP* 12-4-15

Project Review Engineer Date
** Lee Upkins / KLP* 12-3-15

State Utilities Engineer Date

District Engineer Date
** Bill DuVall / KLP* 1-4-16

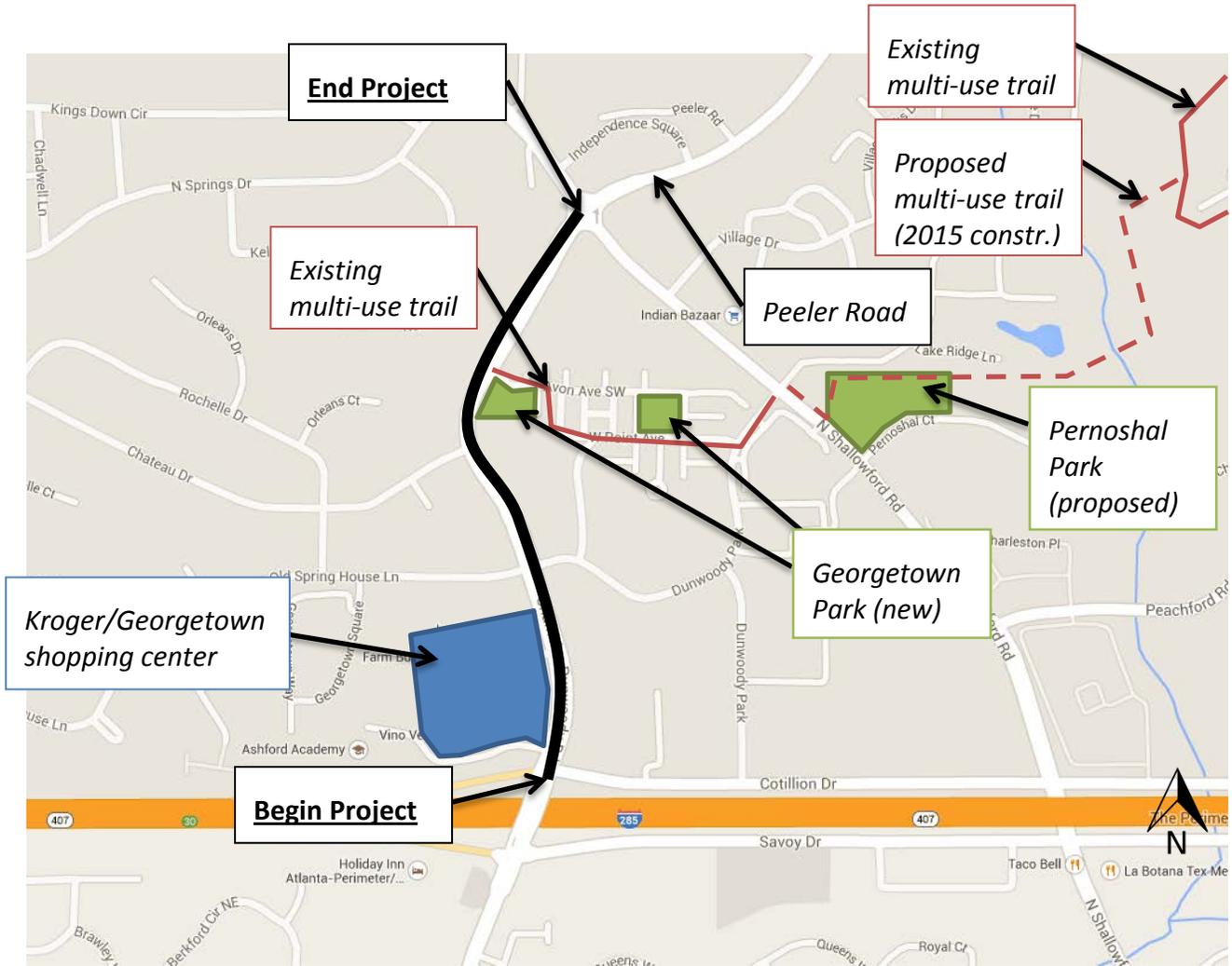
State Bridge Engineer Date

State Transportation Financial Management Administrator Date

** Recommendations on file*
 The concept as presented herein and submitted for approval is consistent with that which is included in the Regional Transportation Plan (RTP) and/or the State Transportation Improvement Program (STIP).

Cynthia S. Nangle
 State Transportation Planning Administrator 2-10-16
Date

PROJECT LOCATION MAP



CR 329 & CR 5156 – Georgetown Area Multimodal Improvements

DeKalb County
P.I. No. 0012875

County: DeKalb

PLANNING AND BACKGROUND

Project Justification Statement:

The project justification statement has been prepared by AECOM. The primary need of the proposed improvements is to provide complete street improvements with context sensitive cross sections on Chamblee Dunwoody Road between Cotillion Drive and Peeler Road. Landscaping and pedestrian lighting are also proposed to enhance the project's context sensitivity. This section of the corridor lacks continuous sidewalks and bicycle facilities on both sides of the road. Commercial service providers, restaurants, public parks, as well as the area's only major grocery store are located on this section of Chamblee Dunwoody Road. The northern area of the project corridor includes existing MARTA bus stops. Several multi-family housing developments are located along Chamblee Dunwoody Road, which currently lack connectivity to many of these facilities. Pedestrian activity and the need for pedestrian improvements along the corridor are evident by density of residential and commercial uses, as well as the presence of worn paths in the sidewalk gaps. The proposed improvements along this corridor will accommodate active transportation modes by providing dedicated space for non-motorized users to operate.

The need for the development of pedestrian and bicycle connectivity in the Georgetown area was identified in both the Georgetown/North Shallowford Master Plan as well as the City of Dunwoody's and DeKalb County's transportation plans. This plan incorporates a multimodal corridor through Georgetown on Chamblee Dunwoody Road.

The non-motorized facilities that are currently present introduce a high number of potential conflict points between pedestrian and bicycle traffic and motor vehicle traffic, particularly on the eastern side of the road. Therefore, driveway consolidation and raised medians in select locations are proposed to minimize these potential conflict points.

Existing conditions:

Chamblee Dunwoody Road is an important link between the residential and commercial areas both north and south of I-285 through the Georgetown/N. Shallowford Road areas. The corridor currently fluctuates through a number of different cross-sectional configurations between Cotillion Drive and Peeler Road including 2, 3, and 4-lane sections (with auxiliary lanes frequently added and dropped). Lane widths vary between 10 and 12 feet throughout the corridor.

Chamblee Dunwoody Road is currently a mixture of both rural and urban sections and is classified as an urban minor arterial street. A curb and gutter runs along both sides between I-285 and Dunwoody Park/Old Springhouse Lane. Further north, between Dunwoody Park/Old Springhouse Lane and Peeler Road, the curbing on either side varies between curb and gutter, header curb, rolled curb and no curb. The pedestrian environment is characterized by frequent driveway crossings along the southern half of the corridor and inconsistent sidewalk coverage, particularly on the east side of the road. There are currently no true bike lanes along the corridor.

The bridge over I-285 consists of 5 lanes and sidewalks on either side. The 0.2-mile section between Cotillion Drive and the Kroger shopping center north entrance consists of 4 lanes with a flush median, a right turn bay at Cotillion Drive, and a left turn bay at the Kroger shopping center north entrance. There are sidewalks along both sides and frequent driveways, especially along the east side. The 500-foot section between the Kroger shopping center north entrance and Dunwoody Park/Old Spring House Lane consists of 4 lanes. One of the northbound lanes within this segment drops into a left turn lane at Old Spring House Lane. There are only sidewalks on the west side of the road along this segment.

Chamblee Dunwoody Road consists of two lanes between Dunwoody Park/Old Spring House Lane and Peeler Road (0.4 mile) except for near Dunwoody Park/Old Spring House Lane where there is a second

County: DeKalb

through lane in the southbound direction. In addition, there are left turn bays at both Dunwoody Park/Old Springhouse Lane and Peeler Road. There are also several deceleration lanes which are added and dropped along this section. This section has sidewalks along the west side but not the east side. There is a paved shoulder inappropriately marked as a bike lane along one of these southbound right turn bays into an office building. This section also passes by the former “PVC Site”, recently redeveloped into a city park and multi-use trail (Georgetown Park), surrounded by new single- and multi-family housing.

Other projects in the area:

- 0000784 - I-285/SR 400 Interchange Reconstruction
- 721850 - SR 400 Collector-Distributor (CD) Lanes
- 714000 – I-285 from SR 400 to N Shallowford Rd (revive285) – Long range program
- 0001758 – I-285 from I-75/Cobb thru Fulton to I-85/DeKalb for HOV (revive285) – Long range program
- 0002799 – Chamblee Dunwoody Rd from Clairview/Cumberland to Buford Hwy pedestrian/streetscape improvements
- City of Dunwoody – Pernoshal Park and Multi-Use Trail – Connectivity between existing trails at Georgetown and Brook Run Parks
- City of Dunwoody – Chamblee Dunwoody Pedestrian and Bicycle Improvements (Cambridge Drive to Womack Road)
- City of Dunwoody – Chamblee Dunwoody Corridor Improvements (Peeler Road to Vermack Road)
- City of Dunwoody – Multi-use Trail, Cotillion Road (Chamblee Dunwoody to N. Shallowford Road)
- City of Dunwoody – Intersection Improvement at Chamblee Dunwoody Road/N. Shallowford Road/Peeler Road

MPO: Atlanta Regional Commission (ARC)

MPO Project ID: DK-417

Regional Commission: Atlanta Regional Commission

RC Project ID: DK-417

Congressional District(s): 6

Federal Oversight: PoDI Exempt State Funded Other

Projected Traffic: AADT 24 HR T: 3.8%

Current Year (2014): 25,507 Open Year (2020): 26,283 Design Year (2040): 29,038
Traffic Projections Performed by: AECOM

Functional Classification (Mainline): Urban Minor Arterial Street

Complete Streets - Bicycle, Pedestrian, and/or Transit Warrants:

Warrants met: None Bicycle Pedestrian Transit

Is this a 3R (Resurfacing, Restoration, & Rehabilitation) Project? No Yes

Pavement Evaluation and Recommendations

Preliminary Pavement Evaluation Summary Report Required? No Yes
Preliminary Pavement Type Selection Report Required? No Yes
Feasible Pavement Alternatives: HMA PCC HMA & PCC

This is a sidewalk and bicycle connectivity project, and as such does not require a Pavement Evaluation.

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DESIGN AND STRUCTURAL

Description of the proposed project: This project proposes to reconstruct Chamblee Dunwoody Road providing bicycle and pedestrian accommodations from Cotillion Drive to Peeler Road in DeKalb County.

Major Structures:

Structure	Existing	Proposed
None		

Mainline Design Features: Chamblee Dunwoody Road

Feature	Existing	Standard*	Proposed
Typical Section			
- Number of Lanes	4 lanes (between Cotillion Drive and Old Spring House Lane); 2 lanes (between Old Spring House Lane and Peeler Road)	2-4	4 lanes (between Cotillion Drive and Old Spring House Lane); 2 lanes (between Old Spring House Lane and Peeler Road)
- Lane Width(s)	10'-12'	10'-12'	10'
- Median Width & Type	6'-8' raised (north leg of Cotillion Drive intersection – 90' long); 0'-10' flush (between end of raised median and northern Georgetown Shopping Center entrance)	20' raised or 14' flush	14'-24' raised (between Cotillion Drive and northern Georgetown Shopping Center entrance); 13' flush (elsewhere)
- Outside Shoulder or Border Area Width	Varies	16' Urban	19'-6" to 27'-6" Urban (left); 26'-6" Urban (right)
- Outside Shoulder Slope	2:1 max	2:1 max	2:1 max
- Inside Shoulder Width	N/A	N/A	N/A
- Sidewalks	<i>Between Cotillion Drive and Old Spring House Lane:</i> 4'-6' sidewalk (west side); 5' intermittent sidewalk (east side). <i>Between Old Spring House Lane and Peeler Road:</i> 4'-5' sidewalk (west side); None (east side).	5'	<i>Between Cotillion Drive and Old Spring House Lane:</i> 8-12' Multi-use trail (east side); 10' Sidewalk (west side). <i>Between Old Spring House Lane and Peeler Road:</i> 8' Sidewalk (west side); 8-12' Multi-use Trail (east side).
- Auxiliary Lanes	10'-14'	10'-12'	10'
- Bike Lanes	None	4' min	None – Multi-use Trail
Posted Speed	35mph	35mph	35mph
Design Speed	35mph	35mph	35mph

County: DeKalb

Min Horizontal Curve Radius	400'	35 mph – 371'	400'
Maximum Superelevation Rate	4% max	Urban-4% max	4% max
Maximum Grade	7%	7%	7%
Access Control	Permitted	Permitted	Permitted
Design Vehicle	WB-40	WB-40 or BUS-40	WB-40 and BUS-40
Pavement Type	Asphalt	Asphalt	Asphalt

*According to current GDOT design policy if applicable

Major Intersections: The intersections of Chamblee Dunwoody Road with Cotillion Drive, the Kroger shopping center north entrance, Old Spring House Lane/Dunwoody Park, and N Shallowford Road/Peeler Road are all currently signalized intersections.

Lighting required: No Yes
 Commitment letter attached. Pedestrian lighting will be provided.

Off-site Detours Anticipated: No Undetermined Yes
 Off-site detour is not required for this project.

Transportation Management Plan [TMP] Required: No Yes *KLP*
 If Yes: Project classified as: Non-Significant Significant
 TMP Components Anticipated: TTC TO PI

Design Exceptions to FHWA/AASHTO controlling criteria anticipated:

FHWA/AASHTO Controlling Criteria	No	Undetermined	Yes	Appvl Date (if applicable)
1. Design Speed	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
2. Lane Width	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
3. Shoulder Width	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
4. Bridge Width	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
5. Horizontal Alignment	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
6. Superelevation	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
7. Vertical Alignment	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
8. Grade	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
9. Stopping Sight Distance	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
10. Cross Slope	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
11. Vertical Clearance	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
12. Lateral Offset to Obstruction	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
13. Bridge Structural Capacity	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	

Design Variances to GDOT Standard Criteria anticipated:

GDOT Standard Criteria	Reviewing Office	No	Undetermined	Yes	Appvl Date (if applicable)
1. Access Control/Median Openings	DP&S	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
2. Intersection Sight Distance	DP&S	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
3. Intersection Skew Angle	DP&S	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
4. Lateral Offset to Obstruction	DP&S	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
5. Rumble Strips	DP&S	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
6. Safety Edge	DP&S	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
7. Median Usage	DP&S	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	

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County: DeKalb

8. Roundabout Illumination Levels	DP&S	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
9. Complete Streets	DP&S	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
10. ADA & PROWAG	DP&S	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
11. GDOT Construction Standards	DP&S	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
12. GDOT Drainage Manual	DP&S	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
13. GDOT Bridge & Structural Manual	Bridges	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	

VE Study anticipated: No Yes Completed – Date:

UTILITY AND PROPERTY

Temporary State Route needed: No Yes Undetermined

Railroad Involvement: N/A

Utility Involvements:

- Georgia Power
- Atlanta Gas Light Resources
- AT&T/Distribution
- City of Dunwoody
- Charter Communications
- DeKalb County Department of Watershed Management

SUE Required: No Yes Undetermined

Public Interest Determination Policy and Procedure recommended (Utilities)? No Yes

Right-of-Way (ROW): Existing width: 80-110 ft Proposed width: 80-110 ft

Required Right-of-Way anticipated: None Yes Undetermined
 Easements anticipated: None Temporary Permanent Utility Other

Anticipated total number of impacted parcels: 26 *12 KLP*
 Displacements anticipated: Businesses: 0
 Residences: 0
 Other: 0
 Total Displacements: 0

Location and Design approval: Not Required Required

ROUNDABOUTS

The intersection of Chamblee Dunwoody Road with North Shallowford Road and Peeler Road (the northern terminus of the project) was studied for roundabout as a potential operation improvement to this intersection, which is currently controlled by a traffic signal. The traffic study, done by AECOM, shows that a hybrid (dual and single) lane roundabout would provide an adequate LOS.

However, it has been determined that a roundabout at the intersection of North Shallowford/Peeler/Chamblee Dunwoody Road is outside of the scope of this project, which is to provide bike/ped accommodations; further operational improvements may be studied as part of a separate project in the future.

County: DeKalb

Roundabout Lighting Agreement/Commitment Letter received:

No

Yes

N/A

Roundabout Planning Level Assessment:

N/A

Roundabout Feasibility Study:

N/A

Roundabout Peer Review Required: No Yes Completed – Date:

N/A

CONTEXT SENSITIVE SOLUTIONS

Issues of Concern:

Extensive dialogue with the project stakeholders has resulted in a firm awareness of the needs and concerns of the community. The public comments received have not only revealed that this project is supported by the community, but have also resulted in alterations to the preferred design, which safeguards that this project is designed in a context sensitive manner. The public will continue to be engaged at key milestones throughout the life of the project in compliance with the GDOT’s public meeting process.

The Georgetown/North Shallowford Master Plan identified the goal to enhance the area’s identity, character, and appearance as a gateway into the City of Dunwoody. The streetscaping element of the proposed designs will harmonize well with the aforementioned goals. Well-designed, visually appealing bicycle and pedestrian facilities that are well-integrated into the streetscape will also create a sense of place and encourage more users. These elements will also addressing the goal identified in the Georgetown/North Shallowford Master Plan to improve bicycle and pedestrian connectivity and walkability.

According to the City of Dunwoody’s 2010 Comprehensive Land Use Plan, the goal for the Georgetown/North Shallowford area is for it to “redevelop into a pedestrian and bicycle-oriented activity center with medium-scaled intensity of activity” by 2030. The proposed designs will therefore go beyond minimum accommodation for pedestrian and bicycle traffic to promote an environment where non-motorized travelers feel welcome.

Based on stakeholder comments, the multi-use trail was removed from the west side of the road north of Old Spring House Lane to better fit with the context of a residential area.

ENVIRONMENTAL & PERMITS

Anticipated Environmental Document:

GEPA:

NEPA: CE

EA/FONSI

EIS

MS4 Permit Compliance – Is the project located in a MS4 area?

No

Yes

County: DeKalb

Environmental Permits/Variations/Commitments/Coordination anticipated:

Permit/ Variance/ Commitment/ Coordination Anticipated	No	Yes	Remarks
1. U.S. Coast Guard Permit	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
2. Forest Service/Corps Land	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
3. CWA Section 404 Permit	<input checked="" type="checkbox"/>	<input type="checkbox"/>	No impacts to waters anticipated
4. Tennessee Valley Authority Permit	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
5. Buffer Variance	<input checked="" type="checkbox"/>	<input type="checkbox"/>	No impacts to waters anticipated
6. Coastal Zone Management Coordination	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
7. NPDES	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
8. FEMA	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
9. Cemetery Permit	<input checked="" type="checkbox"/>	<input type="checkbox"/>	
10. MS4 Permit	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
11. Other Commitments	<input checked="" type="checkbox"/>	<input type="checkbox"/>	None known
12. Other Coordination	<input checked="" type="checkbox"/>	<input type="checkbox"/>	None known

Is a PAR required? No Yes Completed – Date:

Environmental Comments and Information:

Chamblee Dunwoody Road lies within a wide (100') right-of-way through much of the corridor sufficient to accommodate proposed improvements; however, some impacts to surrounding properties may be necessary in some areas. It is anticipated that this project will not be a cause of significant public controversy, nor will it disproportionately impact disadvantaged communities.

This project must comply with the NPDES GDOT MS4 Permit requirement to install post-construction stormwater BMPs. At the concept planning level, enhanced swales and infiltration trenches are proposed as structural post construction stormwater BMPs to meet this requirement. More detailed designs of the BMPs will be developed during the PFPR phase of the project to meet MS4 requirements.

NEPA/GEPA: As a trail/sidewalk project, this project should qualify as a categorical exclusion. Permitting for Jurisdictional Waters of the US will not be necessary, neither will a buffer variance.

Ecology: No Formal Section 7 coordination or biological assessment will be required. An aquatic species survey will not be needed. The corridor will not contain suitable habitat for endangered bats.

History: Historic resources and public recreational resource will be avoided such that an Individual Section 4(f) evaluation will not be needed.

Archaeology: Only a Phase I survey for archaeology will be needed. No archaeological investigations of cemeteries will be required.

Air Quality:

Is the project located in a PM 2.5 Non-attainment area? No Yes
 Is the project located in an Ozone Non-attainment area? No Yes
 Is a Carbon Monoxide hotspot analysis required? No Yes

County: DeKalb

The project will comply with the Atlanta Metropolitan Planning Organization (AMPO)/Atlanta Regional Commission (ARC) regarding transportation planning and clean-air standards. Phase 1 of this project will not add capacity, nor will it signalize intersections; therefore, carbon monoxide modeling for Phase 1 is not possible. Carbon Monoxide modeling may be required for Phase 2 improvements. The proposed project is a type not anticipated to be a cause for air quality concern. A qualitative air quality analysis should satisfy this requirement.

Noise Effects: Non anticipated due to the minor changes in roadway geometry

Public Involvement:

Four public involvement workshops were held as part of the development of the Georgetown/North Shallowford Master Plan, during a time period spanning from September 15, 2010 through January 19, 2011. A series of public open houses and council member interviews were also held during that time period.

The results of the public workshops demonstrate that the public supports the goal to create better bicycle and pedestrian connectivity – particularly with bicycle and pedestrian facilities – within the Georgetown/North Shallowford area as well as to surrounding areas (Brook Run, Dunwoody Village, and Perimeter Center). Another goal identified at the meetings is to enhance the area’s identity, character, and appearance as a gateway into the City of Dunwoody.

A community preference survey was also conducted both at meetings and online to gather stakeholder input about the Georgetown/North Shallowford area. Survey results revealed a preference for making the area more walkable and encouraging high-quality development. Most survey respondents (74%) believed that the area needed extensive redevelopment.

An open house for commercial stakeholders on the corridor (property owners and tenants) was held during business hours followed by a roundtable discussion during business hours on November 11, 2014. At this open house, aerials along the corridor of the project were on display and an opportunity to discuss the needs and concerns of the attendees. Later that evening, a roundtable discussion was held with residents from the adjacent neighborhoods. The discussion reflected largely the efforts of the Georgetown Area Master Plan that a multi-modal corridor on Chamblee Dunwoody Road, and Cotillion would have a positive impact on the community.

A Public Information Open House (PIOH) was held on December 11, 2014. Multiple, viable concepts were presented separately for both Phase 1 and Phase 2. Alternatives for the operational improvements of Chamblee Dunwoody Road with Peeler Road/N. Shallowford Road and the Chamblee Dunwoody Road corridor to the next signalized intersection at Vermack Road were also on display. Comments regarding these options were assembled from several sources including comment cards, an online survey, court reporter transcripts, and other written communication received. A second public meeting is anticipated to be held prior to the right of way acquisition phase.

A presentation to City Council was held on February 13, 2015. The concepts and community feedback from the PIOH were discussed and the most favorable alternate for both phases of this project were revealed. A second stakeholder meeting was held on July 16, 2015. This meeting resulted in some alterations to the preferred alternate. The City Council supports the preferred alternatives in a Council meeting held on August 24, 2015.

County: DeKalb

Major stakeholders:

Travelling Public, commercial and residential property owners, commercial tenants, City of Dunwoody, GDOT, ARC, DeKalb County, and MARTA.

CONSTRUCTION

Issues potentially affecting constructability/construction schedule: None

Early Completion Incentives recommended for consideration: No Yes

COORDINATION, ACTIVITIES, RESPONSIBILITIES, AND COSTS

Coordination Meeting with District 1 Staff: *District 7 Mike Lobdell*
 Held on 6/30/2014. See attached meeting minutes.

Concept Team Meeting:
 Held on 4/30/2015. See attached meeting minutes.

Team Responsibilities:

Project Activity	Party Responsible for Performing Task(s)
Concept Development	AECOM
Design	AECOM
Right-of-Way Acquisition	City of Dunwoody
Utility Relocation	Utility Company/City of Dunwoody
Letting to Contract	City of Dunwoody
Construction Supervision	City of Dunwoody
Providing Material Pits	Contractor
Providing Detours	Contractor
Environmental Studies, Documents, & Permits	AECOM
Environmental Mitigation	City of Dunwoody
Construction Inspection & Materials Testing	City of Dunwoody

Project Cost Estimate Summary and Funding Responsibilities:

Note: Table has been updated to show current costs. KLP

	Breakdown of <i>SCP & PE</i> ★	ROW	Reimburs- able Utility	★★ CST*	Environmental Mitigation	Total Cost
Funded By	GDOT/City <i>Federal</i>	City	City	GDOT/City <i>Federal</i>	City	
\$ Amount	\$575,000 DOT \$150,000 city \$725,000 tot.	\$750,000 <i>\$316,000</i>	\$1,200,000	\$2,535,750 DOT \$3,248,229.19 city \$5,783,979.19 tot.	\$0	\$3,110,750 DOT \$5,348,229.19 city \$8,458,979.19 tot.
Date of Estimate	6/18/2015	8/24/2015 <i>2/18/2016</i>	8/21/2015 <i>2/2/2016</i>	8/24/2015	8/24/2015	\$7,674,979.19

*CST Cost includes: Construction, 5% Engineering and Inspection, 15% Contingencies, and Liquid AC Cost Adjustment.

★ SCP = Fed + local = \$200,000 + \$50,000 = \$250,000
 PE = Fed + local = \$100,000 + \$25,000 = \$125,000
 ★★ CST = Fed + local = \$540,000 + \$5,243,979.19 = \$5,783,979.19

County: DeKalb

ALTERNATIVES DISCUSSION

Alternative selection:

Multiple alternatives were evaluated. Four alternatives are described below, the first being the preferred alternative, and the fourth being the “no-build” alternative. All three build alternatives propose constructing median refuge islands for mid-block pedestrian crossings at several points between Old Spring House Lane and the northern terminus of the project, including one near the entrance to Georgetown Park. The three build alternatives also include a planted median separating northbound and southbound traffic between Cotillion Drive and the northern (signalized) Georgetown Shopping Center entrance. Auxiliary lanes are provided throughout the corridor as required for all build alternatives.

All three build alternatives involve minimal right-of-way acquisition, are approximately equal to one another in total width, and require minimal changes to the footprint of the original roadway. All three build alternatives maintain approximately the existing roadway widths between Cotillion Drive and Old Spring House Lane while requiring some additional roadway widening in certain sections between Old Spring House Lane and Peeler Road. None of the three build alternatives would impede future improvements to the existing signalized intersection at Chamblee Dunwoody Road, Peeler Road, and North Shallowford Road.

The **preferred alternative** shares the same roadway characteristics as Alternative B, differing only in multi-use trail and sidewalk configuration. This alternative includes four 10-foot travel lanes (two in each direction) between Cotillion Drive and Old Spring House Lane. At Old Spring House Lane, the outer northbound travel lane drops into a right turn lane. North of Old Spring House Lane, this alternative includes two 10-foot travel lanes (one in each direction). A 13-foot shared turn lane separates northbound and southbound lanes throughout the corridor, with the exception of the portion between Cotillion Drive and the northern (signalized) Georgetown Shopping Center entrance where there is a raised, planted median instead. A 12-foot multi-use trail runs along the west side of the corridor between Cotillion Drive and Old Spring House Lane, while a 10-foot sidewalk runs along the east side of the corridor throughout that segment. North of Old Spring House Lane, an 8-foot sidewalk runs along the west side of the corridor, while a 12-foot multi-use trail runs along the east side. All sidewalks and multi-use trails are separated from the roadway by a 6-inch header curb and a 6-foot planted and hardscaped buffer area. Right-of-way limitations may necessitate altering the width of landscape or hardscape area on the west side of Chamblee Dunwoody Road in some locations north of Old Spring House Lane. This alternative was found to be most favorable by the public and Dunwoody City Council.

Alternative A includes two 11-foot travel lanes, one in each direction, separated by a 14-foot shared turn lane and flanked on both sides by 5-foot bike lanes, which are separated from motor vehicle traffic by a 3-foot painted buffer. A 10-foot sidewalk follows both sides of the street, separated from the roadway by a 6-inch header curb and a 5 ½-foot planting strip. A 5-foot outside shoulder runs along the back of sidewalk. A traffic study comparing this alternative to the existing roadway conditions predicted no significant change in overall delay as a result of implementing this design along the corridor. For more details, see attachment: Traffic Memorandum. This alternative was found to be least favorable to the public and Dunwoody City Council.

Alternative B shares the same roadway characteristics as the preferred alternative, differing only in multi-use trail and sidewalk configuration. This alternative includes four 10-foot travel lanes (two in each direction) between Cotillion Drive and Old Spring House Lane. At Old Spring House Lane, the outer northbound travel lane drops into a right turn lane. North of Old Spring House Lane, this alternative includes two 10-foot travel lanes (one in each direction). A 13-foot shared turn lane separates northbound and southbound lanes throughout the corridor, with the exception of the portion between Cotillion Drive and the northern (signalized) Georgetown Shopping Center entrance where there is a raised, planted median instead. Two 12-foot multi-use trails run along both sides of the entire corridor, separated from the roadway by a 6-inch header curb and a 6-foot planted and hardscaped buffer area.

County: DeKalb

Preferred Alternative: Four-lane cross-section south of Old Spring House Lane with raised and flush medians, varying by location; 8-to-12-foot multi-use trail on west side of road and 10-foot sidewalk on east side. North of Old Spring House Lane, a three-lane cross-section including shared turn lane; 8-foot sidewalk on west side of road and 8-to-12-foot multi-use trail on east side.

Estimated Property Impacts:	26 12	Estimated Total Cost:	\$5.8M RVP \$5.3M (CST only)
Estimated ROW Cost:	\$273,000 \$316,000	Estimated CST Time:	12 months

Rationale: This is the preferred alternative because it provides dedicated pedestrian and bicycle facilities while not adding vehicular travel time delays compared with existing conditions. This alternative also aligns with the public’s preference for off-road bicycle facilities and retaining the existing number of vehicular travel lanes. Unlike Alternative B, this alternative does not provide a multi-use trail along both sides of the corridor throughout. Instead, it provides a multi-use trail along one side of the corridor at a time, while providing a wide sidewalk on the other side of the street to maintain pedestrian connectivity.

Alternative A: Three-lane cross-section with buffered bike lanes

Estimated Property Impacts:	26	Estimated Total Cost:	\$6.0M (CST only)
Estimated ROW Cost:	TBD	Estimated CST Time:	12 months

Rationale: This alternative meets project goals by providing dedicated pedestrian and bicycle facilities while not adding vehicular travel time delays compared with existing conditions. However, this alternative does not achieve the public’s preference for off-road bicycle facilities and retaining all existing vehicular travel lanes.

Alternative B: Four-lane cross-section south of Old Spring House Lane with raised and flush medians, varying by location. Three-lane cross-section north of Old Spring House Lane including shared turn lane. Multi-use trails along both sides of roadway throughout corridor.

Estimated Property Impacts:	26	Estimated Total Cost:	\$5.4M (CST only)
Estimated ROW Cost:	TBD	Estimated CST Time:	12 months

Rationale: This alternative meets the project goals by providing dedicated pedestrian and bicycle facilities while not adding vehicular travel time delays compared with existing conditions. This alternative also aligns with the public’s preference for off-road bicycle facilities and retaining all existing vehicular travel lanes. However, the high number of driveways along the eastern side of the south end of the corridor combined with the low currently known demand for bicycle capacity in this area suggests that providing a multi-use trail along both sides of the road along this segment would be excessive and potentially dangerous for cyclists in some locations.

No-Build Alternative:

Estimated Property Impacts:	0	Estimated Total Cost:	\$0
Estimated ROW Cost:	\$0	Estimated CST Time:	none

Rationale: This alternative does not meet the project goal of providing pedestrian and bicycle access along the corridor.

County: DeKalb

LIST OF ATTACHMENTS/SUPPORTING DATA

1. Concept Layouts
2. Typical sections
3. Detailed Cost Estimates:
 - a. Construction including Engineering and Inspection
 - b. Completed Fuel & Asphalt Price Adjustment forms
 - c. Right-of-Way
 - d. Utilities
4. Traffic diagrams
5. Traffic Study
6. Minutes of Concept meetings
7. Minutes of any meetings that shows support or objection to the concept
8. PFA
9. Lighting Commitment Letter
10. Preconstruction Status Report

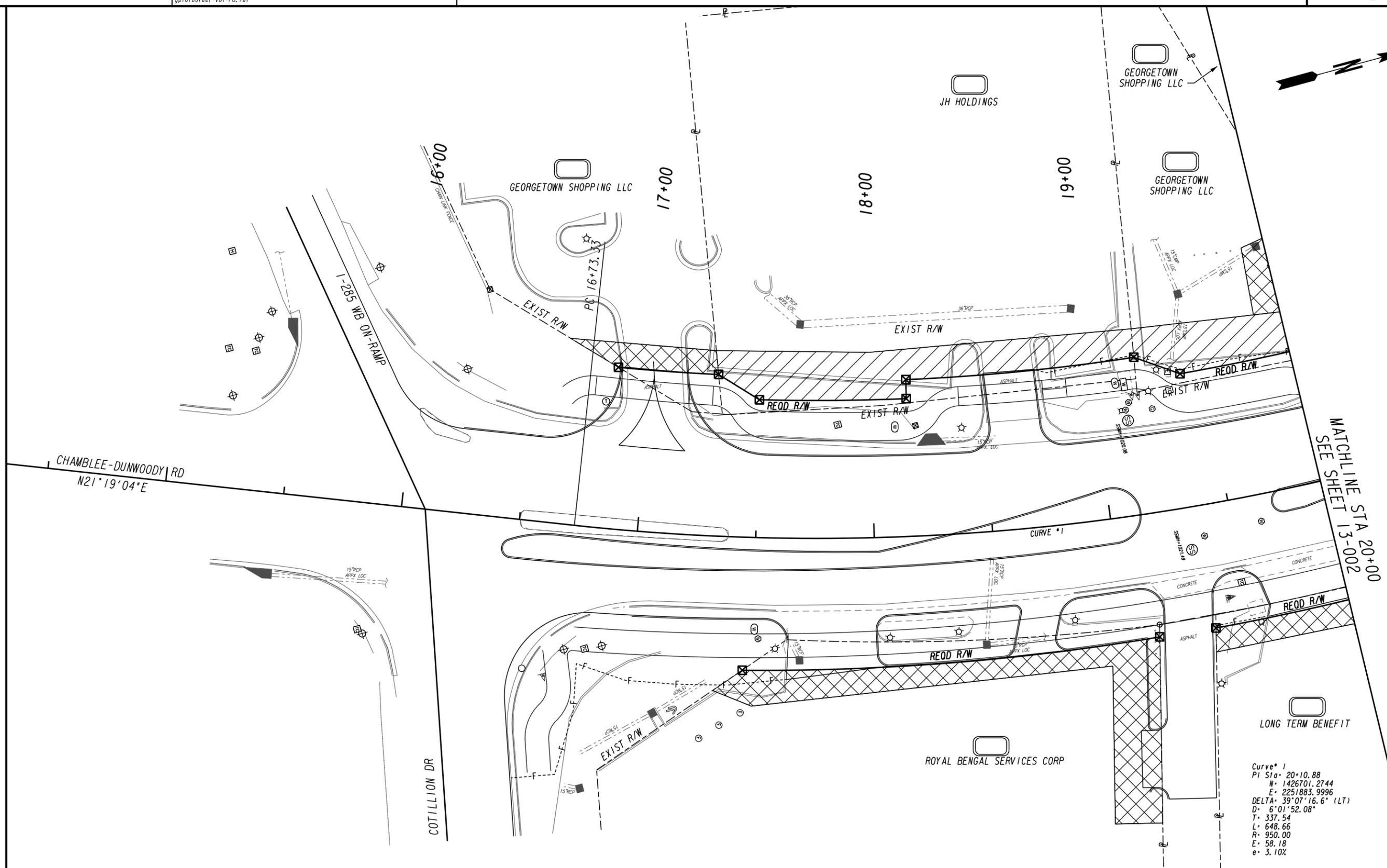
APPROVALS

Concur: *Heidi Pirell*
Director of Engineering

Approve: *Margaret B. Pirell*
Chief Engineer

2.23.16
Date

ATTACHMENT #1



MATCHLINE STA 20+00
SEE SHEET 13-002

Curve* 1
 PI Sta= 20+10.88
 N= 1426701.2744
 E= 2251883.9996
 DELTA= 39°07'16.6" (LT)
 D= 6°01'52.08"
 T= 337.54
 L= 648.66
 R= 950.00
 E= 58.18
 e= 3.10%

PROPERTY AND EXISTING R/W LINE	---
REQUIRED R/W LINE	---
CONSTRUCTION LIMITS	---
EASEMENT FOR CONSTR	---
& MAINTENANCE OF SLOPES	---
EASEMENT FOR CONSTR OF SLOPES	---
EASEMENT FOR CONSTR OF DRIVES	---

BEGIN LIMIT OF ACCESS.....	BLA
END LIMIT OF ACCESS.....	ELA
LIMIT OF ACCESS	---
REQ'D R/W & LIMIT OF ACCESS	---
ORANGE BARRIER FENCE	---
ESA - ENV. SENSITIVE AREA	---
(SEE ERIT TABLE)	---

GEORGIA
DEPARTMENT
OF
TRANSPORTATION

Dunwoody*

AECOM

400 NORTHPARK TOWN CENTER
1000 ABERNATHY ROAD, N.E., SUITE 900
ATLANTA, GEORGIA 30328
TEL: (678) 808-8800 FAX: (678) 808-8400

SCALE IN FEET

0 20 40 60

REVISION DATES	

MAINLINE PLAN

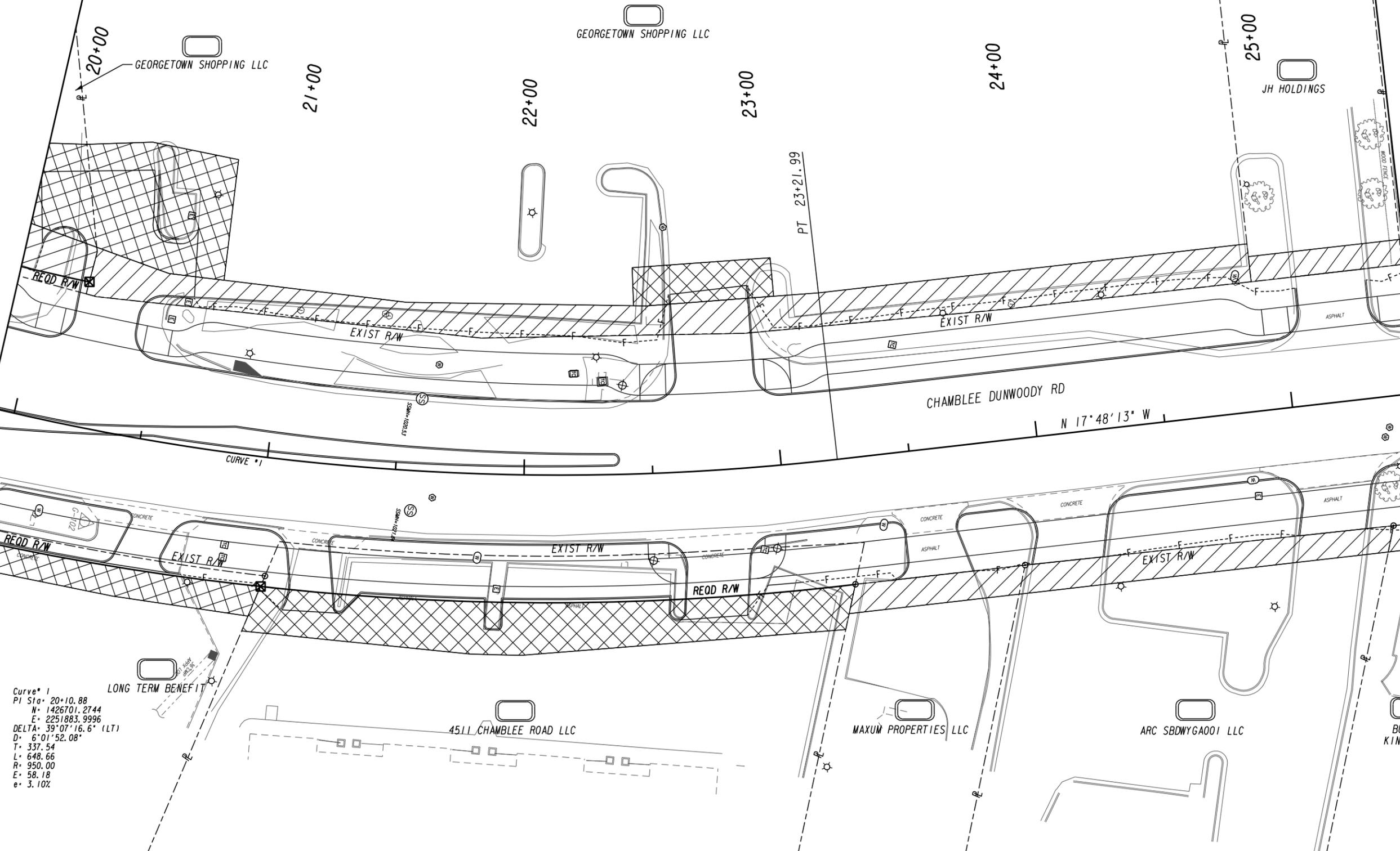
CR 3329 & CR 5156 - GEORGETOWN
AREA MULTIMODAL IMPROVEMENTS

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BACKCHECKED:	DATE:	13-001
CORRECTED:	DATE:	
VERIFIED:	DATE:	



MATCHLINE STA 20+00
SEE SHEET 13-001

MATCHLINE STA 25+00
SEE SHEET 13-003



PROPERTY AND EXISTING R/W LINE	---
REQUIRED R/W LINE	---
CONSTRUCTION LIMITS	---
EASEMENT FOR CONSTR & MAINTENANCE OF SLOPES	---
EASEMENT FOR CONSTR OF SLOPES	---
EASEMENT FOR CONSTR OF DRIVES	---

BEGIN LIMIT OF ACCESS.....BLA	---
END LIMIT OF ACCESS.....ELA	---
LIMIT OF ACCESS	---
REQ'D R/W & LIMIT OF ACCESS	---
ORANGE BARRIER FENCE	---
ESA - ENV. SENSITIVE AREA (SEE ERIT TABLE)	---

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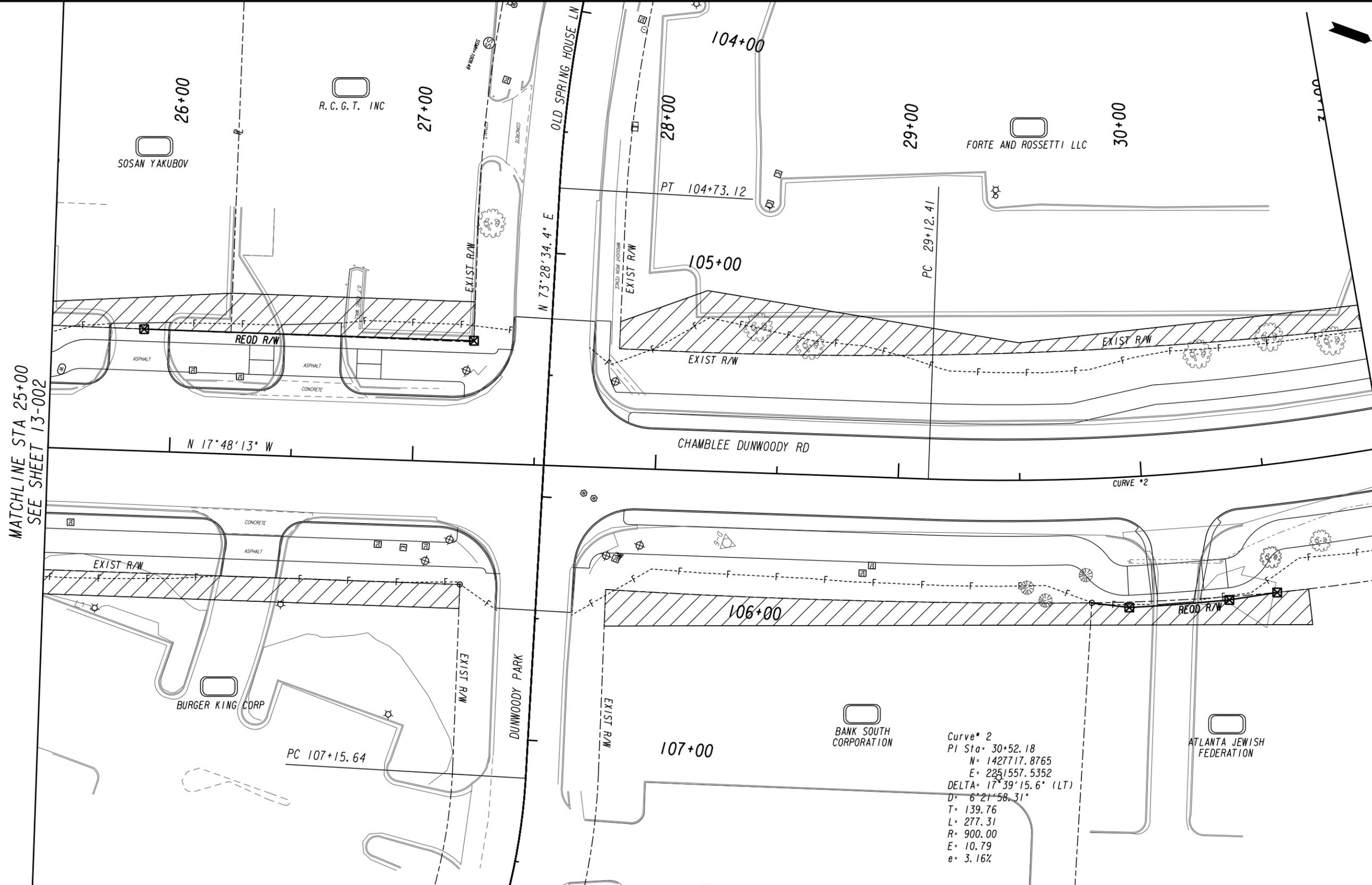
SCALE IN FEET

REVISION DATES	

MAINLINE PLAN

CR 3329 & CR 5156 - GEORGETOWN
AREA MULTIMODAL IMPROVEMENTS

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BACKCHECKED:	DATE:	13-002
CORRECTED:	DATE:	
VERIFIED:	DATE:	



MATCHLINE STA 25+00
SEE SHEET 13-002

MATCHLINE STA 31+00
SEE SHEET 13-004

---P---	PROPERTY AND EXISTING R/W LINE
---	REQUIRED R/W LINE
---C---	CONSTRUCTION LIMITS
---F---	EASEMENT FOR CONSTR & MAINTENANCE OF SLOPES
---S---	EASEMENT FOR CONSTR OF SLOPES
---D---	EASEMENT FOR CONSTR OF DRIVES

---BLA---	BEGIN LIMIT OF ACCESS.....BLA
---ELA---	END LIMIT OF ACCESS.....ELA
---	LIMIT OF ACCESS
---	REQ'D R/W & LIMIT OF ACCESS
---	ORANGE BARRIER FENCE
---	ESA - ENV. SENSITIVE AREA (SEE ERIT TABLE)

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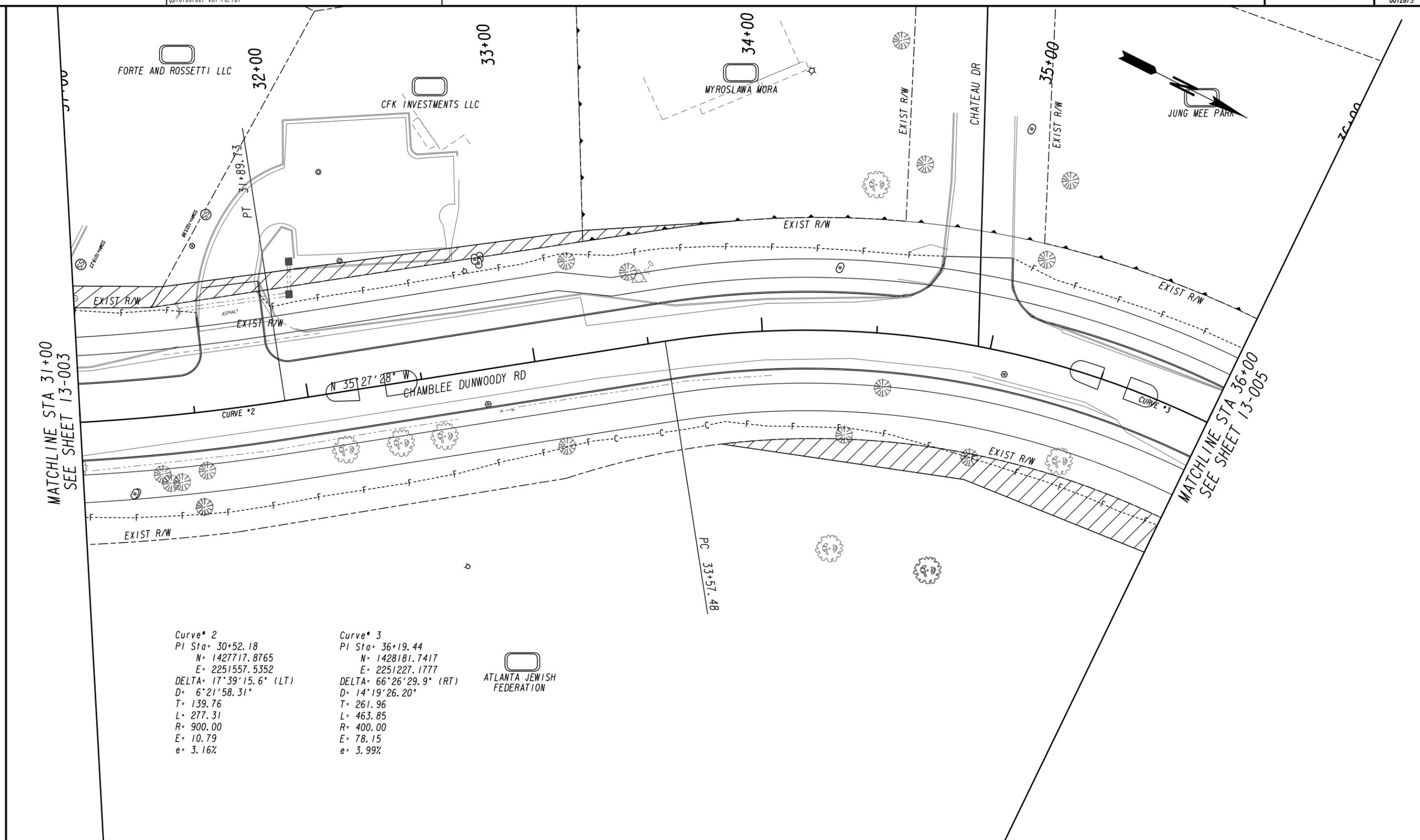
SCALE IN FEET

REVISION DATES	

MAINLINE PLAN

CR 3329 & CR 5156 - GEORGETOWN
AREA MULTIMODAL IMPROVEMENTS

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	13-003
CORRECTED:	DATE:	
VERIFIED:	DATE:	



MATCHLINE STA 31+00
SEE SHEET 13-003

MATCHLINE STA 36+00
SEE SHEET 13-005

Curve* 2
 PI Sta: 30+52.18
 N= 1427717.8765
 E= 2251557.5352
 DELTA: 17°39'15.6" (LT)
 D= 6°21'58.31"
 T= 139.76
 L= 277.31
 R= 900.00
 E= 10.79
 e= 3.16%

Curve* 3
 PI Sta: 36+19.44
 N= 1428181.7417
 E= 2251227.1777
 DELTA: 66°26'29.9" (RT)
 D= 14°19'26.20"
 T= 261.96
 L= 463.85
 R= 400.00
 E= 78.15
 e= 3.99%

ATLANTA JEWISH
FEDERATION

PROPERTY AND EXISTING R/W LINE	
REQUIRED R/W LINE	
CONSTRUCTION LIMITS	
EASEMENT FOR CONSTR & MAINTENANCE OF SLOPES	
EASEMENT FOR CONSTR OF SLOPES	
EASEMENT FOR CONSTR OF DRIVES	

BEGIN LIMIT OF ACCESS.....BLA	
END LIMIT OF ACCESS.....ELA	
LIMIT OF ACCESS	
REQ'D R/W & LIMIT OF ACCESS	
ORANGE BARRIER FENCE	
ESA - ENV. SENSITIVE AREA (SEE ERIT TABLE)	

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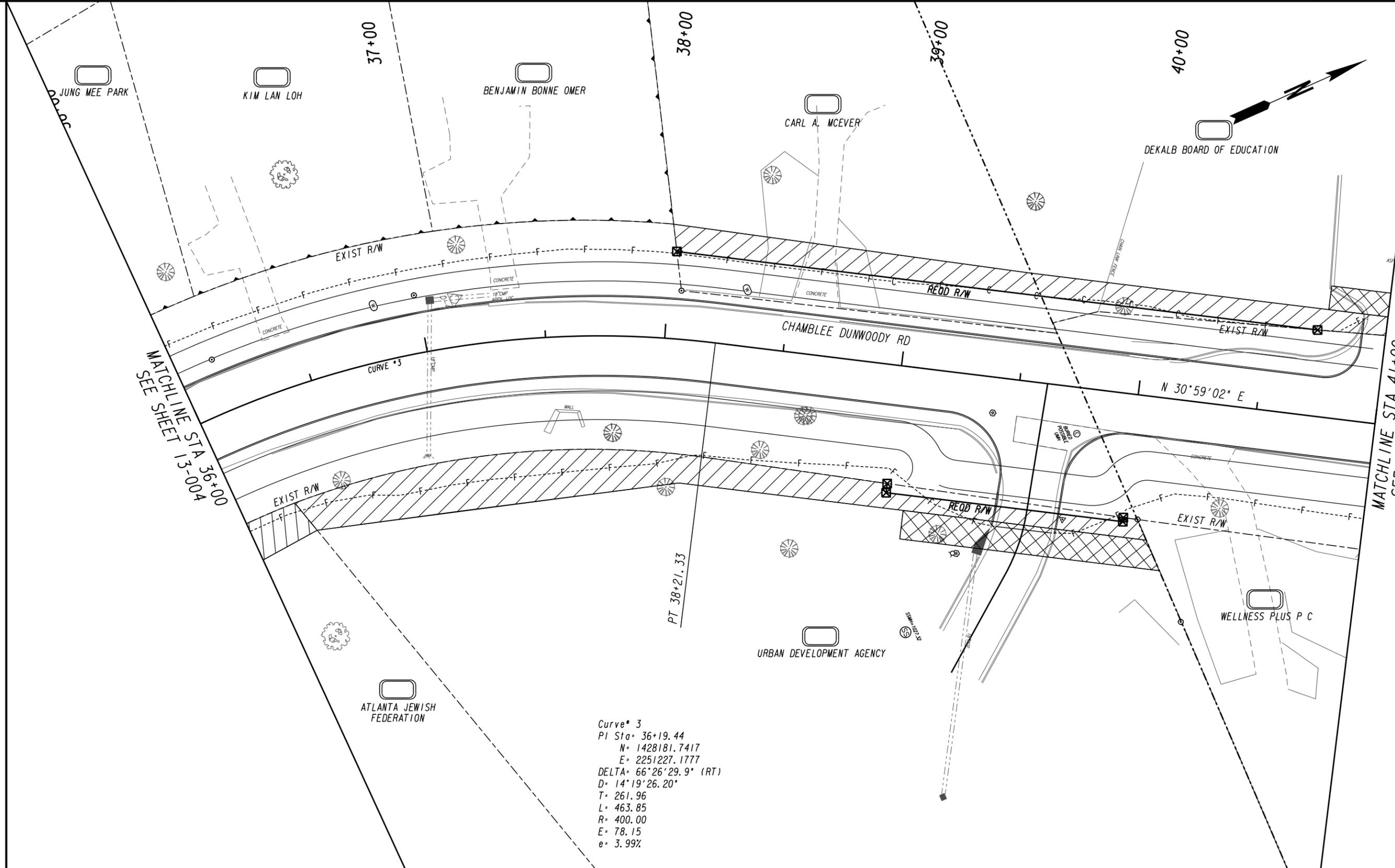
SCALE IN FEET

REVISION DATES	

MAINLINE PLAN

CR 3329 & CR 5156 - GEORGETOWN
AREA MULTIMODAL IMPROVEMENTS

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	13-004
CORRECTED:	DATE:	
VERIFIED:	DATE:	



MATCHLINE STA 36+00
 SEE SHEET 13-004

MATCHLINE STA 41+00
 SEE SHEET 13-006

Curve # 3
 PI Sta= 36+19.44
 N= 1428181.7417
 E= 2251227.1777
 DELTA= 66°26'29.9" (RT)
 D= 14°19'26.20"
 T= 261.96
 L= 463.85
 R= 400.00
 E= 78.15
 e= 3.99%

PROPERTY AND EXISTING R/W LINE	
REQUIRED R/W LINE	
CONSTRUCTION LIMITS	
EASEMENT FOR CONSTR & MAINTENANCE OF SLOPES	
EASEMENT FOR CONSTR OF SLOPES	
EASEMENT FOR CONSTR OF DRIVES	

BEGIN LIMIT OF ACCESS.....BLA	
END LIMIT OF ACCESS.....ELA	
LIMIT OF ACCESS	
REQ'D R/W & LIMIT OF ACCESS	
ORANGE BARRIER FENCE	
ESA - ENV. SENSITIVE AREA (SEE ERIT TABLE)	

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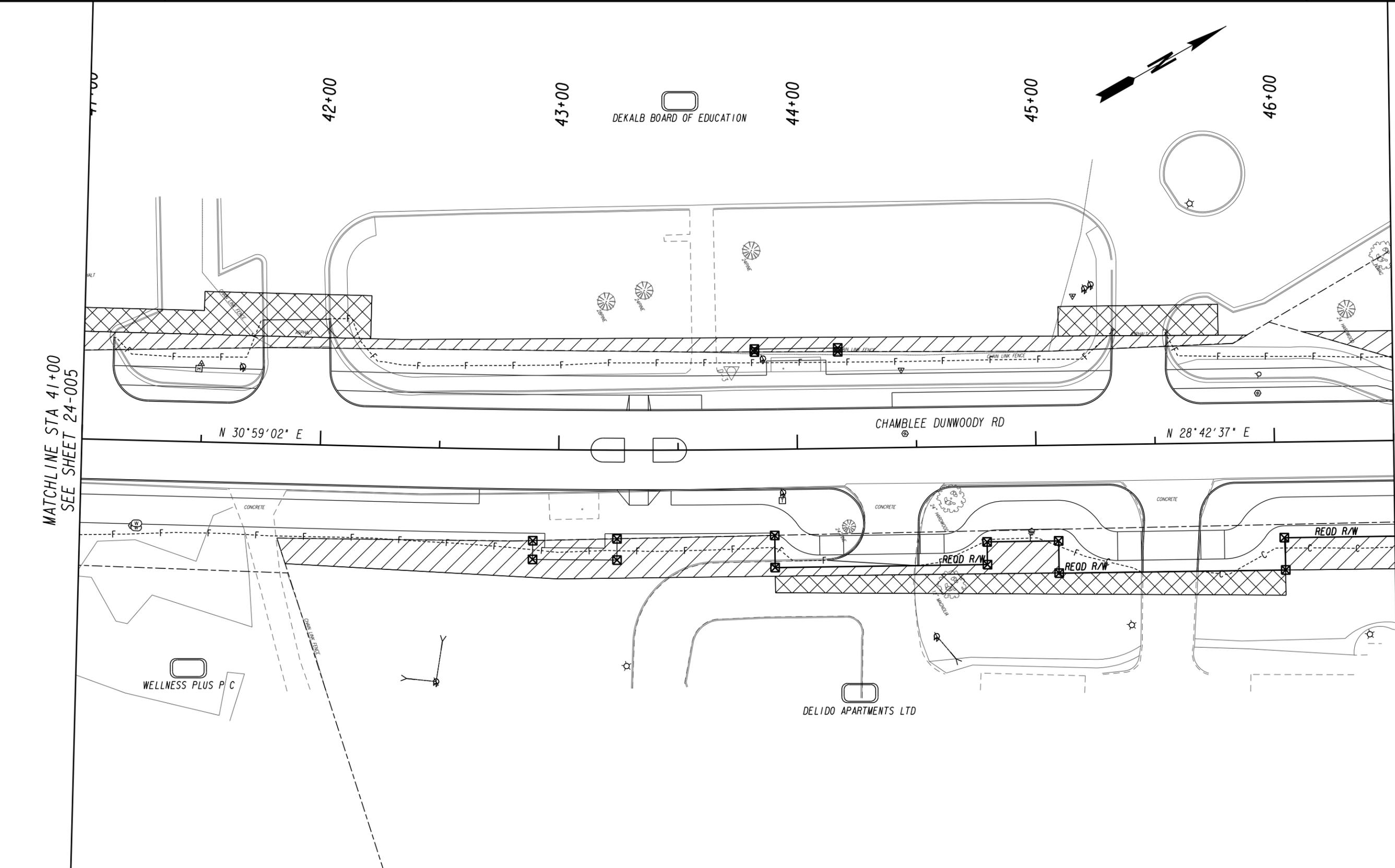
SCALE IN FEET

REVISION DATES	

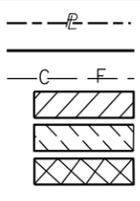
MAINLINE PLAN

CR 3329 & CR 5156 - GEORGETOWN AREA MULTIMODAL IMPROVEMENTS

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	13-005
CORRECTED:	DATE:	
VERIFIED:	DATE:	



PROPERTY AND EXISTING R/W LINE
 REQUIRED R/W LINE
 CONSTRUCTION LIMITS
 EASEMENT FOR CONSTR
 & MAINTENANCE OF SLOPES
 EASEMENT FOR CONSTR OF SLOPES
 EASEMENT FOR CONSTR OF DRIVES



BEGIN LIMIT OF ACCESS.....BLA
 END LIMIT OF ACCESS.....ELA
 LIMIT OF ACCESS
 REQ'D R/W & LIMIT OF ACCESS
 ORANGE BARRIER FENCE
 ESA - ENV. SENSITIVE AREA
 (SEE ERIT TABLE)

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SCALE IN FEET
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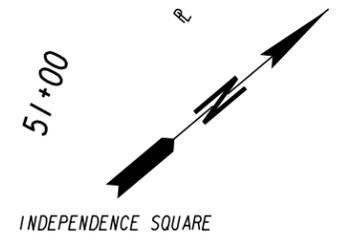
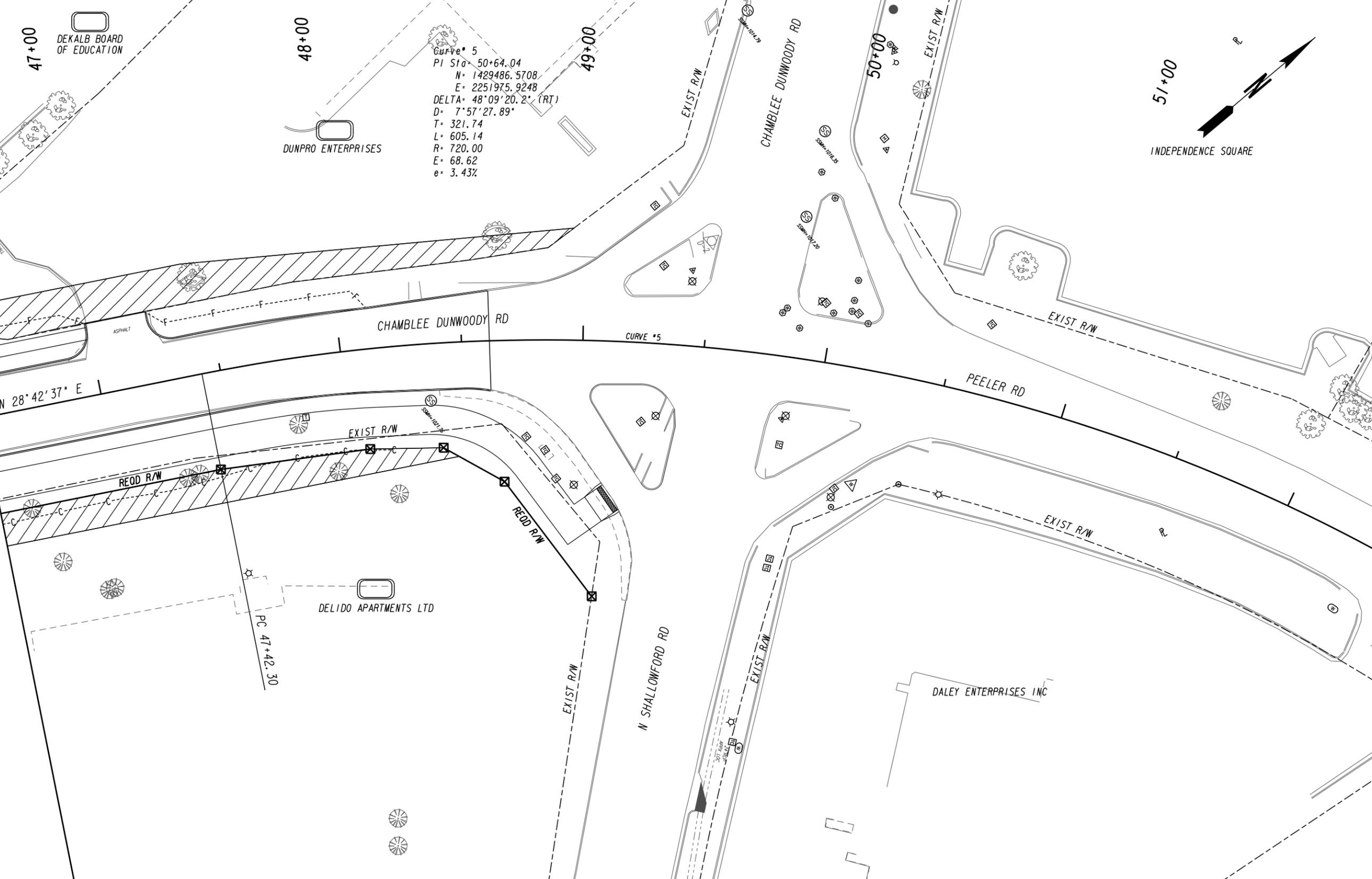
REVISION DATES	

UTILITY PLANS

CR 3329 & CR 5156 - GEORGETOWN
 AREA MULTIMODAL IMPROVEMENTS

CHECKED:	DATE:	DRAWING No. 24-006
BACKCHECKED:	DATE:	
CORRECTED:	DATE:	
VERIFIED:	DATE:	

MATCHLINE STA 46+50
SEE SHEET 13-007



PROPERTY AND EXISTING R/W LINE	
REQUIRED R/W LINE	
CONSTRUCTION LIMITS	
EASEMENT FOR CONSTR & MAINTENANCE OF SLOPES	
EASEMENT FOR CONSTR OF SLOPES	
EASEMENT FOR CONSTR OF DRIVES	

BEGIN LIMIT OF ACCESS.....BLA	
END LIMIT OF ACCESS.....ELA	
LIMIT OF ACCESS	
REQ'D R/W & LIMIT OF ACCESS	
ORANGE BARRIER FENCE	
ESA - ENV. SENSITIVE AREA (SEE ERIT TABLE)	

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SCALE IN FEET

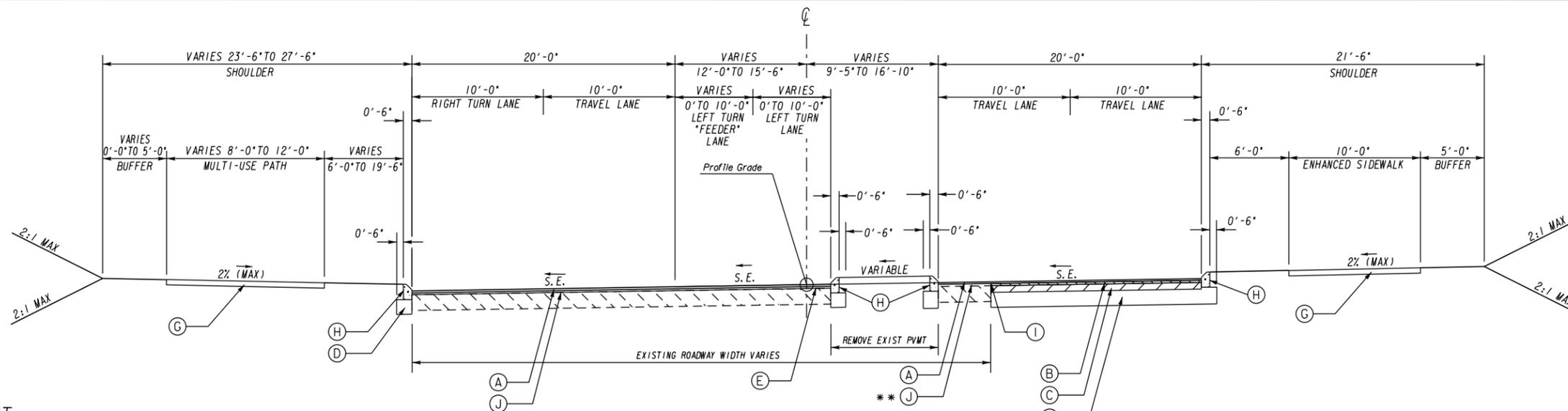
REVISION DATES	

MAINLINE PLAN

CR 3329 & CR 5156 - GEORGETOWN AREA MULTIMODAL IMPROVEMENTS

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	13-007
CORRECTED:	DATE:	
VERIFIED:	DATE:	

ATTACHMENT #2



TS-01

CHAMBLEE-DUNWOODY ROAD
APPLIES TO STA 16+44.00 TO 19+68.00

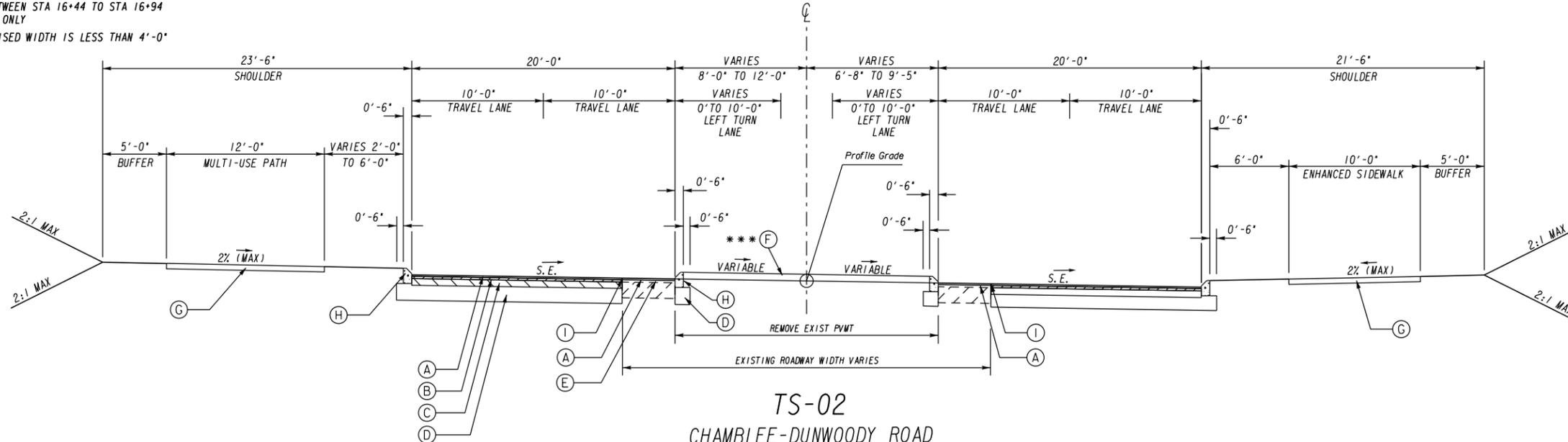
REQUIRED PAVEMENT

- (A) RECYCLED ASPH CONC 12.5 MM SUPERPAVE, GP 2 ONLY, INCL BITUM MATL & H LIME (165 LB/SQ YD)
- (B) RECYCLED ASPH CONC 19 MM SUPERPAVE, GP 1 OR 2, INCL BITUM MATL & H LIME (220 LB/SQ YD)
- (C) RECYCLED ASPH CONC 25 MM SUPERPAVE, GP 1 OR 2, INCL BITUM MATL & H LIME (440 LB/SQ YD)
- (D) GR AGGR BASE CRS, 10 INCH, INCL MATL
- (E) RECYCLED ASPH CONC LEVELING, INCL BITUM MATL & H LIME
- (F) CONCRETE MEDIAN, 7 1/2 IN (TYPE 7 FACE)
- (G) 4 IN CONCRETE SIDEWALK *
- (H) CONCRETE HEADER CURB, 6 IN, TP 7
- (I) PAVEMENT REINFORCEMENT FABRIC, 18" WIDE
- (J) MILL ASPHALT CONC PAVEMENT, VARIABLE DEPTH (1 1/2" TYP) * *

* NOTE: ALL RAMPS SHALL BE 8" CONCRETE

* * MILL EXIST ASPHALT CONC. BETWEEN STA 16+44 TO STA 16+94 AND STA 47+62 TO STA 48+12 ONLY

* * * ONLY USE (C) WHEN OVERALL RAISED WIDTH IS LESS THAN 4'-0"



TS-02

CHAMBLEE-DUNWOODY ROAD
APPLIES TO STA 19+68.00 TO 22+43.00

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NOT TO SCALE

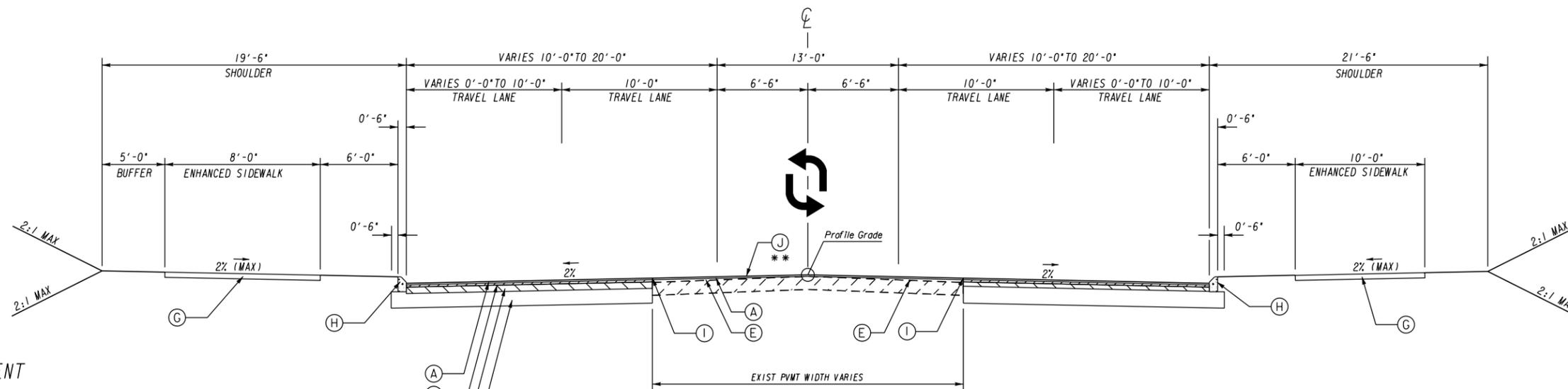
REVISION DATES

NO.	DATE	DESCRIPTION

TYPICAL SECTIONS

CR 3329 & CR 5156 - GEORGETOWN
AREA MULTIMODAL IMPROVEMENTS

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	05-001
CORRECTED:	DATE:	
VERIFIED:	DATE:	

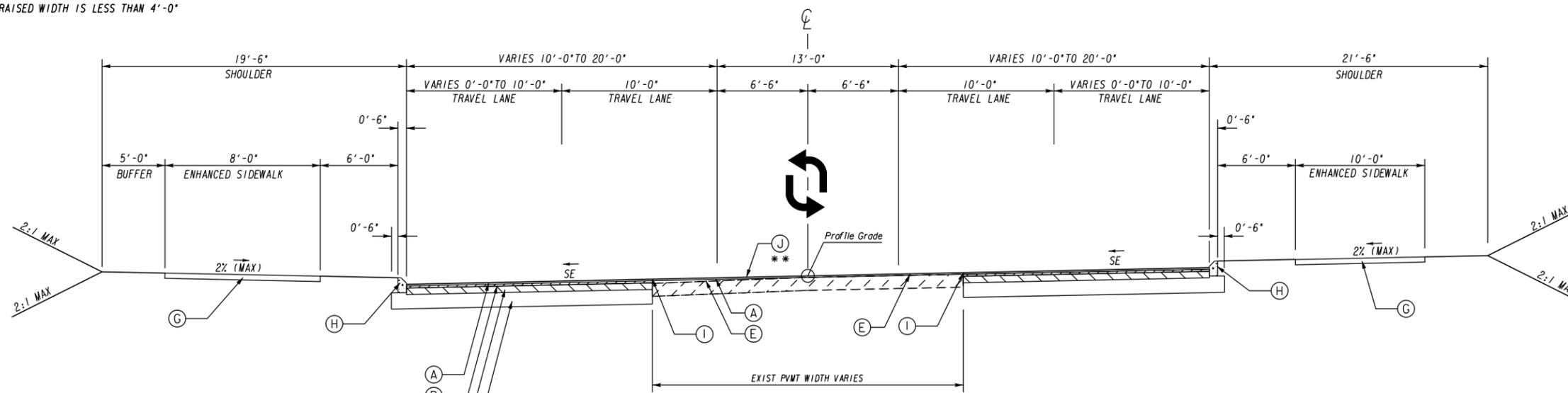


REQUIRED PAVEMENT

- (A) RECYCLED ASPH CONC 12.5 MM SUPERPAVE, GP 2 ONLY, INCL BITUM MATL & H LIME (165 LB/SQ YD)
- (B) RECYCLED ASPH CONC 19 MM SUPERPAVE, GP 1 OR 2, INCL BITUM MATL & H LIME (220 LB/SQ YD)
- (C) RECYCLED ASPH CONC 25 MM SUPERPAVE, GP 1 OR 2, INCL BITUM MATL & H LIME (440 LB/SQ YD)
- (D) GR AGGR BASE CRS, 10 INCH, INCL MATL
- (E) RECYCLED ASPH CONC LEVELING, INCL BITUM MATL & H LIME
- (F) CONCRETE MEDIAN, 7 1/2 IN (TYPE 7 FACE)
- (G) 4 IN CONCRETE SIDEWALK *
- (H) CONCRETE HEADER CURB, 6 IN, TP 7
- (I) PAVEMENT REINFORCEMENT FABRIC, 18" WIDE
- (J) MILL ASPHALT CONC PAVEMENT, VARIABLE DEPTH (1 1/2" TYP) * *

* NOTE: ALL RAMPS SHALL BE 8" CONCRETE
 * * WILL EXIST ASPHALT CONC. BETWEEN STA 16+44 TO STA 16+94 AND STA 47+62 TO STA 48+12 ONLY
 * * * ONLY USE (J) WHEN OVERALL RAISED WIDTH IS LESS THAN 4'-0"

TS-03
 CHAMBLEE-DUNWOODY ROAD
 TANGENT SECTION
 APPLIES TO STA 27+55.00 TO 48+12.00



TS-03
 CHAMBLEE-DUNWOODY ROAD
 SUPERELEVATED SECTION

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 OF
 TRANSPORTATION

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 ATLANTA, GEORGIA 30328
 TEL: (678) 808-8800 FAX: (678) 808-8400

NOT TO SCALE

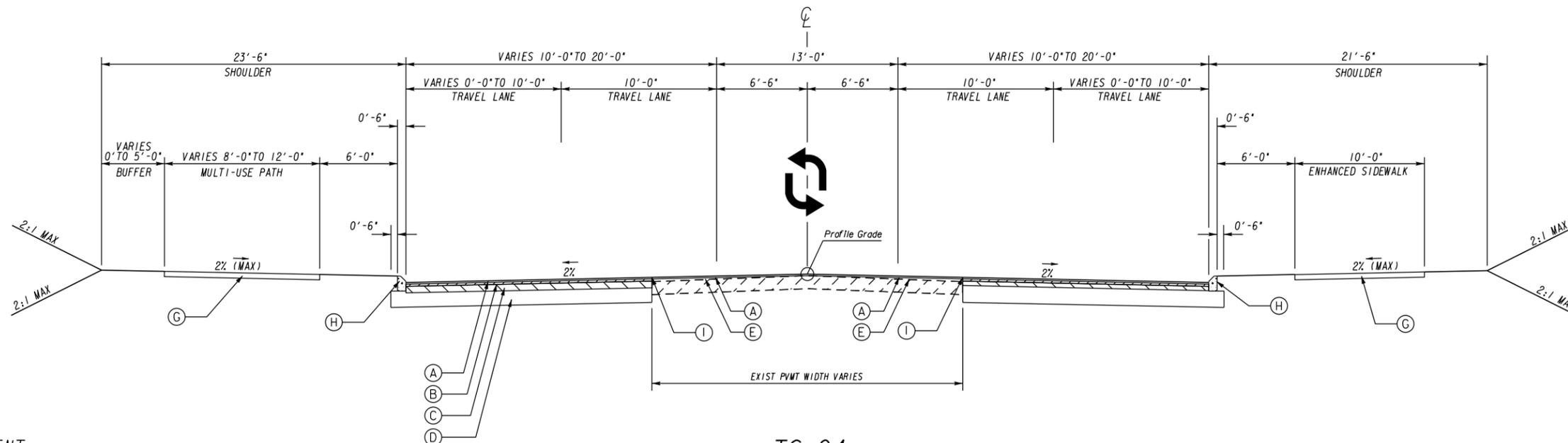
REVISION DATES

NO.	DATE	DESCRIPTION

TYPICAL SECTIONS

CR 3329 & CR 5156 - GEORGETOWN
 AREA MULTIMODAL IMPROVEMENTS

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	05-002
CORRECTED:	DATE:	
VERIFIED:	DATE:	



REQUIRED PAVEMENT

- (A) RECYCLED ASPH CONC 12.5 MM SUPERPAVE, GP 2 ONLY, INCL BITUM MATL & H LIME (165 LB/SQ YD)
 - (B) RECYCLED ASPH CONC 19 MM SUPERPAVE, GP 1 OR 2, INCL BITUM MATL & H LIME (220 LB/SQ YD)
 - (C) RECYCLED ASPH CONC 25 MM SUPERPAVE, GP 1 OR 2, INCL BITUM MATL & H LIME (440 LB/SQ YD)
 - (D) GR AGGR BASE CRS, 10 INCH, INCL MATL
 - (E) RECYCLED ASPH CONC LEVELING, INCL BITUM MATL & H LIME
 - (F) CONCRETE MEDIAN, 7 1/2 IN (TYPE 7 FACE)
 - (G) 4 IN CONCRETE SIDEWALK *
 - (H) CONCRETE HEADER CURB, 6 IN, TP 7
 - (I) PAVEMENT REINFORCEMENT FABRIC, 18" WIDE
 - (J) MILL ASPHALT CONC PAVEMENT, VARIABLE DEPTH (1 1/2" TYP) * *
- * NOTE: ALL RAMPS SHALL BE 8" CONCRETE
- * * WILL EXIST ASPHALT CONC. BETWEEN STA 16+44 TO STA 16+94 AND STA 47+62 TO STA 48+12 ONLY
- * * * ONLY USE (J) WHEN OVERALL RAISED WIDTH IS LESS THAN 4'-0"

TS-04
 CHAMBLEE-DUNWOODY ROAD
 APPLIES TO STA 22+43.00 TO 27+55.00

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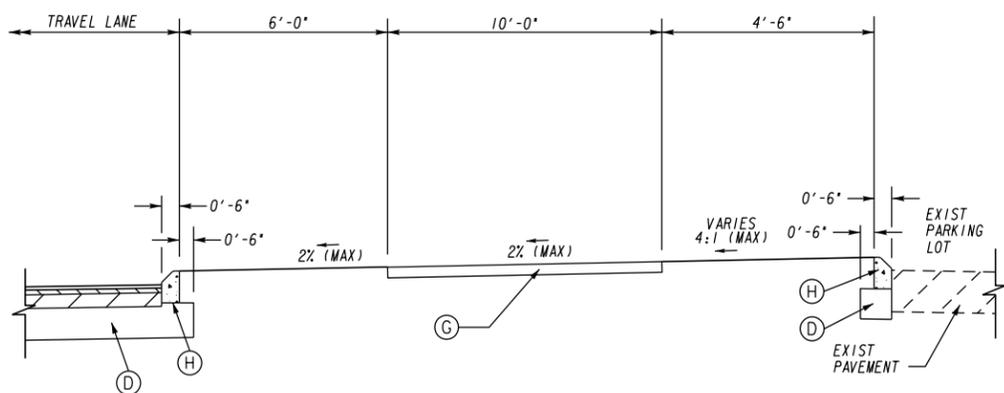
NOT TO SCALE

REVISION DATES	

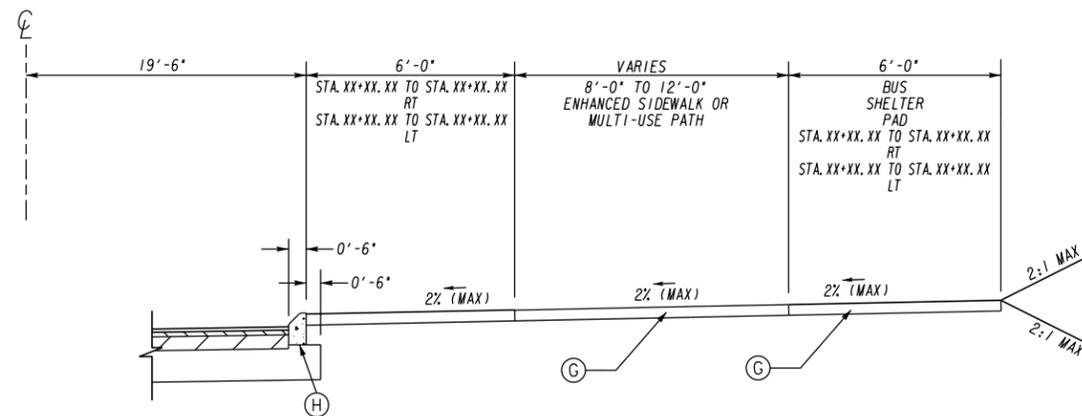
TYPICAL SECTIONS

CR 3329 & CR 5156 - GEORGETOWN
 AREA MULTIMODAL IMPROVEMENTS

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	05-003
CORRECTED:	DATE:	
VERIFIED:	DATE:	



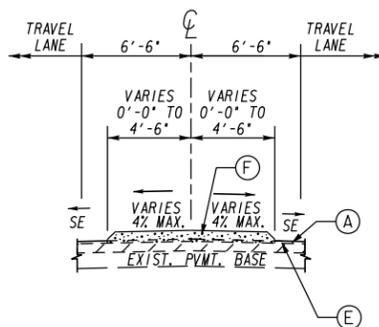
CHAMBLEE-DUNWOODY ROAD
 ADJACENT PARKING LOT DETAIL
 APPLIES TO STA. XX+XX.XX TO STA. XX+XX.XX RT
 STA. XX+XX.XX TO STA. XX+XX.XX LT



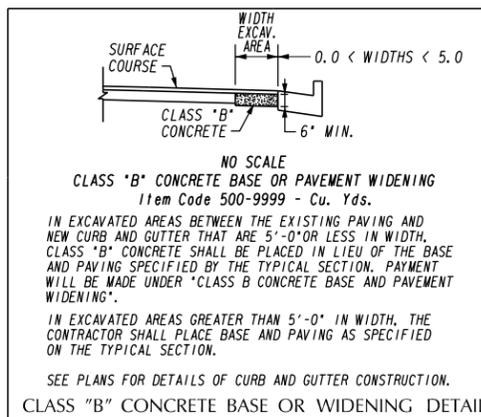
CHAMBLEE-DUNWOODY ROAD
 BUS STOP DETAIL
 APPLIES TO STA. XX+XX.XX TO STA. XX+XX.XX RT
 STA. XX+XX.XX TO STA. XX+XX.XX LT

REQUIRED PAVEMENT

- (A) RECYCLED ASPH CONC 12.5 MM SUPERPAVE, GP 2 ONLY, INCL BITUM MATL & H LIME (165 LB/SQ YD)
 - (B) RECYCLED ASPH CONC 19 MM SUPERPAVE, GP 1 OR 2, INCL BITUM MATL & H LIME (220 LB/SQ YD)
 - (C) RECYCLED ASPH CONC 25 MM SUPERPAVE, GP 1 OR 2, INCL BITUM MATL & H LIME (440 LB/SQ YD)
 - (D) GR AGGR BASE CRS, 10 INCH, INCL MATL
 - (E) RECYCLED ASPH CONC LEVELING, INCL BITUM MATL & H LIME
 - (F) CONCRETE MEDIAN, 7 1/2 IN (TYPE 7 FACE)
 - (G) 4 IN CONCRETE SIDEWALK *
 - (H) CONCRETE HEADER CURB, 6 IN, TP 7
 - (I) PAVEMENT REINFORCEMENT FABRIC, 18" WIDE
 - (J) MILL ASPHALT CONC PAVEMENT, VARIABLE DEPTH (1 1/2" TYP) * *
- * NOTE: ALL RAMPS SHALL BE B" CONCRETE
- * * MILL EXIST ASPHALT CONC BETWEEN STA 16+44 TO STA 16+94 AND STA 47+62 TO STA 48+12 ONLY
- * * * ONLY USE (J) WHEN OVERALL RAISED WIDTH IS LESS THAN 4'-0"



CHAMBLEE-DUNWOODY ROAD
 MID-BLOCK RAISED ISLAND DETAIL
 APPLIES TO STA. XX+XX.XX TO STA. XX+XX.XX RT
 STA. XX+XX.XX TO STA. XX+XX.XX LT



CLASS "B" CONCRETE BASE OR PAVEMENT WIDENING
 Item Code 500-9999 - Cu. Yds.

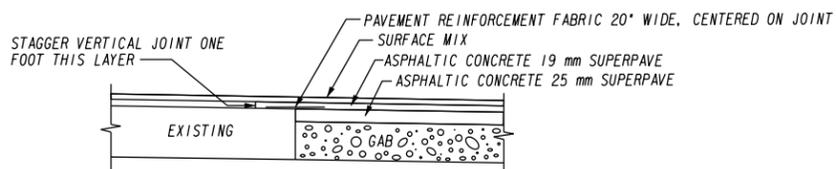
IN EXCAVATED AREAS BETWEEN THE EXISTING PAVING AND NEW CURB AND GUTTER THAT ARE 5'-0" OR LESS IN WIDTH, CLASS "B" CONCRETE SHALL BE PLACED IN LIEU OF THE BASE AND PAVING SPECIFIED BY THE TYPICAL SECTION. PAVEMENT WILL BE MADE UNDER "CLASS B CONCRETE BASE AND PAVEMENT WIDENING".

IN EXCAVATED AREAS GREATER THAN 5'-0" IN WIDTH, THE CONTRACTOR SHALL PLACE BASE AND PAVING AS SPECIFIED ON THE TYPICAL SECTION.

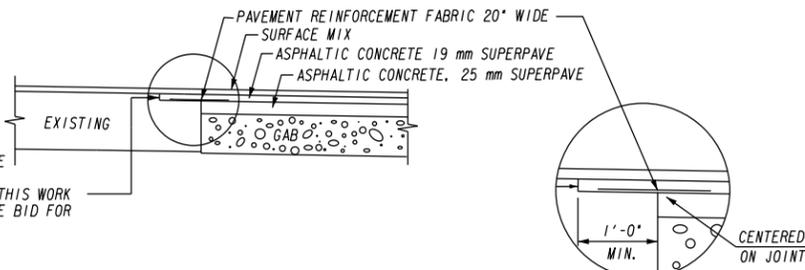
SEE PLANS FOR DETAILS OF CURB AND GUTTER CONSTRUCTION.

CLASS "B" CONCRETE BASE OR WIDENING DETAIL

TYPICAL SECTION DETAIL TO BE USED WHEN EXISTING PAVEMENT IS TO BE RESURFACED WITH TWO INCHES OR MORE OF ASPHALTIC CONCRETE



TYPICAL SECTION DETAIL TO BE USED WHEN EXISTING PAVEMENT IS TO BE RESURFACED WITH LESS THAN TWO INCHES OF ASPHALTIC CONCRETE



MILL EXISTING LANE ONE FOOT WIDE TO DEPTH OF ADJOINING LAYER TO BE PLACED. COST OF MILLING FOR THIS WORK TO BE INCLUDED IN THE UNIT PRICE BID FOR PAVEMENT REINFORCING FABRIC.

SLOPE: SEE ROADWAY PLANS FOR SUPERELEVATION RATES AND TRANSITIONS.

NOTE: FOR METHOD OF SUPERELEVATION SEE CONSTR PLAN & PROFILE SHEETS-CURVE DATA. LOCATIONS OF NORMAL CROWN & FULL S.E. NOTED ON CONSTR CENTERLINE.

* SEE PLAN SHEETS.

SHOULDER MAY BE GRADED AWAY FROM ROADWAY TO FACILITATE THE SLOPE TIE TO EXISTING GROUND.

ALLOWABLE RANGES TABLE

FOR THIS PROJECT, CROSS SLOPES THAT ARE ADJUSTED TO "BEST FIT" EXISTING PAVEMENT SLOPES ARE SUBJECT TO THE FOLLOWING LIMITS:

A. NORMAL CROWN	
SECTION WITH GRADES 0.5% OR GREATER	SECTION WITH GRADES LESS THAN 0.5%
0.0150 FT/FT - MINIMUM	0.0156 FT/FT - MINIMUM
0.0208 FT/FT - DESIRABLE	0.0208 FT/FT - DESIRABLE
0.0250 FT/FT - MAXIMUM	0.0300 FT/FT - MAXIMUM

- B. SUPERELEVATION RATE
 S.E. RATE SHOWN ON PLANS OR SE RATE EXISTING IN FIELD, WHICHEVER IS GREATER.
- C. SUPERELEVATION TRANSITION LENGTH (LENGTH FROM FLAT POINT TO FULL SE)

RATE OF CHANGE	CORRESPONDING DIFFERENCE IN GRADE BETWEEN PIVOT POINT AND EDGE OF PAVEMENT
MINIMUM 1:150	0.67%
DESIRABLE 1:200	0.50%
MAXIMUM 1:300	0.33%

LENGTH SHALL BE SET TO AVOID CREATING A FLAT GUTTER GRADE ON LOW SIDE AND TO AVOID FLAT CROSS SLOPES AT OR NEAR THE LOW POINT OF VERTICAL CURVES.

- D. POSITIONING OF SUPERELEVATION TRANSITION LENGTH ON SIMPLE CURVES

- 50% OF TRANSITION INSIDE CURVE - MAXIMUM
- 33% OF TRANSITION INSIDE CURVE - DESIRABLE
- 20% OF TRANSITION INSIDE CURVE - MINIMUM

NOTE: CROWN WIPE-OUT SHALL BE AT THE SAME RATE AS THE SE TRANSITION.

- E. SMOOTHING OF BREAKS IN EDGE PROFILE AT BEGIN AND END OF TRANSITION SHALL BE ACCOMPLISHED BY VERTICAL CURVE WITH A MINIMUM LENGTH (IN FEET) EQUAL TO THE SPEED DESIGN (IN MPH).

TO DEPTH OF ADJOINING LAYER TO BE PLACED. COST OF MILLING FOR THIS WORK TO BE INCLUDED IN THE UNIT PRICE BID FOR PAVEMENT REINFORCING FABRIC.

GEORGIA
 DEPARTMENT
 OF
 TRANSPORTATION

Dunwoody*

AECOM

400 NORTHPARK TOWN CENTER
 1000 ABERNATHY ROAD, N.E., SUITE 900
 ATLANTA, GEORGIA 30328
 TEL: (678) 808-8800 FAX: (678) 808-8400

NOT TO SCALE

REVISION DATES

TYPICAL SECTIONS

CR 3329 & CR 5156 - GEORGETOWN
 AREA MULTIMODAL IMPROVEMENTS

CHECKED:	DATE:	DRAWING No.
BACKCHECKED:	DATE:	05-004
CORRECTED:	DATE:	
VERIFIED:	DATE:	

ATTACHMENT #3

DEPARTMENT OF TRANSPORTATION STATE OF GEORGIA

INTERDEPARTMENT CORRESPONDENCE

FILE DeKalb County
P.I. Number 0012875
Chamblee Dunwoody Road from
Cotillion Drive to Peeler Rd

OFFICE Program Delivery
DATE 2/2/2016

FROM Sean Pharr, AECOM

TO Lisa Myers, Project Review Engineer

SUBJECT REVISIONS TO PROGRAMMED COSTS

PROJECT MANAGER Carleton Fisher

MNGT LET DATE 6/2018

MNGT R/W DATE 6/2017

PROGRAMMED COST (TPro W/OUT INFLATION)

LAST ESTIMATE UPDATE

CONSTRUCTION N/A

DATE N/A

RIGHT OF WAY \$750,000.00

8/24/2015

UTILITIES \$1,200,000.00

8/21/2015

REVISED COST ESTIMATES

CONSTRUCTION* \$5,783,979.19

RIGHT OF WAY N/A

UTILITIES** N/A

* Costs contain 5% Engineering and Inspection and 15% Construction Contingencies and Fuel and Liquid AC Adjustments.

REASON FOR COST INCREASE Further development of concept

CONTINGENCY SUMMARY

Construction Cost Estimate:	\$4,791,032.84	(Base Estimate)
Engineering and Inspection:	\$239,551.64	(Base Estimate x 5 %)
Construction Contingency:	\$718,654.93	(Base Estimate x 15 %) (The Construction Contingency is based on the Project Improvement Type in TPro.)
Total Liquid AC Adjustment	\$34,739.78	(From attached worksheet)
Construction Total:	\$5,783,979.19	
Utility Cost Estimate:	\$1,200,000.00	
Utility Contingency:	\$0	
Utility Total:	\$1,200,000.00	

Utility Owner
Georgia Power

Reimbursable Cost
\$1,200,000.00

c: Bobby Hilliard, Office of Program Control
Angela Whitworth, Financial Management Administrator

JOB DETAIL ESTIMATE

JOB NUMBER : 0012875 - 2 SPEC YEAR: 01
 DESCRIPTION: CR 3329 & CR 5156 - GEORGETOWN AREA MULTIMODAL IMPROVEMENTS

COST GROUPS FOR JOB 0012875 - 2

COST GROUP	DESCRIPTION	QUANTITY	PRICE	AMOUNT	ACTIVE?
MISC	LANDSCAPING (LS)	1.000	557240.00000	557240.00	Y
MISC	HARDSCAPING (LS)	1.000	457900.00000	457900.00	Y
MISC	EROSION CONTROL (LS)	1.000	111450.00000	111450.00	Y
MISC	MISCELLANEOUS (LS)	1.000	200000.00000	200000.00	Y
ACTIVE COST GROUP TOTAL				1326590.00	
INFLATED COST GROUP TOTAL				1326590.00	

ITEMS FOR JOB 0012875 - 2

LINE	ITEM	ALT	UNITS	DESCRIPTION	QUANTITY	PRICE	AMOUNT
0005	150-1000		LS	TRAFFIC CONTROL - LUMP SUM ITEM	1.000	80000.00	80000.00
0010	210-0100		LS	GRADING COMPLETE - LUMP SUM ITEM	1.000	300000.00	300000.00
0015	310-1101		TN	GR AGGR BASE CRS, INCL MATL	1258.000	26.61	33476.44
0020	318-3000		TN	AGGR SURF CRS	500.000	22.76	11384.23
0025	402-1802		TN	RECYL AC PATCHING, INCL BM&HL	100.000	136.40	13640.78
0030	402-1812		TN	RECYL AC LEVELING, INC BM&HL	200.000	88.02	17605.92
0035	402-3130		TN	RECYL AC 12.5MM SP,GP2,BM&HL	1337.000	96.26	128712.84
0040	402-3190		TN	RECYL AC 19 MM SP,GP 1 OR 2 ,INC BM&HL	244.000	97.60	23814.60
0045	402-3121		TN	RECYL AC 25MM SP,GP1/2,BM&HL	611.000	84.47	51612.73
0049	413-1000		GL	BITUM TACK COAT	836.000	3.65	3057.96
0050	432-5010		SY	MILL ASPH CONC PVMT,VARB DEPTH	17222.000	3.36	57943.94
0055	441-0104		SY	CONC SIDEWALK, 4 IN	6900.000	29.13	201003.49
0060	441-0754		SY	CONC MEDIAN, 7 1/2 IN	1444.000	51.26	74024.10
0065	441-4030		SY	CONC VALLEY GUTTER, 8 IN	890.000	44.75	39835.81
0070	441-6740		LF	CONC CURB & GUTTER/ 8"X30" TP7	11000.000	13.70	150736.08
0075	446-1100		LF	PVMT REF FAB STRIPS, TP2,18 INCH WIDTH	1000.000	6.01	6011.00
0080	550-1180		LF	STM DR PIPE 18",H 1-10	5250.000	38.64	202892.71
0085	550-1240		LF	STM DR PIPE 24",H 1-10	1750.000	49.93	87393.02
0090	550-1360		LF	STM DR PIPE 36",H 1-10	100.000	83.15	8315.48
0095	668-1100		EA	CATCH BASIN, GP 1	47.000	2330.69	109542.80
0100	653-1501		LF	THERMO SOLID TRAF ST 5 IN, WHI	19600.000	0.41	8135.76
0105	653-1502		LF	THERMO SOLID TRAF ST, 5 IN YEL	4900.000	0.50	2473.18
0110	653-1704		LF	THERM SOLID TRAF STRIPE,24",WH	240.000	6.35	1524.51
0115	653-1804		LF	THERM SOLID TRAF STRIPE, 8",WH	1440.000	2.25	3252.67
0120	653-3501		GLF	THERMO SKIP TRAF ST, 5 IN, WHI	5000.000	0.30	1516.40
0125	654-1001		EA	RAISED PVMT MARKERS TP 1	53.000	4.37	231.76
0130	654-1003		EA	RAISED PVMT MARKERS TP 3	210.000	3.44	724.21
0135	653-6004		SY	THERM TRAF STRIPING, WHITE	389.000	3.95	1538.78

JOB DETAIL ESTIMATE

0140	653-6006	SY	THERM TRAF STRIPING, YELLOW	100.000	4.34	434.04
0145	636-1020	SF	HWY SGN,TP1MAT,REFL SH TP3	220.000	14.99	3299.32
0150	636-1033	SF	HWY SIGNS, TP1MAT,REFL SH TP 9	330.000	18.38	6068.39
0155	636-2070	LF	GALV STEEL POSTS, TP 7	280.000	7.99	2239.89
0160	652-0120	EA	PAVEMENT MARKING, ARROW, TP 2	20.000	50.00	1000.00
0165	647-1000	LS	TRAF SIGNAL INSTALLATION NO - CHAMBLEE DUNWOODY AT OLD SPG HOUSE	1.000	125000.00	125000.00
0170	647-1000	LS	TRAF SIGNAL INSTALLATION NO - CHAMBLEE DUNWOODY AT KROGER SHOPPING CTR	1.000	125000.00	125000.00
0175	647-1000	LS	TRAF SIGNAL INSTALLATION NO - CHAMBLEE DUNWOODY AT COTILLION	1.000	125000.00	125000.00
0180	681-4120	EA	LT STD, 12' MH, POST TOP	104.000	8000.00	832000.00
0185	681-4278	EA	LT STD, 25' MH, 8' ARM	52.000	12000.00	624000.00
ITEM TOTAL						3464442.84
INFLATED ITEM TOTAL						3464442.84

TOTALS FOR JOB 0012875 - 2

ESTIMATED COST:	4791032.84
CONTINGENCY PERCENT (0.0):	0.00
ESTIMATED TOTAL:	4791032.84

TASKS FOR JOB ITEM LINE NUMBER: 0005

=====

TASK: REF 001 TYPE: REFERENCE PRICE ACTIVE?: Y
 VALUE: 80000.00000
 FORMULA:
 COMMENT: ADHOC REFERENCE PRICE

TASKS FOR JOB ITEM LINE NUMBER: 0010

=====

TASK: REF 001 TYPE: REFERENCE PRICE ACTIVE?: Y
 VALUE: 300000.00000
 FORMULA:
 COMMENT: ADHOC REFERENCE PRICE

TASKS FOR JOB ITEM LINE NUMBER: 0015

=====

TASK: BID 001 TYPE: BID-BASED ACTIVE?: Y
 REGRESSION: 26.61
 MODEL: 3 WORKTYPE: ASPH AREA: NCEN SEASON: SPRI HIGHWAY TYPE: ASPH URBAN/RURAL: URBN QUANTITY LEVEL: 2
 COMMENT: REGRESSION MODEL 3

MPO TIP NO.

DK-417

CALL NO.

P.I. NO.

0012875

DATE

8/21/2015

INDEX (TYPE)

REG. UNLEADED
DIESEL
LIQUID AC

DATE	INDEX
Aug-15	\$ 2.497
	\$ 2.725
	\$ 470.00

Link to Fuel and AC Index:

<http://www.dot.ga.gov/doingbusiness/Materials/Pages/asphaltcementindex.aspx>

LIQUID AC ADJUSTMENTS

PA=[((APM-APL)/APL)]xTMTxAPL

Asphalt

Price Adjustment (PA)				33727.2	\$	33,727.20
Monthly Asphalt Cement Price month placed (APM)	Max. Cap	60%	\$	752.00		
Monthly Asphalt Cement Price month project let (APL)			\$	470.00		
Total Monthly Tonnage of asphalt cement (TMT)				119.6		

ASPHALT	Tons	%AC	AC ton
Leveling	200	5.0%	10
12.5 OGFC		5.0%	0
12.5 mm	1337	5.0%	66.85
9.5 mm SP		5.0%	0
25 mm SP	611	5.0%	30.55
19 mm SP	244	5.0%	12.2
	2392		119.6

BITUMINOUS TACK COAT

Price Adjustment (PA)			\$	1,012.58	\$	1,012.58
Monthly Asphalt Cement Price month placed (APM)	Max. Cap	60%	\$	752.00		
Monthly Asphalt Cement Price month project let (APL)			\$	470.00		
Total Monthly Tonnage of asphalt cement (TMT)				3.590704371		

Bitum Tack

Gals	gals/ton	tons
836	232.8234	3.59070437

MPO TIP NO.

DK-417

CALL NO.

P.I. NO.

0012875

DATE

8/21/2015

BITUMINOUS TACK COAT (surface treatment)

Price Adjustment (PA)						0	\$	-
Monthly Asphalt Cement Price month placed (APM)		Max. Cap	60%	\$		752.00		
Monthly Asphalt Cement Price month project let (APL)				\$		470.00		
Total Monthly Tonnage of asphalt cement (TMT)						0		

Bitum Tack	SY	Gals/SY	Gals	gals/ton	tons
Single Surf. Trmt.		0.20	0	232.8234	0
Double Surf.Trmt.		0.44	0	232.8234	0
Triple Surf. Trmt		0.71	0	232.8234	0
					0

TOTAL LIQUID AC ADJUSTMENT	\$	34,739.78
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**GEORGIA DEPARTMENT OF TRANSPORTATION
PRELIMINARY ROW COST ESTIMATE SUMMARY**

Date: 2/18/2016 Project: 0012875
 Revised: County: Dekalb County
 PI: 0012875

Description: Chamblee Dunwoody Multi Modal
 Project Termini: Chamblee Dunwoody Multi Modal

Existing ROW: Varies
 Required ROW: Varies
 Parcels: 12

Land and Improvements _____ \$62,250.00

Proximity Damage	\$0.00
Consequential Damage	\$10,000.00
Cost to Cures	\$10,000.00
Trade Fixtures	\$0.00
Improvements	\$15,000.00

Valuation Services _____ \$45,000.00

Legal Services _____ \$83,100.00

Relocation _____ \$24,000.00

Demolition _____ \$0.00

Administrative _____ \$101,500.00

TOTAL ESTIMATED COSTS _____ \$315,850.00

TOTAL ESTIMATED COSTS (ROUNDED) _____ \$316,000.00

Preparation Credits	Hours	Signature

Prepared By: Dathone Alexander CG#: 286999 02/18/2016 (DATE)

Approved By: Dathone Alexander CG#: 286999 02/18/2016 (DATE)

NOTE: No Market Appreciation is included in this Preliminary Cost Estimate



AECOM
400 Northpark Town Center
1000 Abernathy Road, Suite 900
Atlanta, GA 30328
www.aecom.com

678.808.8800 tel
678.808.8400 fax

Project No: N/A
P.I.# 0012875

Date: February 2, 2016

Description: CR 3329 & CR 5156- Georgetown Area Multimodal Improvements

From: Eric Rickert, P.E.-AECOM

To: Mindy Sanders, P.E., Capital Projects Manager-City of Dunwoody

SUBJECT: UTILITY ESTIMATE OF PROBABLE COSTS

A review of utilities located on the above project has been conducted without a design concept. Listed below is a breakdown of the anticipated reimbursable and non-reimbursable cost.

<u>Utility Owner</u>	<u>Reimbursable</u>	<u>Non-Reimbursable</u>	<u>Estimate Based on</u>
Atlanta Gas Light	\$0.00	TBD	Site Visit/ Available Drawings
AT&T –Distribution	\$0.00	TBD	Site Visit/ Available Drawings
AT&T-Long Distance	\$0.00	TBD	Site Visit/ Available Drawings
Comcast	\$0.00	TBD	Site Visit/ Available Drawings
DeKalb County Watershed Management	\$0.00	TBD	Site Visit/ Available Drawings
Georgia Power Distribution	\$1,200,000.00	TBD	Site Visit/ Available Drawings
XO Communications	\$0.00	TBD	Site Visit/ Available Drawings
Level 3	\$0.00	TBD	Site Visit/ Available Drawings
Zayo Fiber Solutions	\$0.00	TBD	Site Visit/ Available Drawings
Windstream	\$0.00	TBD	Site Visit/ Available Drawings
Hotwire Communications	\$0.00	TBD	Site Visit/ Available Drawings
Total:	\$1,200,000.00	TBD	

This estimate is based on the best available information at the current stage, unforeseen prior rights information may be provided by the Utility Company at a later date that could potentially cause some non-reimbursable costs to shift to the reimbursable costs column.

If additional information is needed, please contact me at 678-808-8800.

ATTACHMENT #4

Department of Transportation State of Georgia

INTERDEPARTMENT CORRESPONDENCE

FILE DeKalb County **OFFICE** Planning
P.I. # 0012875 **DATE** August 5, 2015

FROM Cynthia L. VanDyke, State Transportation Planning Administrator

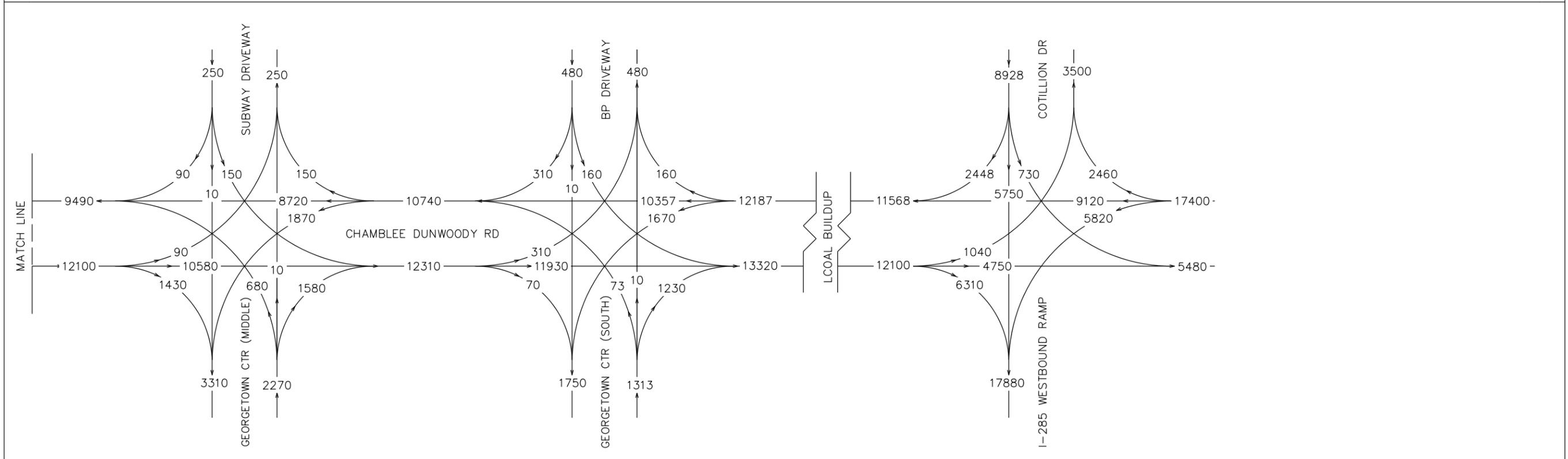
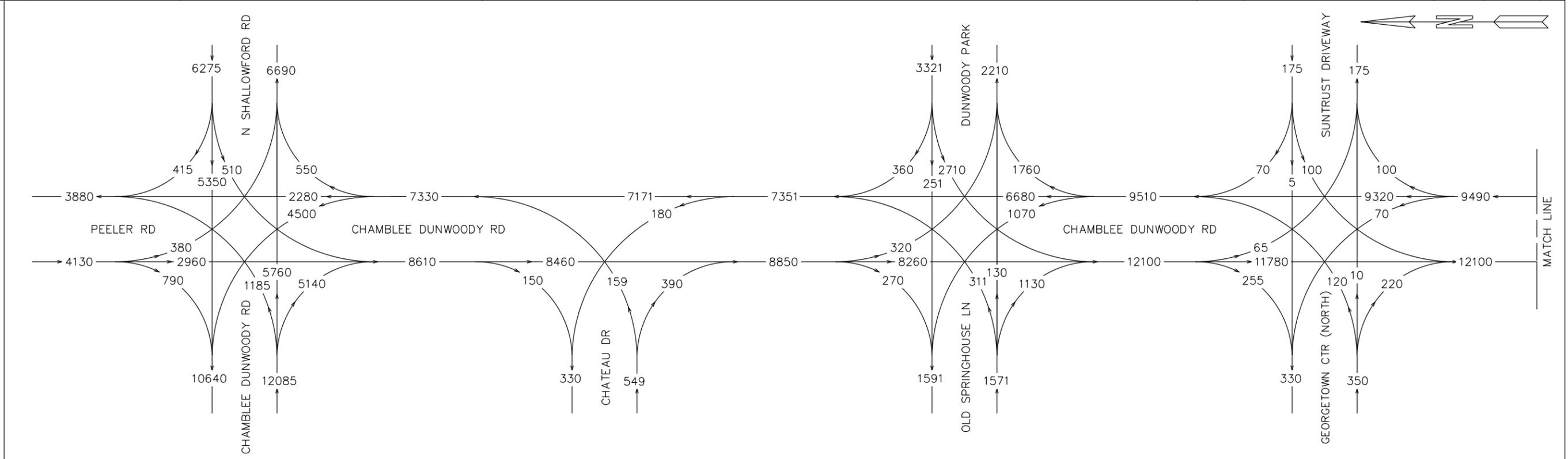
TO Albert Shelby, State Program Delivery Engineer
Attention: Elaine Armster

SUBJECT **Reviewed** Updated Design Traffic Report for CR 3329 & CR 5156 –
GEORGETOWN AREA MULTIMODAL IMPROVEMENTS.

We have reviewed the consultant's Updated Design Traffic for the above project. The Updated Design Traffic is approved.

If you have any questions concerning this information, please contact Rhonda Niles at (404) 631-1924.

CLV/RFN



400 NORTHPARK TOWN CENTER
1000 ABERNATHY ROAD, N.E., SUITE 900
ATLANTA, GEORGIA 30328
TEL: (678) 808-8800 FAX: (678) 808-8400

2014 ADT

24 HOUR T= 3.8%
SU= 3.5%
COMB= 0.3%

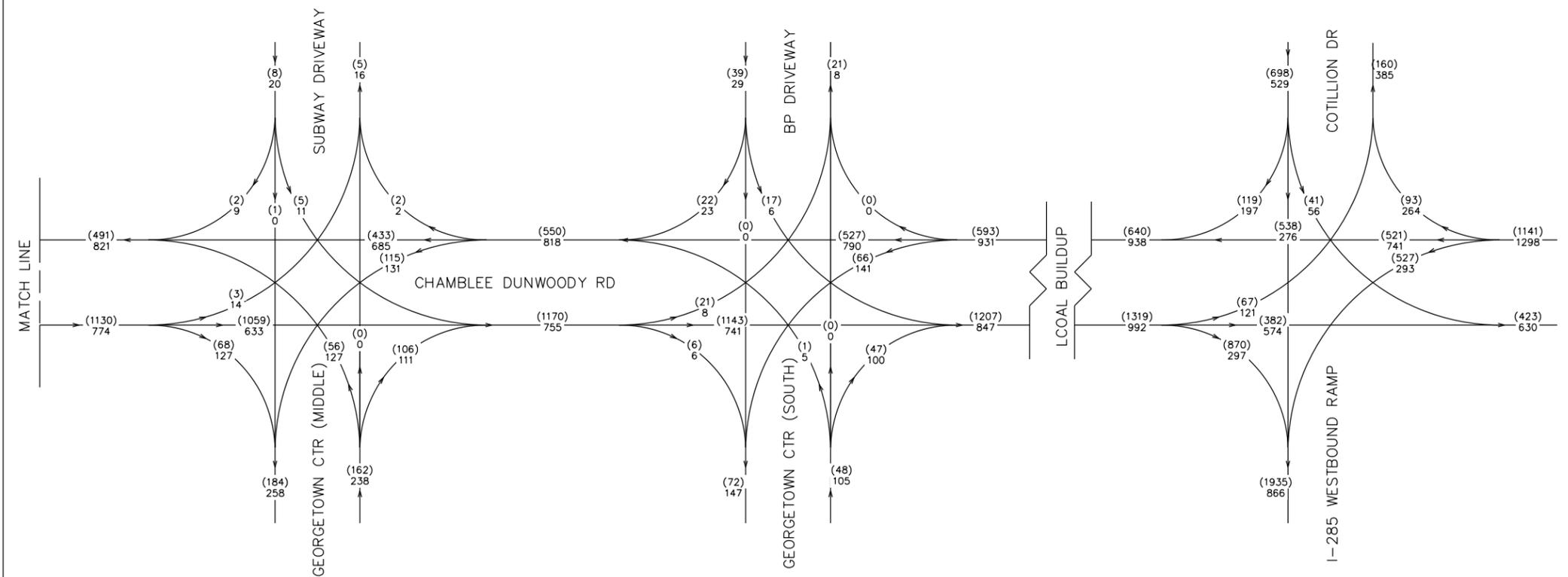
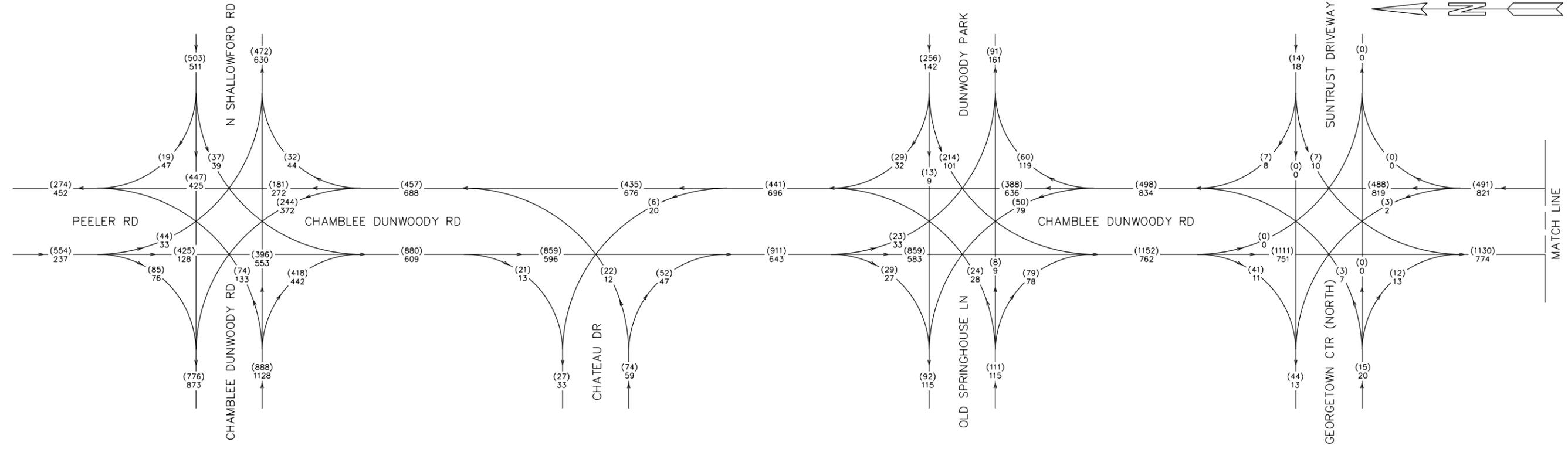
REVISION DATES

NO.	DATE	DESCRIPTION

STATE OF GEORGIA
DEPARTMENT OF TRANSPORTATION
OFFICE: PLANNING
PI# 0012875

CHAMBLEE DUNWOODY ROAD FROM
COTILLION DRIVE TO PEELER ROAD

DRAWING No.
10-001



AECOM
400 NORTHPARK TOWN CENTER
1000 ABERNATHY ROAD, N.E., SUITE 900
ATLANTA, GEORGIA 30328
TEL: (678) 808-8800 FAX: (678) 808-8400

2014 DHV
(AM)
PM

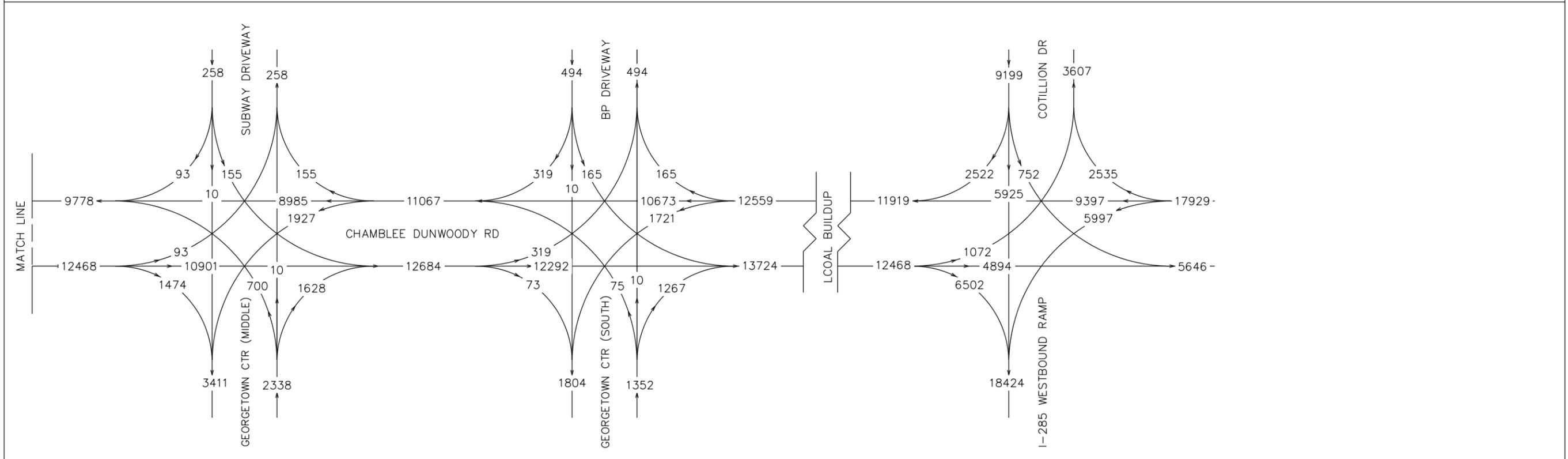
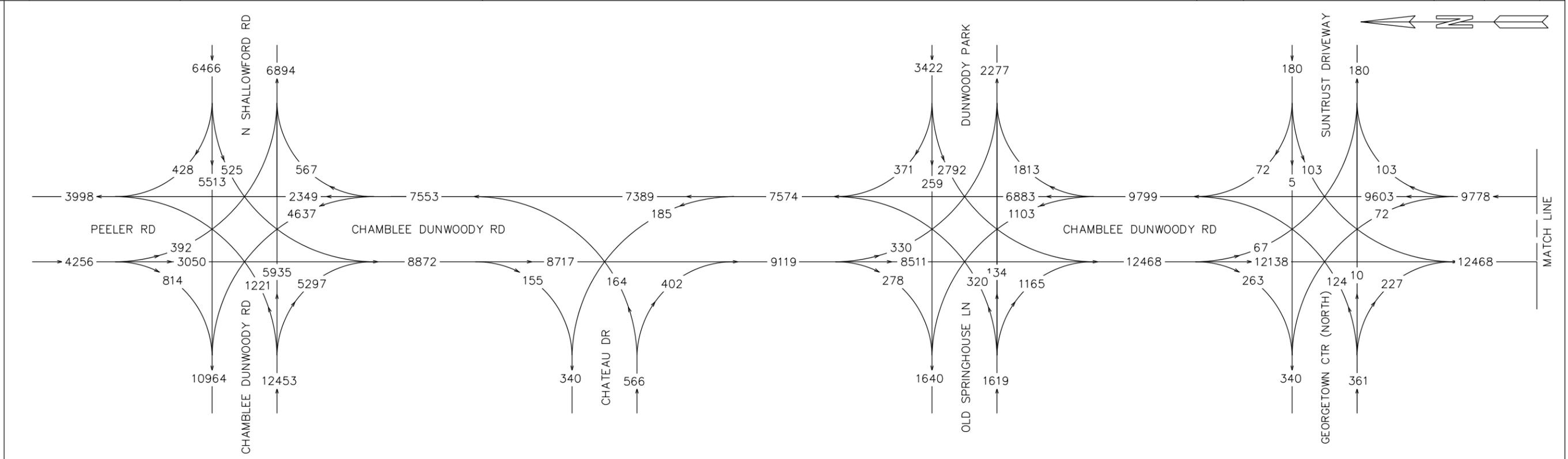
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SU= 2.7%
COMB= 0.3%

REVISION DATES	

STATE OF GEORGIA
DEPARTMENT OF TRANSPORTATION
OFFICE: PLANNING
PI# 0012875

CHAMBLEE DUNWOODY ROAD FROM
COTILLION DRIVE TO PEELER ROAD

DRAWING No.
10-002



400 NORTHPARK TOWN CENTER
1000 ABERNATHY ROAD, N.E., SUITE 900
ATLANTA, GEORGIA 30328
TEL: (678) 808-8800 FAX: (678) 808-8400

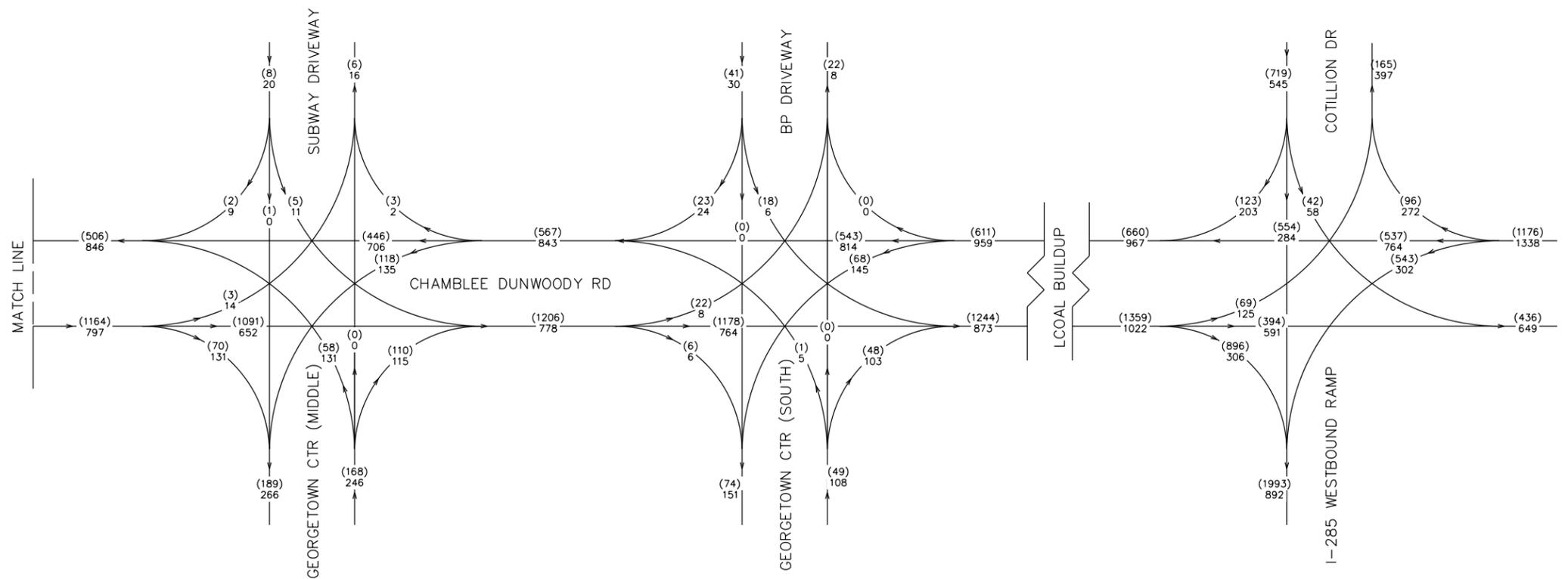
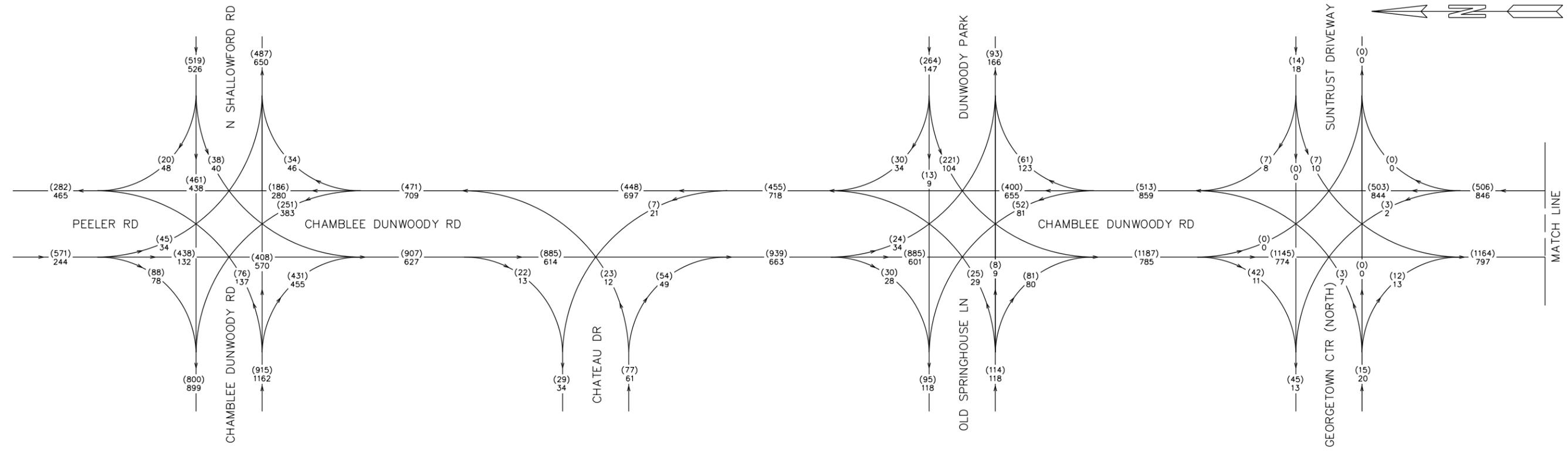
2020 ADT
NO BUILD = BUILD

24 HOUR T= 3.8%
SU= 3.5%
COMB= 0.3%

REVISION DATES	

STATE OF GEORGIA
DEPARTMENT OF TRANSPORTATION
OFFICE: PLANNING
PI# 0012875
CHAMBLEE DUNWOODY ROAD FROM
COTILLION DRIVE TO PEELER ROAD

DRAWING No.
10-003



AECOM
400 NORTHPARK TOWN CENTER
1000 ABERNATHY ROAD, N.E., SUITE 900
ATLANTA, GEORGIA 30328
TEL: (678) 808-8800 FAX: (678) 808-8400

2020 DHV
(AM)
PM
NO BUILD = BUILD

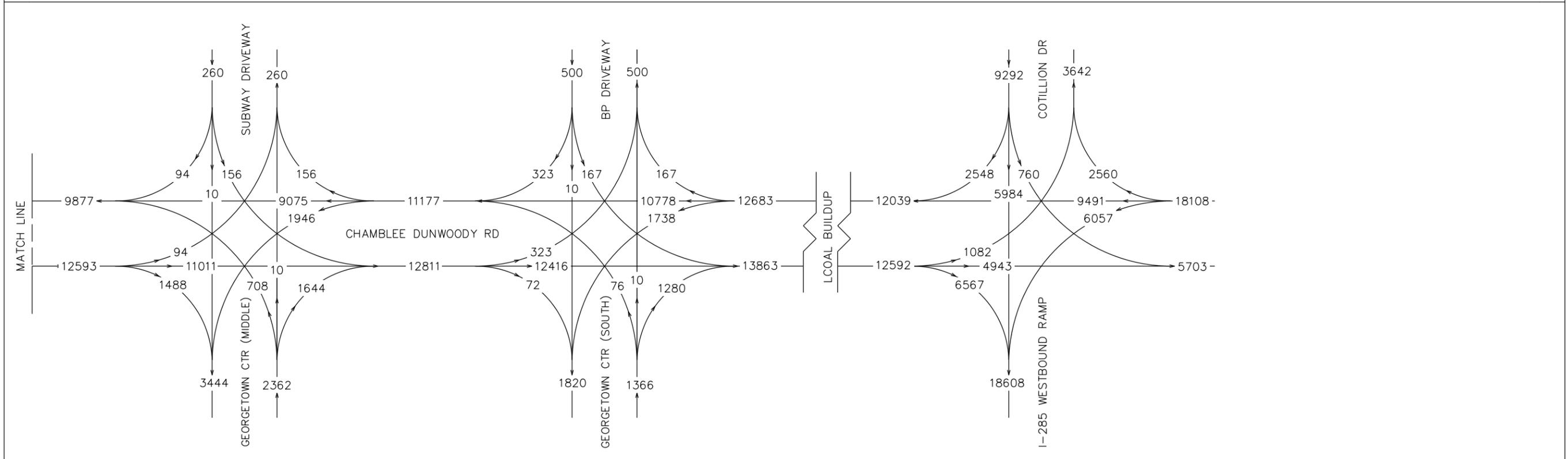
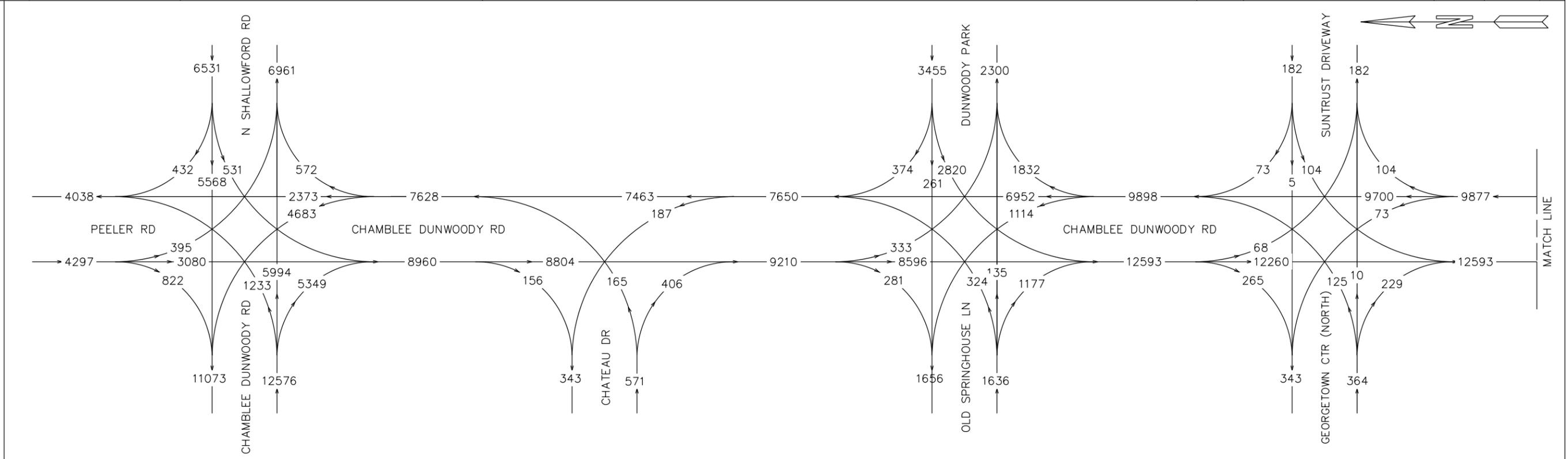
T = 3.0%
SU = 2.7%
COMB = 0.3%

REVISION DATES	

STATE OF GEORGIA
DEPARTMENT OF TRANSPORTATION
OFFICE: PLANNING
PI# 0012875

CHAMBLEE DUNWOODY ROAD FROM
COTILLION DRIVE TO PEELER ROAD

DRAWING No.
10-004



400 NORTHPARK TOWN CENTER
1000 ABERNATHY ROAD, N.E., SUITE 900
ATLANTA, GEORGIA 30328
TEL: (678) 808-8800 FAX: (678) 808-8400

2022 ADT
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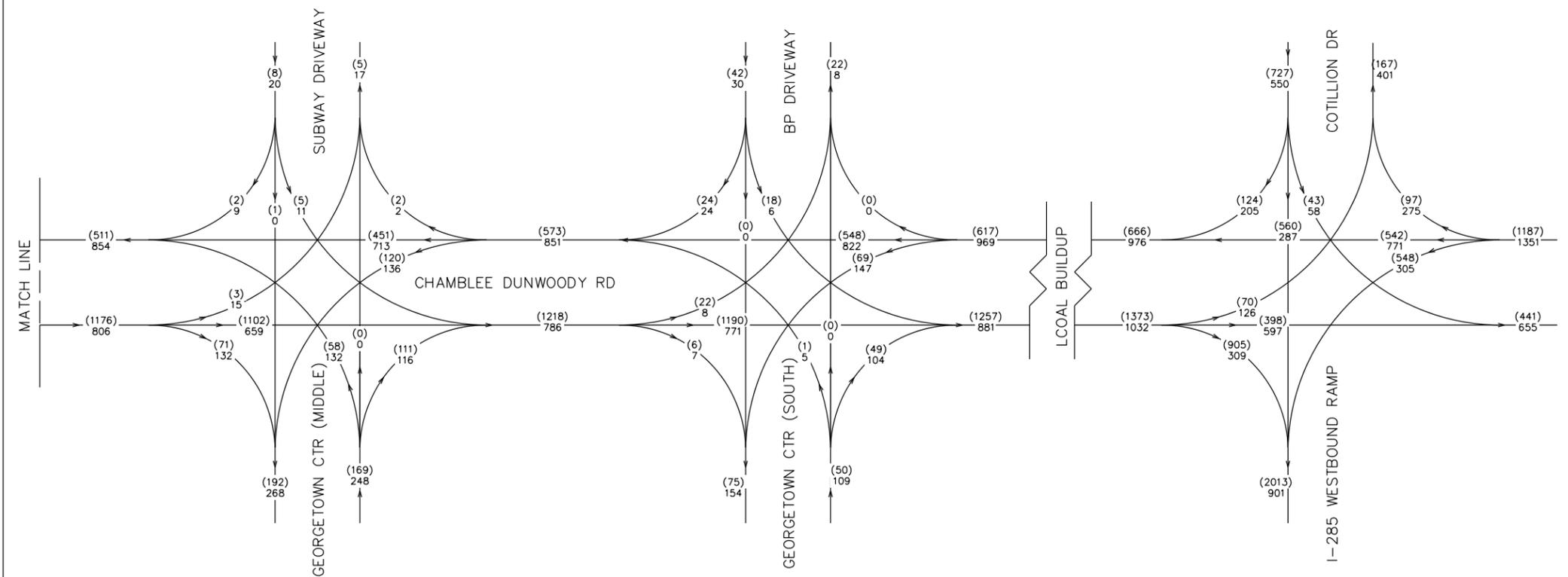
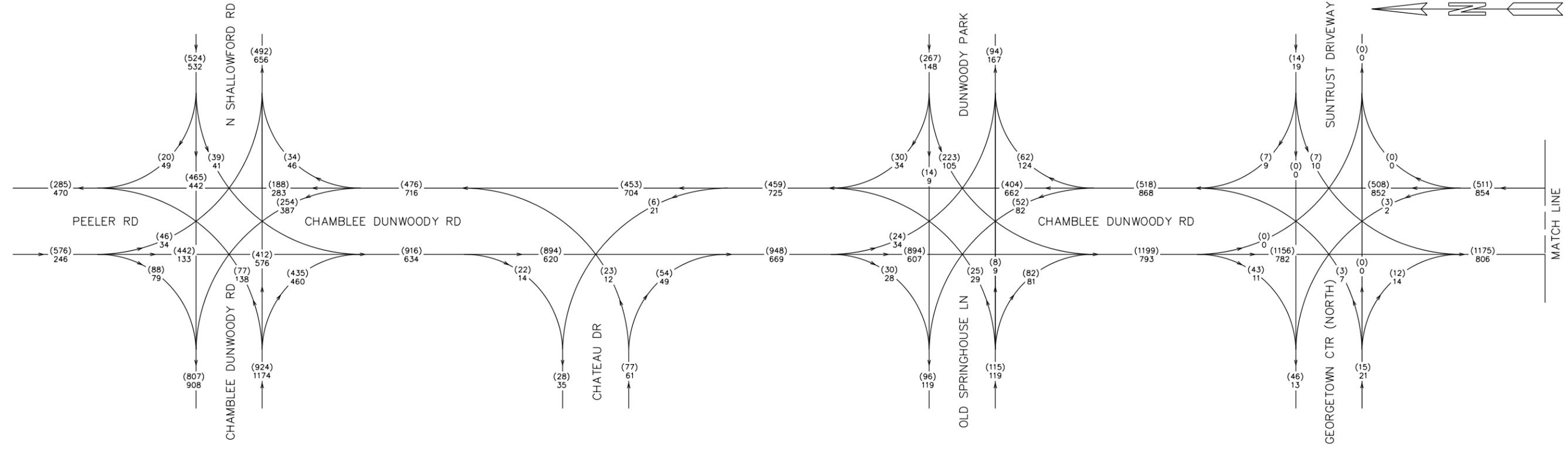
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SU= 3.5%
COMB= 0.3%

REVISION DATES

STATE OF GEORGIA
DEPARTMENT OF TRANSPORTATION
OFFICE: PLANNING
PI# 0012875

CHAMBLEE DUNWOODY ROAD FROM
COTILLION DRIVE TO PEELER ROAD

DRAWING No.
10-005



AECOM
400 NORTHPARK TOWN CENTER
1000 ABERNATHY ROAD, N.E., SUITE 900
ATLANTA, GEORGIA 30328
TEL: (678) 808-8800 FAX: (678) 808-8400

2022 DHV
(AM)
PM
NO BUILD = BUILD

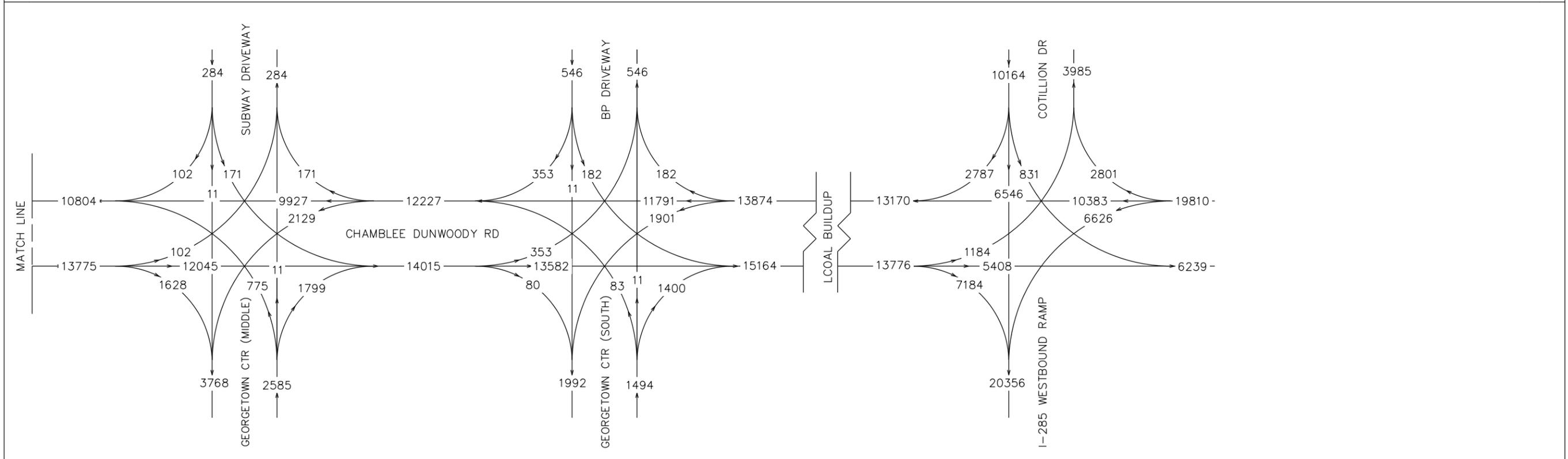
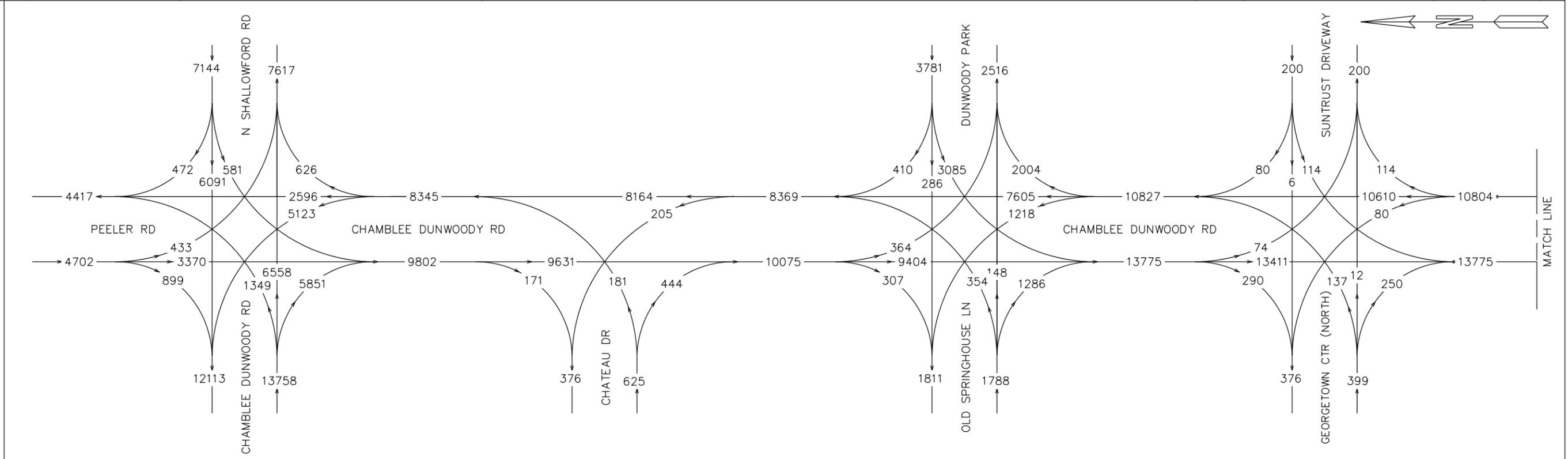
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SU= 2.7%
COMB= 0.3%

REVISION DATES	

STATE OF GEORGIA
DEPARTMENT OF TRANSPORTATION
OFFICE: PLANNING
PI# 0012875

CHAMBLEE DUNWOODY ROAD FROM
COTILLION DRIVE TO PEELER ROAD

DRAWING No.
10-006



AECOM
400 NORTHPARK TOWN CENTER
1000 ABERNATHY ROAD, N.E., SUITE 900
ATLANTA, GEORGIA 30328
TEL: (678) 808-8800 FAX: (678) 808-8400

2040 ADT
NO BUILD = BUILD

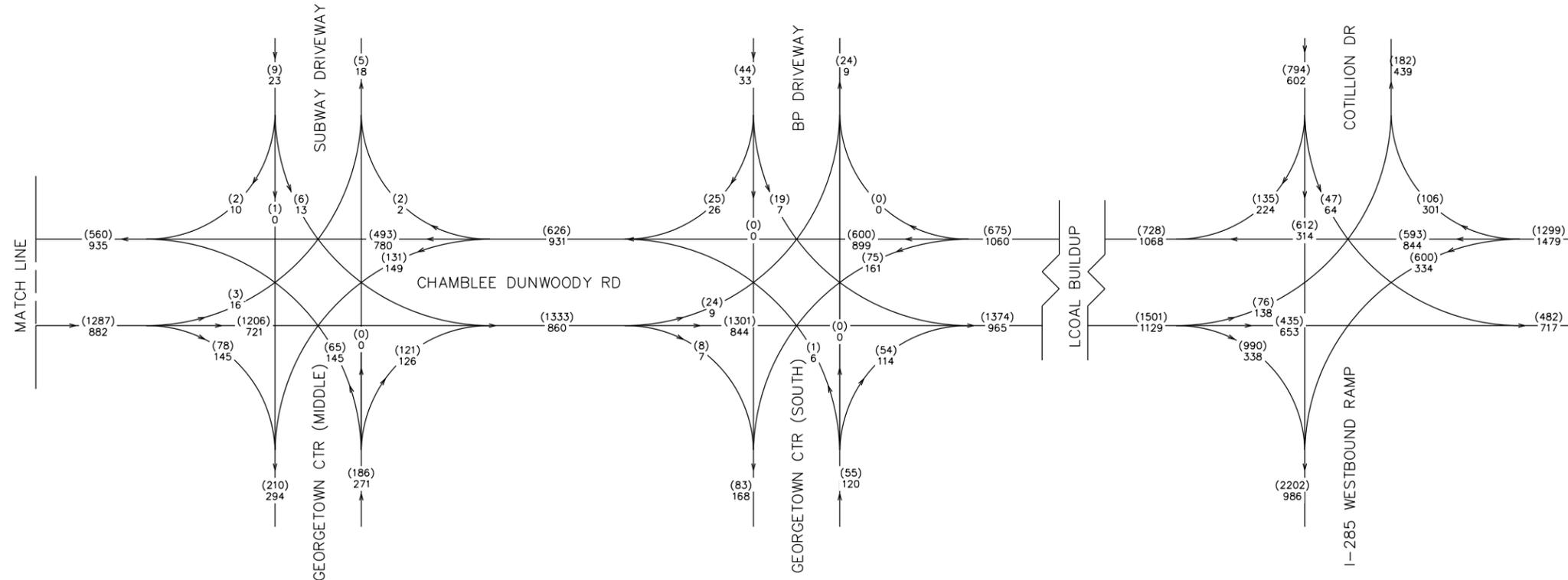
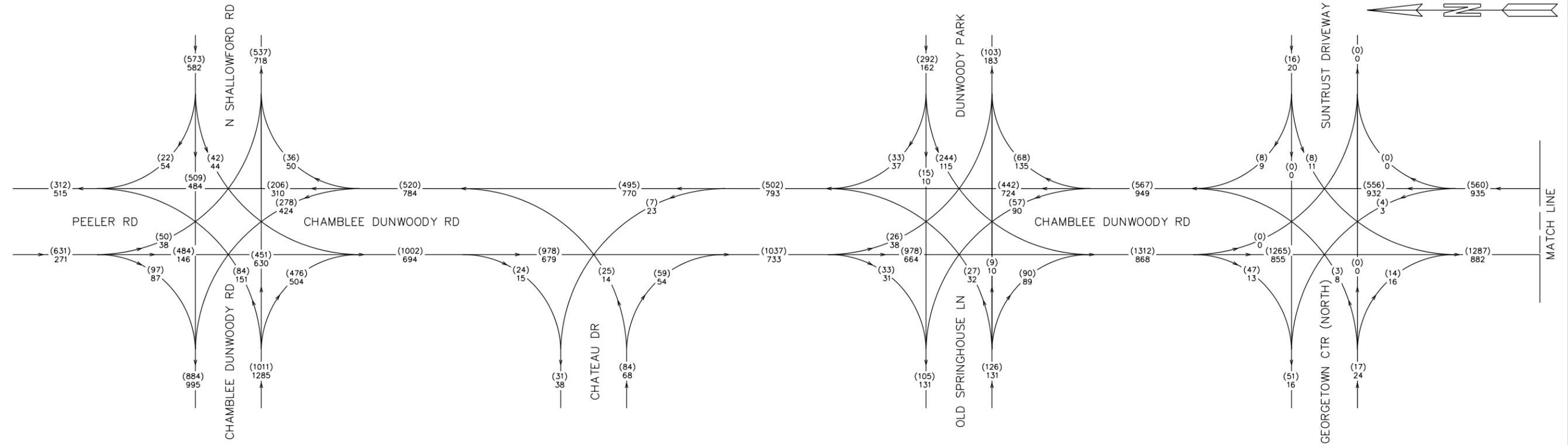
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SU= 3.5%
COMB= 0.3%

REVISION DATES	

STATE OF GEORGIA
DEPARTMENT OF TRANSPORTATION
OFFICE: PLANNING
PI# 0012875

CHAMBLEE DUNWOODY ROAD FROM
COTILLION DRIVE TO PEELER ROAD

DRAWING No.
10-007



400 NORTHPARK TOWN CENTER
1000 ABERNATHY ROAD, N.E., SUITE 900
ATLANTA, GEORGIA 30328
TEL: (678) 808-8800 FAX: (678) 808-8400

2040 DHV
(AM)
PM
NO BUILD = BUILD

T = 3.0%
SU = 2.7%
COMB = 0.3%

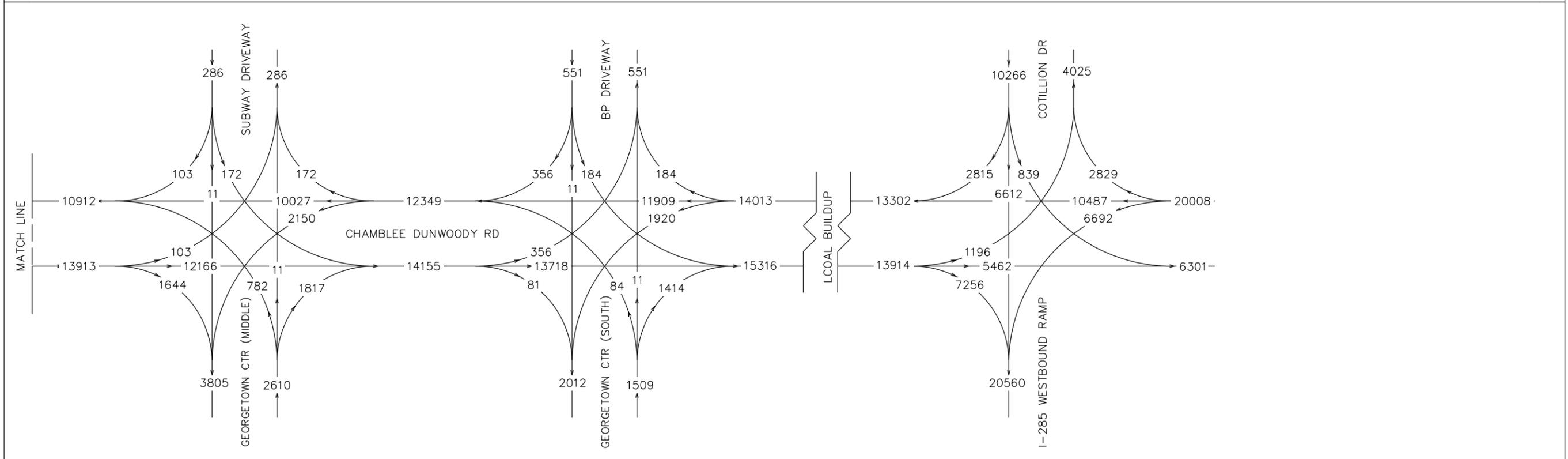
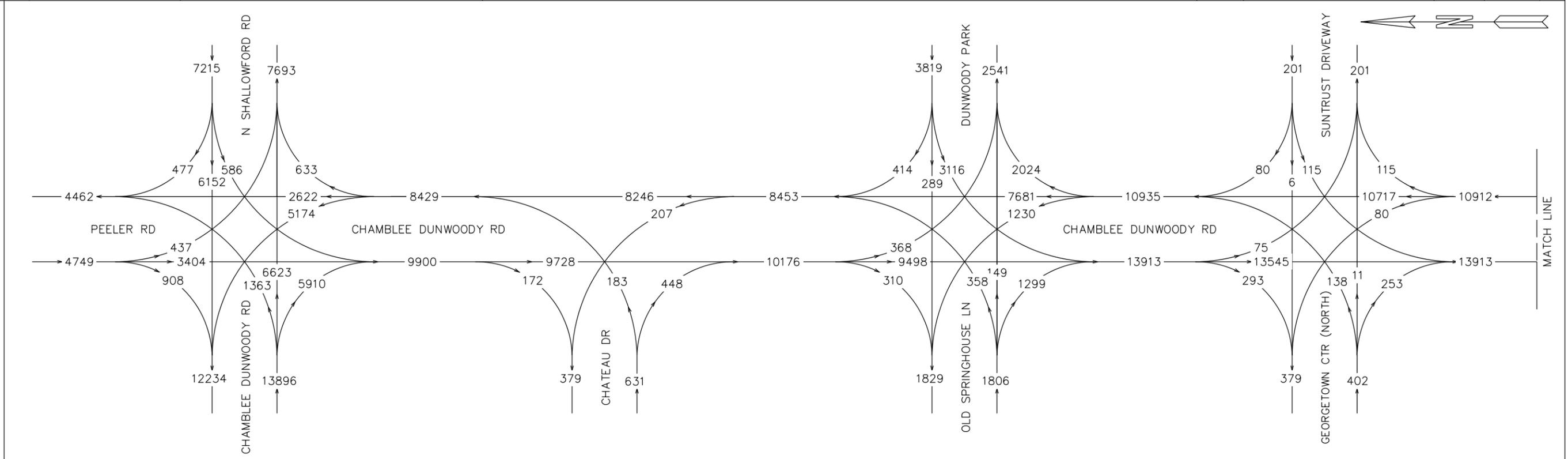
REVISION DATES

STATE OF GEORGIA
DEPARTMENT OF TRANSPORTATION

OFFICE: PLANNING
PI# 0012875

CHAMBLEE DUNWOODY ROAD FROM
COTILLION DRIVE TO PEELER ROAD

DRAWING No.
10-008



400 NORTHPARK TOWN CENTER
1000 ABERNATHY ROAD, N.E., SUITE 900
ATLANTA, GEORGIA 30328
TEL: (678) 808-8800 FAX: (678) 808-8400

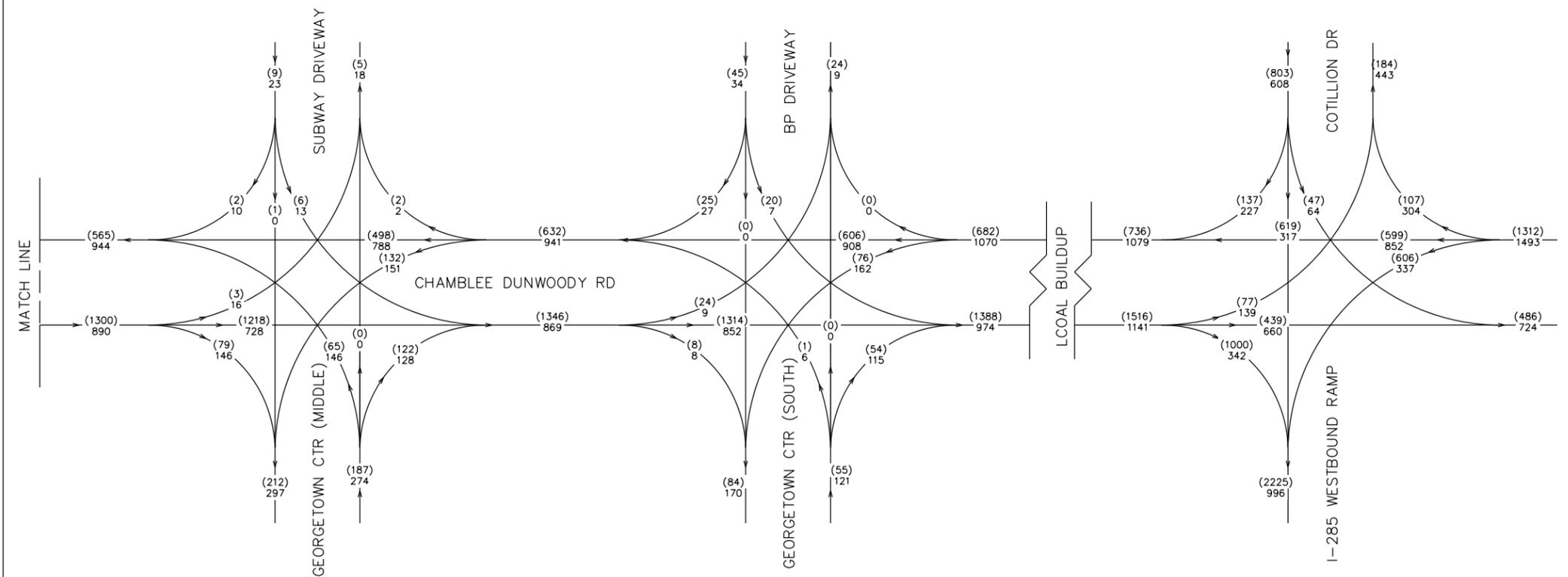
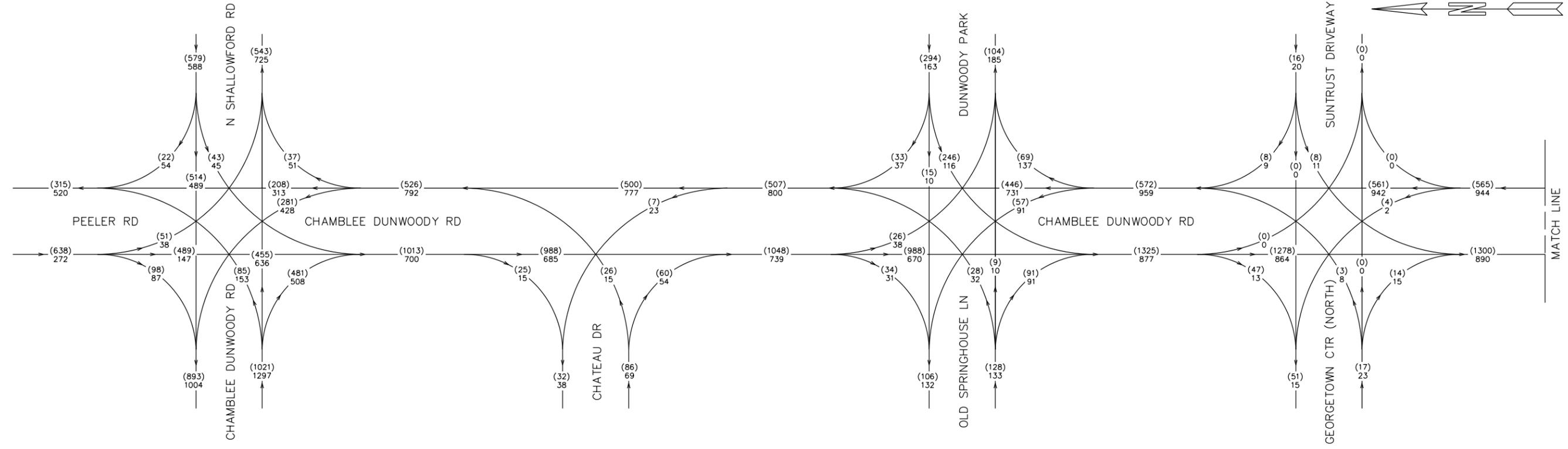
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NO BUILD = BUILD

24 HOUR T= 3.8%
SU= 3.5%
COMB= 0.3%

REVISION DATES	

STATE OF GEORGIA
DEPARTMENT OF TRANSPORTATION
OFFICE: PLANNING
PI# 0012875
CHAMBLEE DUNWOODY ROAD FROM
COTILLION DRIVE TO PEELER ROAD

DRAWING No.
10-009



AECOM
 400 NORTHPARK TOWN CENTER
 1000 ABERNATHY ROAD, N.E., SUITE 900
 ATLANTA, GEORGIA 30328
 TEL: (678) 808-8800 FAX: (678) 808-8400

2020 DHV
 (AM)
 PM
 NO BUILD = BUILD

T = 3.0%
 SU = 2.7%
 COMB = 0.3%

REVISION DATES	

STATE OF GEORGIA
 DEPARTMENT OF TRANSPORTATION
 OFFICE: PLANNING
 PI# 0012875

CHAMBLEE DUNWOODY ROAD FROM
 COTILLION DRIVE TO PEELER ROAD

DRAWING No.
 10-010

ATTACHMENT #5



Sprinkle CONSULTING

Planners + Engineers

To: Sean H. Pharr, P.E.
From: Theo Petritsch, P.E., PTOE
Date: July 6, 2015
Subject: Chamblee Dunwoody Road - Crash Information

Sprinkle Consulting has collected and summarized crash information for the Chamblee Dunwoody Road study corridor. Crash data for Chamblee Dunwoody Road, from to just south of I-285 Westbound Ramp-Cotillion Road to just north of Chamblee Dunwoody Road-North Shallowford Road (approximately 0.6 miles), was obtained from the Georgia Department of Transportation for the years 2011, 2012, and 2013.

Table 1 details the crash data along the Chamblee Dunwoody Road corridor. The crash information includes the four signalized intersections along Chamblee Dunwoody Road. For each of the intersections, the number of crashes, injuries, and fatalities are provided. Additionally, types of crashes are provided to show the number of angle, head on, rear end, sideswipe, and non-motor vehicle collisions that have occurred. The information is shown for the entire three-year period.

Table 2 and Figure 1 summarize the number of crashes, injuries, and fatalities for 2011-2013. Rates for crashes, injuries, and fatalities were based on one million vehicle miles traveled. Crash rates for 2011-2012 from the 2015 GOHS Highway Safety Plan were used. More current data was requested from the GOHS; however, they responded that 2012 was the most recent. For this reason, statewide crash rates for 2013 were assumed to be consistent with 2012.

The crash data for this segment indicates 220 crashes with 73 total injuries and zero fatalities. Eighteen additional crashes were reported; however, they were omitted from this study due to missing information. The crash data reveals that the majority of the crashes during the three-year period were angle and rear end collisions (43% and 42%, respectively). During the three-year period, the data indicates the average crash rate per 100 million vehicle miles was 1915, approximately six times higher than the statewide average over the same three year period. The average injury rate per 100 million vehicle miles traveled was 636, approximately six times higher than the statewide average. The average fatality rate per 100 million vehicle miles was zero. The difference in crash and injury rates, as compared to statewide averages, may be the result of the corridor being a wide and heavily traveled suburban corridor; a comparison to comparable corridors would be needed to determine whether this corridor exhibits unusually high crash statistics.

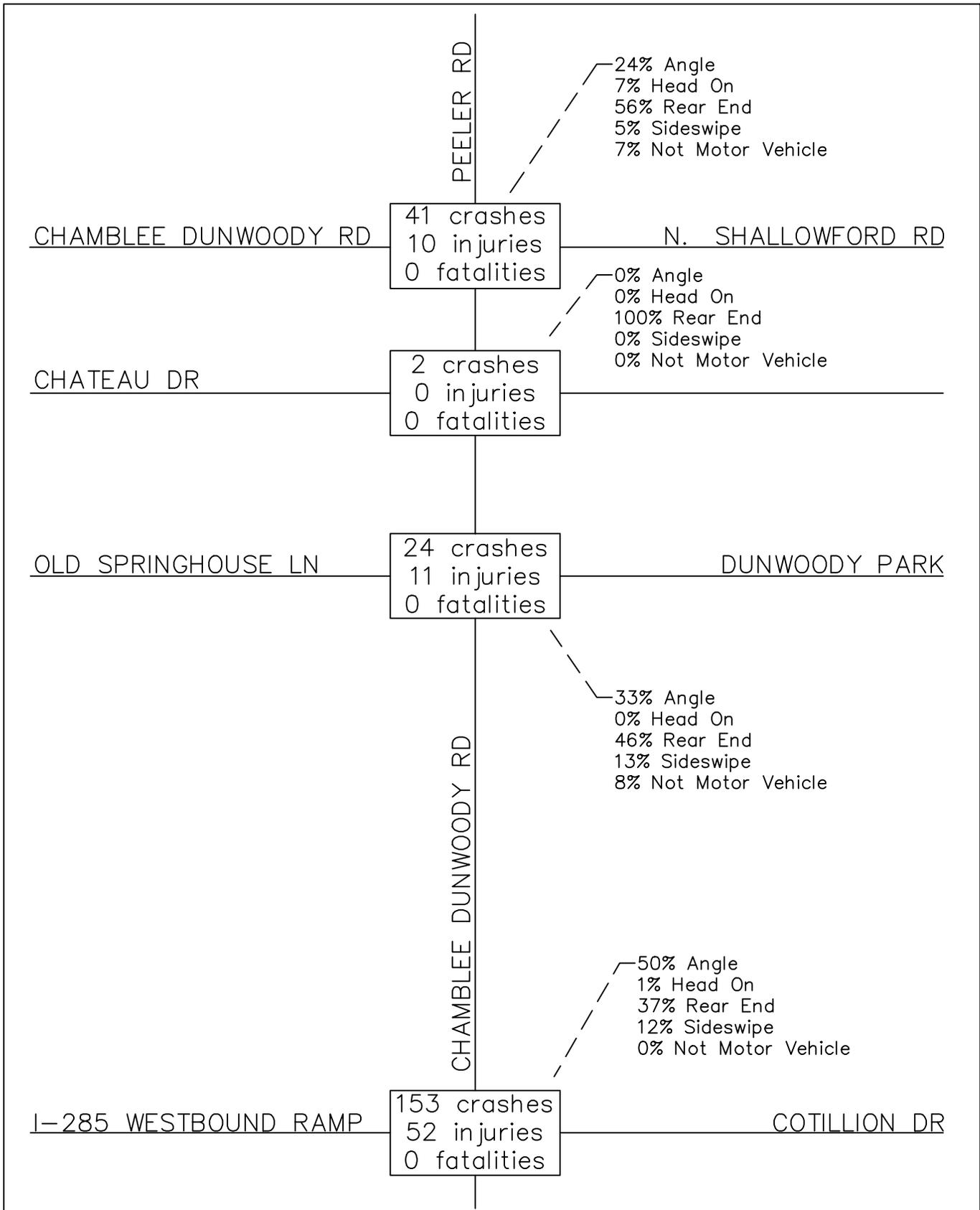
Table 1 Crash Summary by Intersection

Intersection	Year	# of crashes	Injuries	Fatalities	Angle	Head On	Rear End	Sideswipe	Not a Motor Vehicle Collision
285 Ramp/ Cotillion	2011	47	20	0	25	1	12	9	0
	2012	51	20	0	29	0	17	5	0
	2013	55	12	0	22	1	28	4	0
	TOTAL	153	52	0	76	2	57	18	0
Old Springhouse/ Dunwoody Park	2011	6	4	0	1	0	3	1	1
	2012	7	1	0	3	0	3	1	0
	2013	11	6	0	4	0	5	1	1
	TOTAL	24	11	0	8	0	11	3	2
Chateau	2011	1	0	0	0	0	1	0	0
	2012	0	0	0	0	0	0	0	0
	2013	1	0	0	0	0	1	0	0
	TOTAL	2	0	0	0	0	2	0	0
Chamblee Dunwoody/ Shallowford	2011	16	4	0	5	2	9	0	0
	2012	15	1	0	3	1	8	1	2
	2013	10	5	0	2	0	6	1	1
	TOTAL	41	10	0	10	3	23	2	3

Table 2 Crash History (2011-2013)

Year	Number of Crashes	Number of Injuries	Number of Fatalities	Crash Rate	Injury Rate	Fatality Rate	Statewide Average Crash Rate	Statewide Average Injury Rate	Statewide Average Fatality Rate
2011	70	28	0	1837	735	0	269	95	1.09
2012	73	22	0	1906	575	0	307	108	1.11
2013	77	23	0	2001	598	0	N/A	N/A	N/A

Crash and injury rates are per 100 million Vehicle Miles Traveled.



<p>Chamblee Dunwoody Road Dekalb County Road Improvement Project</p>	<p>Crash Exhibit Years 2011–2013</p>	<p>Figure 1</p>
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Table 1 2014 Analysis

2014					
Intersection	Movement	AM		PM	
		LOS	Total Delay	LOS	Total Delay
I-285 WB Ramp/Cotillion Dr. & Chamblee Dunwoody Road	WB L	C	25.4	D	41.1
	WB T	E	59.3	E	63.4
	WB R	A	6.5	A	8.4
	NB L	A	1.7	A	1
	NB T	A	4.4	B	12.2
	SB L	A	8.2	D	37.9
	SB T	A	6.0	A	6.1
	SB R	C	33.0	A	4
Overall		C		B	
Intersection	Movement	LOS	Total Delay	LOS	Total Delay
Chamblee Dunwoody Road & Kroger Ctr	EB L	E	57.7	E	60.6
	EB T	A	6.3	A	1.1
	WB T	D	39.9	A	0.6
	NB L	C	23.4	A	1.8
	NB T	A	4.7	A	2.8
	NB R	A	0.0	A	0
	SB L	A	2.0	A	4.5
	SB T	B	11.0	A	7.4
	SB R	A	0.1	A	0.8
Overall		B		A	
Intersection	Movement	LOS	Total Delay	LOS	Total Delay
Chamblee Dunwoody Road & Old Springhouse Lane	EB L	D	51.9	D	39
	EB T	C	21.6	C	21.9
	WB L	F	119.1	D	50.3
	WB T	C	22.4	C	21.2
	NB L	B	17.7	A	5
	NB T	B	11.9	A	9.3
	NB R	A	0.7	A	0.8
	SB L	A	5.6	A	3.6
	SB T	B	15.0	A	9.6
Overall		C		B	
Intersection	Movement	LOS	Total Delay	LOS	Total Delay
Chamblee Dunwoody Road & Peeler Road	EB L	C	28.7	B	16.7
	EB T	D	44.8	C	28.1
	EB R	A	0.0	A	0.1
	WB L	D	45.2	B	19.9
	WB T	D	44.7	C	34.7
	WB R	A	7.4	A	6.9
	NB L	D	51.5	E	65.8
	NB T	B	15.7	C	25.5
	NB R	A	0.6	A	1.4
	SWB L	B	15.8	C	25
	SWB T	E	67.6	E	63.8
Overall		D		C	

Table 2 2020 Analysis

2020					
Intersection	Movement	AM		PM	
		LOS	Total Delay	LOS	Total Delay
I-285 WB Ramp/Cotillion Dr. & Chamblee Dunwoody Road	WB L	C	25.5	D	40.1
	WB T	E	64.4	E	76.5
	WB R	A	6.9	A	8.9
	NB L	A	1.7	A	1.1
	NB T	A	3.6	B	14.0
	SB L	A	7.1	D	42.0
	SB T	A	5.1	A	5.1
	SB R	D	42.7	A	4.0
Overall		C		B	
Intersection	Movement	LOS	Total Delay	LOS	Total Delay
Chamblee Dunwoody Road & Kroger Ctr	EB L	E	57.8	E	62.1
	EB T	A	7.4	A	1.1
	WB T	D	38.9	A	0.6
	NB L	D	35.6	A	3.2
	NB T	A	5.5	A	5.0
	NB R	A	0.3	A	0.0
	SB L	A	4.0	A	4.6
	SB T	B	13.9	A	8.3
	SB R	A	0.5	A	0.9
Overall		B		A	
Intersection	Movement	LOS	Total Delay	LOS	Total Delay
Chamblee Dunwoody Road & Old Springhouse Lane	EB L	D	52.0	D	39.5
	EB T	C	21.6	C	21.9
	WB L	F	137.6	D	52.8
	WB T	C	22.3	C	21.2
	NB L	C	21.9	A	4.6
	NB T	B	11.9	A	9.7
	NB R	A	1.0	A	0.8
	SB L	A	6.0	A	3.3
	SB T	B	16.3	A	9.2
Overall		C		B	
Intersection	Movement	LOS	Total Delay	LOS	Total Delay
Chamblee Dunwoody Road & Peeler Road	EB L	C	29.2	B	18.6
	EB T	D	47.3	C	29.1
	EB R	A	0.1	A	0.2
	WB L	D	48.1	B	22.4
	WB T	D	46.9	D	38.6
	WB R	A	8.6	A	7.9
	NB L	D	54.1	E	63.6
	NB T	B	15.3	B	23.0
	NB R	A	0.6	A	1.2
	SWB L	B	15.8	C	24.6
	SWB T	E	72.8	E	65.5
Overall		D		C	

Table 3 2022 Analysis

2022					
Intersection	Movement	AM		PM	
		LOS	Total Delay	LOS	Total Delay
I-285 WB Ramp/Cotillion Dr. & Chamblee Dunwoody Road	WB L	C	24.8	D	39.1
	WB T	E	60.6	E	69.4
	WB R	A	6.6	A	8.5
	NB L	A	1.9	A	1.1
	NB T	A	4.3	B	15.9
	SB L	A	6.0	E	64.7
	SB T	A	4.1	A	6.4
	SB R	D	50.3	A	5.1
Overall		C		B	
Intersection	Movement	LOS	Total Delay	LOS	Total Delay
Chamblee Dunwoody Road & Kroger Ctr	EB L	E	57.8	E	62.3
	EB T	A	7.8	A	1.2
	WB T	D	38.9	A	0.6
	NB L	C	29.0	A	3.2
	NB T	A	4.5	A	5.3
	NB R	A	0.0	A	0.0
	SB L	A	5.7	A	4.0
	SB T	C	20.5	A	8.5
	SB R	A	1.3	A	1.0
Overall		B		A	
Intersection	Movement	LOS	Total Delay	LOS	Total Delay
Chamblee Dunwoody Road & Old Springhouse Lane	EB L	D	52.0	D	39.4
	EB T	C	21.6	C	21.9
	WB L	F	139.2	D	53.2
	WB T	C	22.5	C	21.1
	NB L	C	26.1	A	4.0
	NB T	B	13.5	A	8.0
	NB R	A	1.2	A	0.7
	SB L	A	5.8	A	3.6
	SB T	B	16.4	A	9.4
Overall		C		B	
Intersection	Movement	LOS	Total Delay	LOS	Total Delay
Chamblee Dunwoody Road & Peeler Road	EB L	C	29.8	B	19.7
	EB T	D	49.1	C	30.3
	EB R	A	0.1	A	0.2
	WB L	D	49.0	C	22.8
	WB T	D	48.7	D	44.4
	WB R	A	9.2	A	8.4
	NB L	D	52.4	E	67.3
	NB T	C	20.4	C	24.3
	NB R	A	0.9	A	1.5
	SWB L	B	15.6	C	24.6
	SWB T	E	71.6	E	66.3
Overall		D		D	

Table 4 2040 Analysis

2040					
Intersection	Movement	AM		PM	
		LOS	Total Delay	LOS	Total Delay
I-285 WB Ramp/Cotillion Dr. & Chamblee Dunwoody Road	WB L	C	29.9	D	50.2
	WB T	F	93.0	F	142.5
	WB R	B	11.8	B	11.6
	NB L	A	4.0	A	1.2
	NB T	A	9.1	B	19.3
	SB L	A	7.6	F	125.3
	SB T	A	4.7	A	4.9
	SB R	E	70.0	A	4.7
Overall		D		C	
Intersection	Movement	LOS	Total Delay	LOS	Total Delay
Chamblee Dunwoody Road & Kroger Ctr	EB L	E	71.6	E	73.7
	EB T	B	11.1	A	1.4
	WB T	D	46.8	A	0.7
	NB L	F	89.9	A	3.8
	NB T	A	4.9	A	6.4
	NB R	A	0.0	A	0.0
	SB L	A	5.0	A	4.9
	SB T	D	40.8	A	9.4
	SB R	A	1.4	A	1.1
Overall		C		B	
Intersection	Movement	LOS	Total Delay	LOS	Total Delay
Chamblee Dunwoody Road & Old Springhouse Lane	EB L	E	61.5	D	47.8
	EB T	C	25.2	C	25.7
	WB L	F	152.5	E	62.3
	WB T	C	24.0	C	22.3
	NB L	C	28.7	A	5.7
	NB T	B	12.2	B	11.9
	NB R	A	0.4	A	0.9
	SB L	A	4.7	A	4.1
	SB T	B	19.2	B	10.5
Overall		C		B	
Intersection	Movement	LOS	Total Delay	LOS	Total Delay
Chamblee Dunwoody Road & Peeler Road	EB L	C	34.8	B	18.4
	EB T	E	61.0	D	36.0
	EB R	A	0.1	A	0.2
	WB L	E	77.6	C	23.1
	WB T	E	61.2	E	55.7
	WB R	B	15.5	B	13.3
	NB L	F	105.5	F	86.2
	NB T	C	27.6	C	29.0
	NB R	A	1.5	A	3.0
	SWB L	B	18.1	C	29.8
	SWB T	F	85.6	F	108.3
Overall		E		D	

Table 5 2042 Intersection Analysis

2042					
Intersection	Movement	AM		PM	
		LOS	Total Delay	LOS	Total Delay
I-285 WB Ramp/Cotillion Dr. & Chamblee Dunwoody Road	WB L	C	27.4	D	43.8
	WB T	F	90.7	F	160.2
	WB R	A	9.8	B	12.8
	NB L	A	2.3	A	1.1
	NB T	A	5.7	C	20.7
	SB L	B	10.6	F	167.9
	SB T	A	6.9	A	5.2
	SB R	E	79.1	A	5.4
Overall		D		C	
Intersection	Movement	LOS	Total Delay	LOS	Total Delay
Chamblee Dunwoody Road & Kroger Ctr	EB L	E	65.0	E	64.1
	EB T	B	13.1	A	1.4
	WB T	D	42.8	A	0.6
	NB L	F	94.9	A	4.4
	NB T	A	2.1	A	7.3
	NB R	A	0.0	A	0.0
	SB L	A	6.0	A	5.2
	SB T	D	53.5	B	10.2
	SB R	A	2.0	A	1.3
Overall		D		B	
Intersection	Movement	LOS	Total Delay	LOS	Total Delay
Chamblee Dunwoody Road & Old Springhouse Lane	EB L	D	44.7	D	53.0
	EB T	C	23.7	C	21.7
	WB L	F	86.7	F	87.2
	WB T	C	26.2	C	22.5
	NB L	C	22.9	A	5.3
	NB T	A	6.9	B	10.1
	NB R	A	0.6	A	0.3
	SB L	A	4.8	A	3.3
	SB T	B	14.8	A	10.0
Overall		B		B	
Intersection	Movement	LOS	Total Delay	LOS	Total Delay
Chamblee Dunwoody Road & Peeler Road	EB L	C	34.8	C	29.6
	EB T	E	69.3	D	35.7
	EB R	A	0.1	A	0.2
	WB L	E	66.5	E	56.9
	WB T	E	68.9	E	67.2
	WB R	B	15.5	B	11.4
	NB L	F	97.7	F	97.5
	NB T	B	16.9	C	22.9
	NB R	A	0.6	A	0.3
	SWB L	B	16.6	C	24.3
	SWB T	F	87.8	E	74.7
Overall		E		D	

ATTACHMENT #6

DEPARTMENT OF TRANSPORTATION STATE OF GEORGIA

SUMMARY OF MEETING MINUTES

MEETING DATE: April 30, 2015, 10:00AM at GDOT District 7 Office

PARTICIPANTS: See attached sign in sheet for additional detail:
Elaine Armster, GDOT
Michael Murdoch, GDOT
Sharon Witherspoon, GDOT
Wade Woodard, GDOT
Michael Smith, City of Dunwoody
Mindy Sanders, City of Dunwoody
Sean Pharr, AECOM
Patrick Smith, AECOM
Jonathan DiGioia, AECOM

DISCUSSION: **Concept Team Meeting for Georgetown Area Multi-Modal Improvements (Chamblee Dunwoody Rd and Cotillion Dr), PI# 0012875, Dunwoody, GA**

A Concept Team Meeting with the above listed participants was held on April 30, 2015 in conference room 144/145, GDOT District 7 Office, 5025 New Peachtree Rd, Chamblee, GA 30341. The meeting was held to discuss the draft concept report and to coordinate expectations and responsibilities for the project going forward.

Elaine Armster called the meeting to order at 10:15AM and asked all the participants to introduce themselves to the group.

Sean Pharr then gave a summary of the project's background. A brief summary is as follows:

The need for the Georgetown Area Multi-Modal Improvements project was established in both the Georgetown Area Master Plan (2011) and the City of Dunwoody Comprehensive Transportation Plan (2011) to provide a multi-modal corridor along Chamblee Dunwoody Rd between Cotillion Dr. and Peeler Rd, as well as on Cotillion Dr between Chamblee Dunwoody Rd and North Shallowford Rd. URS/AECOM has been hired by the City of Dunwoody for PE and concept work for both of these segments through funding from Atlanta Regional Commission's Livable Centers Initiative program. To date, URS/AECOM has completed the Scoping Report for ARC, obtained survey for the roadway corridor, completed a Draft Concept Report for GDOT, conducted stakeholder meeting, and one public information open house (PIOH) specific to this project to gather public input and establish what "multi-modal" means to the community.

Alternatives considered so far by the design team have included in-road bike lanes(traditional and buffered), travel lane consolidation(reduction), multi-use trails

weighed into consideration with dedicated bike facilities (one and two way cycle tracks), and combinations of sidewalks and multi-use trails. After receiving input from the public, the team gravitated toward a combination of multi-use trails and wide, buffered sidewalks to accommodate non-motorized users along the corridor. The design team has also laid out an alternative which “cleans up” lane allocations on the roadway to improve predictability and consistency for motor vehicle drivers. To implement any of the build alternatives included in the draft concept report, some localized right-of-way acquisition will be necessary in order to fit trails and sidewalks within the right-of-way.

This GDOT PI number (0012875) currently includes multi-modal improvements on both Chamblee Dunwoody Rd and Cotillion Dr, where the Cotillion Dr portion is “Phase 1,” and the Chamblee Dunwoody Rd portion is “Phase 2.” There is an opportunity for Phase 1 (Cotillion Dr) to be transitioned to completely local funding to expedite its implementation.

Patrick Smith continued summarizing the project’s background with its environmental aspects. His summary is as follows:

There are no streams or bodies of water in close proximity to the project. The Chateau Woods subdivision may qualify as a ranch house district. If so, it is not known presently what its boundaries would be. A categorical exclusion environmental document is anticipated for this project.

Michael Murdoch concurred that he anticipated a categorical exclusion, and a programmatic categorical exclusion could be warranted.

Sean Pharr asked whether Georgetown Park (a new park near the center of the Chamblee Dunwoody Rd study corridor) was on public or private land. **Michael Smith** responded that the city owned the park as well as land around it, but that the city planned to sell its land on the far side of the park from Chamblee Dunwoody Rd to private developers. Michael Smith said the city intended to keep the streets through the development behind the park public. Sean Pharr stated that he did not anticipate the need to acquire right-of-way from the park. Patrick Smith said that even if minor right-of-way acquisition from the park was necessary, it would likely qualify as *de minimis*.

Sean Pharr reported that the project team studied, under separate scope with the City of Dunwoody, the intersection of Chamblee Dunwoody Rd with Peeler Rd and North Shallowford Rd; the findings were traffic backups in the existing and no-build conditions. The team studied a potential roundabout application at that intersection and found that the public was receptive to the idea and that a roundabout could potentially handle traffic well.

Sean Pharr changed the discussion to the subject of utilities, pointing out that when the design team begins work on drainage design, it will need to coordinate with Georgia Power and other utilities (particularly relating to burying power lines).

Sharon Witherspoon suggested putting in a One Call ticket for the corridor to see what utilities are present. She said the report would include all utilities present besides water and sewer.

Michael Smith said that Dunwoody had a franchise agreement with Georgia Power such that Georgia Power had the responsibility for its own utility relocation costs in conjunction with roadway projects. However, the agreement did not cover the cost of burying power lines. Sharon Witherspoon suggested that that Dunwoody could approach Georgia Power about the franchise agreement and offer to pay the upgrade fee for burying power lines instead of relocating them in kind.

Sharon Witherspoon noted that there may be a need for manhole modifications, and she advised including that in the scope of work and in agreements with Georgia Power and other utility companies. She said GDOT needed these agreements prior to certification of the project. **Wade Woodard** advised to also be aware of the potential presence of fiber lines, which are becoming increasingly prevalent around Atlanta.

Elaine Armster brought up the subject of lighting. Sharon Witherspoon stated that lighting types (street lighting, pedestrian lighting, or both) and photometrics need to be included in the lighting agreement. **Michael Smith** stated that both street lighting and pedestrian lighting would be used. Michael Smith and Mindy Sanders both said they believed the existing street lighting could be maintained through the residential (northern) section of the corridor without the need for total replacement.

Elaine Armster distributed a memorandum from GDOT's Office of Design Policy and Support which provided specific comments on the draft concept report and began going through the list (see attached memorandum).

Elaine Armster paused from going through the list and brought up the fact that the project currently had a GDOT-let status; she had heard that it may become local-let. Mindy Sanders confirmed that the City of Dunwoody intended to change the status to local-let at a later time prior to project letting.

Wade Woodard made a comment about wall design and emphasized the importance of being aware of utilities near proposed wall footings. Elaine Armster suggested identifying proposed walls as structures before utility coordination.

Sharon Witherspoon asked for clarification whether sidewalks and/or trails were proposed to continue throughout the entire length of the project area. **Mindy Sanders** responded that they are. Sharon Witherspoon pointed out that existing utilities along the shoulder of the road would need to be raised to grade if they were below the proposed sidewalk and would need slip-resistant covers at grade and could not be located on wheelchair ramps.

Wade Woodard and Sharon Witherspoon warned about private utilities in the existing or proposed right-of-way and advised potentially including re-location funds for those cases as part of the project. They prefer to get manholes outside of the wheel-line and will not allow utility vaults to be relocated within GDOT r/w.

Elaine Armster resumed and finished going through the comment list from GDOT Design and Policy Support. With regard to the comment about Revive I-285, Sean Pharr remarked that its boundaries do not overlap the boundaries of this project.

Elaine Armster reminded the team that right-of-way costs need to be approved by GDOT.

Sharon Witherspoon advised the team to be aware of a large pole that was recently moved on the corner of Chamblee Dunwoody Rd and Peeler Rd.

Michael Murdoch pointed out that the Need and Purpose Statement in the draft concept report only stated bicycle and pedestrian needs goals, but the preferred alternative described operational improvements for motorized traffic as well; he suggested clarifying this and pointed out the need for level-of-service data in conjunction with motor vehicle operational improvements. Sean Pharr concurred.

Michael Murdoch asked for clarification on what lanes are existing and what lanes are proposed in the concept report alternatives description. Sean Pharr noted this would be included in the need and purpose.

Elaine Armster suggested that the team use the meeting minutes and the Design Policy and Support memorandum as a checklist for moving forward with the project in the near term. Sean replied that AECOM would aim to respond to the comments discussed in the meeting within 6 weeks. Elaine Armster called the meeting to a close.

The action items from the meeting are as follows:

Action Items:

- a. AECOM email draft meeting minutes
- b. AECOM responds to comments from meeting and memorandum within 6 weeks
- c. AECOM/City of Dunwoody to provide R/W costs to GDOT for review

ATTACHMENT #7



SUMMARY OF MEETING MINUTES

MEETING DATE: June 30, 2014
1:30PM at Dunwoody City Hall

PARTICIPANTS: Elaine Armster, GDOT
Mike Lobdell, GDOT
Patrick Bradshaw, ARC
Michael Smith, City of Dunwoody
Steve Foote, City of Dunwoody
Mindy Sanders, City of Dunwoody
Rebecca Keefer, City of Dunwoody
Kimberly Greer, City of Dunwoody
Sean Pharr, URS
Mickey O'Brien, URS
John Hightower, URS
(via conf. call)
Bruce Landis, Sprinkle

DISCUSSION: **Kickoff Meeting/Initial Concept Team Meeting for Georgetown Area Scoping Phase, GDOT PI 0012875, City of Dunwoody**

A kickoff meeting of the above listed participants was held on June 30, 2014 at the City of Dunwoody. The meeting was held to discuss project scope, roles and responsibilities, schedule, utilities, environment and general concerns. Sean Pharr opened the meeting providing a brief overview of the project;

Sean Pharr: The project area lies within the Georgetown Area Master Plan (LCI), and encompasses the following;

- Chamblee-Dunwoody Road from Cotillion Drive to the intersection of Peeler/Shallowford Road – the project segment goals includes enhancing multi-modal operation, including bike/ped and transit stop access. (See attached presentation for details).
- Cotillion Drive improvements – provide sidewalk and/or bike facilities along Cotillion Drive from Chamblee-Dunwoody Road to Shallowford Drive.
- Operational Study of the intersections of Peeler and Vermack along Chamblee-Dunwoody Road(concept and traffic study only).

Currently TAP and last mile connectivity and last mile connectivity funds are programmed for this project. The project is in the Scoping Phase. The PFA states that utilities, engineering, right of way, and construction are the responsibility of the local government.

URS will prepare a baseline schedule that will be submitted to GDOT for review. GDOT will provide an approved baseline schedule. Currently right of way authorization date is Fiscal Year 2016 and construction is 2018. The level of environmental documentation required is anticipated to be a Categorical Exclusion(CE).

URS Corporation
400 Northpark Town Center
1000 Abernathy Road, NE
Suite 900
Tel: 678.808.8800
Fax: 678.808.8400
www.urscorp.com

Michael Smith: Noted that the planning effort (Georgetown Area Master Plan) was adopted by ARC's LCI program. And the burial of utilities will be sought within the Chamblee-Dunwoody Corridor.

Mike Lobdell: Questioned if crash data was available, and noted GDOT receives a lot of call regarding traffic backing up on Cotillion during peak hours and around church hours on the weekend.

Elaine Armster: Relayed the current funding and question when the next call for projects would be through the ARC.

Patrick Bradshaw: Noted the 2014 funding authorization is complete and 2015 funding call schedule is not known at this time.

Sean Pharr: URS will begin working on the following activities under contract with the City;

- Environmental Screening and special studies
- Public outreach plan(lead is Sprinkle Consulting)
- Scoping report, including traffic studies
- Project survey(lead is Accura)
- MS4 report

A question was raised regarding property and environmental access letters.

Elaine Armster: Responded that the local government is responsible for these and GDOT did not need to approve.

Mike Lobdell: Noted that any work in the I-285 right of way, may require review by FHWA and should be coordinated with Revive I-285. It was suggested that an initial concept team meeting be held to present and discuss options and alternatives.

Patrick Bradshaw: Relayed the ARC's support to assisting in coordination, and public outreach/involvement.

Steve Foote: The design team should look 15 to 20 years out and take into consideration future conditions and economic development of the area.

Rebecca Keefer: Noted the design team should keep in mind a flexible solution as noted in previous interview and that this project is a high priority of the City and should have a economic impact/benefit to the Georgetown Area.

Kimberly Greer: Relayed this project is high priority for the City of Dunwoody.

Action Items:

- Distribute copy of sign in sheet and meeting minutes
- Property Access letters for environment
- Property Access letters for survey
- Consultant baseline schedule to GDOT
- Draft Public Involvement plan to City
- Electronic copy of presentation to both City of Dunwoody and Byron Rushing at ARC.
- Monthly status reports to GDOT
- Bi-monthly updates to ARC

ATTACHMENT #8

Keith Golden, P.E., Commissioner



GEORGIA DEPARTMENT OF TRANSPORTATION

One Georgia Center, 600 West Peachtree Street, NW
Atlanta, Georgia 30308
Telephone: (404) 631-1000

April 15, 2014

Mr. John Gates
Purchasing Manager, City of Dunwoody
41 Perimeter Center East
Dunwoody, GA 30346

Dear Mr. Gates:

I am returning for your files an executed agreement between the Georgia Department of Transportation and the City of Dunwoody for the following project:

DeKalb County, PI# 0012875

We look forward to working with you on the successful completion of the joint project.
Should you have any questions, please contact the Project Manager Elaine Armster at (404) 631-1784.

Sincerely,

A handwritten signature in blue ink that reads "Angela Robinson".

Angela Robinson,
Financial Management Administrator

AR:kp

Enclosure

c: Bob Rogers
Rachel Brown – District 7 Engineer
Kevin Cowan – District 7 Planning & Programming Engineer
Patrick Allen, P.E. – District 7 Utilities Engineer
Mike Bolden – State Utilities Engineer

**AGREEMENT
BETWEEN
DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA
AND
THE CITY OF DUNWOODY
FOR
TRANSPORTATION FACILITY IMPROVEMENTS**

This Framework Agreement is made and entered into this 7th day of April, 2014, by and between the DEPARTMENT OF TRANSPORTATION, an agency of the State of Georgia, hereinafter called the "DEPARTMENT", and the CITY OF DUNWOODY, acting by and through its Mayor and City Council, hereinafter called the "LOCAL GOVERNMENT".

WHEREAS, the LOCAL GOVERNMENT has represented to the DEPARTMENT a desire to improve the transportation facility described in Attachment "A", attached and incorporated herein by reference and hereinafter referred to as the "PROJECT"; and

WHEREAS, the LOCAL GOVERNMENT has represented to the DEPARTMENT a desire to participate in certain activities, including the funding of certain portions of the PROJECT and the DEPARTMENT has relied upon such representations; and

Revised: 12/2011

WHEREAS, the DEPARTMENT has expressed a willingness to participate in certain activities of the PROJECT as set forth in this Agreement; and

WHEREAS, the DEPARTMENT has provided an estimated cost to the LOCAL GOVERNMENT for its participation in certain activities of the PROJECT; and

WHEREAS, the Constitution authorizes intergovernmental agreements whereby state and local entities may contract with one another "for joint services, for the provision of services, or for the joint or separate use of facilities or equipment; but such contracts must deal with activities, services or facilities which the parties are authorized by law to undertake or provide." Ga. Constitution Article IX, §III, ¶I(a).

NOW THEREFORE, in consideration of the mutual promises made and of the benefits to flow from one to the other, the DEPARTMENT and the LOCAL GOVERNMENT hereby agree each with the other as follows:

1. The LOCAL GOVERNMENT has applied for and received "Qualification Certification" to administer federal-aid projects. The GDOT Local Administered Project (LAP) Certification Committee has reviewed, confirmed and approved the certification for the LOCAL GOVERNMENT to develop federal project(s) within the scope of its certification using the DEPARTMENT'S Local Administered Project Manual procedures. The LOCAL GOVERNMENT shall contribute to the PROJECT by funding all or certain portions of the PROJECT costs for the preconstruction engineering (design) activities,

Revised: 12/2011

hereinafter referred to as "PE", all reimbursable utility relocations, all non-reimbursable utilities owned by the LOCAL GOVERNMENT, railroad costs, right of way acquisitions and construction, as specified in Attachment "A", affixed hereto and incorporated herein by reference. In addition, the September 17, 2010 Planning Office memorandum titled "Preliminary Engineering Oversight for Project Managers/Project Delivery Staff", outlines the five (5) conditions when the LOCAL GOVERNMENT will be requested to fund the PE oversight activities at 100%, and is enclosed as Attachment "C" and incorporated herein by reference. Expenditures incurred by the LOCAL GOVERNMENT prior to the execution of this AGREEMENT or subsequent funding agreements shall not be considered for reimbursement by the DEPARTMENT. PE expenditures incurred by the LOCAL GOVERNMENT after execution of this AGREEMENT shall be reimbursed by the DEPARTMENT once a written notice to proceed is given by the DEPARTMENT.

2. The DEPARTMENT shall contribute to the PROJECT by funding all or certain portions of the PROJECT costs for the PE, right of way acquisitions, reimbursable utility relocations, railroad costs, or construction (specified in Attachment "A") affixed hereto and incorporated herein by reference, and none of the five (5) conditions apply from the Planning Office memorandum dated September 17, 2010 (specified in Attachment "C").

3. The DEPARTMENT shall provide a PE Oversight Estimate to the LOCAL GOVERNMENT, if appropriate, appended as Attachment "D" and incorporated by reference as if fully set out herein. The LOCAL GOVERNMENT will be responsible for

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providing payment, which represents 100% of the DEPARTMENT's PE Oversight Estimate at the time of the Project Framework Agreement execution.

If at any time the PE Oversight funds are depleted within \$5,000 of the remaining PE Oversight balance and project activities and tasks are still outstanding, the LOCAL GOVERNMENT shall, upon request, make additional payment to the DEPARTMENT. The payment shall be determined by prorating the percentage complete and using the same estimate methodology as provided in Attachment "D". If there is an unused balance after completion of all tasks and phases of the project, then pending a final audit, the remainder will be refunded to the sponsor.

4. It is understood and agreed by the DEPARTMENT and the LOCAL GOVERNMENT that the funding portion as identified in Attachment "A" of this Agreement only applies to the PE. The Right of Way and Construction funding estimate levels as specified in Attachment "A" are provided herein for planning purposes and do not constitute a funding commitment for right of way and construction. The DEPARTMENT will prepare LOCAL GOVERNMENT Specific Activity Agreements for funding applicable to other activities when appropriate.

Further, the LOCAL GOVERNMENT shall be responsible for repayment of any expended federal funds if the PROJECT does not proceed forward to completion due to a lack of available funding in future PROJECT phases, changes in local priorities or

cancellation of the PROJECT by the LOCAL GOVERNMENT without concurrence by the DEPARTMENT.

5. In accordance with Georgia Code 32-2-2, the LOCAL GOVERNMENT shall be responsible for all costs for the continual maintenance and operations of any and all sidewalks and the grass strip between the curb and sidewalk within the PROJECT limits. The LOCAL GOVERNMENT shall also be responsible for the continual maintenance and operation of all lighting systems installed to illuminate any roundabouts constructed as part of this PROJECT. Furthermore, the LOCAL GOVERNMENT shall also be responsible for the maintaining of all landscaping installed as part of any roundabout constructed as part of this PROJECT.

6. Both the LOCAL GOVERNMENT and the DEPARTMENT hereby acknowledge that Time is of the Essence. It is agreed that both parties shall adhere to the schedule of activities currently established in the approved Transportation Improvement Program/State Transportation Improvement Program, hereinafter referred to as "TIP/STIP". Furthermore, all parties shall adhere to the detailed project schedule as approved by the DEPARTMENT, attached as Attachment "B" and incorporated herein by reference. In the completion of respective commitments contained herein, if a change in the schedule is needed, the LOCAL GOVERNMENT shall notify the DEPARTMENT in writing of the proposed schedule change and the DEPARTMENT shall acknowledge the change through written response letter; provided that the DEPARTMENT shall have final authority for approving any change.

Revised: 12/2011

If, for any reason, the LOCAL GOVERNMENT does not produce acceptable deliverables in accordance with the approved schedule, the DEPARTMENT reserves the right to delay the PROJECT's implementation until funds can be re-identified for right of way or construction phases, as applicable.

7. The LOCAL GOVERNMENT shall certify that the regulations for "CERTIFICATION OF COMPLIANCES WITH FEDERAL PROCUREMENT REQUIREMENTS, STATE AUDIT REQUIREMENTS, and FEDERAL AUDIT REQUIREMENTS" are understood and will comply in full with said provisions.

8. The LOCAL GOVERNMENT shall accomplish the PE activities for the PROJECT. The PE activities shall be accomplished in accordance with the DEPARTMENT's Plan Development Process hereinafter referred to as "PDP", the applicable guidelines of the American Association of State Highway and Transportation Officials, hereinafter referred to as "AASHTO", the DEPARTMENT's Standard Specifications Construction of Transportation Systems, and all applicable design guidelines and policies of the DEPARTMENT to produce a cost effective PROJECT. Failure to follow the PDP and all applicable guidelines and policies will jeopardize the use of Federal Funds in some or all categories outlined in this agreement, and it shall be the responsibility of the LOCAL GOVERNMENT to make up the loss of that funding. The LOCAL GOVERNMENT's responsibility for PE activities shall include, but is not limited to the following items:

Revised: 12/2011

a. Prepare the PROJECT Concept Report and Design Data Book in accordance with the format used by the DEPARTMENT. The concept for the PROJECT shall be developed to accommodate the future traffic volumes as generated by the LOCAL GOVERNMENT as provided for in paragraph 8b and approved by the DEPARTMENT. The concept report shall be approved by the DEPARTMENT prior to the LOCAL GOVERNMENT beginning further development of the PROJECT plans. It is recognized by the parties that the approved concept may be updated or modified by the LOCAL GOVERNMENT as required by the DEPARTMENT and re-approved by the DEPARTMENT during the course of PE due to updated guidelines, public input, environmental requirements, Value Engineering recommendations, Public Interest Determination (PID) for utilities, utility/railroad conflicts, or right of way considerations.

b. Prepare a Traffic Study for the PROJECT that includes Average Daily Traffic, hereinafter referred to as "ADT", volumes for the base year (year the PROJECT is expected to be open to traffic) and design year (base year plus 20 years) along with Design Hour Volumes, hereinafter referred to as "DHV", for the design year. DHV includes morning (AM) and evening (PM) peaks and other significant peak times. The Study shall show all through and turning movement volumes at intersections for the ADT and DHV volumes and shall indicate the percentage of trucks on the facility. The Study shall also include signal warrant evaluations for any additional proposed signals on the PROJECT.

c. Prepare environmental studies, documentation reports and complete Environmental Document for the PROJECT along with all environmental re-

evaluations required that show the PROJECT is in compliance with the provisions of the National Environmental Policy Act or the Georgia Environmental Policy Act as per the DEPARTMENT's Environmental Procedures Manual, as appropriate to the PROJECT funding. This shall include any and all archaeological, historical, ecological, air, noise, community involvement, environmental justice, flood plains, underground storage tanks, and hazardous waste site studies required. The completed Environmental Document approval shall occur prior to Right of Way funding authorization. A re-evaluation is required for any design change as described in Chapter 7 of the Environmental Procedures Manual. In addition, a re-evaluation document approval shall occur prior to any Federal funding authorizations if the latest approved document is more than 6 months old. The LOCAL GOVERNMENT shall submit to the DEPARTMENT all studies, documents and reports for review and approval by the DEPARTMENT, the FHWA and other environmental resource agencies. The LOCAL GOVERNMENT shall provide Environmental staff to attend all PROJECT related meetings where Environmental issues are discussed. Meetings include, but are not limited to, concept, field plan reviews and value engineering studies.

d. Prepare all PROJECT public hearing and public information displays and conduct all required public hearings and public information meetings with appropriate staff in accordance with DEPARTMENT practice.

e. Perform all surveys, mapping, soil investigations and pavement evaluations needed for design of the PROJECT as per the appropriate DEPARTMENT Manual.

Revised: 12/2011

f. Perform all work required to obtain all applicable PROJECT permits, including, but not limited to, Cemetery, TVA and US Army Corps of Engineers permits, Stream Buffer Variances and Federal Emergency Management Agency (FEMA) approvals. The LOCAL GOVERNMENT shall provide all mitigation required for the project, including but not limited to permit related mitigation. All mitigation costs are considered PE costs. PROJECT permits and non-construction related mitigation must be obtained and completed 3 months prior to the scheduled let date. These efforts shall be coordinated with the DEPARTMENT.

g. Prepare the stormwater drainage design for the PROJECT and any required hydraulic studies for FEMA Floodways within the PROJECT limits. Acquire of all necessary permits associated with the Hydrology Study or drainage design.

h. Prepare utility relocation plans for the PROJECT following the DEPARTMENT's policies and procedures for identification, coordination and conflict resolution of existing and proposed utility facilities on the PROJECT. These policies and procedures, in part, require the Local Government to submit all requests for existing, proposed, and relocated facilities to each utility owner within the project area. Copies of all such correspondence, including executed agreements for reimbursable utility/railroad relocations, shall be forwarded to the DEPARTMENT's Project Manager and the District Utilities Engineer and require that any conflicts with the PROJECT be resolved by the LOCAL GOVERNMENT. If it is determined that the PROJECT is located on an on-system route or is a DEPARTMENT LET PROJECT, the LOCAL GOVERNMENT and the District Utilities Engineer shall ensure that permit applications are approved for each utility company in conflict with

Revised: 12/2011

the project. If it is determined through the DEPARTMENT's Project Manager and State Utilities Office during the concept or design phases the need to utilize Overhead/Subsurface Utility Engineering, hereinafter referred to as "SUE", to obtain the existing utilities, the LOCAL GOVERNMENT shall be responsible for acquiring those services. SUE costs are considered PE costs.

i. Prepare, in English units, Preliminary Construction plans, Right of Way plans and Final Construction plans that include the appropriate sections listed in the Plan Presentation Guide, hereinafter referred to as "PPG", for all phases of the PDP. All drafting and design work performed on the project shall be done utilizing Microstation V8i and InRoads software respectively using the DEPARTMENT's Electronic Data Guidelines. The LOCAL GOVERNMENT shall further be responsible for making all revisions to the final right of way plans and construction plans, as deemed necessary by the DEPARTMENT, for whatever reason, as needed to acquire the right of way and construct the PROJECT.

j. Prepare PROJECT cost estimates for construction, Right of Way and Utility/railroad relocation at the following project stages: Concept, Preliminary Field Plan Review, Right of Way plan approval (Right of Way cost only), Final Field Plan Review and Final Plan submission using the applicable method approved by the DEPARTMENT. The cost estimates shall also be updated annually if the noted project stages occur at a longer frequency. Failure of the LOCAL GOVERNMENT to provide timely and accurate cost estimates may delay the PROJECT's implementation until additional funds can be identified for right of way or construction, as applicable.

Revised: 12/2011

k. Provide certification, by a Georgia Registered Professional Engineer, that the Design and Construction plans have been prepared under the guidance of the professional engineer and are in accordance with AASHTO and DEPARTMENT Design Policies.

l. Provide certification, by a Level II Certified Design Professional that the Erosion Control Plans have been prepared under the guidance of the certified professional in accordance with the current Georgia National Pollutant Discharge Elimination System.

m. Provide a written certification that all appropriate staff (employees and consultants) involved in the PROJECT have attended or are scheduled to attend the Department's PDP Training Course. The written certification shall be received by the Department no later than the first day of February of every calendar year until all phases have been completed.

9. The Primary Consultant firm or subconsultants hired by the LOCAL GOVERNMENT to provide services on the PROJECT shall be prequalified with the DEPARTMENT in the appropriate area-classes. The DEPARTMENT shall, on request, furnish the LOCAL GOVERNMENT with a list of prequalified consultant firms in the appropriate area-classes. The LOCAL GOVERNMENT shall comply with all applicable state and federal regulations for the procurement of design services and in accordance with the Brooks Architect-Engineers Act of 1972, better known as the Brooks Act, for any consultant hired to perform work on the PROJECT.

Revised: 12/2011

10. The DEPARTMENT shall review and has approval authority for all aspects of the PROJECT provided however this review and approval does not relieve the LOCAL GOVERNMENT of its responsibilities under the terms of this agreement. The DEPARTMENT will work with the FHWA to obtain all needed approvals as deemed necessary with information furnished by the LOCAL GOVERNMENT.

11. The LOCAL GOVERNMENT shall be responsible for the design of all bridge(s) and preparation of any required hydraulic and hydrological studies within the limits of this PROJECT in accordance with the DEPARTMENT's policies and guidelines. The LOCAL GOVERNMENT shall perform all necessary survey efforts in order to complete the hydraulic and hydrological studies and the design of the bridge(s). The final bridge plans shall be incorporated into this PROJECT as a part of this Agreement.

12. The LOCAL GOVERNMENT unless otherwise noted in Attachment "A" shall be responsible for funding all LOCAL GOVERNMENT owned utility relocations and all other reimbursable utility/railroad costs. The utility costs shall include but are not limited to PE, easement acquisition, and construction activities necessary for the utility/railroad to accommodate the PROJECT. The terms for any such reimbursable relocations shall be laid out in an agreement that is supported by plans, specifications, and itemized costs of the work agreed upon and shall be executed prior to certification by the DEPARTMENT. The LOCAL GOVERNMENT shall certify via written letter to the DEPARTMENT's Project Manager and District Utilities Engineer that all Utility owners' existing and proposed facilities are shown on the plans with no conflicts 3 months prior

Revised: 12/2011

to advertising the PROJECT for bids and that any required agreements for reimbursable utility/railroad costs have been fully executed. Further, this certification letter shall state that the LOCAL GOVERNMENT understands that it is responsible for the costs of any additional reimbursable utility/railroad conflicts that arise during construction.

13. The DEPARTMENT will be responsible for all railroad coordination on DEPARTMENT Let and/or State Route (On-System) projects; the LOCAL GOVERNMENT shall address concerns, comments, and requirements to the satisfaction of the Railroad and the DEPARTMENT. If the LOCAL GOVERNMENT is shown to LET the construction in Attachment "A" on off-system routes, the LOCAL GOVERNMENT shall be responsible for all railroad coordination and addressing concerns, comments, and requirements to the satisfaction of the Railroad and the DEPARTMENT for PROJECT.

14. The LOCAL GOVERNMENT shall be responsible for acquiring a Value Engineering Consultant for the DEPARTMENT to conduct a Value Engineering Study if the total estimated PROJECT cost is \$50 million or more. The Value Engineering Study cost is considered a PE cost. The LOCAL GOVERNMENT shall provide project related design data and plans to be evaluated in the study along with appropriate staff to present and answer questions about the PROJECT to the study team. The LOCAL GOVERNMENT shall provide responses to the study recommendations indicating whether they will be implemented or not. If not, a valid response for not implementing

shall be provided. Total project costs include PE, right of way, and construction, reimbursable utility/railroad costs.

15. The LOCAL GOVERNMENT, unless shown otherwise on Attachment "A", shall acquire the Right of way in accordance with the law and the rules and regulations of the FHWA including, but not limited to, Title 23, United States Code; 23 CFR 710, et. Seq., and 49 CFR Part 24 and the rules and regulations of the DEPARTMENT. Upon the DEPARTMENT's approval of the PROJECT right of way plans, verification that the approved environmental document is valid and current, a written notice to proceed will be provided by the DEPARTMENT for the LOCAL GOVERNMENT to stake the right of way and proceed with all pre-acquisition right of way activities. The LOCAL GOVERNMENT shall not proceed to property negotiation and acquisition whether or not the right of way funding is Federal, State or Local, until the right of way agreement named "Contract for the Acquisition of Right of Way" prepared by the DEPARTMENT's Office of Right of Way is executed between the LOCAL GOVERNMENT and the DEPARTMENT. Failure of the LOCAL GOVERNMENT to adhere to the provisions and requirements specified in the acquisition contract may result in the loss of Federal funding for the PROJECT and it will be the responsibility of the LOCAL GOVERNMENT to make up the loss of that funding. Right of way costs eligible for reimbursement include land and improvement costs, property damage values, relocation assistance expenses and contracted property management costs. Non reimbursable right of way costs include administrative expenses such as appraisal, consultant, attorney fees and any in-house property management or staff expenses. The LOCAL GOVERNMENT

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shall certify that all required right of way is obtained and cleared of obstructions, including underground storage tanks, 3 months prior to advertising the PROJECT for bids.

16. The DEPARTMENT unless otherwise shown in Attachment "A" shall be responsible for Letting the PROJECT to construction, solely responsible for executing any agreements with all applicable utility/railroad companies and securing and awarding the construction contract for the PROJECT when the following items have been completed and submitted by the LOCAL GOVERNMENT:

- a. Submittal of acceptable PROJECT PE activity deliverables noted in this Agreement.
- b. Certification that all needed rights of way have been obtained and cleared of obstructions.
- c. Certification that the environmental document is current and all needed permits and mitigation for the PROJECT have been obtained.
- d. Certification that all Utility/Railroad facilities, existing and proposed, within the PROJECT limits are shown, any conflicts have been resolved and reimbursable agreements, if applicable, are executed.

If the LOCAL GOVERNMENT is shown to LET the construction in Attachment "A", the LOCAL GOVERNMENT shall provide the above deliverables and certifications and shall follow the requirements stated in Chapters 10, 11, 12 and 13 of the DEPARTMENT's Local Administered Project Manual. The LOCAL GOVERNMENT

Revised: 12/2011

shall be responsible for providing qualified construction oversight with their personnel or by employing a Consultant firm prequalified in Area Class 8.01 to perform construction oversight. The LOCAL GOVERNMENT shall be responsible for employing a GDOT prequalified consultant in area classes 6.04a and 6.04b for all materials testing on the PROJECT, with the exception of field concrete testing. All materials testing, including field concrete testing shall be performed by GDOT certified technicians who are certified for the specific testing they are performing on the PROJECT. The testing firm(s) and the individual technicians must be submitted for approval prior to Construction.

17. The LOCAL GOVERNMENT shall provide a review and recommendation by the engineer of record concerning all shop drawings prior to the DEPARTMENT review and approval. The DEPARTMENT shall have final authority concerning all shop drawings.

18. The LOCAL GOVERNMENT agrees that all reports, plans, drawings, studies, specifications, estimates, maps, computations, computer files and printouts, and any other data prepared under the terms of this Agreement shall become the property of the DEPARTMENT if the PROJECT is being let by the DEPARTMENT. This data shall be organized, indexed, bound, and delivered to the DEPARTMENT no later than the advertisement of the PROJECT for letting. The DEPARTMENT shall have the right to use this material without restriction or limitation and without compensation to the LOCAL GOVERNMENT.

19. The LOCAL GOVERNMENT shall be responsible for the professional quality, technical accuracy, and the coordination of all reports, designs, drawings, specifications, and other services furnished by or on behalf of the LOCAL GOVERNMENT pursuant to this Agreement. The LOCAL GOVERNMENT shall correct or revise, or cause to be corrected or revised, any errors or deficiencies in the reports, designs, drawings, specifications, and other services furnished for this PROJECT. Failure by the LOCAL GOVERNMENT to address the errors, omissions or deficiencies within 30 days of notification shall cause the LOCAL GOVERNMENT to assume all responsibility for construction delays and supplemental agreements caused by the errors and deficiencies. All revisions shall be coordinated with the DEPARTMENT prior to issuance. The LOCAL GOVERNMENT shall also be responsible for any claim, damage, loss or expense, to the extent allowed by law that is attributable to errors, omissions, or negligent acts related to the designs, drawings, specifications, and other services furnished by or on behalf of the LOCAL GOVERNMENT pursuant to this Agreement.

20. The DEPARTMENT shall be furnished with a copy of all contracts and agreements between the LOCAL GOVERNMENT and any other agency or contractor associated with construction activities. The DEPARTMENT's Project Manager shall be the primary point of contact unless otherwise specified.

21. The LOCAL GOVERNMENT shall provide the DEPARTMENT with a detailed project schedule that reflects milestones, deliverables with durations for all pertinent

Revised: 12/2011

activities to develop critical path elements. An electronic project schedule shall be submitted to the Project Manager after execution of this agreement.

This Agreement is made and entered into in FULTON COUNTY, GEORGIA, and shall be governed and construed under the laws of the State of Georgia.

The covenants herein contained shall, except as otherwise provided, accrue to the benefit of and be binding upon the successors and assigns of the parties hereto.

IN WITNESS WHEREOF, the DEPARTMENT and the LOCAL GOVERNMENT have caused these presents to be executed under seal by their duly authorized representatives.

DEPARTMENT OF
TRANSPORTATION

CITY OF DUNWOODY

BY: [Signature]
Commissioner

BY: [Signature]
Mike Davis
Mayor

ATTEST: [Signature]
Treasurer



Signed, sealed and delivered this 9th
day of December, 2013 in the
presence of:

[Signature]
Witness

[Signature]
Notary Public

This Agreement approved by the City of
Dunwoody, the 9th day of
December, 2013.

Attest

[Signature]
Sharon Lowery, City Clerk

FEIN: 26-3687535

Attachment "A" Funding Sources and Distribution
 Project No.: 0012875 Sponsor: City of Dunwoody

Project # 0012875 City of Dunwoody

Attach "Project Manager" Project Charging Form for Approval

Preliminary Engineering Phase I	Preliminary Engineering - Phase I ¹					GDOT Oversight for PE (Phase I) ²			Preliminary Engineering Grand Total (Phase I)	
	Percentage	PE Amount	Maximum PE Participation Amount (\$)	Participant	PE Activity Sponsor	Percentage	Amount	Participant	Percentage	Amount
1	80%	\$200,000.00	\$200,000.00	Federal	Local Government	#DIV/0!	\$0.00	Federal	80%	\$200,000.00
2	0%	\$0.00	\$0.00	State		#DIV/0!	\$0.00	State	0%	\$0.00
3	20%	\$50,000.00	N/A	Local		0%	\$0.00	Local	20%	\$50,000.00
4	0%	\$0.00	\$0.00	Other		#DIV/0!	\$0.00	Other	0%	\$0.00
Total	100%	\$250,000.00			#DIV/0!	\$0.00		100%	\$250,000.00	

Right of Way Phase II	Right of Way - Phase II ³					
	Percentage	ROW Amount	Maximum ROW Participation Amount (\$)	Participant	Acquisition By:	Acquisition Funding By:
1	#DIV/0!	\$0.00	\$0.00	Federal	Local Government	Local Government
2	#DIV/0!	\$0.00	\$0.00	State		
3	#DIV/0!	\$0.00	N/A	Local		
4	#DIV/0!	\$0.00	\$0.00	Other		
Total	#DIV/0!	\$0.00				

Utility Phase IV	Utility Relocation - Phase IV	
	Utility Funding By:	Railroad Funding By:
	Local Government	Local Government
Total	100%	100%

Construction Phase III	Construction - Phase III ³				
	Percentage	CST Amount	Maximum CST Participation Amount (\$)	Participant	Letting By:
1	#DIV/0!	\$0.00	\$0.00	Federal	Local Govt
2	#DIV/0!	\$0.00	\$0.00	State	
3	#DIV/0!	\$0.00	N/A	Local	
4	#DIV/0!	\$0.00	\$0.00	Other	
Total	#DIV/0!	\$0.00			

Construction Oversight Phases V & VI	GDOT Oversight for CST (Phase III) ²	
	Testing (Phase V) Funding By:	Inspection (Phase VI) Funding By:
	Local Government	Local Government
Total	100%	100%

Summary of Phases I Through III	Grand Total - All Phases I through III			
	Percentage	TOTAL Amount	Maximum Participation Amount (\$)	Participant
1	80%	\$200,000.00	\$200,000.00	Federal
2	0%	\$0.00	\$0.00	State
3	20%	\$50,000.00	N/A	Local
4	0%	\$0.00	\$0.00	Other
Total	100%	\$250,000.00		

¹The maximum allowable GDOT participating amounts for PE phase are shown above. The local government will only be reimbursed the percentage of the accrued invoiced amounts up to but not to exceed the maximum amount indicated.

²GDOT Oversight for PE (Phase I) is detailed in Attachment "D".

³Right-of-Way and Construction amounts shown are for budget planning purposes only.

NOTE: Separate GDOT P.O.s will be established for each funding phase.

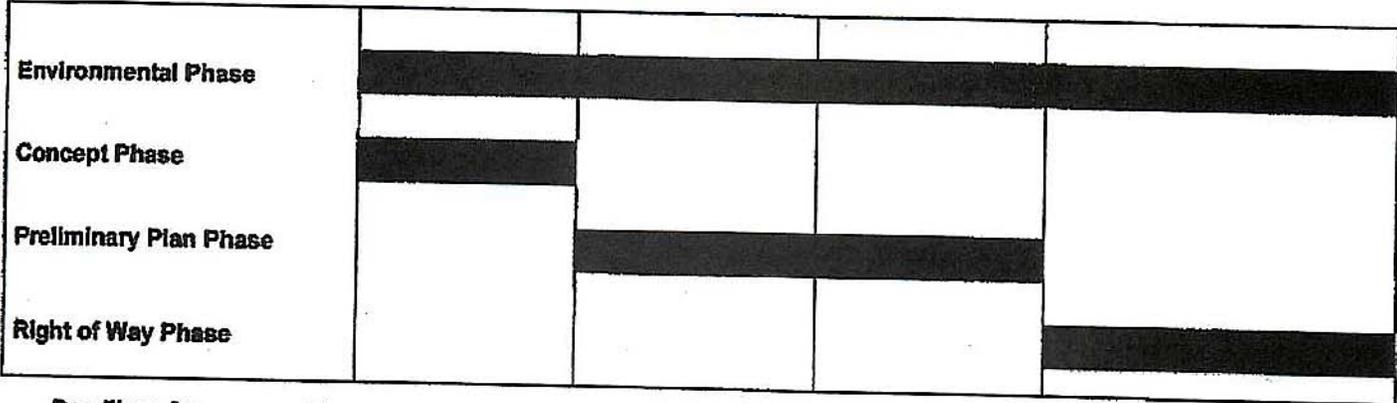
The GDOT Oversight check shall be remitted to the District Planning and Programming Engineer along with the signed Project Framework Agreement (PFA).

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ATTACHMENT "B" Project Timeline

PI # 0012875 – City of Dunwoody

Proposed Project Timeline



Deadlines for Responsible Parties	Execute Agreement	Month/Year (Approve Concept)	Month/Year (Approve Env. Document)	Month/Year (Authorize Right of Way funds)	Month/Year (Authorize Const. funds)
	2/2014	9/2014	3/2015	12/2015	12/2016

Annual Reporting Requirements

The Local Government shall provide a written status report to the Department's Project Manager with the actual phase completion date(s) and the percent complete/proposed completion date of incomplete phases. The written status report shall be received by the Department no later than the first day of February of every calendar year until all phases have been completed.

Revised: 12/2011

ATTACHMENT "C"

Project # 0012875 City of Dunwoody

D.O.T. 66

DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA

INTERDEPARTMENTAL CORRESPONDENCE

FILE OFFICE Planning
 DATE September 17, 2010
 FROM  Angela T. Alexander, State Transportation Planning Administrator
 TO Todd I. Long, PE, PTOE, Director of Planning
 Gerald M. Ross, PE, Chief Engineer/Deputy Commissioner
 SUBJECT Preliminary Engineering Oversight for Project Managers/Project Delivery Staff

Note: This memo supersedes the previous PE Oversight Memo, dated August 17, 2010. PE Oversight funding for Safe Route to School (SRTS) projects are eligible for PE Oversight funds, paid for with funding from the SRTS program. No other changes were made to the memo.

As you are aware, the Department is unable to continue funding PE oversight with 100% motor fuel funds due to the decline in motor fuel revenues. As a result, the Department needs an established procedure detailing the circumstances under which the Department will fund PE oversight with federal-aid funds (matched with state motor fuel funds) and when the Department will request that the local government/project sponsor fund the Department's expenses associated with PE oversight. The PE Oversight funds will be used to fund staff man-hours and any other associated expenses incurred by any GDOT employee working on the project. Please note that the process detailed below applies equally to routes both on and off the state highway system.

GDOT Funds PE Oversight with Federal-Aid:

The Department will fund PE oversight with federal-aid funds (and matching motor fuel funds), only if a subsequent project phase (ROW, UTL, CST) is programmed within the first 4 active years of the currently approved TIP/STIP. The source of federal-aid funds to be used for the PE oversight activities is as follows:

- 1) Projects on the National Highway System will use NHS funds (L050) to finance GDOT's PE oversight expenses
- 2) Projects *not* on the National Highway System but eligible for Surface Transportation Program (STP) funds, will follow one of the scenarios below:
 - a) Projects in urban areas between 5,000 and 199,999 in population will use L200 funds (with MPO approval, if applicable)
 - b) Projects in urban areas with a population greater than 200,000 will use L230 funds (with MPO approval)
 - c) Projects in rural areas with a population less than 5,000 will use L250 funds
 - d) The Department may, at the joint discretion of the Chief Engineer and Director of Planning, apply L240 funds to any federal-aid eligible project

- 3) Projects which have received an earmark in federal legislation, will use a portion of the earmark funding for GDOT's PE oversight expenses, pending MPO approval if applicable. (Note: earmark funded projects could receive PE oversight funding regardless of the funding being programmed within the first 4 active years of a currently approved TIP/STIP).
- 4) Projects funded with Safe Route to School (SRTS) funds will use SRTS funds to finance GDOT's PE oversight expenses, regardless of whether or not a subsequent phase of the project appears in the STIP/TIP.

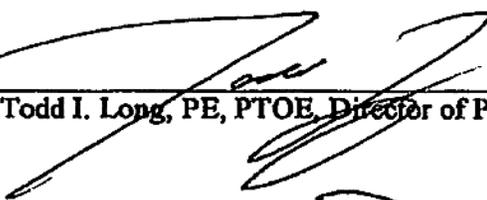
GDOT Requests Local Government/Project Sponsor to Fund PE Oversight:

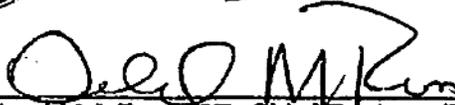
The Department will request that the local government fund PE oversight with 100% local funds under the following conditions:

- 1) A subsequent phase of the project is not programmed within the first 4 active years of the Currently approved TIP/STIP
- 2) The MPO has elected to not approve the use of L200 or L230 funds for GDOT's PE oversight expenses
- 3) The project is funded with CMAQ funds
- 4) The project is funded with an earmark identified in federal legislation and the local government/entity which secured the earmark (or MPO, if applicable) declines to allow GDOT to use a portion of the earmark for PE oversight expenses
- 5) The project is currently funded entirely with local funds; however, the local government intends to secure federal funding at a future date

Once the PE oversight process is implemented, it will be the responsibility of the GDOT Project Manager to work with the GDOT Office of Financial Management to establish an appropriate amount of federal-aid funded PE oversight funding, or work with the local government to secure locally sourced PE oversight funds.

If you approve of this process, please sign below. Once an acceptable process is developed and approved by both the Chief Engineer and Director of Planning, we will provide the finalized process to the Office of Program Control for distribution to the GDOT Project Managers and incorporation into future Project Framework Agreements. If you have any questions, please contact Matthew Fowler at 404-631-1777.

Approved:  _____ 7/27/10
Todd I. Long, PE, PTOE, Director of Planning Date

Approved:  _____ 10/7/20
Gerald M. Ross, PE, Chief Engineer/Deputy Commissioner Date

ATTACHMENT "D"
GDOT Oversight Estimate for Locally Administered Project

Wednesday, November 06, 2013 2:42 PM

PI Number	<input type="text" value="0012875"/>	Project Number	<input type="text"/>
County	<input type="text" value="DeKalb"/>	Project Length	<input type="text" value="1.22"/> Miles
Project Manager	<input type="text" value="Fisher, Carleton"/>	Project Cost	<input type="text" value="\$250,000.00"/>
Project Type	<input type="text" value="Enhancement"/>		
Project Description	<input type="text" value="CR 3329 & CR 5156 – Georgetown Area Multimodal Improvements"/>		
Expected Life of Project	<input type="text" value="1.00"/> Years		

Project Phase	Oversight Hours	Oversight Cost
1. Project Initiation	0	\$ 0.00
2. Concept Development	0	\$ 0.00
3. Database Preparation*	0	\$ 0.00
4. Preliminary Design	0	\$ 0.00
5. Environmental	0	\$ 0.00
6. Final Design	0	\$ 0.00
Travel Expenses	-	\$ -
Total Oversight Estimate	0	\$ 0.00
Percentage of Project Cost	.00%	

C:\Documents and Settings\vgavalas\My Documents\Oversight Estimate 0012875.docx

**ATTACHMENT "E"
GEORGIA SECURITY AND IMMIGRATION COMPLIANCE ACT
AFFIDAVIT**

Name of Contracting Entity: City of Dunwoody

Contract No. and Name: PI# 0012875

By executing this affidavit, the undersigned person or entity verifies its compliance with O.C.G.A. § 13-10-91, stating affirmatively that the individual, firm, or entity which is contracting with the Georgia Department of Transportation has registered with, is authorized to participate in, and is participating in the federal work authorization program commonly known as E-Verify, or any subsequent replacement program, in accordance with the applicable provisions and deadlines established in O.C.G.A. § 13-10-91.

The undersigned person or entity further agrees that it will continue to use the federal work authorization program throughout the contract period, and it will contract for the physical performance of services in satisfaction of such contract only with subcontractors who present an affidavit to the undersigned with the information required by O.C.G.A. § 13-10-91(b).

The undersigned person or entity further agrees to maintain records of such compliance and provide a copy of each such verification to the Georgia Department of Transportation within five (5) business days after any subcontractor is retained to perform such service.

224297
E-Verify / Company Identification Number

[Signature]
Signature of Authorized Officer or Agent

6/23/09
Date of Authorization

Michael G. Davis
Printed Name of Authorized Officer or Agent

Mayor
Title of Authorized Officer or Agent

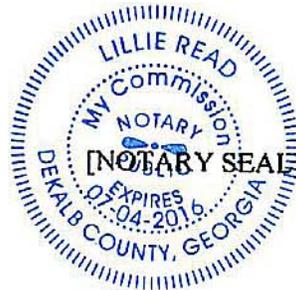
1/24/14
Date

SUBSCRIBED AND SWORN
BEFORE ME ON THIS THE

24th DAY OF January, 2014

Lillie Read
Notary Public

My Commission Expires: 7/4/16



Revised: 12/2011

ATTACHMENT "F"

TITLE VI INTRODUCTION

As a sub-recipient of federal funds from Georgia Department of Transportation, all municipalities are required to comply with Title VI of the Civil Rights Act of 1964 which provides that:

"No person in the United States shall, on the grounds of race, color, or national origin, be excluded from participation in, or be denied the benefits of, or be subjected to discrimination under any program or activity receiving federal assistance under This title or carried out under this title."

Additionally, the Civil Rights Restoration Act of 1987, expanded the definition of the terms "programs and activities" to include all programs or activities of federal recipients, subrecipients, and contractors, whether or not such programs and activities are federally assisted.

The provisions of Title VI apply to all contractors, subcontractors, consultants and suppliers. And is a condition for receiving federal funds. All sub recipients must sign Title VI assurances that they will not discriminate as stated in Title VI of the Civil Rights Act of 1964.

In the event that the sub recipient distributes federal aid funds to second tier entity, the sub-recipient shall include Title VI language in all written documents and will monitor for compliance. If, these assurances are not signed, the City or County government may be subjected to the loss of federal assistance.

All sub recipients that receive federal assistance must also include Federal Highways Administrations 1273 in their contracts. The FHWA 1273 sets out guidance for ensuring non discrimination and encouraging minority participation and outreach.

Enclosed you will find Title VI acknowledgment form and the Title VI assurances. The Title VI acknowledgment form and Title VI assurances must be signed by your local government official if it has not been signed.

Revised: 12/2011

ATTACHMENT "F"

TITLE VI ACKNOWLEDGEMENT FORM

The undersigned assures that no person shall on the grounds or race, color, national origin or sex as provided by Title VI of the Civil Rights Act of 1964, and the Civil Rights Restoration Act of 1987 be excluded from participation in, be denied the benefits of, or otherwise be subjected to discrimination under any City or County sponsored program or activity. The undersigned assures that every effort will be made to ensure non discrimination in all of its programs or activities, whether those programs are federally funded or not.

Assurance of compliance therefore falls under the proper authority of the City Council or the County Board of Commissioners. The Title VI Coordinator or Liaison is authorized to ensure compliance with provisions of this policy and with the Law, including the requirements of 23 Code of Federal Regulations (CFR) 200 and 49 CFR 21.

Michael G. Davis, Mayor
Official Name and Title

1/24/14
Date

Citations:

Title VI of the Civil Rights Act of 1964; 42 USC 2000d to 2000d-4; 42 USC 4601 to 4655; 23 USC 109(h); 23 USC 324; DOT Order 1050.2; EO 12250; EO 12898; 28CFR 50.3

Other Nondiscrimination Authorities Expanded the range and scope of Title VI coverage and applicability

- The 1970 Uniform Act (42 USC 4601)
- Section 504 of the 1973 Rehabilitation Act (29 USC 790)
- The 1973 Federal-aid Highway Act (23 USC 324)
- The 1975 Age Discrimination Act (42 USC 6101)
- Implementing Regulations (49 CFR 21 & 23 CFR 200)
- Executive Order 12898 on Environmental Justice (EJ)
- Executive Order 13166 on Limited English Proficiency (LEP)

ATTACHMENT #9

August 19, 2015

Mr. Brent Story, P.E.
Georgia Department of Transportation – Design Policy and Support
One Georgia Center
600 West Peachtree Street, NW
Atlanta, GA 30308

**Subject: Lighting and Maintenance for Proposed Bike and Pedestrian Improvements:
Chamblee Dunwoody Road (PI 0012875)**

Dear Brent:

The City of Dunwoody has contracted with AECOM for design services for the proposed bike and pedestrian improvement project on Chamblee Dunwoody Road between Cotillion Drive and North Shallowford Road (PI 0012875). AECOM's lighting subconsultant, Womack Lumsden & Associates, LLC, will provide a properly designed pedestrian lighting plan for this project. The city and these consultants are aware of the IESNA and AASHTO lighting guidelines for roundabouts.

The City of Dunwoody has committed to funding the Energy, Operations and Maintenance costs of the lighting systems installed for this project. We are currently in the process of investigating energy efficient alternatives including LED's and CFL's for this and other projects with the city in order to reduce future operating costs.

The City of Dunwoody will also commit to maintaining any on street parking areas built by this project so that they are free of trash.

If you need any additional information, please contact me (mindy.sanders@dunwoodyga.gov, 678-382-6812) or Sean Pharr, AECOM Project Manager (sean.pharr@aecom.com, 678-808-8839).

Sincerely,



Mindy Sanders, P.E.
Capital Projects Manager

ATTACHMENT #10

PROJ ID	COUNTY	DESCRIPTION
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0012875 DeKalb **CR 3329 & CR 5156 - GEORGETOWN AREA MULTIMODAL IMPROVEMENTS**

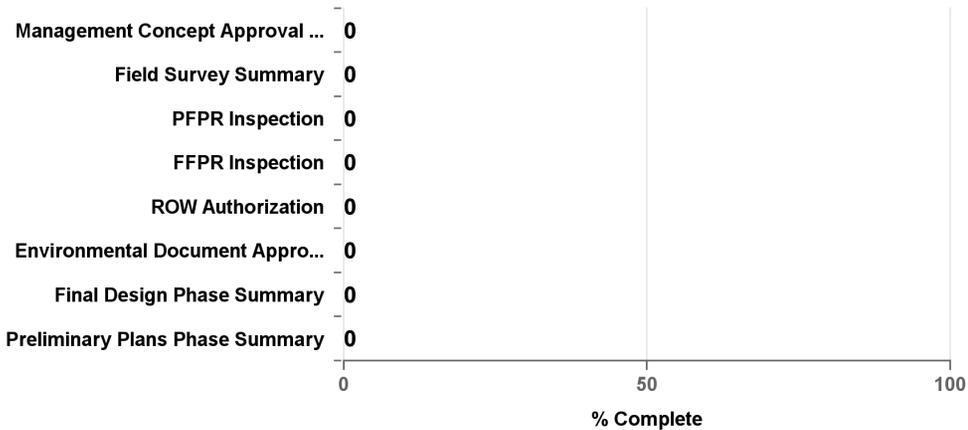
Mgmt Let Date: 1/15/2018 This project will provide enhanced bicycle and pedestrian amenities throughout the previous Master Plan study area.

PROJ NO:
MPO TIP#: DK-417
MPO: Atlanta TMA
PROJ LENGTH (MI): 1.22
TYPE WORK: Bicycle/Ped. Facility
LET RESPONSIBILITY: GDOT Let
BIKE PROVISIONS INCLUDED?: N

SPONSOR: Dunwoody
PROJ MGR: Fisher, Carleton
DOT DIST: 7
CONG DIST: 006
TYPE WORK: Bicycle/Ped. Facility
HOUSE DIST:
SENATE DIST:

Phase	FY Approved	Approved FY Estimate *	Fund	Phase Status
Engineering	2014	\$325,000.00	33C	AUTHORIZED
Right of Way	2016	\$750,000.00	LOC	PRECST
Construction	2018	\$2,535,750.00	M230	PRECST
UTL	2018	\$1,200,000.00	LOC	PRECST
SCP	2014	\$250,000.00	M301	AUTHORIZED

* Inflation Included in Estimate



Activity	Actual Start Date	Actual Finish Date
Management Concept Approval Complete		
Environmental Document Approval Summary (11412)	12/17/2015	
Field Survey Summary		
Preliminary Plans Phase Summary		
FFPR Inspection		
ROW Authorization		
Final Design Phase Summary		
FFPR Inspection		

Right of Way Acquisition Information:

Preliminary Parcel Count: Total Parcel Count: Acquired by: LOC