

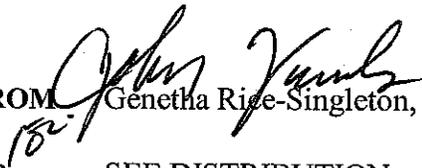
**DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA**

INTERDEPARTMENT CORRESPONDENCE

FILE P. I. No. 0007635, McIntosh County
CSHPP-0007-00(635)
McIntosh Trail

OFFICE Preconstruction

DATE June 4, 2009

FROM  Genetha Rice-Singleton, Assistant Director of Preconstruction

TO SEE DISTRIBUTION

SUBJECT APPROVED PROJECT CONCEPT REPORT

Attached for your files is the approval for subject project.

Attachment

DISTRIBUTION:

Ron Wishon
Glenn Bowman
Ken Thompson
Michael Henry
Keith Golden
Glenn Durrence
Paul Liles
Brad Saxon
Dennis Odom
BOARD MEMBER

**DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA**

INTERDEPARTMENTAL CORRESPONDENCE

FILE: P.I. No. 0007635, McIntosh County
CSHPP-0007-00(635)
McIntosh Trail

OFFICE: Preconstruction

DATE: May 28, 2009


FROM: Genetha Rice-Singleton, Assistant Director of Preconstruction

TO: Gerald M. Ross, P.E., Chief Engineer

SUBJECT: PROJECT CONCEPT REPORT

This project consists of the construction of a segment of the proposed Coastal Georgia Greenway to become part of the East Coast Greenway beginning at the intersection of McIntosh Boulevard/Pine Street with SR 99 in the city of Darien and continuing north 0.30 miles along SR 99 to the intersection of Bond Road. The purpose of this project is to provide a safe means of alternative transportation outside of the city of Darien's existing trail system.

The project proposes a concrete hiking/jogging path varying in width from four to seven feet between the above mention project limits. Construction of the trail will continue north from Bond Road under TEE-0008-00(177) and CSTE-0009-00(109) to connect with the McIntosh County Blue and Hall Park at the intersection of Blue and Hall Road and SR 99. It will continue on to the Sapelo Island Visitor's Center and State Ferry at the intersection of Landing Road and SR 99. The combined projects will be known as the Highlander Trail. The project and design will accommodate the applicable accessibility requirements.

Environmental concerns include requiring a Categorical Exclusion be prepared; a Public Information Open House will be held; time saving procedures are appropriate.

The estimated costs for this project are:

	<u>PROPOSED</u>	<u>APPROVED</u>	<u>FUNDING</u>	<u>PROG DATE</u>
Construction (includes E&C)	\$193,000	\$384,770	LY10	2011(proposed)
Right-of-way & Utilities	Local	Local	Local	Local

* McIntosh County signed PFA on 4-21-09 for PE, ROW, UTIL and 20% CST/ 100% over \$142,400.

P.I. No. 0007635, McIntosh County
Page 2
May 28, 2009

I recommend this project concept be approved.

GRS: JDQ
Attachment

CONCUR

Handwritten signature: Kenneth Rice - Director for District

Director of Preconstruction

APPROVED

Handwritten signature: Gerald M. Ross

Gerald M. Ross, P.E., Chief Engineer

Concept Report

DEPARTMENT OF TRANSPORTATION STATE OF GEORGIA

District 5 Office

Project Number:

County:

Project Number: HPP-0007-00(635) McIntosh Trail
(Former Project STP-0000-00(538) Coastal Georgia Greenway)

County: McIntosh

P.I. Number: 0007635

Federal Route Number: N/A

State Route Number: 99

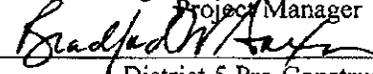
The McIntosh County Highlander Trail is a 7' wide concrete path on SR99, part of the Coastal Georgia Greenway from Savannah to St. Marys and the East Coast Greenway from Maine to Florida.

Recommendation for approval:

DATE 2-19-09

DATE 2-19-09



Project Manager

_____ District 5 Pre-Construction Engineer

The concept as presented herein and submitted for approval is consistent with that which is included in the Regional Transportation Improvement Program (RTP) and/or the State Transportation Improvement Program (STIP).

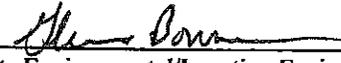
DATE _____

State Transportation Planning Administrator

DATE _____

Office of Financial Management Administrator

DATE 3/3/09


_____ State Environmental/Location Engineer

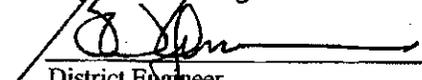
DATE _____

State Traffic Safety and Design Engineer

DATE _____

Project Review Engineer

DATE 2/19/08


_____ District Engineer

Concept Report

DEPARTMENT OF TRANSPORTATION STATE OF GEORGIA

District 5 Office

Project Number:

County:

Project Number: HPP-0007-00(635) McIntosh Trail
(Former Project STP-0000-00(538) Coastal Georgia Greenway)

County: McIntosh

P.I. Number: 0007635

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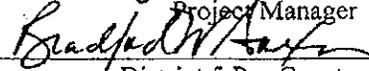
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DATE 2-19-09



Project Manager



District 5 Pre-Construction Engineer

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DATE _____

DATE 2-26-09

DATE _____

DATE _____

DATE _____

DATE 2/19/08

State Transportation Planning Administrator

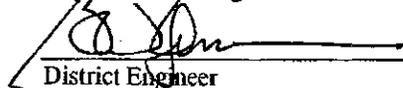


Financial Management Administrator

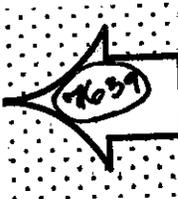
State Environmental/Location Engineer

State Traffic Safety and Design Engineer

Project Review Engineer



District Engineer



Concept Report

DEPARTMENT OF TRANSPORTATION STATE OF GEORGIA

District 5 Office

Project Number:

County:

Project Number: HPP-0007-00(635) McIntosh Trail
(Former Project STP-0000-00(538) Coastal Georgia Greenway)

County: McIntosh

P.I. Number: 0007635

Federal Route Number: N/A

State Route Number: 99

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Recommendation for approval:

DATE 2-19-09

[Signature]

DATE 2-19-09

[Signature]
Project Manager
District 5 Pre-Construction Engineer

The concept as presented herein and submitted for approval is consistent with that which is included in the Regional Transportation Improvement Program (RTP) and/or the State Transportation Improvement Program (STIP).

DATE _____

State Transportation Planning Administrator

DATE _____

Office of Financial Management Administrator

DATE _____

State Environmental/Location Engineer

DATE 3-2-09

[Signature]
State Traffic Safety and Design Engineer

DATE _____

Project Review Engineer

DATE 2/19/08

[Signature]
District Engineer

Concept Report

DEPARTMENT OF TRANSPORTATION STATE OF GEORGIA *District 5 Office*

Project Number:

County:

Project Number: HPP-0007-00(635) McIntosh Trail
(Former Project STP-0000-00(538) Coastal Georgia Greenway)

County: McIntosh

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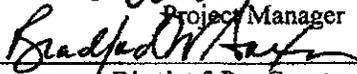
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Project Manager

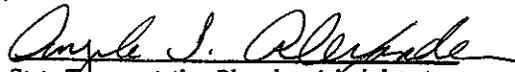
DATE 2-19-09



District 5 Pre-Construction Engineer

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DATE 3-2-09



State Transportation Planning Administrator

DATE _____

Office of Financial Management Administrator

DATE _____

State Environmental/Location Engineer

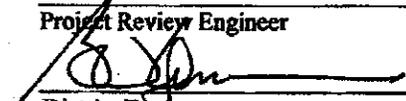
DATE _____

State Traffic Safety and Design Engineer

DATE _____

Project Review Engineer

DATE 2/19/08



District Engineer

DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA

INTERDEPARTMENT CORRESPONDENCE

FILE: P.I. No. 0007635

OFFICE: Environment/Location

PROJECT No. CSHPP-0007-00(635) / MCINTOSH
County

DATE: 3/31/09

SR 99 McIntosh Trail

FROM:


Glenn Bowman, P.E., State Environmental/Location Engineer

TO:

Genetha Rice-Singleton, Assistant Director of Preconstruction

SUBJECT: PROJECT CONCEPT REPORT REVIEW

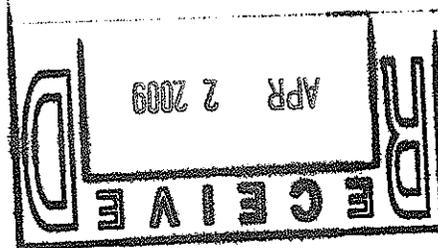
The Concept Report for the above project has been reviewed and appears satisfactory subject to the following comment:

1. To date, GDOT has not approved any special studies for this project. According to the concept report, most studies were completed in 1999 to 2001 and these must be updated. We believe that the five (5) months scheduled for environmental work (page 8) is inadequate. Please contact Jonathan Cox of this office for a more accurate estimate of time needed. *REVISED TO 12 MONTHS PER OEL.*

If you have any questions, please contact Glenn Bowman at (404) 699-4401.

GB:lc

cc: Ron Wishon
Angela Whitworth
Keith Golden
Angela Alexander
Glenn Durrence



Concept Report

DEPARTMENT OF TRANSPORTATION STATE OF GEORGIA

District 5 Office

Project Number:

County:

Project Number: HPP-0007-00(635) McIntosh Trail
(Former Project STP-0000-00(538) Coastal Georgia Greenway)

County: McIntosh

P.I. Number: 0007635

Federal Route Number: N/A

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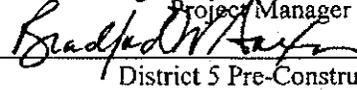
Recommendation for approval:

DATE 2-19-09



Project Manager

DATE 2-19-09



District 5 Pre-Construction Engineer

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DATE _____

State Transportation Planning Administrator

DATE _____

Office of Financial Management Administrator

DATE _____

State Environmental/Location Engineer

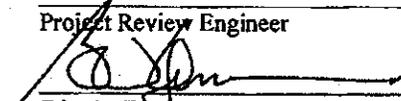
DATE _____

State Traffic Safety and Design Engineer

DATE _____

Project Review Engineer

DATE 2/19/08



District Engineer

NOTICE OF LOCATION AND DESIGN APPROVAL

Project No. HPP-0007-00(635)

P. I. NUMBER 0007635

Notice is hereby given in compliance with Georgia Code 22-2-109 that the Georgia Department of Transportation has approved the Location and Design of this project.

Date of Location Approval: JUNE 4, 2009

Date of Environmental Approval: _____

The project consists of a segment of the proposed Coastal Georgia Greenway. It includes the installation of a concrete hiking/jogging path beginning at the intersection of McIntosh Boulevard/Pine St. with S.R. 99 at M.P. 4.03 in the City of Darien and continuing north 0.30 miles along S.R. 99 to the intersection of Bond Rd. at M.P. 4.33.

Drawings or maps or plats of the proposed project, as approved, are on file and are available for public inspection at the Georgia Department of Transportation:

Bryan G. Czech
GDOT
Bryan.Czech@dot.state.ga.us
128 Public Safety Blvd.
Brunswick, GA 31525
Phone: 912-264-7247

Any interested party may obtain a copy of the drawings or maps or plats or portions thereof by paying a nominal fee and requesting in writing to:

Brad Saxon
GDOT
Brad.Saxon@dot.state.ga.us
P.O. Box 610
Jesup GA 31598
Phone: 912-427-5715

Any written request or communication in reference to this project or notice SHOULD include the Project and P. I. Numbers as noted at the top of this notice.

Concept Report

DEPARTMENT OF TRANSPORTATION STATE OF GEORGIA *District 5 Office*

Project Number:

County:

Project Number: HPP-0007-00(635) McIntosh Trail
(Former Project STP-0000-00(538) Coastal Georgia Greenway)

County: McIntosh

P.I. Number: 0007635

Federal Route Number: N/A

State Route Number: 99

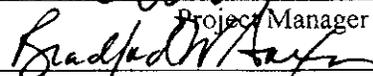
The McIntosh County Highlander Trail is a 7' wide concrete path on SR99, part of the Coastal Georgia Greenway from Savannah to St. Marys and the East Coast Greenway from Maine to Florida.

Recommendation for approval:

DATE 2-19-09

DATE 2-19-09



Project Manager

_____ District 5 Pre-Construction Engineer

The concept as presented herein and submitted for approval is consistent with that which is included in the Regional Transportation Improvement Program (RTP) and/or the State Transportation Improvement Program (STIP).

DATE _____

State Transportation Planning Administrator

DATE _____

Office of Financial Management Administrator

DATE _____

State Environmental/Location Engineer

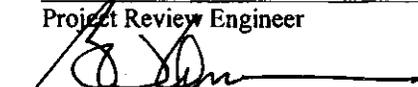
DATE _____

State Traffic Safety and Design Engineer

DATE _____

Project Review Engineer

DATE 2/19/08


_____ District Engineer

Project Map



Darien, Georgia

Need and Purpose:

At present, there are no pedestrian corridors to provide a safe means of alternative transportation outside of the City of Darien's existing trail system, although a majority of McIntosh County's citizens, of all income groups, live in the unincorporated area along or near SR 99.

Although the Fort King George State Historic Site and the Sapelo Island Visitors Center and State Ferry are McIntosh County's two primary State owned tourist attractions connected by the Altamaha Scenic Byway, there is not a safe route between them for visitor use on foot.

The Highlander Trail is a demonstration project for the proposed Coastal Georgia Greenway hiking/jogging trail from Savannah to St. Marys. Also, it is a proposed segment of the East Coast Greenway from Calais, Maine to Key West, Florida, which will benefit tourism in McIntosh County, as well as serve residents.

Description of the proposed project:

The HPP project is a pedestrian corridor demonstration project for a segment of the proposed Coastal Georgia Greenway to become a part of the East Coast Greenway. This segment consists of a concrete hiking/jogging path varying in width from four to seven feet on the East side of SR99. It begins at the intersection of SR99 and McIntosh Boulevard/Pine Street, mile 4.03 in the City of Darien. It connects to the existing City trail system and Fort King George State Historic Site by going north 0.30 mile to the intersection of SR99 and Bond Road, mile 4.33.

Construction of the trail will continue north from this point under project numbers TEE-0008-00(177) and CSTEE-0009-00(109) to connect with the McIntosh County Blue and Hall Park at the intersection of Blue and Hall Rd. and SR99. It will continue on to the Sapelo Island Visitor's Center and State Ferry at the intersection of Landing Rd. and SR99. The combined projects will be known as the Highlander Trail.

Future projects will continue the trail and connect it to the proposed Coastal Georgia Greenway/East Coast Greenway proposed routes from the North. The trail will continue along the right of way of SR99 to the McIntosh County Park at Crescent, then on to US17 at Eulonia. There it will connect to the proposed Coastal Georgia Greenway/East Coast Greenway.

Is the project located in a Non-attainment area?YesX No.

PDP Classification: Major _____ Minor X

Federal Oversight: Full Oversight (), Exempt (X), State Funded (), or Other ()

Functional Classification: Major Collector

U. S. Route Number(s): N/A State Route Number(s): 99

Traffic (AADT):

Current Year: N/A Design Year: N/A

Existing design features:

- Typical Section: *The existing route of SR99 consists of a right of way varying from 60 to 100 feet wide with an approximately 22 feet wide asphalt two-lane highway in the middle of the right of way.*
- Posted speed 35-40-45 mph Maximum degree of curvature: N/A
- Maximum grade: 2 %
- Width of right of way: 60-100 ft.
- Major structures: None
- Major interchanges or intersections along the project: None
- Existing length of roadway segment: The project begins at the intersection of SR 99 with McIntosh Boulevard/Pine St. at mile 4.03 and ends .30 mile north at the intersection of SR 99 and Bond Rd. at mile 4.33.

Proposed Design Features:

- Proposed typical section(s): *See Attachment Number 2, a seven foot wide sidewalk of four inch deep concrete with six inch deep driveway crossings and eight inch commercial driveway crossings.*
- Proposed Design Speed Mainline: 35-40-45 mph on SR99
- Proposed Maximum grade Mainline: 2% Maximum grade allowable: 2%.
- Proposed Maximum grade Side Street N/A Maximum grade allowable: N/A
- Proposed Maximum grade driveway: N/A
- Proposed Maximum degree of curve: N/A Maximum degree allowable: N/A

- Right of way
 - Width: 60-100 feet
 - Easements: Temporary (X)
 - Type of access control: Full (X)
 - Number of parcels: None Number of displacements:
 - Business: None
 - Residences: None
 - Mobile homes: None
 - Other: None
- Structures:
 - Bridges: Wooden walkways to be constructed over drainage systems.
 - Retaining walls: None
- Major intersections and interchanges: None
- Traffic control during construction: *Temporary closure of one lane of SR 99 will be necessary for short periods during construction. No detours are anticipated.*
- Design Exceptions to controlling criteria anticipated:

	<u>UNDETERMINED</u>	<u>YES</u>	<u>NO</u>
HORIZONTAL ALIGNMENT:	()	()	(X)
ROADWAY WIDTH:	()	()	(X)
SHOULDER WIDTH:	()	()	(X)
VERTICAL GRADES:	()	()	(X)
CROSS SLOPES:	()	()	(X)
STOPPING SIGHT DISTANCE:	()	()	(X)
SUPERELEVATION RATES:	()	()	(X)
HORIZONTAL CLEARANCE:	()	()	(X)
SPEED DESIGN:	()	()	(X)
VERTICAL CLEARANCE:	()	()	(X)
BRIDGE WIDTH:	()	()	(X)
BRIDGE STRUCTURAL CAPACITY:	()	()	(X)

- Design Variances: None
- Environmental concerns:

No wetlands identified in the survey of the project area by Moore Land surveying dated April 30, 1999 and incorporated into the project construction plans will be filled. Consequently, no Section 404 permit will be required. See Attachment Number 9, construction plans labeled "Coastal Georgia Greenway, McIntosh County".

Measures to prevent soil erosion have been incorporated into the design plans for construction to prevent degradation of water quality in adjacent wetlands identified in the Moore Survey.

Project Concept Report Page 6
Project Number: HPP-0007-00(635)
P.I. Number: 0007635
County: McIntosh

No hazardous waste sites were found during the same survey of the project highway right of way by Moore Land Surveying.

A survey of archeological and historical sites in the project area by New South Associates, Technical Report 701, dated Jan. 21, 1999 found three archaeological sites (9Mc393), (9Mc394), (9Mc395), and one Isolated Find (IF1). None of the three sites recorded appeared to qualify for nomination to the National Register of Historic Places, according to a review of the New South report completed by W. Ray Luce, Director of the State Historic Preservation Division dated Sept. 22, 2002 for the Coastal Georgia Greenway Trail, Section IV, McIntosh County, STP-0000-00(538): PI 0000538: TE 021112-002 which concluded "The project as proposed will have no adverse effect to archaeological resources on or eligible for listing on the National Register of Historic Places."

One historic building whose porch was partially on the SR 99 right of way in the project area was identified in the New South Report. In the same letter referenced above, the SHP director said, "HPD agrees that The Ridge Historic District is listed on the National Register of Historic Places and that the Reames Store should be considered eligible for listing in the National Register of Historic Places for the purposes of this project and review. It is HPD's opinion that the project as proposed will meet the Secretary of Interior's Standards for the Treatment of Historic Properties with no adverse effect on the historic structural resources."

Subsequently, the Reames Store building was demolished in December of 2006 by the resident of the property who lives in the adjacent house, Mrs. Mattie Reames Gladstone of The Ridge and her contractor, Mr. Wayne Hutchinson of Sterling, Ga. The adjacent property where Mrs. Gladstone lives is owned by the Reames Estate.

- Anticipated level of environmental analysis:
 - Are Time Savings Procedures appropriate? Yes (X), No (),
 - Categorical exclusion (X),

- Utility involvements:
 - Communications: Darien Telephone Company, Comcast*
 - Power: Georgia Power Company*
 - Water and Sewer: City of Darien (Inside City Limits Only).*

Project responsibilities:

- Design: McIntosh County
- Right of Way Acquisition: None. Project is entirely on State owned right of way of SR99.
- Relocation of Utilities: McIntosh County, Darien Telephone Co, Ga. Power Company, City of Darien
- Letting to contract: McIntosh County
- Supervision of construction: McIntosh County, Engineer to be selected, and GDOT.
- Providing material pits: Contractor
- Providing detours: Contractor

Coordination

- Concept Meeting held Sept. 26, 2007. See attached minutes.
- Public involvement. *Two public meetings were held: Sept. 19, 2001 to review the completed concept plan, and Jan. 30, 2002, to review the 60% completed plan.*

A wide variety of questions were asked about the proposed project and answered at the first public meeting on Sept. 19, 2001, where most participants, but not all expressed general support for the project. The most controversial issue was the possibility of various alternative routes for the trail, particularly through the area known locally as The Ridge or Ridgeville, where some residents were divided in support and opposition to the project, including whether or not it should go on the east side of SR99 through the area, be placed on the west side of SR 99 instead, or take side roads to the west around the area, or not be built at all. See Attachment Number 6.

Consequently, the second public meeting on Jan. 30, 2002, focused specifically on four alternative routes for the project through The Ridge. Participants were asked to vote for their most favored route and least favored route. 17 voted for the east side of SR99, with one person voting against it. On the separate question of routing the trail around The Ridge on roads to the west of it, 12 persons voted against alternate routes, and one person voted for them. See Attachment Number 7.

- Local government comments: *After hearing from project supporters and opponents at its regular monthly meeting of the McIntosh County Commission on April 9, 2002, the Commissioners voted unanimously 5-0 to support the project, specifically including the route on the east side of SR99 through The Ridge. See Attachment Number 8.*

- *Other projects in the area:*

STP-000-E(146) P.I. 571412, the Historic Darien Pedestrian Corridor, and the Bicycle Route & Trail Center.

- *Other coordination to date:*

- 1. On March 20, 2007, the McIntosh County Commission, by a unanimous vote of 5-0, adopted a resolution designating Mr. Jim Morrison as project liaison. With this project, he is the Point of Contact (POC) between the county, Georgia Dept. of Transportation (GDOT), and all other concerned parties. Mr. Morrison; the County Commission Chairman, Boyd Gault; and 4th District County Commissioner, Owen Green met with representatives of the GDOT's Jesup office on March 28, 2007, to begin necessary coordination for the project.*
- 2. On September 26, 2007, the Concept Meeting was held at the McIntosh County Courthouse. It was attended by representatives of the GDOT, McIntosh County, and utility owners in the project area. Minutes of the meeting are Attachment Number 3.*

Scheduling – Responsible Parties' Estimate

- Time to complete the environmental process: 12 Months
- Time to complete preliminary construction plans: 3 Months
- Time to complete right of way plans: N/A
- Time to complete the Section 404 Permit: N/A
- Time to complete final construction plans: 3 Months
- Time to complete to purchase right of way: N/A
- List other major items that will affect the project schedule: None

Other alternates considered: *(1) Not building the project, (2) Rerouting traffic on side streets, or (3) Placing the route on the west side of SR99. The east side route was selected due to widespread public support. Supporting endorsements came from the McIntosh County Bicycle Club, the Darien Lions Club, the American Legion, the Darien-McIntosh County Chamber of Commerce, the McIntosh Rotary Club, and the McIntosh County Board of Commissioners. No objections to the project were registered by state or federal environmental officials during the environmental review for the previous Project Number of STP-0000-00(538), P.I. Number 0000538, East Coast Greenway, McIntosh County.*

Comments: *At present, residents and visitors to McIntosh County are hampered by a lack of safe walkways and handicapped accessible, alternative transportation options outside of the City of Darien. This project will meet this need and will serve as a demonstration project for a portion of the Coastal Georgia Greenway from Savannah to St. Marys and the East Coast Greenway from Calais, Maine to Key West, Florida, when completed.*

Concept Report Page 9
Project Number: HPP-0007-00(635)
P.I. Number: 0007635
County: McIntosh

Attachments:

1. Cost Estimates:
 - a. Construction including E&C: \$192,100 *See Attachment Number 1.*
 - b. Right of Way: N/A
 - c. Utilities: \$14,800
2. Sketch location map: *See Page 2.*
3. Typical sections: *See Attachment number 2.*
4. Minutes of Concept meeting: *See Attachment Number 3.*
5. Project Framework Agreement: *See Attachment Number 4.*
6. Location and Design Notice. *See Attachment Number 5.*
7. Minutes of the Sept. 9, 2001 Public Meeting. *See Attachment Number 6*
8. Minutes of the Jan. 30, 2002 Public Meeting. *See Attachment Number 7.*
9. Minutes of the April 9, 2002 meeting of the McIntosh County Commissioners. *See Attachment Number 8.*
10. Construction plans, sheet Number C1, Coastal Georgia Greenway, including the survey of Moore Surveying showing no wetlands in the project area. *See Attachment Number 9.*

DEPARTMENT OF TRANSPORTATION STATE OF GEORGIA

INTERDEPARTMENT CORRESPONDENCE

FILE PROJECT No. CSHPP-0007-00(635), McIntosh **OFFICE** D5/RD
SR 99 MCINTOSH PATH BTWN DARIEN & SAPELO ISLD VISITOR CENTER

P.I. No. 0007635

DATE 2/18/2009

FROM Glenn Durrence, P.E., District 5 Engineer

TO Ron Wishon, Project Review Engineer

SUBJECT REVISIONS TO PROGRAMMED COSTS

PROJECT MANAGER Dennis Odom

MNGT LET DATE 9/1/2009

MNGT R/W DATE Select Date

PROGRAMMED COST (TPro W/OUT INFLATION)

LAST ESTIMATE UPDATE

CONSTRUCTION \$200,000

DATE Select Date

RIGHT OF WAY \$N/A

DATE Select Date

UTILITIES \$N/A

DATE Select Date

REVISED COST ESTIMATES

CONSTRUCTION* \$192,064

RIGHT OF WAY \$N/A

UTILITIES** \$14,801

* Costs contain 5% Engineering and Inspection and 1% Construction Contingencies and Fuel and Liquid AC Adjustments.

** Costs contain 30% contingency.

REASON FOR COST INCREASE Increased cost estimate and revised concept.

CONTINGENCY SUMMARY

Construction Cost Estimate:	\$181,192	(Base Estimate)
Engineering and Inspection:	\$9,060	(Base Estimate x 5 %)
Construction Contingency:	\$1,812	(Base Estimate x 1 %) (The Construction Contingency is based on the Project Improvement Type in TPro.)
Total Fuel Adjustment	\$ N/A	(From attached worksheet)
Total Liquid AC Adjustment	\$ N/A	(From attached worksheet)
Construction Total:	\$192,064	
Utility Cost Estimate:	\$11,385	
Utility Contingency:	\$3,416	30 %
Utility Total:	\$14,801	

REIMBURSABLE UTILITY COST

Utility Owner	Reimbursable Costs
_____	_____
_____	_____
_____	_____
_____	_____
_____	_____
_____	_____
_____	_____
_____	_____
_____	_____
_____	_____
_____	_____

Attachments

c: Genetha Rice - Singleton, Assistant Director of Preconstruction

Angela Whitworth, Financial Management Administrator

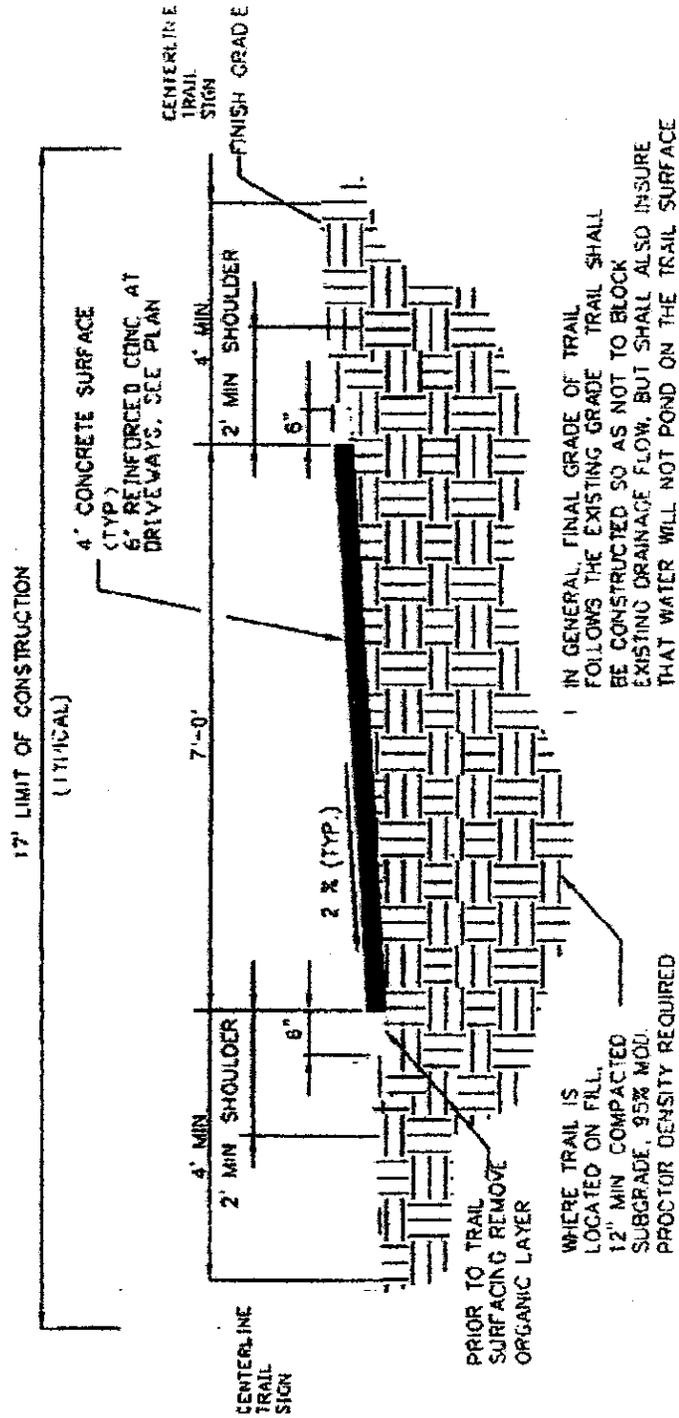
Estimate Report for file "McIntosh Blvd"

Section General					
Item Number	Quantity	Units	Unit Price	Item Description	Cost
150-1000	1	LS	2900.00	TRAFFIC CONTROL -	2900.00
163-0232	2	AC	385.22	TEMPORARY GRASSING	770.44
165-0020	835	LF	1.00	MAINTENANCE OF TEMPORARY SILT FENCE, TP B	835.00
171-0020	835	LF	3.50	TEMPORARY SILT FENCE, TYPE B	2922.50
201-1500	1	LS	4650.00	CLEARING & GRUBBING -	4650.00
210-0100	1	LS	84963.00	GRADING COMPLETE -	84963.00
610-6515	1	EA	32.34	REM HIGHWAY SIGN, STD	32.34
611-5360	1	EA	140.70	RESET HIGHWAY SIGN	140.70
700-6910	2	AC	825.66	PERMANENT GRASSING	1651.32
Section Sub Total:					\$98,865.30

Section Storm Drainage					
Item Number	Quantity	Units	Unit Price	Item Description	Cost
550-1181	16	LF	50.60	STORM DRAIN PIPE, 18 IN, H 10-15	809.60
550-1241	440	LF	51.83	STORM DRAIN PIPE, 24 IN, H 10-15	22805.20
550-4218	1	EA	616.11	FLARED END SECTION 18 IN, STORM DRAIN	616.11
550-4224	4	EA	744.82	FLARED END SECTION 24 IN, STORM DRAIN	2979.28
668-2200	4	EA	3135.95	DROP INLET, GP 2	12543.80
Section Sub Total:					\$39,753.99

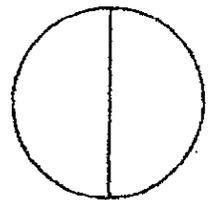
Section Pavement					
Item Number	Quantity	Units	Unit Price	Item Description	Cost
441-0104	1075	SY	32.82	CONC SIDEWALK, 4 IN	35281.50
441-0106	121	SY	52.00	CONC SIDEWALK, 6 IN	6292.00
652-9001	444	SY	2.25	TRAFFIC STRIPE, WHITE	999.00
Section Sub Total:					\$42,572.50

Total Estimated Cost: \$181,191.79



TYPICAL TRAIL CROSS SECTION

NOT TO SCALE



Attachment # 3

MINUTES OF THE CONCEPT MEETING

Project No. CSHPP-0007-00(635) County McIntosh

P.I. NUMBER 0007635

DATE: 9-26-2007

The meeting began at 10:00 A.M. at the McIntosh County Courthouse, at which time the description of the project was read, and the main headings from the Project Concept Report were covered. During this time a sign in sheet was passed around for those in attendance to sign.

Those in attendance: See Attachment

Need & Purpose, description of project, Capacity: Pedestrian Corridor or Bikeway to provide a safe means of alternative transportation outside of the City of Darien's existing trail system, although a majority of McIntosh County's citizens of all income groups in the unincorporated area live along or near SR 99. This specific project will address the first mile of the county's plan for the pedestrian corridor.

Environmental Issues

An environmental assessment has been conducted in the past, but a re-evaluation will need to be done prior to the letting of this project. Remove all notes about environmental issues that do NOT have anything to do with Phase I.

Right of Way – Parcels and relocations: None needed; Possible temporary or permanent easements only.

Hazardous waste sites or USTS: none

Maintenance Problems: none anticipated

Constructability: no problems anticipated

Utilities: Comcast, Atlanta Gas Light, Darien Telephone, GA Power Co., City of Darien (water and sewer); Plans need to be sent to all utility companies as soon as possible once the concept report has been approved. Also our utility department wants to get the electronic files from the engineering firm that drew up the plans.

From the meeting on 9/27/2007: comments from the meeting

It was understood that this project will be reduced in size to encompasses only the original Phase I. Project limits for the HPP project to end at Ashantilly Road, and the TE project to end at Blue and Hall Road. This needs to be specifically outlined in the concept report.

All reference to the Bike Route needs to be removed from the concept report, because the shoulder of the road will serve as a bike route. The GADOT traffic ops department (Cynthia Phillips) has concerns with no bike route being available. SR99 currently has no shoulder but new trail is not set up for cyclists.

The GADOT district construction office recommends to end this project at Ashantilly Road due to clear-zone issues for pedestrians.

Functional classification needs to be changed to a Major Collector.

Signing for this one mile is not included in the county's cost estimate. Grant money will be used to sign the project and the county feels there is not a problem getting this money.

On the typical sections sheet: use the typical from the plans with a select number of typicals, instead of just the one typical section.

Adjust Scheduling.

Additional Comments added on 10/3/07:

To not acquire R/W or Easements from property owners: It is suggested that from Bond Road up to the driveway prior to Andrews Cemetery Road, the walkway be shifted in toward the Edge of Pavement by 2 feet. By doing this, construction of the walkway and final grading of the shoulder can be completed without encroaching on a property owner.

All drainage pipes with percent grades on pipes as well as flow lines need to be shown under driveways. This needs to be done so that there is no water backing up over the state route in the case of heavy rain. No ponding of rain water needs for occur in ditches. Ditch x-sections are recommended.

On the typical section sheet: Show no swelling or ditching in the 5' distance between the EOP of the State Route and the pedestrian Walkway needs to be shown. By doing this, and keeping the 5% slope of the shoulder uniform, no yard drains will be needed. The walkway needs to be on the same 5% slope as the 5' grassed sections and inset into the ground so that water can sheet off of the roadway, over the walkway, and into the ditch behind the walkway. The typical section also needs to be changed to show the shoulder and walkway on a 5% slope. This is ONLY in the section where there is a 5' MINIMUM distance between the edge of pavement and the walkway. This is PER the ADA guideline booklet. As long as we are doing this in a 500' section or less, it is permissible.

Sign In Sheet (Please Print)

Name	Office	Phone	Email
1. DENNIS ODOM	GDOT-JESUP	912-427-5716	DENNIS.ODOM@DOT.STATE.GA.US
2. Cynthia Phillips	GDOT-T.O.	912-427-5767	Cynthia.Y.Phillips@DOT...
3. Rebecca Thigpen	GDOT-RoadDesign	912-427-5794	Rebecca.Thigpen@dot.state.ga.us
4. Wright Aldridge	MAAI	770-263-5945	waldridge@maai.net
5. Tom Frankie	GDOT	912-638-7555	JTfranklin@bellsouth.net
6. Jeffery Young	GDOT Location	912-370-2711	Jeffery.Young@Dot.State.GA.US
7. GEORGE SHANK	GDOT UTILITIES	912 427 5859	GEORGE.SHANK@DOT.STATE.GA.US
8. Jason F. Coley	GPC-Darien	912-437-4419	JFCOLEY@Sathewco.com
9. Xev Miller	Darien Tel	912-437-6621	KM2@darienTel.net
10. RONALD SMITH	DARIEN Tel co	912-437-7020	Ronnie.Smith@Dariencommunications.com
11. JEROME SHEPHERD	GDOT-JESUP	(912) 427-5760	
12. ANTHONY COOK	GDOT JESUP	(912) 427-5797	
13. Jo Hickson	Coastal GA Greenway (prior designer)	912-236-2225	johickson@comcast.net
14. Boyd Gault	MCINTOSH Co	437-6312-437-6671	
15. Luther Smart	MCINTOSH Co	437-6671	
16. JIM MORANSON	MCINTOSH Co	437-6808	WJamesmoranson@eol

Boyd Gault, Chairman
Nathaniel Grovner, Vice-Chairman
Charles Jordan
Steve Jessup
Owen Greene

Attachment # 4

James Coppage
County Attorney



McIntosh County Board of Commissioners

P.O. Box 584 • 310 North Way • Darien, Georgia 31305 • 912-437-6671 • FAX 912-437-6416
Ms. Teresa A. Scott Oct. 24, 2008
District Planning & Programming Engineer
Georgia Department of Transportation
P.O. Box 610
Jesup, GA 31598

RE: CSHP-0007-00(635), McIntosh County, PI 0007635

Dear Ms. Scott:

As you requested in your letter of March 28, 2007 to former county commission chairman George J. Deverger, Jr., enclosed are three copies of the Project Framework Agreements signed by our current county commission chairman Boyd Gault.

Our proposed project schedule requested in your "Attachment B" is as follows for Deadlines for Responsible Parties:

Execute Agreement: Oct. 21, 2008

Approve Concept: November, 2008

Approve Environmental Document: April, 2009

Authorize Right of Way Funds: Not Applicable

Authorize Construction Funds: July, 2009

As you also requested, this is to certify that I have attended and completed the Georgia Department of Transportation's Plan Development Training Course in Atlanta on August 14-16, 2007 as you requested. No other county employees or consultants are involved in the project.

Sincerely yours,

Jim Morrison
Project Liaison, McIntosh-Highlander Trail



Department of Transportation

HAROLD E. LINNENKOHL
COMMISSIONER
(404) 656-5206

DAVID E. STUDSTILL, JR., P.E.
CHIEF ENGINEER
(404) 656-5277

State of Georgia
#2 Capitol Square, S.W.
Atlanta, Georgia 30334-1002

March 28, 2007

LARRY E. DENT
DEPUTY COMMISSIONER
(404) 656-5212

EARL L. MAHFUZ
TREASURER
(404) 656-5224

Honorable George J. Deverger, Jr., Commission Chairman
McIntosh County Board of Commissioners
Post Office Box 584
Darien, Georgia 31305

RE: CSHPP-0007-00(635), McIntosh County, PI 0007635

Commissioner Deverger:

Attached are three Project Framework Agreements (PFA's) for your review and execution. These agreements are needed in order to authorize funding for the enhancement project listed above. Please execute all three agreements and return to Georgia Department of Transportation, Attn: Teresa Scott, Post Office Box 610, Jesup, Georgia 31598.

Also, the Sponsor is required to submit a schedule of activities for the project. Please use the last page of this agreement as a guide; and if you have any questions regarding any of the above, please contact Teresa at (912) 427-5788.

Yours Very Truly,

For: Glenn W. Durrence
District Engineer

A handwritten signature in black ink, appearing to read "Glenn W. Durrence", is written over a horizontal line.

By: Teresa A. Scott
District Planning & Programming Engineer

GWD:AJC:TAS:tas

CC: Bryan Czech, Area Engineer, Brunswick
File

AGREEMENT
BETWEEN
DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA
AND
McIntosh County
FOR
TRANSPORTATION FACILITY IMPROVEMENTS

This Framework Agreement is made and entered into this ____ day of _____, 200_, by and between the DEPARTMENT OF TRANSPORTATION, an agency of the State of Georgia, hereinafter called the "DEPARTMENT", and the McIntosh County, acting by and through its Board of Commissioners, hereinafter called the "LOCAL GOVERNMENT".

WHEREAS, the LOCAL GOVERNMENT has represented to the DEPARTMENT a desire to improve the transportation facility described in Attachment A, attached and incorporated herein by reference and hereinafter referred to as the "PROJECT"; and

WHEREAS, the LOCAL GOVERNMENT has represented to the DEPARTMENT a desire to participate in certain activities including the funding of certain portions of the PROJECT and the DEPARTMENT has relied upon such representations; and

WHEREAS, the DEPARTMENT has expressed a willingness to participate in certain activities of the PROJECT as set forth in this Agreement; and

WHEREAS, the Constitution authorizes intergovernmental agreements whereby state and local entities may contract with one another "for joint services, for the provision of services, or for the joint or separate use of facilities or equipment; but such contracts must deal with activities, services or facilities which the parties are authorized by law to undertake or provide." Ga. Constitution Article IX, §III, ¶I(a).

NOW THEREFORE, in consideration of the mutual promises made and of the benefits to flow from one to the other, the DEPARTMENT and the LOCAL GOVERNMENT hereby agree each with the other as follows:

1. The LOCAL GOVERNMENT shall contribute to the PROJECT by funding all or certain portions of the PROJECT costs for the preconstruction engineering (design), all reimburseable utility relocation costs, right of way acquisitions and construction, as specified in Attachment A, attached hereto and incorporated herein by reference. Expenditures incurred by the LOCAL GOVERNMENT and eligible for reimbursement by the DEPARTMENT shall not be considered reimbursible to the LOCAL GOVERNMENT until the LOCAL GOVERNMENT receives a written notice to proceed for each phase of the PROJECT.

2. The DEPARTMENT shall contribute to the PROJECT by funding all or certain portions of the PROJECT costs for the preconstruction engineering (design) activities, right of way acquisitions or construction as specified in Attachment A.

3. It is understood and agreed by the DEPARTMENT and the LOCAL GOVERNMENT that the funding portion as identified in Attachment "A" of this Agreement only applies to the Preconstruction Engineering Activities. The Right of Way and Construction funding estimate levels as specified in Attachment A are provided herein for planning purposes and does not constitute a funding commitment for right of way and construction activities. The DEPARTMENT will prepare LOCAL GOVERNMENT Specific Activity Agreements for applicable Right of Way and Construction when appropriate.

Further, the LOCAL GOVERNMENT shall be responsible for repayment of any expended federal funds, if the PROJECT does not proceed forward to completion due to a lack of available funding in future Project phases.

4. The LOCAL GOVERNMENT shall be responsible for all costs for the continual maintenance of the project and the continual operations of any and all sidewalks and the grass strip between the curb and gutter and the sidewalk within the PROJECT limits.

5. Both the LOCAL GOVERNMENT and the DEPARTMENT hereby acknowledge that Time is of the Essence. It is agreed that both parties shall adhere to the schedule of activities currently established in the approved Transportation

Improvement Program/State Transportation Improvement Program (TIP/STIP). Furthermore, all parties shall adhere to the detailed project schedule as approved by the DEPARTMENT, attached as Attachment B and incorporated herein by reference. In the completion of respective commitments contained herein, if a change in the schedule is needed, the LOCAL GOVERNMENT shall notify the DEPARTMENT in writing of the proposed schedule change and the DEPARTMENT shall acknowledge the change through written response letter; provided that the DEPARTMENT shall have final authority for approving any change.

If, for any reason, the LOCAL GOVERNMENT does not produce acceptable deliverables in accordance with the approved schedule, the DEPARTMENT reserves the right to delay the project's implementation until funds can be re-identified for construction or right of way, as applicable.

6. The LOCAL GOVERNMENT shall certify that they have read and understands the regulations for "CERTIFICATION OF COMPLIANCES WITH FEDERAL PROCUREMENT REQUIREMENTS, STATE AUDIT REQUIREMENTS, AND FEDERAL AUDIT REQUIREMENTS" and will comply in full with said provisions.

7. The LOCAL GOVERNMENT shall accomplish all of the design activities for the PROJECT. The design activities shall be accomplished in accordance with the DEPARTMENT's Plan Development Process, the applicable guidelines of the American Association of State Highway and Transportation Officials, hereinafter referred to as "AASHTO", the DEPARTMENT's Standard Specifications Construction

of Transportation Systems, the DEPARTMENT's Plan Presentation Guide, PROJECT schedules, and applicable guidelines of the DEPARTMENT. The LOCAL GOVERNMENT responsibility for design shall include, but is not limited to the following items:

a. Prepare the PROJECT concept report in accordance with the format used by the DEPARTMENT. The concept for the PROJECT shall be developed to accommodate the future traffic volumes as generated by the LOCAL GOVERNMENT as provided for in paragraph 7b and approved by the DEPARTMENT. The concept report shall be approved by the DEPARTMENT prior to the LOCAL GOVERNMENT beginning further development of the PROJECT plans. It is recognized by the parties that the approved concept may be modified by the LOCAL GOVERNMENT as required by the DEPARTMENT and re-approved by the DEPARTMENT during the course of design due to public input, environmental requirements, or right of way considerations.

b. Develop the PROJECT base year (year facility is expected to be open to traffic) and design year (base year plus 20 years) traffic volumes. This shall include average daily traffic (ADT) and morning (am) and evening (pm) peak hour volumes. The traffic shall show all through and turning movement volumes at intersections for the ADT and peak hour volumes and shall indicate the percentage of trucks expected on the facility.

c. Validate (check and update) the approved PROJECT concept and prepare a PROJECT Design Book for approval by the DEPARTMENT prior to the beginning of preliminary plans.

d. Prepare environmental studies, documentation, and reports for the PROJECT that show the PROJECT is in compliance with the provisions of the National Environmental Protection Act and Georgia Environmental Protection Act, as appropriate to the PROJECT funding. This shall include any and all archaeological, historical, ecological, air, noise, underground storage tanks (UST), and hazardous waste site studies required as well as any environmental reevaluations required. The LOCAL GOVERNMENT shall submit to the DEPARTMENT all environmental documents and reports for review and approval by the DEPARTMENT and the FHWA.

e. Prepare all public hearing and public information displays and conduct all required public hearings and public information meetings in accordance with DEPARTMENT practice.

f. Perform all surveys, mapping, soil investigation studies and pavement evaluations needed for design of the PROJECT.

g. Perform all work required to obtain project permits, including, but not limited to, US Army Corps of Engineers 404 and Federal Emergency Management Agency (FEMA) approvals. These efforts shall be coordinated with the DEPARTMENT. As part of the design an environmental assessment will be conducted, which may or may not result in environmental impacts of the surrounding area. If it is determined that there will be impacts that will require mitigation, then it will be the responsibility of the LOCAL GOVERNMENT to resolve the matter.

h. Prepare the PROJECT drainage design including erosion control plans and the development of the hydraulic studies for the Federal

Emergency Management Agency Floodways and acquisition of all necessary permits associated with the drainage design.

i. Prepare traffic studies, preliminary construction plans including a cost estimate for the Preliminary Field Plan Review, preliminary and final utility plans, preliminary and final right of way plans, staking of the required right of way, and final construction plans including a cost estimate for the Final Field Plan Review, erosion control plans, lighting plans, traffic handling plans, and construction sequence plans and specifications including special provisions for the PROJECT.

j. Provide certification, by a Georgia Registered Professional Engineer, that the construction plans have been prepared under the guidance of the professional engineer and are in accordance with AASHTO and DEPARTMENT guidelines.

k. Failure of the LOCAL GOVERNMENT to follow the DEPARTMENT's Plan Development Process will jeopardize the use of Federal funds in some or all of the categories outlined in this Agreement, and it shall be the responsibility of the LOCAL GOVERNMENT to make up the loss of that funding.

8. All Primary Consultant firms hired by the LOCAL GOVERNMENT to provide services on the PROJECT shall be prequalified with the DEPARTMENT in the appropriate area-classes. The DEPARTMENT shall, on request, furnish the LOCAL GOVERNMENT with a list of prequalified consultant firms in the appropriate area-classes. Any Consultant hired by the Local Government to perform work on

the Project must be compliant to applicable state and federal regulations relating to the procurement of design services in accordance with the Brooks Architect-Engineers Act of 1972, better known as the Brooks Act.

9. The PROJECT construction and right of way plans shall be prepared in English units.

10. All drafting and design work performed on the project shall be done utilizing the DEPARTMENT's latest approved software respectively, and shall be organized as per the Department's guidelines on electronic file management.

11. The DEPARTMENT shall review and has approval authority for all aspects of the PROJECT provided however this review and approval does not relieve the LOCAL GOVERNMENT of its responsibilities under the terms of this agreement. The DEPARTMENT will work with the FHWA to obtain all needed approvals as deemed necessary with information furnished by the LOCAL GOVERNMENT.

12. The LOCAL GOVERNMENT shall be responsible for the design of all bridge(s) and preparation of any required hydraulic and hydrological studies within the limits of this PROJECT in accordance with the DEPARTMENT's policies and guidelines. The LOCAL GOVERNMENT shall perform all necessary survey efforts in order to complete the design of the bridge(s) and prepare any required hydraulic

and hydrological studies. The final bridge plans shall be incorporated into this PROJECT as a part of this Agreement.

13. The LOCAL GOVERNMENT shall follow the DEPARTMENT's procedures for identification of existing and proposed utility facilities on the PROJECT. These procedures, in part, require all requests for existing, proposed, or relocated facilities to flow through the DEPARTMENT's Project Liaison and the District Utilities Engineer.

14. The LOCAL GOVERNMENT shall address all railroad concerns, comments, and requirements to the satisfaction of the DEPARTMENT.

15. If the right of way phase is 100% local funding with no Federal or State reimbursement, upon the DEPARTMENT's approval of the project right of way plans, verification that the approved environmental document is current, which shall mean that the approval of the environmental document occurred within six (6) months of the approval notice by the DEPARTMENT's for project right of way plans, and delivery of a written notice to proceed, the LOCAL GOVERNMENT may proceed with the acquisition of the necessary right of way for the PROJECT. If the right of way phase involves federal and/or state funding reimbursement, upon the Department's approval of the project right of way plans, the Local Government may proceed with all pre-acquisition right of way activities, however, property negotiation and acquisition cannot commence until right of way funding authorization is approved. Right-of-way costs will include mitigation required for Section 404 of the

Clean Water Act permits, if needed. These permits, issued by the US Army Corp of Engineers, regulate the discharge of degraded and/or fill material into jurisdictional waters of the US. Right of way acquisition shall be in accordance with the law and the rules and regulations of the FHWA including, but not limited to, Title 23, United States Code; 23 CFR 710, et. Seq., and 49 CFR Part 24 and the rules and regulations of the DEPARTMENT and in accordance with the "Contract for the Acquisition of Right of Way" to be prepared by the Office of Right of Way and executed between the LOCAL GOVERNMENT and the DEPARTMENT prior to the commencement of any right of way activities. Failure of the LOCAL GOVERNMENT to adhere to the provisions and requirements specified in the acquisition contract may result in the loss of Federal funding for the PROJECT and it will be the responsibility of the LOCAL GOVERNMENT to make up the loss of that funding. In the event the LOCAL GOVERNMENT is to receive reimbursement of all or part of the acquisition funding, reimbursable right of way costs are to include land and improvement costs, property damage values, relocation assistance expenses and contracted property management costs. Non reimbursable costs include administrative expenses such as appraisal, consultant, attorney fees and any in-house property management or staff expenses. All required right of way shall be obtained and cleared of obstructions, including underground storage tanks, prior to advertising the PROJECT for bids. The LOCAL GOVERNMENT shall further be responsible for making all revisions to the approved right of way plans, as deemed necessary by the DEPARTMENT, for whatever reason, as needed to purchase the required right of way.

16. Upon completion and approval of the PROJECT plans, certification that all needed rights of way have been obtained and cleared of obstructions, and certification that all needed permits for the PROJECT have been obtained by the LOCAL GOVERNMENT the PROJECT shall be let for construction. The DEPARTMENT, unless shown otherwise on Attachment A, shall be solely responsible for securing and awarding the construction contract for the PROJECT.

17. The LOCAL GOVERNMENT shall review and make recommendations concerning all shop drawings prior to submission to the DEPARTMENT. The DEPARTMENT shall have final authority concerning all shop drawings.

18. The LOCAL GOVERNMENT agrees that all reports, plans, drawings, studies, specifications, estimates, maps, computations, computer diskettes and printouts, and any other data prepared under the terms of this Agreement shall become the property of the DEPARTMENT if required. This data shall be organized, indexed, bound, and delivered to the DEPARTMENT no later than the advertisement of the PROJECT for letting. The DEPARTMENT shall have the right to use this material without restriction or limitation and without compensation to the LOCAL GOVERNMENT.

19. The LOCAL GOVERNMENT shall be responsible for the professional quality, technical accuracy, and the coordination of all designs, drawings, specifications, and other services furnished by or on behalf of the LOCAL GOVERNMENT pursuant to this Agreement. The LOCAL GOVERNMENT shall

correct or revise, or cause to be corrected or revised, any errors or deficiencies in the designs, drawings, specifications, and other services furnished for this PROJECT. Failure by the LOCAL GOVERNMENT to address the errors or deficiencies within 30 days shall cause the LOCAL GOVERNMENT to assume all responsibility for construction delays caused by the errors and deficiencies. All revisions shall be coordinated with the DEPARTMENT prior to issuance. The LOCAL GOVERNMENT shall also be responsible for any claim, damage, loss or expense, to the extent allowed by law that is attributable to errors, omissions, or negligent acts related to the designs, drawings, specifications, and other services furnished by or on behalf of the LOCAL GOVERNMENT pursuant to this Agreement.

This Agreement is made and entered into in FULTON COUNTY, GEORGIA, and shall be governed and construed under the laws of the State of Georgia.

The covenants herein contained shall, except as otherwise provided, accrue to the benefit of and be binding upon the successors and assigns of the parties hereto.

IN WITNESS WHEREOF, the DEPARTMENT and the LOCAL GOVERNMENT have caused these presents to be executed under seal by their duly authorized representatives.

RECOMMENDED:

MCINTOSH COUNTY

[Signature]
District Engineer

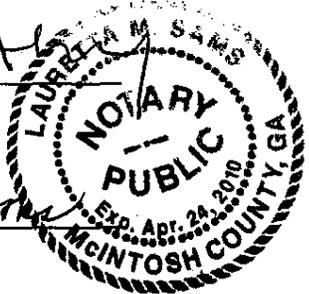
BY: [Signature]
Boyd Gault
Chairman

Preconstruction Director

Signed, sealed and delivered this 16th day of December, 2008, in the presence of:

Chief Engineer

DEPARTMENT OF TRANSPORTATION

[Signature]
Witness
[Signature]
Notary Public


BY: _____
Commissioner

This Agreement approved by Local Government, the 16th day of December, 2008.

ATTEST:

Treasurer

Attest
[Signature]
Name and Title
Lauretta M. Sams, Clerk

REVIEWED AS TO LEGAL FORM:

Office of Legal Services

FEIN: 58-6000862

ATTACHMENT "A"
Project Number: CSHPP-0007-00(635) – McIntosh County

Project (PI#, Project #Description)	Work Type	Preliminary Engineering		Right of Way		Construction		Utilities Relocation Costs by
		Funding	Design	Funding of Real Property	Acquisition & Administrative Cost by	Funding	Letting by	
PI0007635 CSHPP-0007-00(635) SR 99 McIntosh Path between Darien & Sapelo Island Visitors Center	Enhance- ment	100% County	County	100% County	County	\$160,000 – Fed \$40,000 – County >\$200,000 – County	County	100% County

Note: 1. Maximum allowable GDOT reimbursible amount may be shown above in lieu of percentages when applicable. Local Government will only be reimbursed the percentage of the accrued invoiced amounts up to but not to exceed the maximum amount indicated.
 2. Cash participation limits may be shown above in lieu of percentages when applicable.

ATTACHMENT "B"
CSHPP-0007-00(635), McIntosh County

Proposed Project Schedule

Environmental Phase						
Concept Phase						
Preliminary Plan Phase						
Right of Way Phase						

Deadlines for Responsible Parties	Execute Agreement	March/2009 (Approve Concept)	June/2009 (Approve Env. Document)	Aug/2009 (Authorize Right of Way funds)	Nov/2009 (Authorize Const. funds)
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Annual Reporting Requirements

The Local Government shall provide a written status report to the Department's Project Manager with the actual phase completion date(s) and the percent complete/proposed completion date of incomplete phases. The written status report shall be received by the Department no later than the first day of February of every calendar year until all phases have been completed.

Training Certification Requirement

The Local Government shall provide a written certification that all appropriate staff (employees and consultants) involved in the Project have attended or are scheduled to attend the Department's Plan Development Process Training Course. The written certification shall be received by the Department no later than the first day of February of every calendar year until all phases have been completed.

The Local Government shall provide a written certification that all appropriate staff (employees and consultants) involved in the Project have attended or are scheduled to attend the Department's Plan Development Process Training Course. The written certification shall be received by the Department no later than the first day of February of every calendar year until all phases have been completed.

NOTES FROM THE SEPTEMBER 19, 2001 MCINTOSH COUNTY MEETING TO REVIEW THE 30% COMPLETED CONCEPT PLAN FOR A TRAIL FROM DARIEN TO MERIDIAN ALONG HIGHWAY 99

Jo Hickson (HHA) convened the meeting at 4:10 p.m. She indicated that McIntosh County and the Center each donated \$1,000 in matching funds toward this GDOT Transportation Enhancement Project, the Coastal Georgia Greenway—Multi-Purpose Trails. The total project funding of \$501,000 is for design and limited acquisition and construction work in five coastal counties. The part of the project to be discussed at this meeting is for design of the 7.15-mile trail from McIntosh Blvd to Landings Road.

Twenty-four people attended and nine letters were hand delivered at the meeting. Four of these were opposed to a paved trail through the Ridge. Three were opposed to the greenway in general. One from a McIntosh County Commissioner stated that the West side should be considered or an alternate route found through the Ridge and his opposition to the paved trail on the East Side of SR 99 through the Ridge. However, the Commissioner otherwise supported the trail project. Finally a letter from the County Attorney indicated that he did think there was sufficient room on either the east or West side of the road through the Ridge. He asked for clarification of the location of the proposed Greenway through McIntosh County, specifically through the Ridge. These letters will be placed with the minutes and concept plans as the record of comments received about the McIntosh Co. Trail.

The EPA Challenge Grant obtained by the Center for a Sustainable Coast funded previous meetings held in McIntosh County. That grant produced the topographic, wetland and archaeological surveys being utilized in this project as base information. Jo Hickson read a summary of the comments from the previous public meeting.

Jo Hickson stated that written comments could be sent to her by 9/26/01. Her address was written on a flip chart, and is on the handout information.

The previous meeting's comments, information from this meeting, and any written comments will be given to the design firm, Clemmons Engineers, for their use to update the Concept Plans. GDOT has also received the Concept Plans, Categorical Exclusion Report, and Concept Report for review. McIntosh County will also review and comment on the concept plans.

Jo Hickson then presented an overview of the concept plan. The trail would be located on the east side of SR 99, between McIntosh Boulevard (where it will join the City of Darien Trail now under construction) and Landings Road, that leads to the Sapelo Island Ferry and Visitor's Center. The 7.15-mile path will be wholly within the existing right of way. The trail will be a maximum width of 7-feet and a minimum width of 3-feet. The width will vary so as to minimize impacts to trees and historic sites, including the Ridge. The trail will be composed of concrete, 4" standard depth, but 6" with reinforcement where driveways cross it. The trail will be located a minimum of 5-feet from the edge of pavement throughout its length when 7-feet wide. Where the path is 3-4-feet wide it is located 2-feet from the edge of pavement, which is standard for sidewalks. It will meander from this closest point to behind the ditch line so as to avoid existing trees, wetlands, etc. Where needed, wooden boardwalks will be utilized to cross drainageways and wetlands. One bridge is proposed to cross the creek near Landings Road.

The US Corps of Engineers has accepted the jurisdictional determination locating 12,936.3 square feet of wetlands as identified August 2, 2000, east of SR 99 within the right-of-way. A maximum of 500 square feet of these wetlands may be impacted by construction of the path as described in the concept plans. On August 7, 2000, the Corps authorized fill up to 0.3 acres (13,068 SF) of wetlands for construction of a hike /bike trail east of Georgia Highway 99.

The concept plan has been reviewed in accordance with Section 7 of the Endangered Species Act. Based on the information submitted prior to August 7, 2000, the Corps has determined that the project would have no effect on any threatened or endangered species nor any critical habitat for such species.

On January 21, 1999, New South Associates completed a Phase I Archaeological Survey of the proposed corridor. The survey recorded three archaeological sites and one Isolated Find. These sites were recorded as 9Mc393, 9Mc394, 9Mc395 and IF 1. None of these sites appear to qualify for nomination to the National Register of Historic Places. For the most part, the assemblages appear to represent opportunistic dumping rather than historic occupation. Of the sites, only 9Mc395 has the possibility to contribute to the understanding of the National Register Historic District—The Ridge. No further archaeological study of the deposits is recommended. A copy of this document was given to Mattie Gladstone for use by those in the county interested in it's history.

Following the presentation, participants inspected the concept plans, which had been spread across five tables. Some participants choose to provide comments about specific sites to Jo Hickson or Jim Feldt, the facilitator. Jo and Jim recorded these site-specific comments in pencil directly on the plans.

Comments from the Facilitated Session

Jim Feldt explained that he was there as the facilitator to manage the comment period. He set some simple ground rules for the session. Then he opened the session for comments. He continually sought comments from all participants by calling on those who had not previously said anything before returning to those who had already commented.

The section that follows reports all of the comments that were heard. The comments have been organized around themes rather than being presented in the order in which they were heard. The themes were teased out form the comments and appear in italics above the comments. Themes have been ordered so that those containing more individual comments appear first.

The final section reports thoughts about possible names for the trail.

Routing and alternative routes:

- The plan appears to include disturbances to property. It calls for fences to be moved. This does not seem to be fair given that people have already lost property to the right of way, especially in the Carnigan area. Project Manager response: the trail is located entirely in the right of way.
- Any fences that are called to be moved are located in the right of way. These may have been

fences that had been built before the right of way was designated.

- Were alternatives, such as the use of the Old Ridge Road, ever looked at? (Project Manager note: alternative corridors were identified and SR 99 proved to be the most viable, for most of its length the ROW is 100' and it is in public ownership. Swamp Road may be utilized in the future to link from SR 99 to US 17. The old access road to the Sapelo Visitor's Center was investigated and ruled out. However, an attendee indicated a contact person at the Sapelo Island Visitor's Center, owned by the Georgia Department of Natural Resources, who may be able to determine the advisability of using this route as an alternate on a small loop. (Following the meeting the Project Manager contacted GDNR who advised that part of this route is in private ownership.) The Project Manager is unaware of which road was called Old Ridge Road, but suspects that it was the former rail corridor. She will follow-up on these matters.
- Could the railroad right of way be used for the trail? (Jo Hickson indicated that the railroad right of way is used for vehicular traffic and is not wide enough to add a trail.)
- What was the rationale for placing the trail on the east side of the road? (Jo Hickson indicated that the rationale was safety, that is, to minimize or eliminate the trail having to cross from one side of the road to another. Whenever a trail crosses a road there are safety problems, especially if the trail crosses at locations other than an intersection.)
- At the Hudson Cemetery some of the graves are quite close to the road so there is very little right of way. (Project Manger note: this may mean that the graves in the cemetery are quite close to the Right of Way, the historic and archaeological survey did not identify impacts to the cemetery from construction of the trail.)
- By having the trail on the east side of Highway 99, users will have easy and safe access to side roads that lead into neighborhoods and to the marsh.
- With the trail on the east side of the road some mailboxes will have to be moved. If the trail were on the west side of the road, there would be no problem with mailboxes.

Historic/aesthetic character and the Ridge:

- It is my hope that for now and into the future that there will be no thought of widening Route 99. The plans should keep to the scale of a two lane meandering road.
- The plans and construction should keep tree preservation as a foremost concern, especially through the Ridge.
- An attorney for the County indicated that the County Commission is extremely concerned about the location of the trail on the east side of the road, especially through the Ridge. When the Commission had previously heard about and approved the concept of the trail, they did not expect the trail to be located on the east side of the road. (A letter was presented to Jo Hickson stating this concern.) Project Manager note: at the conclusion of the previous public meetings utilizing the EPA grant, public comment indicated that the west side of the road through the ridge should be considered. The topographic survey was then expanded to include the west side in areas where the right-of-way is less than 80 feet (included the Ridge). The design firm has represented the trail only on the east side and being 3-feet wide, the Project Manager will direct the engineer to prepare a concept where a 3-foot wide trail is also located on the west side (or only located on the west side), if feasible.
- Personally, I really like the idea of having the bike path. At the same time I understand the concerns of the property owners in the Ridge. An alternative route through the Ridge might be a good choice.

- I am concerned that the trail will compromise the historic integrity of the Ridge. This is the county's only designated historic district. (Project Manager's Note: actually the City of Darien also contains an historic district.)
- If the trail goes through the Ridge, you might seek out Georgia Power's cooperation to bury the power lines at the same time the trail was built. (Project Manager's Note: burying utility lines would damage the root system of old trees within the right-of-way and power easement.)
- I would like for the trail to go in front of the historic houses in the Ridge, rather than along some alternate route. The proposed trail will offer a good view of the pretty fronts of these houses.

Funding and costs of construction:

- I am 99% in favor of this trail, but where will the money come from of the trail? (Jo Hickson states that funds for the design of the project have come from federal grants with only a small local matching requirement. For construction we hope that private sources will contribute during a capital campaign. State and federal funds will also be sought for construction. Funding should be a private and public effort.)
- What sorts of costs are anticipated for construction? (Jo Hickson stated that the cost estimates would be available when the plan is at the 60% completion point. A meeting is planned for January 30, 2002 in McIntosh County to review the plans at that point. A general rule of thumb that has been used is that a mile of trail will cost about \$150,000.)
- Earlier this was called a demonstration project. Is this still a demonstration project or is there another place you can point to where a public-private effort has built a trail? (Jo Hickson responded that this trail will link to the City of Darien Trail, and will connect the proposed Trail Head Building in Darien to the Sapelo Island Visitor Center. This is the original route identified as the Demonstration Project.)
- The city of Darien is now constructing sidewalks. Their experience and costs can provide information.
- Glynn County has also been re-paving much of their bike paths on St. Simons and constructing new paths in other parts of the county. They might be asked for their cost experience.

Concerns about the delays and progress:

- Are the plans dependent upon being approved by the County Commission? (Jo Hickson indicated that the County Commission would review the plans. Their approval would be needed.)
- I hope that this trail is completed while I am still alive to use it and to enjoy it.
- I am concerned that all of the questions being raised and the comments being made will further delay development of the trail. At the pace that things are going it could take twenty years. Let's just get on with things and build this trail.
- It has been three years since the concept of the trail was first presented to the County Commissioners. Why has so much time elapsed and we are only at this point in the planning? (Jo Hickson stated that progress has only been possible when grant funding has been received. Grants had to be applied for, approval given, and then funds made available.)
- Does this trail overlap the trails that Darien is building? (Jo Hickson stated that the trail will connect with Darien's and will not overlap their work.)

Safety:

- I am concerned about the trail being only two feet from the road. It does not seem to be a safe distance.
- Any trail, even one only two feet from the road, is better than having to walk or bike in the road.
- I worry about children in the Meridian area who now walk in the road to get to a neighbor's or relative's house. The trail will be safer for these children to use.
- I would much rather have the bike path than have a scenic byway. What is important is the safety of bikers and children.

Maintenance concerns:

- Has proper thought been given to what will happen to this paved trail if it is located close to the ditches? Heavy equipment that is used to maintain the ditches will have to cross the trail that is located at the edge of the road.
- How will maintenance of the trail be handled? (Jo Hickson indicated that a steering committee, comprised of representatives from the six counties and four non-profit organizations, is addressing this issue. Maintenance will probably be a shared burden with public and private support.)
- Check the county's plans and maps for the location of water lines along the roadway. Trail maintenance costs could be reduced if the paving did not have to be ripped up to get access to waterlines.

Driveways and parking:

- Some properties contain multiple driveways. The plans may not indicate that the primary drive would be paved with the 6" of concrete. (Jo Hickson indicated that the project engineer would be directed to come back to check the properties to ensure that their plan calls for the proper driveway to be paved.)
- People park their cars on the lawns around the churches along this road. The paved trail near the churches might need to be 6" deep and reinforced to support the cars rolling over the trail.

Trail on Landings Road:

- Will a path be built along Landings Road? (Jo Hickson stated that the plan is to designate this as a shared road and to place share the road signs along the roadside. A path cannot easily be placed along Landing Road because widening the causeway would impact extensive wetlands.)
- There are logging trucks and other heavy equipment using Landings Road daily. In order for hikers and bikers to safely share the road the speed limit might need to be lowered.

Signs:

- The sign for Cowhorn Road is actually located in the wrong place and should be relocated into Carnigan.

Suggestions for the name of the trail

- In preparing the Concept Plan, Theresa Wexel, project engineer for Clemmons Engineers, suggested that the trail be named the **Cowhorn Trail**, after Cowhorn Road.

- The trail could use the same name as the proposed scenic byway, that is, the Altamaha Scenic Byway. This might reduce the need for two sets of signs and support simultaneous promotion of the byway and the trail.
- The Altamaha River is something that we are proud of, but it is not near to this trail.
- A name that connects to the county's Scottish history would be nice. The name might be the Highlander Trail or the McIntosh Trail.
- The Highlander Trail has more sizzle.

(Project Manager Note: the county commission will be asked to name the trail.)

Following the comment period, there was a brief discussion of two related projects:

Karen Moore described the Better Hometown Community – Downtown Development Authority, City of Darien Scenic Byway Project: the proposed Altamaha Scenic Byway, would be the first in SE GA.

Jo Hickson briefly described the involvement of coastal SC, GA and FLA to develop a Coastal Heritage Trail from Charleston SC, through Georgia's six coastal counties, to St. Augustine, FLA. National Heritage Corridor Status will be applied for, and the area would then be promoted as a region of national heritage significance.

The meeting concluded at 6:00 p.m.

Following the meeting, Mattie Gladstone invited the Project Manager to a meeting of the county's historic society, October 2, 2001. The Project Manager agreed and suggested that the meeting become a workshop to work out issues related to The Ridge. Ms. Gladstone agreed to this format.

The following is a summary of the letters received at the meeting and during the comment period, closing 9/26/01.

7 letters were hand delivered at the meeting in opposition to the trail by landowners from the Ridge community, who could not attend the meeting.

2 letters were submitted by McIntosh County – one from the Commissioner representing the Ridge, indicates concern about the trail being located on the east side through the Ridge. He states his objection to placing the trail on the east side, but otherwise wished the project success.

The County Attorney indicated that he too thought the trail would be located on the west side through the Ridge, he thinks that there is not enough room on either side for the trail. He wishes clarification about the trail location through the Ridge. (Which was given by the Project Manager during the meeting, it is 3 feet wide, located 2 feet from the edge of pavement (like a sidewalk) on the east side, and meanders around existing trees.

Subsequent to the meeting 2 letters were received; one by a resident of the Ridge who indicated support for the trail and one from a resident of the Ridge who indicated opposition to the trail.

NOTES FROM THE JANUARY 30, 2002 MCINTOSH COUNTY MEETING TO REVIEW THE 60% COMPLETED PLAN FOR A TRAIL FROM DARIEN TO MERIDIAN ALONG STATE ROUTE 99

Jo Hickson (HHA) convened the meeting at 5:00 p.m. She first indicated that the meeting would allow for the plans to be reviewed and for input to be sought from participants at least two times. She then described the trail as being generally seven feet wide, and paved with concrete. The trail would be paved with a four-inch thick slab of concrete and would be six inches deep with reinforcing where driveways cross it or where cars would pass over it to park near churches, etc. It would begin in Darien at McIntosh Blvd., travel along the east side of Route 99, and end in Meridian near the Sapelo dock.

Jo Hickson described the four alternative routes that had been designed for the trail, as it would pass through or around Ridgeville. She explained the routes and then asked participants to rise from their seats to view the plans that had been spread across four tables in the room. When everyone had had a chance to view the plans and to seek some clarifications about the plans, the facilitator, Jim Feldt, had the first set of participants (the larger group) take their seats. The second group (the smaller group from 6:00 to 7:00) continued to stand around the plans while Jo Hickson and the facilitator conducted a more informal assessment of the plans and obtained their input.

The facilitator provided a summary of what has heard at the September 19, 2001 meeting in McIntosh County when the plans were reviewed at the 30% completion stage. He noted the following:

- There was general interest in and support for the trail from Darien to Meridian.
- The primary concerns dealt with the design for the trail, as it would pass through the Ridge. People expressed a desire to have alternate routes considered.
- There were inquiries raised about the funding to the trail and when it might be completed.
- Support for the trail was based in part on the fact that a trail would remove bikers, walkers, and children from the roadside.
- Questions were raised about who would be responsible for maintenance of the trail and whether the constructed trail would make maintenance of the ditches more difficult.
- Concerns were expressed about where the trail would cross driveways and pass in front of churches where people pull up onto the church grounds to park.

The facilitator indicated that the participants would first be asked to express a preference among the four alternate routes through the Ridge. Then there would be time to discuss any other aspects of the trail. To assess the preferred alternate routes, the facilitator had participants place sticking dots on the options. Participants who wished to participate in clarifying preferences were given two sticking dots. They were asked to place the green dot on the option that they most preferred and the yellow dot on the option that they least preferred. Once the dots had been placed among the options, the facilitator reviewed the outcome and asked participants to explain the reasons for the preference function. It should be noted that most participants attended the meeting in the 5:00 to 6:00 timeframe. Approximately three or four additional people attended between 6:00 and 7:00. These people were also asked to use dots to indicate their preferences. Later they were also shown how the earlier group had allocated their dots among the options.

Preferred Route through the Ridge

The sections that follow present the outcome of this process of clarifying the preferred route through the Ridge. In reporting the sticking dot allocation among the four alternative routes, the overall placement of dots is described, that is, the total number of dots that were placed on the alternatives is listed and not the number placed by the two groups of participants. Some participants indicated that they had no preference among the four alternate routes and decided not to place dots among the alternatives. Some participants used the dots but opted to only place a green dot to indicate their most preferred option. At least a few people said that they just wanted the trail built and that the specific route did not matter.

Alternative Routes through the Ridge	Green Dots	Yellow Dots
1.) A three to four foot wide "sidewalk" running along the east side of Rt. 99 with no damage to trees and in the right of way.	17	1
2.) A three to four foot wide "sidewalk" that would cross to the west side of Rt. 99 in Ridgeville, be located in the right of way, and then cross back to the east side of Rt. 99 on the other side of Ridgeville.		
3.) A fence would block the trail with a sign posted "Please dismount and walk your bike." The trail would be unpaved and be located in the right of way on the east side of Rt. 99. Paving would begin again on the other side of Ridgeville.		2
4.) Bicycle traffic on the trail would be directed to go around Ridgeville. This trail would pass along Blount Crossing Rd. the old railroad track, and Steadwell Rd. The roads would be marked with "share the road" signs. Bicycles would be in the roadway with traffic.	1	12

The facilitator summarized that the outcome by stating that the first alternative seemed to be the most preferred, the fourth alternative the least preferred, and that the other options fell somewhere in between these two preferences. He asked for the participants to explain the reasons for the preferences. The reasons for the preferences reflect what was heard from the two groups, that is, both what was said between 5:00 and 6:00 and between 6:00 and 7:00.

Alternative 1 is most preferred for the following reasons:

- Users of the trail would not have to cross Rt. 99 and, therefore, would be safer.
- The most direct route for the trail is to come out of Darien and stay along the east side of the road.
- This route for the trail is clearly the most scenic.
- By passing along this route, users of the trail would have easy access to side roads and trails onto which they might want to turn.
- This route would carry users of the trail through the most historic area.
- Bicyclists would not have to dismount and walk, which they may be reluctant to do.

Alternative 4 is the least preferred for the following reasons:

- Users of the trail would encounter too many loose dogs back along this route.
- The route would take people through an area that is not scenic and was said to be ugly.
- Houses along these roads are actually closer to the roadside than are the houses on Rt. 99.
- Since bicyclists would have to "share the road" with vehicle traffic along this route, it would be far less safe than staying on a trail next to Rt. 99.
- Mosquitoes could be even worse along sections of this trail.

Alternative three received two of the yellow dot for the following reason:

- As a bicyclist who would be using this trail, I do not want to interrupt my ride to dismount and walk my bike for about a quarter of the mile.
- Bicyclists would be more inclined to not dismount and to cut over and ride in the road. That would place them in a less safe area.

The facilitator asked if there were any other comments about the route of the trail through the Ridge. A participant stood and read aloud a short resolution from the Lions Club that expressed strong support for the trail being routed consistently along the east side of Rt. 99.

This same participant also vociferously advocated for the trail being wider than the planned three to four feet as it passes through Ridgeville. He stated that such a narrow pathway would make it very difficult for bicyclists traveling in opposite directions to pass one another. A trail that is seven feet wide would also allow for a pickup truck to drive down the trail to pick up litter and conduct other maintenance. There appeared to be some sentiment among the other participants in support of this assertion. The sense seemed to be that the trail should be widened as much as possible and yet not interfere with the trees along the road nor should it exceed the right of way and cross into

private property. One participant stated that she really wanted the trail and that if the only way to get it built was for the trail to be only three feet wide in some places in the Ridge, then she was willing to accept the narrower trail.

It was again stated that it is crucial that the trail not damage any trees in the area of the Ridge. Jo Hickson pointed out where the trail would narrow to accommodate trees and how alternative paving might be used in some spots to minimize adverse effects on trees. She also indicated that a raised wooden segment would be used on the trail in the Ridge to avoid harming some cedar trees.

Other Comments and Questions

With the group that met from 5:00 to 6:00, the facilitator asked if there were any other comments or questions about the trail. With the four people that attended between 6:00 and 7:00, Jo Hickson conducted a less structured discussion, which the facilitator recorded on a legal pad. The section that follows reports the comments and the questions and answers that were covered in throughout the evening. The record of what was said has been organized by topics and descriptive phrases have been placed strategically to make it easier for the reader to follow.

Regarding the location of the trail in front of Elm Grove Baptist Church and traveling to the Sapelo dock:

- Members of the Elm Grove Baptist Church in Meridian, the site of the meeting, stated that they would prefer that the trail not pass directly in front of their church. They expressed strong concerns that the users of the trail might be less safe because of the frequency with which church members would be crossing the trail to park on church grounds. They asked whether it would be possible for the trail to use either William Edmunds Road or the property owned DNR to the right side of this road as the trail progresses to the Sapelo dock.
- Other participants noted that, if the trail passed the church and users wanted to travel to the Sapelo dock, they would have to travel along Landing Road that is often used by large pulpwood trucks.
- One participant stated that William Edmunds Road, which had been used to access the dock while a bridge on Landing Rd. was being rebuilt, travels all of the way to the visitors' center at the dock and even circles the center.
- Even if the trail were to be continued on to Crescent (and there are plans for this to be done eventually), it would still be possible to do so if the trail did not pass directly in front of Elm Grove Baptist Church.

Support for completing the trail:

- Just build the trail.
- If you build it, they will come.
- This trail would add to the set of assets that McIntosh County can promote to tourists.
- In recent ratings of the most livable communities, one criteria that has been used in the presence of trails and greenways. Completing such a trail will make our county more livable and more attractive to visitors.

Question: How far will this trail go?

Answer: This portion of the trail runs from Darien to Meridian. In the future the trail may be continued on to Highway 17 in Eulonia. This trail is not part of the East Coast Greenway.

Question: What will you do where the ditches are very wide and near to the roadside?

Answer: A considerable amount of fill will be added to effectively narrow the ditch and shift the center of the ditch further away from the roadside. The trail will then be built on a widened shoulder.

Question: Is there a typical cross section of the trail that is shown in the plans.

Answer: Jo Hickson stated that the plan did include a page showing cross sections and that anyone could consult the plan.

Question: How will this trail be maintained? I am concerned because I sometimes use the trails on St. Simons Island and I do not think that they are well maintained. There is often gravel and sand across the path, fallen branches, and broken glass. All of these things are hazards to bicycle riders.

Answer: The local government (city or county) is responsible for maintenance of the trail. However, in many locations groups, such as local bicycle clubs, trails have voluntarily adopted trails. These clubs then help to maintain the trails by picking up litter and patrolling the trails to increase the safety of users. A participant

noted that in Florida the Jacksonville to Baldwin trail is a prime example of a great trail that has been adopted and cared for by volunteers.

Question: Who is funding this trail?

Answer: Work on the planning of the trail has been paid for mostly by grants that have been received in bits and pieces. Additionally, the participating partners in the trail, for example, McIntosh County, have contributed a local match.

Question: Who is it that is developing the plans?

Answer: We have established a Coastal Greenway Steering Committee.

Question: How are the members of this committee appointed?

Answer: Each partner gets to appoint a member and a supporting staff person. In McIntosh County the County Commission appointed one of the Commissioners to serve on the committee.

Question: How much money has been received to plan the trail?

Answer: For the region, that is, the five coastal counties excluding Glynn County, \$135,000 was received initially to develop the master plan. About \$247,000 was received from EPA to do surveys of the topography, wetlands, and archeology. About another \$400,000 was received from GDOT for the current planning. Again, the local participants also put in funds as the local match.

Question: Are the plans for any beautification along the trail?

Answer: Right now there are plans for some things (such as fencing) along the trail as it passes through the Ridge, but not for other sections of the trail.

Question: How wide will the trail be?

Answer: In most places it will be seven feet wide. It will narrow as it goes through Ridgeville.

Question: What is state right of way along this road?

Answer: The right of way is about 75 feet wide through the Ridge and about 100 to 110 feet wide along the rest of Rt. 99. The road is not always in the center of the right of way. There is not an equal amount from the centerline of the road to both edges of the right of way.

Question: When will the next meeting be held?

Answer: This is the last planned meeting at this point in time. The next step is to submit the plan to GDAT for their review. The McIntosh County Commission may decide to review the plan and might decide to invite the public to a meeting to review the plan.

Question: Will the plan that is going to GDOT show the three to four foot width of the trail through the Ridge?

Answer: Yes. The plans that you are looking at are the version that will be submitted to GDOT. With the submittal to GDOT we can indicate that we would like to try to widen the trail somewhat in Ridgeville.

Question: What sort of construction costs do you expect for this trail?

Answer: It will be something in the neighborhood of \$1 million. About 80% of that would be federal funding and 20% would have to come from local sources. This could be from local governments and from other sources, such as a capitol campaign.

Question: Will the state be approached for funding?

Answer: We have already gotten money from the state department of transportation. We plan to seek grant funding from the Department of Community Affairs with their One Georgia grants.

Question: Have you sought funds from the Department of Natural Resources?

Answer: No. We have not yet tried to tap them for funding. The Georgia Forestry Commission will provide some grant money for wooden bridges included in trails. We will tap every possible source, including DNR.

Attendance

Name	Address	Phone
John W. Brown	P.O. Box 7, Meridian, GA	832-5903
Ginnie Dery	274 Julienton Dr., Townsend, GA	832-3530
George C. Everson	P.O. Box 271, Darien, GA	437-4857
Judie Gill	P.O. Box 2381, Darien, GA	437-8273
Ralph Gill	P.O. Box 2381, Darien, GA	437-8273
Fred Hay	DNR, One Conservation Way, Brunswick, GA 31520	832-4580
Dee Ingram	310 Julienton Dr., Townsend, GA	832-6864
Harvey L. Jordan, Sr.	Meridian	437-4028
Christie Lambert	Nature Conservancy, P.O. Box 59, Darien, GA	437-2161
Ellie Legg	P.O. Box 667, Darien, GA	437-3737
Sissy Lingle	308 Julienton Dr., Townsend, GA	832-2340
Dr. Fred Marland	P.O. Box 636, Darien, GA	437-6382
Moses McIntosh	P.O. Box 51, Meridian, GA 31319	437-4028
Viola McIntosh	P.O. Box 51, Meridian, GA	437-4028
Jan Mackinnon	DNR, One Conservation Way, Brunswick, GA 31520	262-3048
Billy McKinnon	Darien	437-8500
Bill Merriman	Mgr., DNR, Sapelo Island Welcome Ctr.	437-3224
Chris Milner	346 Julienton Dr., Townsend, GA	832-3122
Ward Milner	346 Julienton Dr., Townsend, GA	832-3122
Elizabeth Mitchell	Rt. 3, P.O. Box 3270, Townsend, GA 31331	832-4659
Jim Morrison	Rt. 1, P.O. Box 1550, Darien, GA 31305	437-6808
Dan Myers, Sr.	P.O. Box 1199, Darien, GA	437-4916
Jimmie L. Palmer, Sr.	P.O. Box 104, Meridian, GA	437-2296
Bill Parker	Pres., Sapelo National Bank, Darien, GA	437-5252
George Parkinson	Meridian, GA	437-4243
Patty Parkinson	Meridian, GA	437-4243
Cindy Peterson	70 Fair Hope Dr., Townsend, GA	832-3445
Sandy Pharr	The Darien News, P.O. Box 496, Darien, GA	437-4251
Sammie Pinckney	P.O. Box 1986, Darien, GA	437-6138
Marilyn Proper	1322 Meadow Lane, Darien, GA	437-6786
Lillian Schaitberger	P.O. Box 1986, Darien, GA	437-4885
Blaine T. Stowasser	Meridian, GA	437-3448
Robert Stokes	Mgr., Ga. Power Co., Darien, GA	437-3081
Kaye Traer	302 Julienton Dr., Townsend, GA 31331	832-5150

Project Staff:

Helen Alexander
James Feldt
Jo Hickson

THE BOARD OF MCINTOSH COUNTY COMMISSIONERS MET IN REGULAR SESSION THIS DATE. CHAIRMAN DAVID EARL LANE PRESIDED. OTHER COMMISSIONERS PRESENT WERE GEORGE J. DEVERGER, JR., FREDRICK JACKSON, STEPHEN JESSUP AND CHARLES JORDAN.

COUNTY ATTORNEY ADAM S. POPPELL, III WAS ALSO PRESENT.

COMMISSIONER JACKSON GAVE THE INVOCATION AND CHAIRMAN LANE LED THE PLEDGE TO THE FLAG.

MINUTES OF MARCH 12, 2002 MEETING WERE APPROVED AS READ.

MRS. JO HICKSON, OF COASTAL GA. GREENWAY PROJECT, ASKED THE BOARD FOR APPROVAL OF THE FINAL PLANS FOR THE BIKE PATH. A DISCUSSION WAS HELD ON THIS TOPIC.

THE BOARD OF COMMISSIONERS LISTENED TO CITIZENS "FOR" AND "AGAINST" THE BIKE PATH ON THE EAST SIDE OF THE RIDGE. STEVE HINSON, OF THE HISTORICAL PRESERVATION COMMISSION, SPOKE FOR THE HISTORICAL PRESERVATION COMMISSION WHO WERE AGAINST THE BIKE PATH.

GEORGE EVERSON, OF ELM GROVE CHURCH, WAS ALSO AGAINST THE BIKE PATH.

MRS. MATTIE GLADSTONE WAS CONCERNED ABOUT THE HISTORY AND LANDSCAPING OF THE RIDGE AND WAS AGAINST THE BIKE PATH.

SAM GORE, RALPH GILL, PAUL MITCHELL AND JIM MORRISON WERE FOR THE BIKE PATH.

CHAIRMAN LANE WANTED TO GO ON RECORD THAT HE SUPPORTED THE EASTSIDE OF THE RIDGE FOR THE BIKE PATH.

MOTION BY COMMISSIONER JESSUP, SECONDED BY COMMISSIONER DEVERGER AND CARRIED UNANIMOUSLY TO ACCEPT THE BIKE PATH PLAN AS SUBMITTED ON THE EAST SIDE OF HIGHWAY 99.

MRS. JO HICKSON ASKED THE BOARD TO NAME THE BIKE PATH AS THE "HIGHLANDER TRAIL". NO ACTION WAS TAKEN BY THE BOARD ON THIS TOPIC.

MOTION BY COMMISSIONER DEVERGER, SECONDED BY COMMISSIONER JESSUP AND CARRIED UNANIMOUSLY TO APPROVE THE 2002-2003 BUDGET FOR THE JUDGES OF SUPERIOR COURT IN THE AMOUNT OF \$494.96 PER MONTH FOR A YEARLY TOTAL OF \$5,939.50.

MOTION BY COMMISSIONER DEVERGER, SECONDED BY COMMISSIONER JORDAN TO APPROVE THE REQUEST OF THE SHERIFF'S DEPARTMENT TO SEND 5 PEOPLE TO THE WILDERNESS SEARCH AND RESCUE TRAINING. COMMISSIONER JACKSON VOTED "YES". COMMISSIONER JESSUP VOTED

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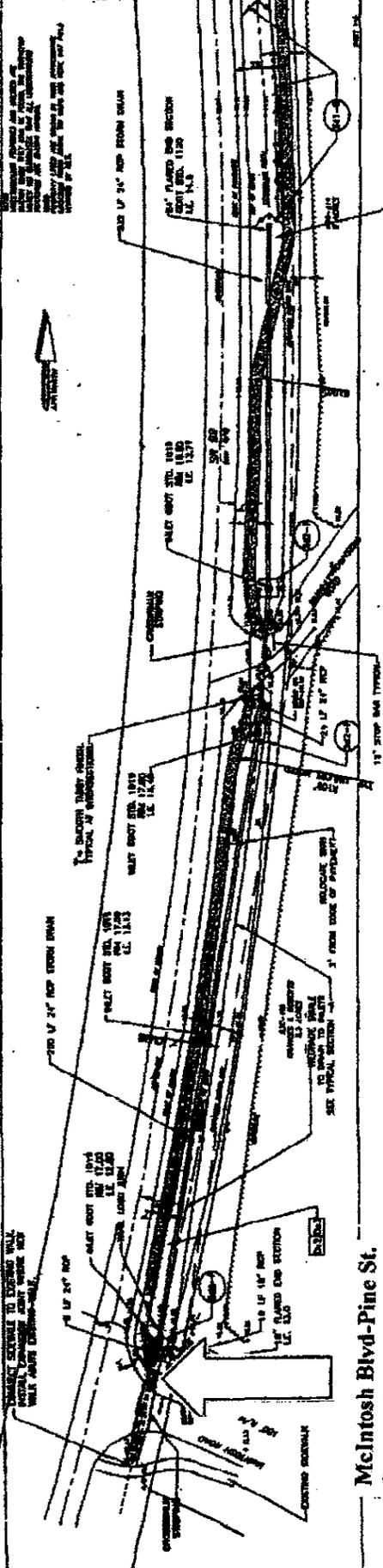
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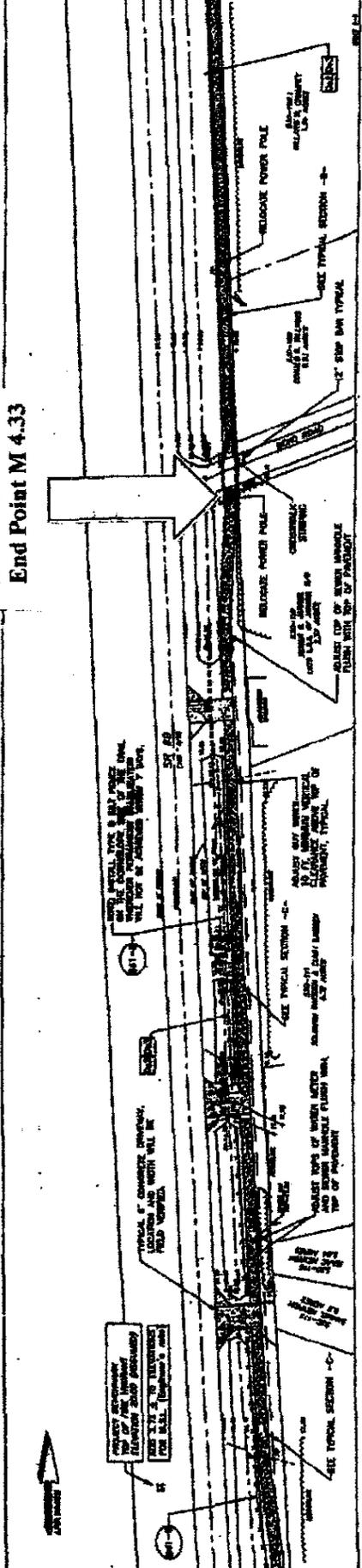
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**Bond Rd.
End Point M 4.33**



- CONSTRUCTION NOTES:**
- SEE SHEET 20 FOR CONSTRUCTION GENERAL DRAINAGE CONTROL NOTES, DRAINAGE DETAILS, AND GEODRAIN SCHEDULE.
 - SEE SHEET 21 FOR TYPICAL SECTION THROUGH TRAIL AND SUBSTRUCTURE DETAILS.
 - SEE SHEET 22 FOR BRIDGE AND PAVEMENT DETAILS.
 - SEE SHEET 23 FOR BRIDGE DRAINAGE DETAILS.
 - REINFORCING DRAWINGS: 1" CONCRETE, UNLESS NOTED OTHERWISE, SEE DETAIL SHEET 21.
 - CONCRETE: UNLESS NOTED OTHERWISE, 4000 PSI, UNLESS NOTED OTHERWISE, 4000 PSI.
 - REINFORCEMENT: UNLESS NOTED OTHERWISE, SEE DETAIL SHEET 21.
 - CONCRETE CURING: UNLESS NOTED OTHERWISE, 7 DAYS.
 - CONCRETE CURING: UNLESS NOTED OTHERWISE, 7 DAYS.

**McIntosh Blvd-Pine St.
Start Point M 4.03**

CLARKSON ENGINEERS
Civil Engineering
401 E. 10th Street
Savannah, Georgia 31405
(912) 232-4300

HINSHLEY-HICKSON ASSOCIATION
Landscape Architecture and Planning
3601 American Street
Savannah, Georgia 31405
(912) 336-0643

COASTAL GEORGIA GREENWAY
McINTOSH CO. - DARIEN TO SABLED LANDING RD.
LAYOUT PLANS

CHAULAH COUNTY BOARD OF COMMISSIONERS
144 BULL ST., P.O. BOX 5181
SAVANNAH, GEORGIA 31402