

**DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA
LIMITED SCOPE PROJECT CONCEPT REPORT**

Project Type: Multi-Use Trail P.I. Number: 0007633
 GDOT District: Three County: Muscogee
 Federal Route Number: N/A State Route Number: N/A
 Project Number: CSHPP-0007-00(633)

The South Lumpkin Road Trail-Phase II project is a grade-separated pedestrian crossing and portion of multi-use trail. It proposes a pedestrian bridge and approaches to cross US 280/SR 520/Victory Drive which is part of a larger Multi-use facility that extends from Matheson Road North to Cusseta Road. The project length is approximately 1000 feet.

Submitted for approval:

W. Heath & Lineback Engineers, Inc. 9/11/14
 Consultant Designer & Firm DATE
Columbus Consolidated Government 9/11/14
 Local Government Sponsor DATE
Albert Shelby 9-19-14
 State Program Delivery Engineer DATE
Sue Anne Decker 9.16.14
 GDOT Project Manager DATE

Recommendation for approval:

HIRAL PATEL*/EKP 10/6/2014
 State Environmental Administrator DATE
KATHY ZAHUL*/EKP 10/6/2014
 State Traffic Engineer DATE
BEN RABUN*/EKP 12/3/2014
 State Bridge Engineer DATE

THIS PROJECT IS CONSISTENT WITH THE MPO ADOPTED REGIONAL TRANSPORTATION PLAN (RTP)/LONG RANGE TRANSPORTATION PLAN (LRTP).
 The concept as presented herein and submitted for approval is consistent with that which is included in the Regional Transportation Plan (RTP) and/or the State Transportation Improvement Program (STIP).

CINDY VANDUYKE*/EKP 9/26/2014
 State Transportation Planning Administrator DATE

Approval:

Concur: Alan Bonner 12/15/14
 GDOT Director of Engineering DATE

Approve: Bill McMillan 12/17/14
 GDOT Chief Engineer DATE

* - RECOMMENDATION ON FILE

PROJECT LOCATION

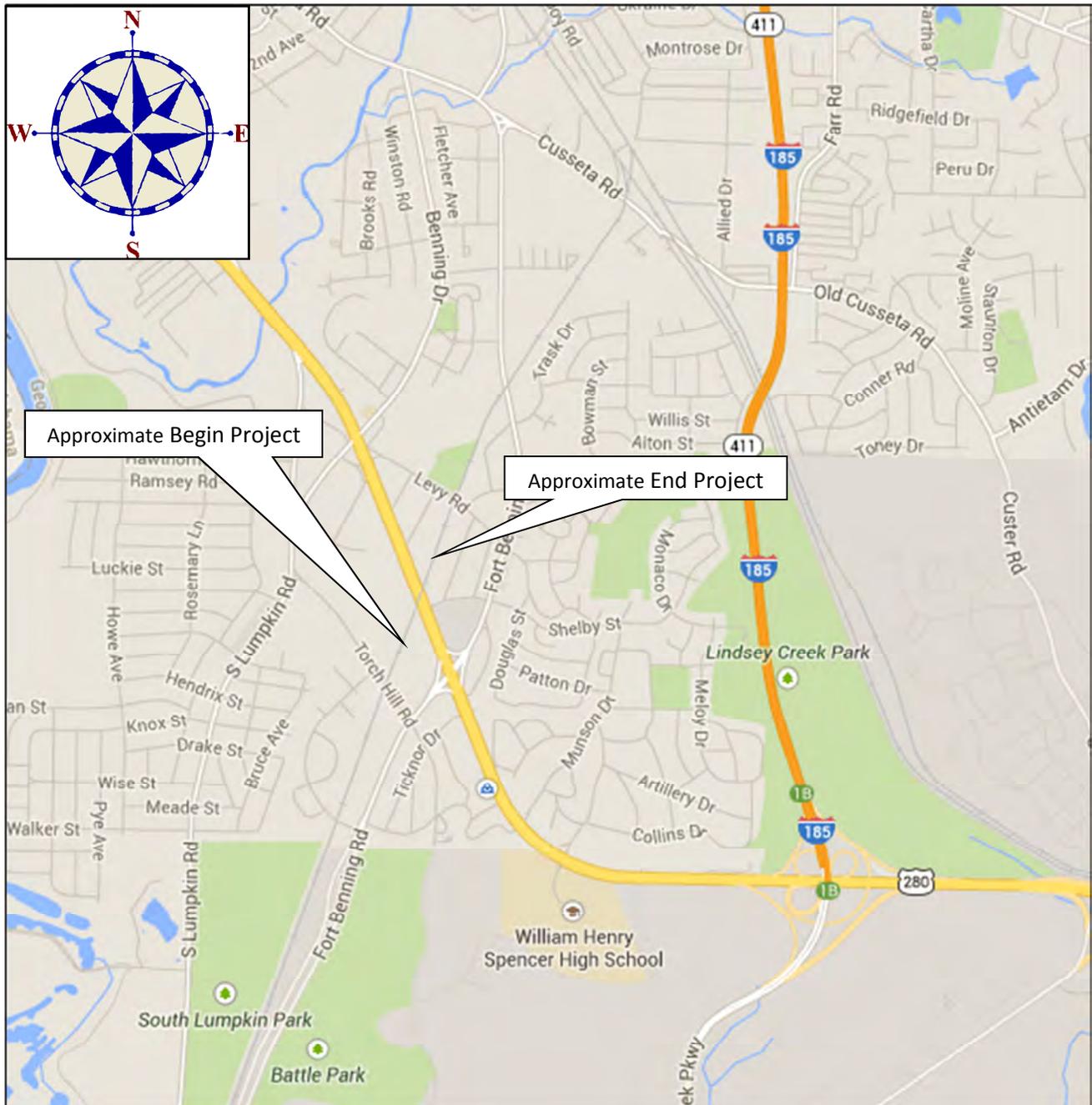


Figure 1 – Project Location Map
South Lumpkin Road Trail-Phase II
PI No. 0007633 – Muscogee County

PLANNING & BACKGROUND DATA

Project Justification Statement: This grade-separated pedestrian crossing is a necessary safety feature of the South Lumpkin Multi-Use trail system, which is a continuation of the city’s expanding rails-to-trails program, providing recreational opportunities for residents throughout the city. The alignment of the South Lumpkin Multi-Use trail crosses *US 280/SR 520/Victory Drive*, a heavily used north-south corridor between the city of Columbus and Fort Benning. Victory Drive is a six lane road with turn lanes, and a raised grassed median. At the point where the trail crosses Victory Drive there is no signalized intersection and no pedestrian crossing. An at-grade pedestrian crossing would consist of a less desirable mid-block crossing. The closest intersection to the proposed trail crossing is Fort Benning Road a distance of approximately 3,000 feet.

Existing conditions: Currently there is no pedestrian facility crossing Victory Drive. Victory Drive at the site is an urban roadway with 3 through lanes in each direction, a raised median and sidewalks. There are overhead utilities on the northwest side of the project.

Other projects in the area:

- PI 0012579 - South Lumpkin Road Multi-use Facility -TIA
PI 0012579 ties to this project (PI 0007633) on both sides
- PI 0010620 – CS 2228/Fort Benning RD from CS 325/Levy RD to Fort Benning Corridor enhancement
- PI 0011434 – CR 62/Cusseta RD from Fort Benning RD to Stanton DR Reconstruction - TIA
- PI 311445 – I-85/Columbus from SR 520 to St Marys Road Reconstruction
- Local – Fort Benning Road Streetscape Improvement
- Local – Fort Benning Road/Cusseta Road/Brennan Road Roundabout
- Local – Cusseta Road/Old Cusseta Road Roundabout

Description of the proposed project: The Victory Drive project (PI No. 0007633) is a grade-separated pedestrian crossing involving a prefabricated steel truss bridge, retaining walls and portion of approach trail that connects on both sides to the Transportation Investment Act (TIA) South Lumpkin Multi-use Facility project (PI No. 0012579). The project length is approximately 1000 feet and is located north of the intersection of Victory Drive and Shelby Street. See attachment 5 for an overall view of the South Lumpkin Multi-use Facility.

MPO: Columbus TMA

TIP #: SL07

Congressional District(s): 2

Federal Oversight: Exempt State Funded Other

DESIGN AND STRUCTURAL

Description of Proposed Project: The Victory Drive project (PI No. 0007633) is a grade-separated pedestrian crossing involving a prefabricated steel truss bridge, retaining walls and portion of approach trail.

Major Structures: Pedestrian Bridge and walls

Structure ID	Existing	Proposed
N/A	None	Bridge - 178 feet in length by 14 feet wide (inside clear width)
N/A	None	Walls – Approximately 1,200 feet of MSE walls, max height of 20 feet

Mainline Design Features: South Lumpkin Road Multi-Use Trail

Feature	Existing	Standard*	Proposed
Typical Section			
- Bike/Pedestrian Lane for Path/Bridge	N/A	10-14'	12'
- Outside Shoulder for Path	N/A	2'	2'
- Outside Shoulder for Bridge	N/A	2'	1'
- Outside Shoulder Slope	N/A	6:1 min	Varies
Posted Speed	N/A		N/A
Design Speed	N/A	18 mph	18 mph
Min Horizontal Curve Radius(recommended)	N/A	60'	40'
Maximum Superelevation Rate	N/A	2%	2%
Maximum Grade	N/A	5%	5%
Design Vehicle for Path	N/A	Bicycle	Bicycle
Design Vehicle for Bridge	N/A	H-10 Truck	H-10 Truck

*According to current GDOT design policy if applicable

Major Interchanges/Intersections: None

Lighting required: No Yes

Transportation Management Plan [TMP] Required: No Yes
 If Yes: Project classified as: Non-Significant Significant
 TMP Components Anticipated: TTC TO PI

Will Context Sensitive Solutions procedures be utilized? No Yes

Design Exceptions to FHWA/AASHTO controlling criteria anticipated: None anticipated

Design Variances to GDOT Standard Criteria anticipated: None anticipated

UTILITY AND PROPERTY

Temporary State Route Needed: No Yes Undetermined

Railroad Involvement: None

Utility Involvements: Coordination and permitting with Columbus Water Works, Georgia Power, and Liberty Gas

SUE Required: No Yes

Public Interest Determination Policy and Procedure recommended? No Yes

Right-of-Way: Existing width: 100-200' at Approaches, 220' at Victory Dr. Proposed width: Same

Required Right-of-Way anticipated: No Yes Undetermined
 Easements anticipated: None Temporary Permanent Utility Other

Anticipated number of impacted parcels:	0
Displacements anticipated:	Total: 0
	Businesses: 0
	Residences: 0
	Other: 0

ENVIRONMENTAL AND PERMITS

Anticipated Environmental Document:

GEPA: NEPA: CE PCE

MS4 Compliance – Is the project located in an MS4 area? No Yes

Environmental Permits, Variances, Commitments, and Coordination anticipated:

A Section 404 permit to comply with the Clean Water Act is anticipated.

Air Quality:

Is the project located in a PM 2.5 Non-attainment area? No Yes
 Is the project located in an Ozone Non-attainment area? No Yes
 Is a Carbon Monoxide hotspot analysis required? No Yes

NEPA/GEPA Comments & Information: Culvert extension would affect stream passing under Victory Drive. The trail approaches would connect to an existing trail to be constructed by locals along the railroad line. Therefore no adverse effect is anticipated to the railroad. Eligible archaeological sites will not be determined until site file research and field survey. Due to the nature of the project for pedestrian improvements, the project is not anticipated to adversely affect air quality or noise. Public controversy is not anticipated.

COORDINATION, ACTIVITIES, RESPONSIBILITIES, AND COSTS

Project Meetings: A meeting was held on August 19, 2014 with the City of Columbus. Part of the meeting agenda was to discuss the current status, action items, and schedule of the South Lumpkin Multi-use Facility. Meeting minutes are attached.

Project Activity	Party Responsible for Performing Task(s)
Concept Development	Heath and Lineback Engineers
Design	Heath and Lineback Engineers
Right-of-Way Acquisition	N/A
Utility Relocation	Utility Owner
Letting to Contract	City of Columbus
Construction Supervision	City of Columbus
Providing Material Pits	N/A
Providing Detours	N/A
Environmental Studies, Documents, & Permits	Edwards-Pitman Environmental
Environmental Mitigation	City of Columbus
Construction Inspection & Materials Testing	City of Columbus

Other coordination to date: There has been utility coordination with utility companies. Plans were sent to utility companies for verification. Responses have been received from Columbus Water Works, Georgia Power, and Liberty Gas.

Project Cost Estimate and Funding Responsibilities:

	Breakdown of PE	ROW	Reimbursable Utility	CST*	Environmental Mitigation	Total Cost
Funded By	City of Columbus					
\$ Amount	\$128,784	\$0	\$0	\$1,360,055	\$45,700	\$1,534,539
Date of Estimate	9/10/2014	9/10/2014	9/10/2014	11/7/2014	9/10/2014	

*CST Cost includes: Construction, Engineering and Inspection, and Liquid AC Cost Adjustment.

ALTERNATIVES DISCUSSION

Preferred Alternative: Alternate 3			
Estimated Property Impacts:	0	Estimated Total Cost:	\$1.53 million
Estimated ROW Cost:	0	Estimated CST Time:	6 months
Rationale: This alternate provides the most cost effective and aesthetic solution. In addition, this alternate utilizes the shortest bridge (178 ft.) that does not require a bent in the median. The steel truss bridge recommended also requires a reduced construction time since the superstructure for the bridge is constructed off-site, transported to the site in two sections and then assembled and erected.			

No-Build Alternative:			
Estimated Property Impacts:	0	Estimated Total Cost:	0
Estimated ROW Cost:	0	Estimated CST Time:	0
Rationale: This alternate does not provide a safe crossing for pedestrians and bicyclist wanting to cross Victory Drive.			

Alternative 1:			
Estimated Property Impacts:	0	Estimated Total Cost:	\$1.70 million
Estimated ROW Cost:	0	Estimated CST Time:	9 months
Rationale: This alternate results in a significant skew crossing Victory Drive and therefore requires a longer bridge (266 ft.) and a bent in the median. Due to the time required to construct a bent in the median, this alternate would result in longer construction duration and impacts to traffic flow. Further, a bent in the median is not desirable due to the proximity to traffic.			

Alternative 2:			
Estimated Property Impacts:	0	Estimated Total Cost:	\$1.75 million
Estimated ROW Cost:	0	Estimated CST Time:	12 months
Rationale: This alternate results in an undesirable dog-leg crossing over Victory Drive that would require two bents in the median. This alternate also results in the longest, most costly bridge (276 ft.). Due to the time required to construct two bents in the median, this alternate would result in longer construction duration and impacts to traffic flow. Further, bents in the median are not desirable due to the proximity to traffic.			

LIST OF ATTACHMENTS/SUPPORTING DATA

1. Columbus Consolidated Government Concept Study/Summary
2. Concept Layouts with Typical Sections
3. Cost Estimates
4. Meeting Minutes
5. Overall Schematic of the South Lumpkin Multi-Use-Facility
6. Rendering of the Victory Drive Crossing
7. Project Framework Agreement

CONCEPT STUDY/SUMMARY

Discussion of alternates for a grade separated crossing of South Lumpkin Multi-use Facility (Trail) and Victory Drive (US280/SR520/SR1)

Summary

This discussion/study will identify constraints, describe assumptions, present options and recommend a preferred alternate for crossing the multi-use trail at Victory Drive.

Assumptions and Constraints

- Assumptions-
 - The trail width is 12 feet between inside face of railings.
 - Victory Drive/US280 is a four lane highway with a 20 foot raised grassed median, curb and gutter and sidewalks.
 - The vertical clearance over Victory Drive is 17.5 feet.
 - The horizontal clearance (clear zone) from the edge of travel lane to the face of piers is 8 feet.
 - The minimum trail horizontal alignment radius is 40 feet (10 feet minimum for dismounting from bicycles options).
 - The maximum allowable trail profile grade is 5%.
 - A tunnel underpass option requires 10 feet vertical clearance from trail to tunnel roof.
- Constraints-
 - An existing 30 inch concrete culvert and drainage channel crossing under Victory Drive running east-west parallel to trail.
 - Overhead power running east-west parallel to trail.

General Approach

An at-grade crossing consists of a mid-block pedestrian crossing with a HAWK signal or directing trail users to an existing at-grade signalized crossing. Observations:

- Does not provide an enjoyable experience.
- There are safety concerns for those with disabilities
- Bicyclist are required to stop and dismount
- There are some costs associated with directing/routing users to other crossing location.
- There is the risk that some users will simply run (or ride) across the road to avoid the diversion route.

South Lumpkin Multi-use Facility – Columbus Consolidated Government

➤ Consideration of a grade separated crossing.

There are two options – over and under.

It is desirable to have ramps at the approach ends crossing facility to encourage bicycle use and to make the use of the crossing an acceptable experience (pedestrian bridges and crossings with steps at each end are often ignored by users) – safety concern.

Under Options

An underpass alternate was studied in the form of a tunnel or box culvert under Victory Drive and is described below:

1. An underpass requires a 270 foot long 10 ft. wide by 10 ft. high concrete box culvert type structure (12 foot wide trail narrows at the culvert).
2. In order hold profile grade at 5% (+/-) maximum and achieve required cover of 2'-6" (min) over the length of the culvert, four walls each approximately 250 foot long are required at the approaches.
3. An underpass culvert requires lighting 24 hours per day with associated long term maintenance costs.
4. Due to the generally flat terrain, the underpass will create a low point in the surrounding area and will not naturally drain. An automatic sump pump system will be required to drain the culvert.
5. An underpass culvert would most likely be constructed by the cut and cover method with traffic shifted for 4 to 6 months onto half of the existing roadway to construct each half of the culvert.
6. Utility facilities will be disrupted and will likely require relocation.
7. Security and safety concerns may deter use by pedestrians/bicyclists.

Over Options

Goals:

- Minimize cost – utility, right of way, construction (total project cost)
- Maximize aesthetics (opportunity to create a visual marker for the trail)
- Minimize impact to roadway users
- Avoid or minimize impact to drainage ways
- Safety
 - Safety of trail users
 - Safety of motorists (people lobbing objects at cars?)

South Lumpkin Multi-use Facility – Columbus Consolidated Government

First, we studied trail alignments in order to compare different alignments, profiles, and skews. These are described below and referenced to attached layouts:

1. **Alternate 1** connects paths that are approximately centered within the corridor right of way on the west and east sides of Victory Drive. This alignment results in a skew crossing over Victory Drive and a bridge length of 266 feet requiring a pier in the median.
2. **Alternate 2** provides a dog legged crossing of Victory Drive with two bents in the median. This reduces the bridge span lengths. The bents in the median potentially are undesirable. The total bridge length is 276 feet long. Sharp curves (10' radius requiring bicyclists to dismount) along the trail are required at the median.
3. **Alternates 3** significantly reduces the bridge skew over Victory Drive and crosses the stream on the east and west sides of Victory Drive.

Second, we studied bridge structure types in order to compare superstructure depths that affect the vertical profile elevation, constructability, and costs. These bridge types are described below:

1. **Prefabricated Steel Truss Bridge** – This truss type is called a 'through girder' in that the superstructure girder is placed on each side of the trail concrete slab and extends above the trail. This enables the trail vertical profile to be set lower than with other girder types placed under the trail concrete slab. The superstructure for this bridge type is constructed off-site, transported to the site in two sections, assembled on site, and erected. The construction time is shorter. The truss structure is aesthetically pleasing.
2. **Built-Up Steel Girder Bridge** – This steel girder type superstructure consists of two steel girders underneath and supporting the trail concrete slab and barrier. The superstructure depth varies based on the span length, and the depth is greater than the truss bridge. The trail profile grade may be as much as 4 feet higher than the steel truss option. This requires taller retaining walls. Decorative barrier and fencing would provide an aesthetic element.
3. **AASHTO Concrete Girder Bridge** – This concrete girder type superstructure consists of two concrete girders underneath and supporting the trail concrete slab and barrier. The superstructure depth varies based on the span length, and the depth is greater than the steel truss bridge. The trail profile grade may be as much as 1.5 feet higher than the steel truss option and a concrete pier is required in the roadway median. Taller retaining walls are required. Decorative barrier and fencing would provide an aesthetic element.

South Lumpkin Multi-use Facility – Columbus Consolidated Government

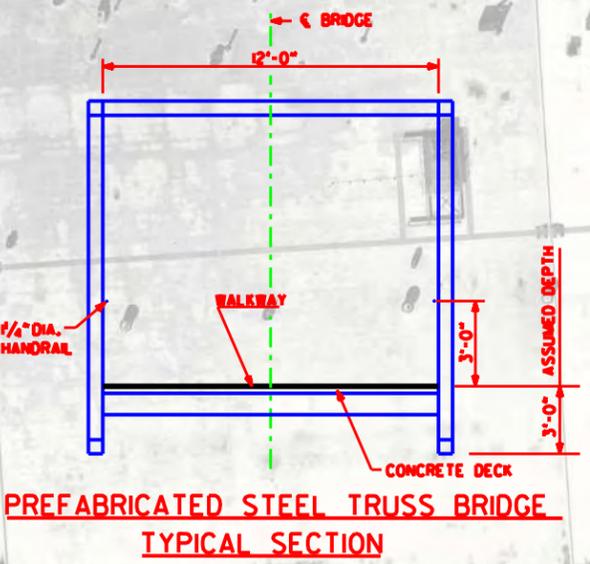
For alternates 2 and 3, multispan structures with bents in the median result in lower superstructure depth, a lower profile grade, and shorter retaining walls.

As a result of this study, H&L recommends alternate 3 with a single span 178 foot prefabricated steel truss bridge crossing over Victory Drive as the preferred solution for the reasons described below:

- ❖ Provides a crossing of Victory drive without a diversion route
- ❖ Creates a pleasing experience with a curvilinear path
- ❖ Allows the opportunity to extend (or bridge over) the existing culvert/stream
- ❖ Requires a lower trail vertical profile
- ❖ Requires the shortest bridge structure length
- ❖ Avoids constructing and protecting a pier in the median
- ❖ Is aesthetically pleasing without needing to add aesthetic features
- ❖ Is the lowest total cost alternate

Layouts and costs are included as attachments and were created and used to evaluate the alternates. Instead of studying all three alternates along with all the different bridge types, alternates 1 and 2 were eliminated after concluding they resulted in the highest total cost options.

Costs begin with bridge structure costs to compare structure type to structure type and then add approach construction costs including earthwork, walls, drainage, etc. Once the recommended preferred alternate is selected to move forward with, design and analysis will be conducted on the approach earthwork and retaining walls in order to reduce wall lengths and heights and associated construction costs. Finally, the funding involving Federal-Aid HPP and Local TIA will require coordination with GADOT to determine necessary and preferred and allowable limits of work.



**KELLOGG'S
SNACKS**

END PROJECT

BEGIN PROJECT

VICTORY DRIVE

**OVERHEAD
TRANSMISSION**

END BRIDGE

WALL TYP.

143 FT

123 FT

**DRAINAGE STRUCTURE
SIZE UNKNOWN**

BEGIN BRIDGE

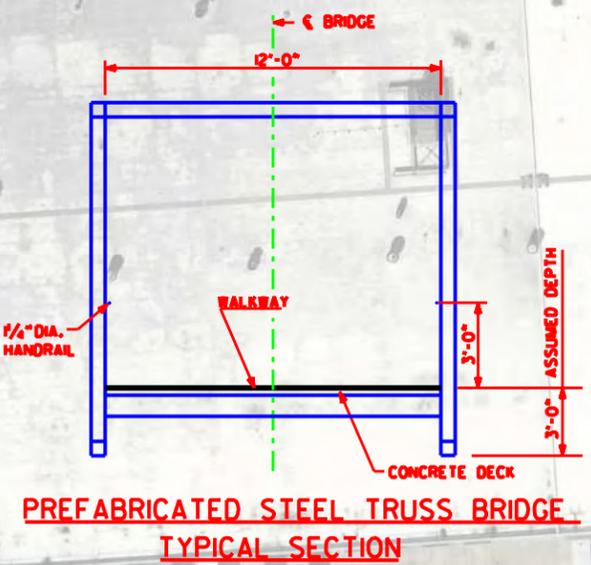
BURGER KING

1. A TWO SPAN 266 FOOT LONG BRIDGE (143' - 123') WILL BE REQUIRED.
2. BENT IN MEDIAN NOT IDEAL DUE TO PROXIMITY TO TRAFFIC.
3. BENT IN THE MEDIAN WILL REQUIRE SHORING TO AVOID THE EXISTING CULVERT.
4. WALLS WILL BE REQUIRED AT THE BRIDGE APPROACHES.

**SOUTH LUMPKIN MULTI-USE FACILITY
AT VICTORY DRIVE**

ALTERNATE #1 - TWO SPAN BRIDGE - PREFABRICATED STEEL TRUSS

2013017/CONCEPT



KELLOGG'S
SNACKS

VICTORY DRIVE

END PROJECT

BEGIN PROJECT

WALL TYP.

BEGIN BRIDGE

END BRIDGE

OVERHEAD TRANSMISSION

10' R

10' R

100' R

100' R

110 FT

71 FT

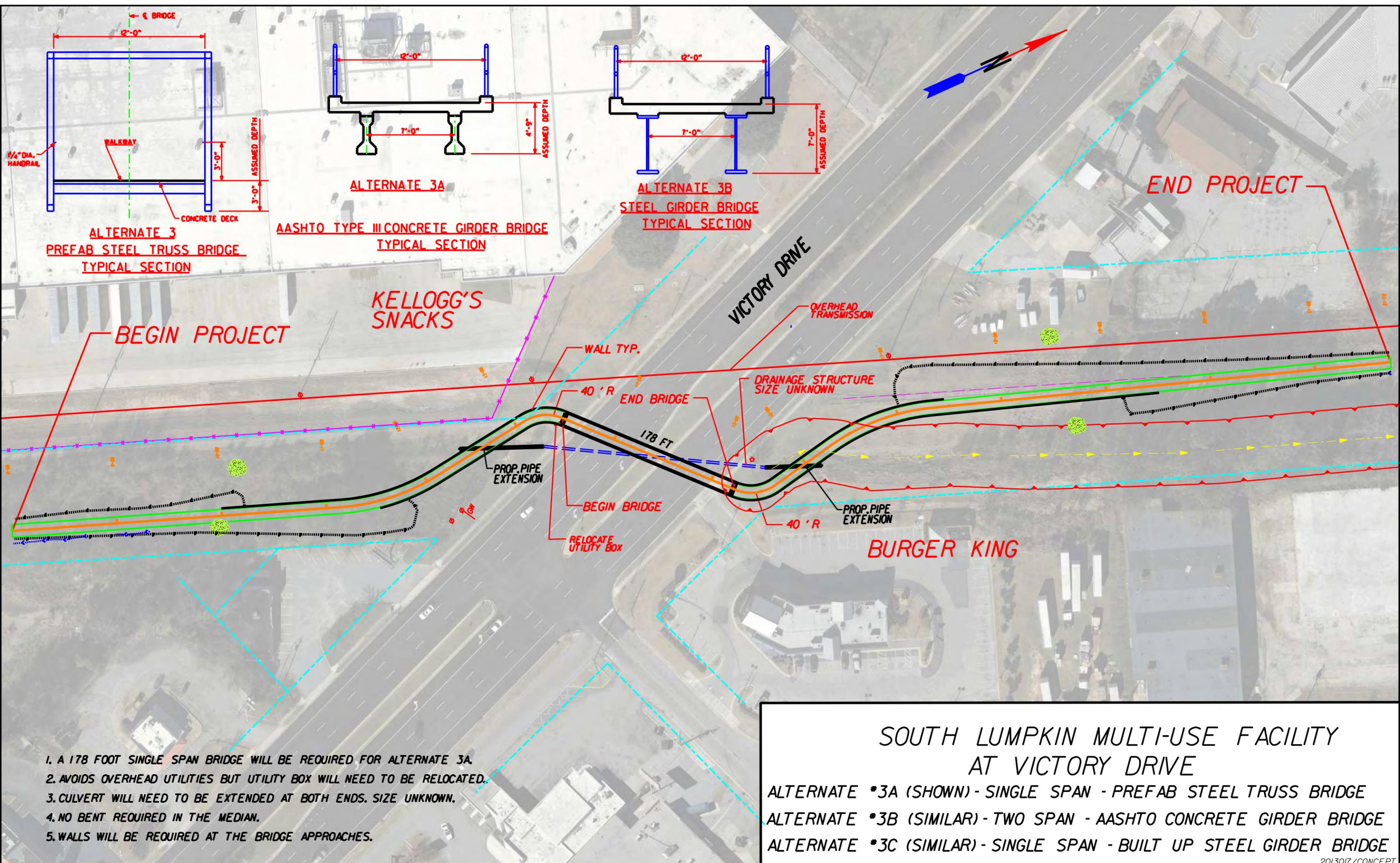
95 FT

DRAINAGE STRUCTURE
SIZE UNKNOWN

BURGER KING

1. A THREE SPAN 276 FOOT BRIDGE (110,71,95).
2. ADDITIONAL COST DUE TO ADDITIONAL BENTS AND BRIDGE.
3. BENTS IN MEDIAN NOT IDEAL DUE TO PROXIMITY TO TRAFFIC.
4. COLUMNS IN MEDIAN WILL REQUIRE PROTECTION/BARRIER WALLS.
5. WALLS WILL BE REQUIRED AT THE BRIDGE APPROACHES.
6. 10 FOOT RADIUS ON BRIDGE NOT DESIREABLE.

SOUTH LUMPKIN MULTI-USE FACILITY
AT VICTORY DRIVE
ALTERNATE #2 - THREE SPAN BRIDGE - PREFABRICATED STEEL TRUSS
2013/017/CONCEPT



ALTERNATE 3
PREFAB STEEL TRUSS BRIDGE
TYPICAL SECTION

ALTERNATE 3A
AASHTO TYPE III CONCRETE GIRDER BRIDGE
TYPICAL SECTION

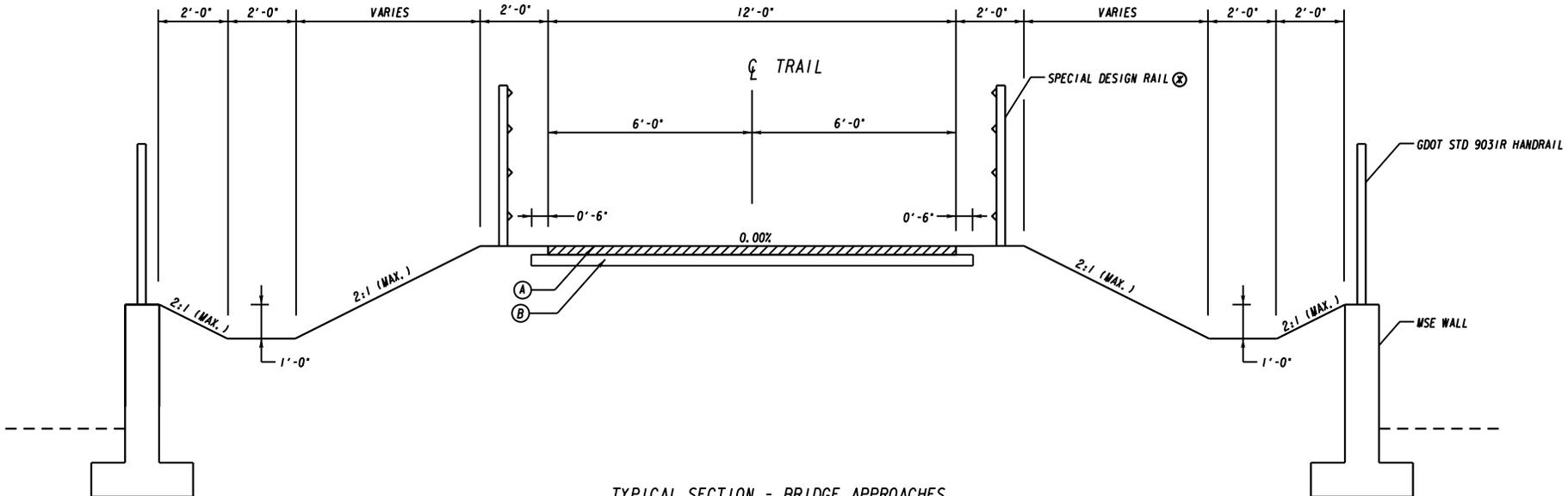
ALTERNATE 3B
STEEL GIRDER BRIDGE
TYPICAL SECTION

1. A 178 FOOT SINGLE SPAN BRIDGE WILL BE REQUIRED FOR ALTERNATE 3A.
2. AVOIDS OVERHEAD UTILITIES BUT UTILITY BOX WILL NEED TO BE RELOCATED.
3. CULVERT WILL NEED TO BE EXTENDED AT BOTH ENDS. SIZE UNKNOWN.
4. NO BENT REQUIRED IN THE MEDIAN.
5. WALLS WILL BE REQUIRED AT THE BRIDGE APPROACHES.

SOUTH LUMPKIN MULTI-USE FACILITY
AT VICTORY DRIVE

ALTERNATE *3A (SHOWN) - SINGLE SPAN - PREFAB STEEL TRUSS BRIDGE
 ALTERNATE *3B (SIMILAR) - TWO SPAN - AASHTO CONCRETE GIRDER BRIDGE
 ALTERNATE *3C (SIMILAR) - SINGLE SPAN - BUILT UP STEEL GIRDER BRIDGE

2013/017/CONCEPT



TYPICAL SECTION - BRIDGE APPROACHES

NOTE: 2:1 SLOPE TIES IN TO EXISTING GRADE WHERE WALLS ARE NOT REQUIRED.
 ⊗ RAILING ONLY REQUIRED WHERE DROP OF 2:1 SLOPE IS GREATER THAN 4'

REQUIRED PAVEMENT:

- Ⓐ RECYCLED ASPH CONC 12.5 MM SUPERPAVE, GP 2 ONLY, INCL BITUM MATL & H LIME (165 LBS/SY)
- Ⓑ GR AGGR BASE CRS, 6 INCH, INCL MATL

HL Heath & Lineback Engineers
 INCORPORATED
 2390 CANTON ROAD, BUILDING 200
 MARIETTA, GEORGIA 30066-5395
 (770)424-6688

N. T. S.

REVISION DATES

COLUMBUS
 CONSOLIDATED GOVERNMENT
 OFFICE: DEPARTMENT OF PLANNING
TYPICAL SECTIONS

SOUTH LUMPKIN ROAD TRAIL - PHASE 2
 DRAWING No. **05-001**

CES Cost Estimate.txt
STATE HIGHWAY AGENCY

DATE : 11/07/2014
PAGE : 1

JOB ESTIMATE REPORT

JOB NUMBER : 0007633 SPEC YEAR: 13
DESCRIPTION: SOUTH LUMPKIN ROAD TRAIL - PHASE II
 VICTORY DRIVE PEDESTRIAN OVERPASS

ITEMS FOR JOB 0007633

LINE	ITEM	ALT	UNITS	DESCRIPTION	QUANTITY	PRICE	AMOUNT
0005	009-2000		LS	LANDSCAPING WITH IRRIGATION	1.000	50000.00	50000.00
0010	150-1000		LS	TRAFFIC CONTROL - VICTORY DRIVE	1.000	20000.00	20000.00
0015	163-0232		AC	TEMPORARY GRASSING	2.000	314.53	629.07
0020	163-0240		TN	MULCH	23.000	273.73	6295.81
0025	163-0300		EA	CONSTRUCTION EXIT	2.000	1306.69	2613.39
0030	163-0503		EA	CONSTR AND REMOVE SILT CONTROL GATE,TP 3	2.000	427.16	854.33
0035	163-0527		EA	CONST/REM RIP RAP CKDM,STN P RIPRAP/SN BG	4.000	247.22	988.89
0040	163-0550		EA	CONS & REM INLET SEDIMENT TRAP	6.000	143.46	860.80
0045	165-0030		LF	MAINT OF TEMP SILT FENCE, TP C	1448.000	0.77	1120.67
0050	165-0041		LF	MAINT OF CHECK DAMS - ALL TYPES	4.000	16.14	64.60
0055	165-0087		EA	MAINT OF SILT CONTROL GATE, TP 3	2.000	116.35	232.70
0060	165-0101		EA	MAINT OF CONST EXIT	2.000	448.28	896.57
0065	165-0105		EA	MAINT OF INLET SEDIMENT TRAP	6.000	41.89	251.38
0070	167-1000		EA	WATER QUALITY MONITORING AND SAMPLING	2.000	223.90	447.81
0075	167-1500		MO	WATER QUALITY INSPECTIONS	9.000	470.18	4231.63
0080	171-0030		LF	TEMPORARY SILT FENCE, TYPE C	2896.000	2.60	7557.75
0085	210-0100		LS	GRADING COMPLETE - BRIDGE APPROACHES	1.000	50000.00	50000.00
0090	310-1101		TN	GR AGGR BASE CRS, INCL MATL	429.000	21.87	9385.22
0095	402-3130		TN	RECYL AC 12.5MM SP,GP2,BM&HL	97.000	74.54	7230.68
0100	441-0600		CY	CONC HEADWALLS	4.000	950.00	3800.00
0105	515-2015		LF	GALV STEEL PIPE HANDRAIL - TRAIL APPROACH RAILING	1756.000	55.50	97458.00
0110	534-1000		LS	PEDESTRIAN OVERPASS BRIDGE,STA BRIDGE OVER VICTORY DRIVE	1.000	400000.00	400000.00
0115	550-1603		LF	STM DR PIPE 60,H 20-25	152.000	186.68	28375.36
0120	626-0320		CY	MSE WALL BACKFILL MATERIAL	9327.000	15.00	139905.00
0125	627-1000		SF	MSE WALL FACE, 0 - 10 FT HT, WALL NO - BRIDGE APPROACHES	518.000	49.48	25635.26
0130	627-1010		SF	MSE WALL FACE, 10 - 20 FT HT, WALL NO - BRIDGE APPROACHES	8762.000	35.98	315317.31
0135	627-1100		LF	COPING A, WALL NO - BRIDGE APPROACHES	540.000	69.49	37528.88
0140	641-1200		LF	GUARDRAIL, TP W	147.000	18.73	2754.51
0145	641-5001		EA	GUARDRAIL ANCHORAGE, TP 1	2.000	803.11	1606.22
0150	641-5012		EA	GUARDRAIL ANCHORAGE, TP 12	2.000	2048.30	4096.62
0155	643-8200		LF	BARRIER FENCE (ORANGE), 4 FT	470.000	1.44	681.40
0160	652-6402		GLF	SKIP TRAF STRIPE, 4 IN, YELLOW	1050.000	0.25	262.50
0165	657-5014		EA	PRF PL PVT MKG,WD/SYM,WH,TP PB	2.000	25.00	50.00
0170	668-4400		EA	STORM SEW MANHOLE, TP 2	1.000	2579.23	2579.24

				CES Cost Estimate.txt			
0175	668-4413	LF	ST SEW MANHOLE,TP 2,A DEP,CL 3		16.000	197.00	3152.00
0180	716-2000	SY	EROSION CONTROL MATS, SLOPES		4830.000	1.05	5113.86
0185	900-0526	EA	BOLLARDS		6.000	739.37	4436.22

STATE HIGHWAY AGENCY

DATE : 11/07/2014
PAGE : 2

JOB ESTIMATE REPORT

ITEM TOTAL	1236413.65
INFLATED ITEM TOTAL	1236413.65
TOTALS FOR JOB 0007633	
<hr/>	
ESTIMATED COST:	1236413.68
CONTINGENCY PERCENT (10.0):	123641.37
ESTIMATED TOTAL:	1360055.05

PRELIMINARY ENVIRONMENTAL MITIGATION COST ESTIMATE

PI No. 0007633 Muscogee County
South Lumpkin Road Trail – Phase II (Victory Drive Pedestrian Overpass)

IMPACTED RESOURCE	CREDITS	UNIT COST	TOTAL
Stream 2, 165 LF	1015.5	\$45.00	\$45,700.00
Totals	1015.5		\$45,700.00

VICTORY DRIVE PEDESTRIAN CROSSING - COST COMPARISON

BRIDGE			APPROACHES					TOTAL	
ALTERNATE	Bridge Construction Cost	Bridge Cost with 5% E & I and 4% Contingency	Earthwork	Pavement	Railing (Off Bridge)	Walls	Approaches Construction Cost	Approach Cost with 5% E & I and 4% Contingency	Total Cost with 5% E & I and 4% Contingency
1 - Two Span Prefabricated Steel Truss Bridge	\$538,900	\$587,401	\$45,000	\$26,250	\$31,500	\$916,000	\$1,018,750	\$1,110,438	\$1,697,839
2 - Three Span Prefabricated Steel Truss Bridge	\$590,400	\$643,536	\$45,000	\$26,250	\$31,500	\$916,000	\$1,018,750	\$1,110,438	\$1,753,974
3A - Single Span Prefabricated Steel Truss Bridge	\$392,440	\$427,760	\$60,000	\$30,000	\$36,000	\$916,000	\$1,042,000	\$1,135,780	\$1,563,540
3B-Two Span AASHTO Type III Concrete Girder Bridge	\$370,300	\$403,627	\$72,000	\$35,000	\$42,000	\$988,920	\$1,137,920	\$1,240,333	\$1,643,960
3C -Single Span Built Up Steel Girder Bridge	\$424,300	\$462,487	\$86,400	\$35,000	\$42,000	\$1,193,360	\$1,356,760	\$1,478,868	\$1,941,355

Note: The total cost of the studied Alternates is for comparison and is not the total cost of the project.

LEGEND:

PREFERRED ALTERNATE

Alternates 1 and 2 were eliminated for the following reasons:

1. A single span prefabricated bridge is more cost effective than the two or three span prefabricated bridges.
2. Alternates 2 and 3 require a bent in the median.
3. The RCP pipe crossing Victory Drive will most likely need relocating to construct the intermediate bent resulting in traffic disruptions.

Alternate 3B was eliminated for the following reasons:

1. Alternate 3B will require a bent in the median and the overall cost of this alternate is higher than that of the preferred.
2. The RCP pipe crossing Victory Drive will most likely need relocating to construct the intermediate bent resulting in traffic disruptions.

Alternate 3C was eliminated due to the high cost and poor aesthetics

ALTERNATES FOR BRIDGE AT VICTORY DRIVE

BRIDGE

ALTERNATE	Span Arrangement	Total Bridge length (ft)	Bridge Foundation Description	Bridge Foundation Cost	Prefab Bridge Cost - For Length < 150 ft (\$ per LF)	Prefab Bridge Cost - For Length > 150 ft (\$ per LF)	Superstructure Erection Cost (10% of Superstructure cost)	MOT for int bent	Bridge Fencing	Superstructure Cost	Total Bridge Cost
1	143-123	266.00	2 end - 1 int	75000	1500	1800	\$39,900	\$25,000	NA	\$438,900	\$538,900
2	110-71-95	276.00	2 end - 2 int	110000	1500	1800	\$41,400	\$25,000	NA	\$455,400	\$590,400
3A	178	178.00	2 end	40000	1500	1800	\$32,040	NA	NA	\$352,440	\$392,440
3B	89-89	178.00	2 end - 1 int	75000	NA	NA	NA	\$25,000	\$35,600	\$270,300	\$370,300
3C	178	178.00	2 end	40000	NA	NA	NA	NA	\$35,600	\$384,300	\$424,300

Alternate 3B: Type III beams were considered which require a bent in the median.

A 74" Bulb-T was considered for Alternate 3B but was eliminated since the profile would need to be raised an additional 4 feet which would significantly increase the project length and cost

Alternate 3C: A steel beam option was considered which does not require a bent in the median.

1. The steel beam could be transported to the site in sections and then assembled at the site, requiring a false bent in the median for erection.
2. The profile would need to be raised an additional 3 to 4 feet which would increase the cost of the project.

LEGEND:

PREFERRED ALTERNATE

ALTERNATES FOR BRIDGE APPROACHES AT VICTORY DRIVE

PAVEMENT			EARTHWORK			RAILING			
ALTERNATE	Total Pavement Length(ft)	Total cost of One Foot of Pavement	Total Pavement Cost	Fill (CY)	Cost Per CY	Total Earthwork Cost	Railing Length	Cost Per LF	Total Railing Cost
1	1050	25	\$26,250	7500	\$6	\$45,000	2100	15	\$31,500
2	1050	25	\$26,250	7500	\$6	\$45,000	2100	15	\$31,500
3A	1200	25	\$30,000	10000	\$6	\$60,000	2400	15	\$36,000
3B	1400	25	\$35,000	12000	\$6	\$72,000	2800	15	\$42,000
3C	1400	25	\$35,000	14400	\$6	\$86,400	2800	15	\$42,000

WALLS

ALTERNATE	MSE Total Wall Length (FT)	MSE Wall Area (SF)	Wall Cost per SF	Coping Cost per LF	Coping Cost	Wall Cost	Total Wall + Coping Cost
1	1200	20200.00	40	90	\$108,000	\$808,000	\$916,000
2	1200	20200.00	40	90	\$108,000	\$808,000	\$916,000
3A	1200	20200.00	40	90	\$108,000	\$808,000	\$916,000
3B - Profile Raised 1.5'	1320	21753.00	40	90	\$118,800	\$870,120	\$988,920
3C - Profile Raised 4'	1520	26414.00	40	90	\$136,800	\$1,056,560	\$1,193,360

Note - Alternates 1, 2 and 3A utilizes the same vertical profile. 3B and 3C require that the profile be raised due to an increase in superstructure depth.

LEGEND:

PREFERRED ALTERNATE

BRIDGE CALCULATIONS

Prefab Bridge Cost:

Spans less than 150 feet + 1,200\$/lf for prefab superstructure + 300\$/lf for deck = 1,500\$/lf not including foundation.

Spans greater than 150 feet + 1,500\$/lf for prefab superstructure + 300\$/lf for deck = 1,800\$/lf not including foundation.

Intermediate bent = Assume 5'x3' single column x 20' and a 10'x10'x3.5' ftg with six HP 14x89 piles x 50' long

Concrete = $(12' \times 5' \times 3' + 5' \times 3' \times 15' + 10' \times 10' \times 3.5') / 27 \times 750 \$/\text{cy}$ incl reinf = \$20,972

Steel Piles = $6 \times 50' \times 45 \$/\text{lf}$ = \$13,500

Total Cost for Int bent = \$34,472 say 35000

End bent = Assume 3'x2' x14' cap with four HP 14x89 piles x 80' long

Concrete = $(3' \times 2' \times 14') / 27 \times 750 \$/\text{cy}$ incl reinf = \$2,333

Steel Piles = $4 \times 80' \times 45 \$/\text{lf}$ = \$14,400

Total Cost for end bent = \$16,733 say 20000

3B. Type III Bridge Superstructure Cost:

Beam Cost = 4 beams x 89' x 150\$/lf = \$53,400

Deck = 350\$/lf x 178' = \$62,300

Concrete Barrier = 178' x 2 x 250\$/lf = \$89,000

Misc Conc/Diaphragm, edge beams = \$30,000

Type III Bridge Total Bridge cost =

\$234,700

3C. Single Span Steel Beam Bridge Superstructure Cost:

Beam Cost = 2 beams x 178' x 400\$/lf = \$142,400

Deck = 350\$/lf x 178' = \$62,300

Concrete Barrier = 178' x 2 x 250\$/lf = \$89,000

Misc Steel/Diaphragm, edge beams = \$40,000

False bent in median = \$15,000

Steel Beam Bridge Total Bridge cost **\$348,700**

MEMO

TO: The File

FROM: Patrick Peters

DATE: August 19, 2014

RE: Columbus Projects Status Update

LOCATION: CCG

Attendees: Rick Jones – CCG
Will Johnson – CCG
Farhard AliFarhani – CCG
Doc Dorsey – CCG
Gary Gullatte – GLA
Allen Krivsky – H&L
Patrick Peters –H&L

The meeting was held to discuss the current status, action items, and schedule of the various projects for the City with a focus on the South Lumpkin Multi-use Facility.

South Lumpkin Multi-use Facility:

- General
 - Patrick opened with a status report on the project. H&L is currently performing a QC check and plans to request a FFPR in the coming weeks to be held in mid to late September.
 - Rick emphasized getting the project complete and under construction as soon as possible.
 - Bid to perform railroad removal due on 8/29/14.
 - The erosion plans will need to be submitted to GAEPD for review and approval.
 - An encroachment permit is needed from GDOT for the sidewalk work at Victory Drive.
 - The City has plans to reconstruct Cusseta Road this fall. There will not be any curb and gutter so we need to remove the curb and gutter shown on the frontage of the Cusseta Road trail head.
 - The City will reach out to bike stakeholders for input on the trail features.
 - Doc mentioned that a City study revealed that only 25% of bicyclists use the rapid flashing pedestrian beacons on other crossing locations.
- Right-of-Way
 - Felton is working with Kellogg Company on donating the required right-of-way and has contacted the businesses currently encroaching on the right-of-way between Victory Drive and Levy Road.
- Trail Heads

Heath & Lineback Engineers, Inc.

- The City's top concerns are safety and maintenance.
- The City wants the Parks and Recreation Department (James) to review building plan and elevations and offer any comments relating to maintenance.
- The restroom doors should NOT lock from the inside – issue with Fall Line Trace buildings.
- Rick wants to be sure that the overhang/patio area will not look weird compared to the other trail head buildings.
- The City desires that the buildings look exactly the same as those on the Fall Line Trace – same quality.
- The overhang/patio area should be visible from the street for safety.
- Rick wants to see a visual of the building – rendering from street.
- Building permit contact is Fred Cobb.
- Landscaping
 - The City approved Gary's concept for an observation area north of Ft. Benning Road.
 - Gary needs final site plans to complete landscaping at trail heads.
 - Gary met with Scott Jones (city arborist) previously. He commented that temporary irrigation will be needed for the landscaping along the trail and at the trail heads.
 - Patrick explained the use of landscaped chicanes in several areas to slow bicyclists approaching roadway intersections. The City approved the locations and method.
 - Team needs to think about options for dressing up the property on the north side of Cusseta Road.
 - The City needs to decide on a name for the trail for the archway monuments.
- Environmental
 - H&L needs to coordinate with Edwards Pittman on what is allowable within the 25' state buffer for observation area north of Ft. Benning Road.
- Utilities
 - Patrick will coordinate with Georgia Power to get the encroachment permit to the City for execution.

Victory Drive Overpass

- The City wants the design team to investigate ways to dress up the bridge.
- Gary met with Todd (with Burger King) to discuss the impacts of the bridge.
- The City will continue coordination with Burger King on impacts to their property – potentially have fence on property.
- Next step is to begin environmental work and start GDOT concept report.

Ft. Benning Rd. Streetscapes:

- Local (Shelby to Cusseta)
 - Felton has right-of-way plans for review and to begin acquisition.
 - Rick needs the conceptual display and rendering of the streetscape to provide to the police department.
- TE (Ft. Benning gates to Shelby – including Victory Drive intersection)
 - Doc will coordinate with Georgia Power on the status of new light standards Ron was working on – H&L will provide current information.
 - H&L is under the assumption that Georgia Power is designing using standard lighting acceptable to the City.
 - CCG will complete real property letter and provide proof of advertising Location and Design Notice for GDOT.

Heath & Lineback Engineers, Inc.

Ft. Benning Road / Cusseta Road / Brennan Road Roundabout:

- H&L delivered revised right-of-way plans to Felton.
- Doc and Allen discussed the skew of the bypass lanes at stations 402+00 RT and 51+00 LT. It was determined that the skews have been softened previous based on conversations with Ron and were acceptable as currently shown.

Other

- Allen provided the City with current schedules for all projects.
- Rick asked Allen to provide a letter of agreement for the utility pot-holing for execution, but that we are fine to move forward with Valley Pipeline.
- Rick provided Allen with contracts for the trail head additional services and the Victory Drive overpass.

Action Items:

South Lumpkin Multi-use Trail

- H&L to complete final plans and request FFPR.
- H&L to submit erosion plans to GA EPD for review and permitting.
- H&L to coordinate with GDOT on encroachment permit at the Victory Drive tie-in.
- H&L to remove curb and gutter along Cusseta Road at trail head frontage.
- H&L to verify overhang/patio area will look appropriate.
- H&L to provide the City with a visual of the buildings.
- H&L to coordinate with 2WR and Fred Cobb on building permitting.
- H&L to provide 2WR and Gullatte final site plans.
- H&L to coordinate with Edwards Pittman on allowable impacts to the 25' buffer for the observation area north of Ft. Benning Road.
- H&L to coordinate with Georgia Power on encroachment permit.
- H&L and Gullatte to investigate ways to dress up property north of Cusseta Road.
- Gullatte to complete landscaping at trail heads.
- CCG to coordinate with bike stakeholders.
- CCG to continue work of Kellogg required right-of-way and right-of-way encroachments.
- CCG to have Parks & Recreation Department review building trail head plans and orientation with regards to maintenance.

Ft. Benning Road Streetscape – TE

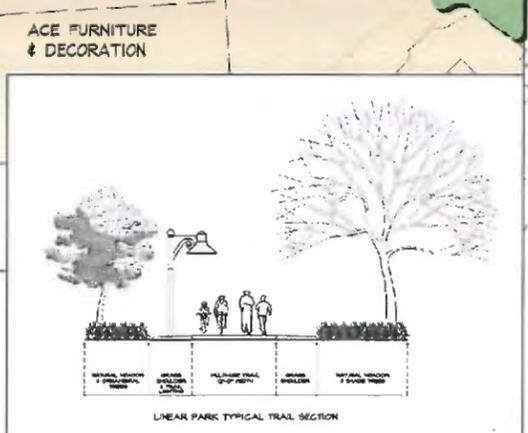
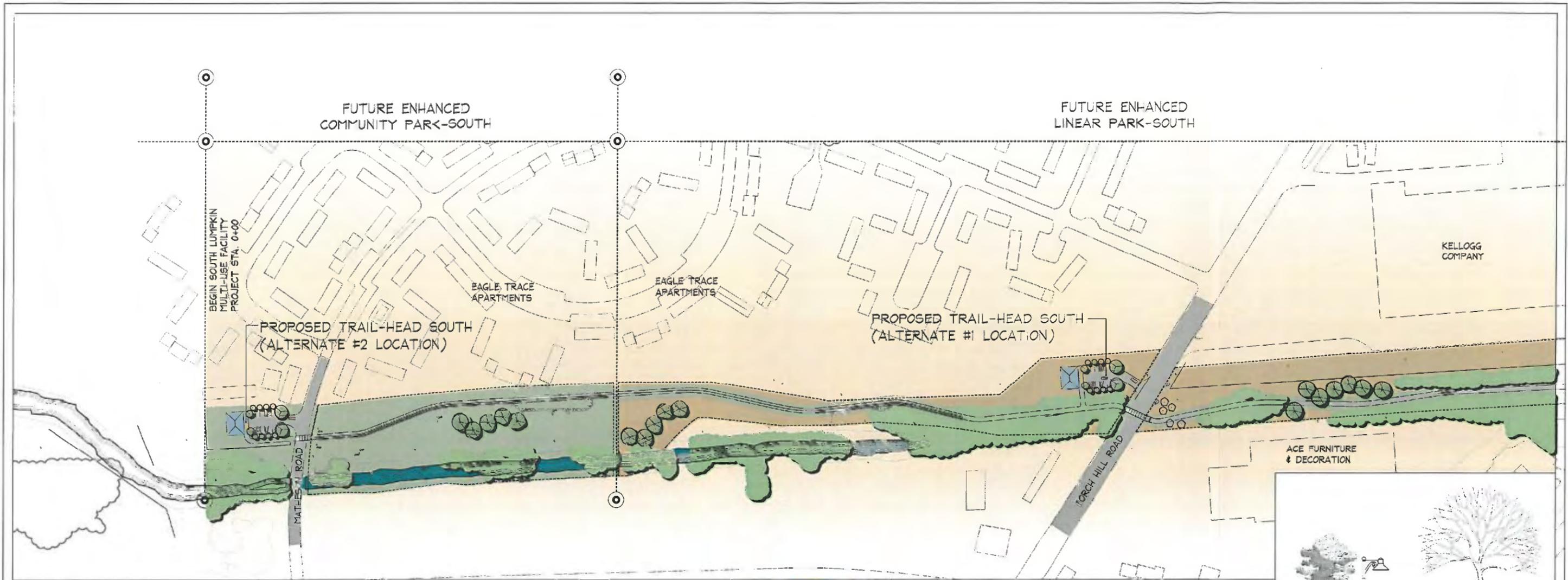
- H&L to send Rick concept display and rendering of streetscapes.
- H&L to send Doc information on lighting provided by Georgia Power.
- CCG to complete Real Property letter and provide proof of advertising Location and Design Notice.
- CCG to coordinate with Georgia Power on lighting standards.

Ft. Benning Road Streetscape – Local

- CCG to review right-of-way plans and begin right-of-way acquisition.

Cusseta/Ft. Benning/Brennan Roundabout

- H&L to provide letter of agreement for utility pot-holing work.
- CCG to continue acquiring required right-of-way.

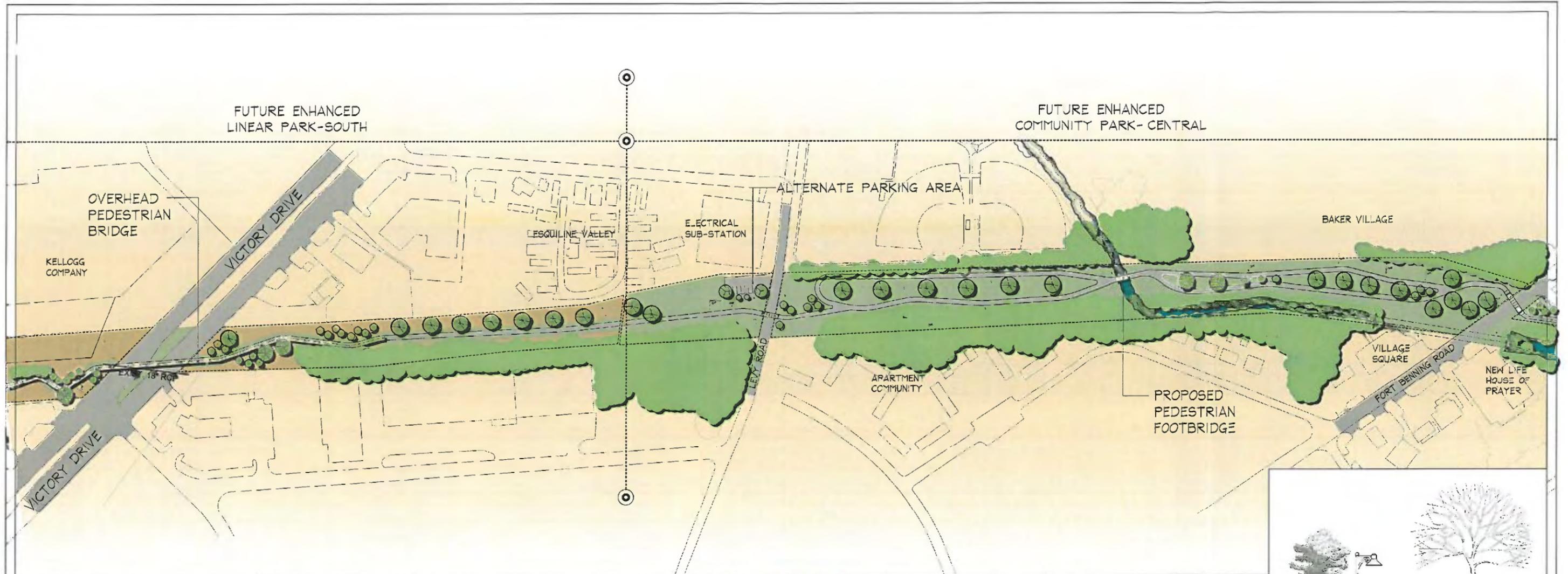


HL Heath & Lineback Engineers
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2390 CANTON ROAD, BUILDING 200
MARIETTA, GEORGIA 30066-5393
(770)424-1668

SOUTH LUMPKIN MULTI-USE FACILITY
COLUMBUS, MUSCOGEE COUNTY, GEORGIA



SHEET 1 of 3



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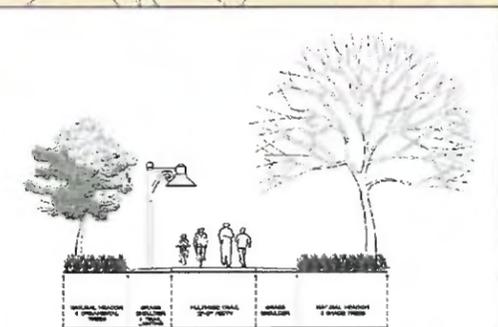
SOUTH LUMPKIN MULTI-USE FACILITY
 COLUMBUS, MUSCOGEE COUNTY, GEORGIA

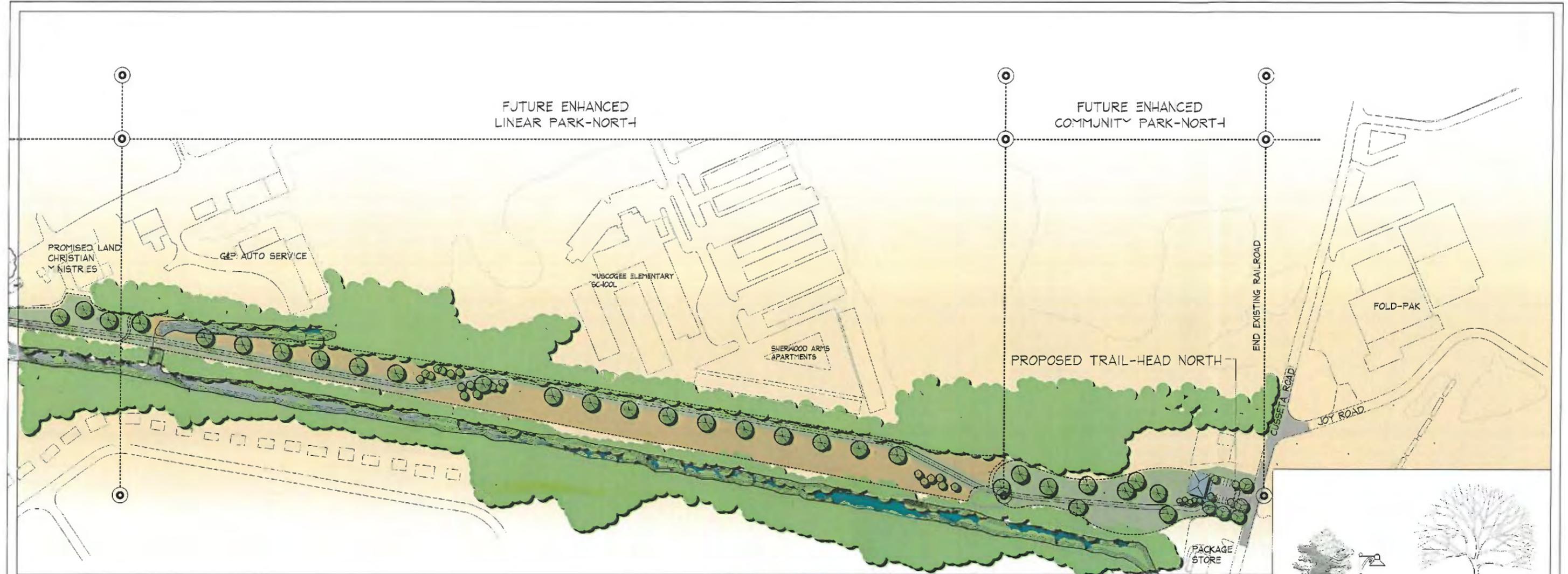


What progress has reserved.



SHEET 2 of 3



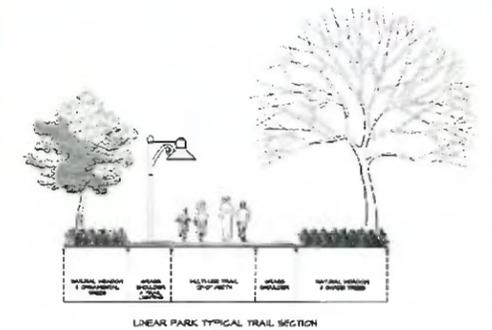


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SOUTH LUMPKIN MULTI-USE FACILITY
COLUMBUS, MUSCOGEE COUNTY, GEORGIA



SHEET 3 of 3





Victory Drive Pedestrian Crossing

Agenda Item # 2

Columbus Consolidated Government

Council Meeting

08/20/2006

Agenda Report # 156

TO: Mayor and Council

SUBJECT: Project Framework Agreement for Transportation High Priority Projects

INITIATED BY: Planning Department

Recommendation: Approval is requested to enter into a Project Framework Agreement (PFA) for the following Georgia Department of Transportation High Priority Projects: Streetscapes in Columbus Phase III (Riverwalk); Trolley Trail and Bike Path, and South Lumpkin Road Trail.

Background: The Safe, Accountable, Flexible, Efficient Transportation Equity Act: A Legacy for Users (SAFETEA-LU) was enacted August 10, 2005, by Congress, which includes projects that are identified as "High Priority Projects." The local Congressional delegation sponsored the following projects for Columbus: Streetscapes in Columbus Phase III (Riverwalk) CSSTP-0007-00 (559) - \$1 million; Trolley Trail and Bike Path CSSTP-0007-00 (563) - \$500,000; and the South Lumpkin Road Trail CSSTP-0007-00 (559) - \$500,000.

Analysis: The projects are federally funded through the administration of the Georgia Department of Transportation. The funds will be used to help leverage existing construction activities for the Riverwalk, and the proposed rails to trails projects. Construction funding from these grants will be made available for the Riverwalk section in FY07 and for the trail and bike paths in FY09.

Financial Considerations: Funding for these projects will require a twenty (20) percent match: Streetscapes in Columbus Phase III (Riverwalk) CSSTP-0007-00 (559) - \$200,000; Trolley Trail and Bike Path CSSTP-0007-00 (563) - \$100,000; and the South Lumpkin Road Trail CSSTP-0007-00 (559) - \$100,000. Funding for this match have been identified from City SPLOST and Capital Improvement Projects.

Legal Considerations: The City will be required to adhere to all legal requirements as established by the GDOT.

Recommendations/ Actions: Authorize the City Manager to sign the Project Framework Agreement (PFA) for the following Georgia Department of Transportation High Priority Projects: Streetscapes in Columbus Phase III (Riverwalk); Trolley Trail and Bike Path, and South Lumpkin Road Trail.

C.M. 06-20-06(2)

261-06

A RESOLUTION

NO. 261-06

A RESOLUTION AUTHORIZING THE CITY MANAGER TO ENTER INTO A PROJECT FRAMEWORK AGREEMENT WITH THE GEORGIA DEPARTMENT OF TRANSPORTATION FOR FUNDING FOR HIGH PRIORITY PROJECTS

WHEREAS, the Safe, Accountable, Flexible, Efficient Transportation Equity Act: A Legacy for Users (SAFETEA-LU) was enacted August 10, 2005, by Congress, which includes projects that are identified as "High Priority Projects.";and,

WHEREAS, the local Congressional delegation sponsored and received funding for the following projects for Columbus: Streetscapes in Columbus Phase III (Riverwalk) CSSTP-0007-00 (559) - \$1 million; Trolley Trail and Bike Path CSSTP-0007-00 (563) - \$500,000; and the South Lumpkin Road Trail CSSTP-0007-00 (559) - \$500,000; and,

WHEREAS, the each grant project requires a 20 percent match, which shall be funded in the amount not to exceed \$200,000 for the Streetscapes in Columbus Phase III (Riverwalk) CSSTP-0007-00 (559); \$100,000 for the Trolley Trail and Bike Path CSSTP-0007-00 (563); and \$100,000 for the South Lumpkin Road Trail CSSTP-0007-00, from city SPLOST; and,

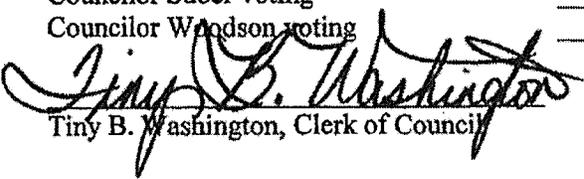
WHEREAS, the funds are administered by the Georgia Department of Transportation and require that the City enter into a Project Framework Agreement for Preliminary Engineering Activities, which outlines how these funds maybe used.

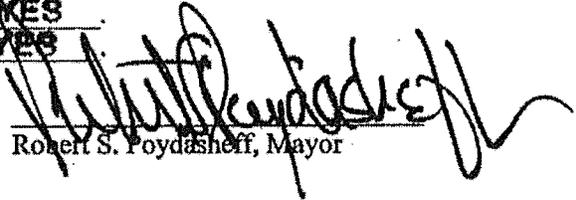
NOW, THEREFORE, THE COUNCIL OF COLUMBUS, GEORGIA, HEREBY RESOLVES AS FOLLOWS:

That the City Manager is hereby authorized to enter into a Project Framework Agreement for Preliminary Engineering Activities with the Georgia Department of Transportation to "High Priority Project" funds for the following projects: Streetscapes in Columbus Phase III (Riverwalk) CSSTP-0007-00 (559) - \$1 million; Trolley Trail and Bike Path CSSTP-0007-00 (563) - \$500,000; and the South Lumpkin Road Trail CSSTP-0007-00 (559) - \$500,000; construction of the Chattahoochee Riverwalk from 13th Street to 14th Street Pedestrian Bridge, in the amount of \$800,000, with a 20 percent match not to exceed \$200,000 for the Streetscapes in Columbus Phase III (Riverwalk) CSSTP-0007-00 (559); \$100,000 for the Trolley Trail and Bike Path CSSTP-0007-00 (563); and \$100,000 for the South Lumpkin Road Trail CSSTP-0007-00.

Introduced at a regular meeting of the Council of Columbus, Georgia, held the 20th day of June 2006, and adopted at said meeting by the affirmative vote of ten members of said Council.

Councilor Allen voting	<u>YES</u>
Councilor Anthony voting	<u>YES</u>
Councilor Davis voting	<u>YES</u>
Councilor Henderson voting	<u>YES</u>
Councilor Hunter voting	<u>YES</u>
Councilor McDaniel voting	<u>YES</u>
Councilor Turner Pugh voting	<u>YES</u>
Councilor Rodgers voting	<u>YES</u>
Councilor Suber voting	<u>YES</u>
Councilor Woodson voting	<u>YES</u>


Tiny B. Washington, Clerk of Council


Robert S. Poydasheff, Mayor

D. Arrington

Isaiah
10/15/06



7093
CITY MANAGER

Department of Transportation

HAROLD E. LINNENKOHL
COMMISSIONER
(404) 656-5206

DAVID E. STUDSTILL, JR., P.E.
CHIEF ENGINEER
(404) 656-5277

State of Georgia
#2 Capitol Square, S.W.
Atlanta, Georgia 30334-1002

October 2, 2006

BUDDY GRATTON, P.E.
DEPUTY COMMISSIONER
(404) 656-5212

EARL L. MAHFUZ
TREASURER
(404) 656-5224

10/6

Isaiah
FYI

The Honorable Robert S. Poydasheff, Mayor
Columbus-Muscogee Consolidated Gov't
P.O. Box 1340
Columbus, Georgia 31902

Dear Mayor Poydasheff:

I am returning for your files an executed agreement between the Georgia Department of Transportation and Muscogee County for the following project:

- PROJECT#:CSSHPP-0007-00(559) Muscogee County, P.I.#0007559**
- PROJECT#:CSSHPP-0007-00 (563) Muscogee County, P.I.#0007563**
- PROJECT#:CSSHPP-0007-00 (633) Muscogee County, P.I.#0007633**

We look forward to working with you on the successful completion of the joint project.
Should you have any questions, please contact the project manager Tom Queen at (706)646-6591.

Sincerely,

James T. Simpson

James T. Simpson,
Financial Management Administrator

JTS:as

Enclosure

- c: Bob Rogers
- Thomas Howell - District 3
- Jeff Baker - Utilities

AGREEMENT
BETWEEN
DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA
AND
Consolidated Government of Columbus-Muscogee County
FOR
TRANSPORTATION FACILITY IMPROVEMENTS

This Framework Agreement is made and entered into this 18th day of September, 2006, by and between the DEPARTMENT OF TRANSPORTATION, an agency of the State of Georgia, hereinafter called the "DEPARTMENT", and the Consolidated Government of Columbus-Muscogee County acting by and through its Mayor and Council, hereinafter called the "LOCAL GOVERNMENT".

WHEREAS, the LOCAL GOVERNMENT has represented to the DEPARTMENT a desire to improve the transportation facility described in Attachment A, attached and incorporated herein by reference and hereinafter referred to as the "PROJECT"; and

WHEREAS, the LOCAL GOVERNMENT has represented to the DEPARTMENT a desire to participate in certain activities including the funding of

certain portions of the PROJECT and the DEPARTMENT has relied upon such representations; and

WHEREAS, the DEPARTMENT has expressed a willingness to participate in certain activities of the PROJECT as set forth in this Agreement; and

WHEREAS, the Constitution authorizes intergovernmental agreements whereby state and local entities may contract with one another "for joint services, for the provision of services, or for the joint or separate use of facilities or equipment; but such contracts must deal with activities, services or facilities which the parties are authorized by law to undertake or provide." Ga. Const. Art. IX, §III, ¶I(a).

NOW THEREFORE, in consideration of the mutual promises made and of the benefits to flow from one to the other, the DEPARTMENT and the LOCAL GOVERNMENT hereby agree each with the other as follows:

1. The LOCAL GOVERNMENT shall contribute to the PROJECT by funding all or certain portions of the PROJECT costs for the preconstruction engineering (design), utility relocations, right of way acquisitions and construction, as specified in Attachment A, attached hereto and incorporated herein by reference. Expenditures incurred by the LOCAL GOVERNMENT and eligible for reimbursement by the DEPARTMENT shall not be considered reimbursable to the LOCAL GOVERNMENT until the LOCAL GOVERNMENT receives a written notice to proceed for each phase of the PROJECT.

If, for any reason, the LOCAL GOVERNMENT does not produce acceptable deliverables in accordance with the approved schedule, the DEPARTMENT reserves the right to delay the project's implementation until funds can be re-identified for construction or right of way, as applicable.

6. The LOCAL GOVERNMENT shall certify that they have read and understands the regulations for "CERTIFICATION OF COMPLIANCES WITH FEDERAL PROCUREMENT REQUIREMENTS, STATE AUDIT REQUIREMENTS, AND FEDERAL AUDIT REQUIREMENTS" and will comply in full with said provisions.

7. The LOCAL GOVERNMENT shall accomplish all of the design activities for the PROJECT. The design activities shall be accomplished in accordance with the DEPARTMENT's Plan Development Process, the applicable guidelines of the American Association of State Highway and Transportation Officials, hereinafter referred to as "AASHTO", the DEPARTMENT's Standard Specifications Construction of Transportation Systems, the DEPARTMENT's Plan Presentation Guide, PROJECT schedules, and applicable guidelines of the DEPARTMENT. The LOCAL GOVERNMENT responsibility for design shall include, but is not limited to the following items:

a. Prepare the PROJECT concept report in accordance with the format used by the DEPARTMENT. The concept for the PROJECT shall be developed to accommodate the future traffic volumes as generated by the LOCAL GOVERNMENT as provided for in paragraph 7b and approved by the

DEPARTMENT. The concept report shall be approved by the DEPARTMENT prior to the LOCAL GOVERNMENT beginning further development of the PROJECT plans. It is recognized by the parties that the approved concept may be modified by the LOCAL GOVERNMENT as required by the DEPARTMENT and re-approved by the DEPARTMENT during the course of design due to public input, environmental requirements, or right of way considerations.

b. Develop the PROJECT base year (year facility is expected to be open to traffic) and design year (base year plus 20 years) traffic volumes. This shall include average daily traffic (ADT) and morning (am) and evening (pm) peak hour volumes. The traffic shall show all through and turning movement volumes at intersections for the ADT and peak hour volumes and shall indicate the percentage of trucks expected on the facility.

c. Validate (check and update) the approved PROJECT concept and prepare a PROJECT Design Book for approval by the DEPARTMENT prior to the beginning of preliminary plans.

d. Prepare environmental studies, documentation, and reports for the PROJECT that show the PROJECT is in compliance with the provisions of the National Environmental Protection Act and Georgia Environmental Protection Act, as appropriate to the PROJECT funding. This shall include any and all archaeological, historical, ecological, air, noise, underground storage tanks (UST), and hazardous waste site studies required as well as any environmental reevaluations required. The LOCAL GOVERNMENT shall

submit to the DEPARTMENT all environmental documents and reports for review and approval by the DEPARTMENT and the FHWA.

e. Prepare all public hearing and public information displays and conduct all required public hearings and public information meetings in accordance with DEPARTMENT practice.

f. Perform all surveys, mapping, soil investigation studies and pavement evaluations needed for design of the PROJECT.

g. Perform all work required to obtain project permits, including, but not limited to, US Army Corps of Engineers 404 and Federal Emergency Management Agency (FEMA) approvals. These efforts shall be coordinated with the DEPARTMENT.

h. Prepare the PROJECT drainage design including erosion control plans and the development of the hydraulic studies for the Federal Emergency Management Agency Floodways and acquisition of all necessary permits associated with the drainage design.

i. Prepare traffic studies, preliminary construction plans including a cost estimate for the Preliminary Field Plan Review, preliminary and final utility plans, preliminary and final right of way plans, staking of the required right of way, and final construction plans including a cost estimate for the Final Field Plan Review, erosion control plans, lighting plans, traffic handling plans, and construction sequence plans and specifications including special provisions for the PROJECT.

j. Provide certification, by a Georgia Registered Professional Engineer, that the construction plans have been prepared under the guidance of the

professional engineer and are in accordance with AASHTO and DEPARTMENT guidelines.

k. Failure of the LOCAL GOVERNMENT to follow the DEPARTMENT's Plan Development Process will jeopardize the use of Federal funds in some or all of the categories outlined in this Agreement, and it shall be the responsibility of the LOCAL GOVERNMENT to make up the loss of that funding.

8. All Primary Consultant firms hired by the LOCAL GOVERNMENT to provide services on the PROJECT shall be prequalified with the DEPARTMENT in the appropriate area-classes. The DEPARTMENT shall, on request, furnish the LOCAL GOVERNMENT with a list of prequalified consultant firms in the appropriate area-classes.

9. The PROJECT construction and right of way plans shall be prepared in English units.

10. All drafting and design work performed on the project shall be done utilizing Microstation and CAiCE software respectively, and shall be organized as per the Department's guidelines on electronic file management.

11. The DEPARTMENT shall review and has approval authority for all aspects of the PROJECT provided however this review and approval does not relieve the LOCAL GOVERNMENT of its responsibilities under the terms of this

agreement. The DEPARTMENT will work with the FHWA to obtain all needed approvals as deemed necessary with information furnished by the LOCAL GOVERNMENT.

12. The LOCAL GOVERNMENT shall be responsible for the design of all bridge(s) and preparation of any required hydraulic and hydrological studies within the limits of this PROJECT in accordance with the DEPARTMENT's policies and guidelines. The LOCAL GOVERNMENT shall perform all necessary survey efforts in order to complete the design of the bridge(s) and prepare any required hydraulic and hydrological studies. The final bridge plans shall be incorporated into this PROJECT as a part of this Agreement.

13. The LOCAL GOVERNMENT shall follow the DEPARTMENT's procedures for identification of existing and proposed utility facilities on the PROJECT. These procedures, in part, require all requests for existing, proposed, or relocated facilities to flow through the DEPARTMENT's Project Liaison and the District Utilities Engineer.

14. The LOCAL GOVERNMENT shall address all railroad concerns, comments, and requirements to the satisfaction of the DEPARTMENT.

15. If the right of way phase is 100% local funding with no Federal or State reimbursement, upon the DEPARTMENT's approval of the project right of way plans, verification that the approved environmental document is current, which shall

mean that the approval of the environmental document occurred within six (6) months of the approval notice by the DEPARTMENT's for project right of way plans, and delivery of a written notice to proceed, the LOCAL GOVERNMENT may proceed with the acquisition of the necessary right of way for the PROJECT. If the right of way phase involves federal and/or state funding reimbursement, upon the Department's approval of the project right of way plans, the Local Government may proceed with all pre-acquisition right of way activities, however, property negotiation and acquisition cannot commence until right of way funding authorization is approved. Right of way acquisition shall be in accordance with the law and the rules and regulations of the FHWA including, but not limited to, Title 23, United States Code; 23 CFR 710, et. Seq., and 49 CFR Part 24 and the rules and regulations of the DEPARTMENT and in accordance with the "Contract for the Acquisition of Right of Way" to be prepared by the Office of Right of Way and executed between the LOCAL GOVERNMENT and the DEPARTMENT prior to the commencement of any right of way activities. Failure of the LOCAL GOVERNMENT to adhere to the provisions and requirements specified in the acquisition contract may result in the loss of Federal funding for the PROJECT and it will be the responsibility of the LOCAL GOVERNMENT to make up the loss of that funding. In the event the LOCAL GOVERNMENT is to receive reimbursement of all or part of the acquisition funding, reimbursable right of way costs are to include land and improvement costs, property damage values, relocation assistance expenses and contracted property management costs. Non reimbursable costs include administrative expenses such as appraisal, consultant, attorney fees and any in-house property management or staff expenses. All required right of way shall be obtained and cleared of

obstructions, including underground storage tanks, prior to advertising the PROJECT for bids. The LOCAL GOVERNMENT shall further be responsible for making all revisions to the approved right of way plans, as deemed necessary by the DEPARTMENT, for whatever reason, as needed to purchase the required right of way.

16. Upon completion and approval of the PROJECT plans, certification that all needed rights of way have been obtained and cleared of obstructions, and certification that all needed permits for the PROJECT have been obtained by the LOCAL GOVERNMENT the PROJECT shall be let for construction. The DEPARTMENT, unless shown otherwise on Attachment A, shall be solely responsible for securing and awarding the construction contract for the PROJECT.

17. The LOCAL GOVERNMENT shall review and make recommendations concerning all shop drawings prior to submission to the DEPARTMENT. The DEPARTMENT shall have final authority concerning all shop drawings.

18. The LOCAL GOVERNMENT agrees that all reports, plans, drawings, studies, specifications, estimates, maps, computations, computer diskettes and printouts, and any other data prepared under the terms of this Agreement shall become the property of the DEPARTMENT if required. This data shall be organized, indexed, bound, and delivered to the DEPARTMENT no later than the advertisement of the PROJECT for letting. The DEPARTMENT shall have the right

to use this material without restriction or limitation and without compensation to the LOCAL GOVERNMENT.

19. The LOCAL GOVERNMENT shall be responsible for the professional quality, technical accuracy, and the coordination of all designs, drawings, specifications, and other services furnished by or on behalf of the LOCAL GOVERNMENT pursuant to this Agreement. The LOCAL GOVERNMENT shall correct or revise, or cause to be corrected or revised, any errors or deficiencies in the designs, drawings, specifications, and other services furnished for this PROJECT. Failure by the LOCAL GOVERNMENT to address the errors or deficiencies within 30 days shall cause the LOCAL GOVERNMENT to assume all responsibility for construction delays caused by the errors and deficiencies. All revisions shall be coordinated with the DEPARTMENT prior to issuance. The LOCAL GOVERNMENT shall also be responsible for any claim, damage, loss or expense, to the extent allowed by law that is attributable to errors, omissions, or negligent acts related to the designs, drawings, specifications, and other services furnished by or on behalf of the LOCAL GOVERNMENT pursuant to this Agreement.

This Agreement is made and entered into in FULTON COUNTY, GEORGIA, and shall be governed and construed under the laws of the State of Georgia.

The covenants herein contained shall, except as otherwise provided, accrue to the benefit of and be binding upon the successors and assigns of the parties hereto.

IN WITNESS WHEREOF, the DEPARTMENT and the LOCAL GOVERNMENT have caused these presents to be executed under seal by their duly authorized representatives.

RECOMMENDED:

[Signature]
District Engineer - Thomaston

[Signature]
Deputy Commissioner

[Signature]
Chief Engineer

LOCAL GOVERNMENT NAME

BY: [Signature]
Name Isaiah Hugley
Title City Manager

Signed, sealed and delivered this 18 day of July, 2006, in the presence of:

DEPARTMENT OF TRANSPORTATION

BY: [Signature]
Commissioner

ATTEST: [Signature]
Treasurer

REVIEWED AS TO LEGAL FORM:
[Signature] 9-14-06
Office of Legal Services

[Signature]

Witness

[Signature]
Notary Public
My Commission Expires March 11, 2007

This Agreement approved on the 20 day of June, 2006.

[Signature]
City/County Clerk (as appropriate), Deputy

FEIN: 58-1097948

EXECUTION AUTHORIZED

By Resolution No. 241-06

[Signature]
Clerk of Council

ATTACHMENT "A"

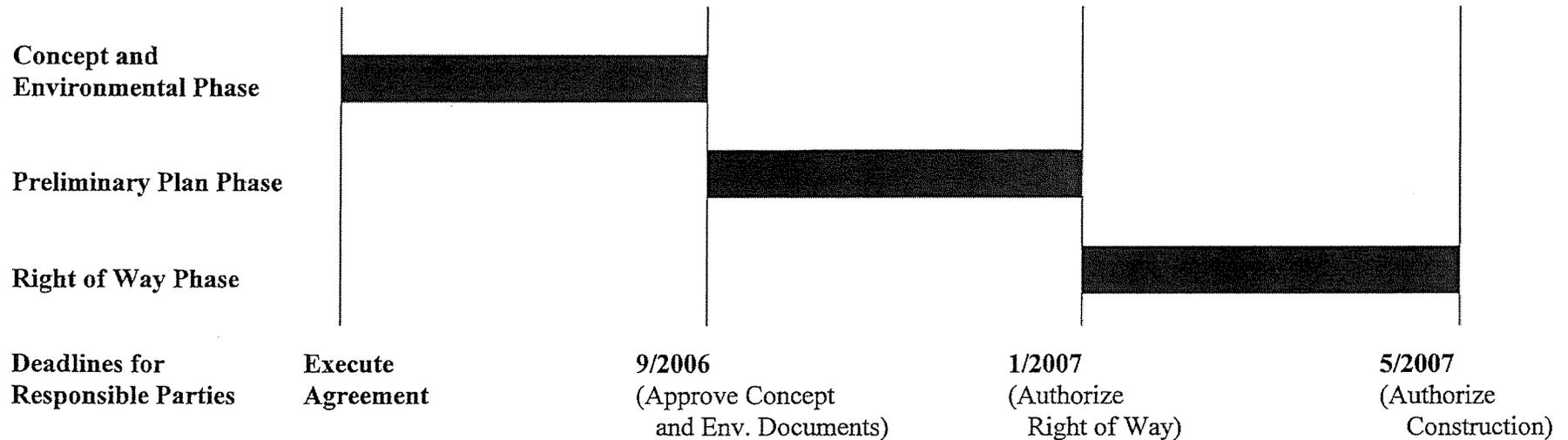
Project Numbers: CSHP-0007-00 (559), (563), & (633) – Muscogee County

Project (PI#, Project #, Description)	Work Type	Preliminary Engineering		Right of Way		Construction		Utilities
		Funding	Design	Funding	Acquisition	Funding	Letting	Relocation Costs
PI# 0007559 CSSHP-0007-00 (559) Streetscapes Phase III	Streetscapes	100% Local	Consolidated Government of Columbus-Muscogee County	100% Local	Consolidated Government of Columbus-Muscogee County	80% DOT/Fed (\$ 800,000) 20% City (\$ 200,000) >100% City	GDOT FY 2007	100% Consolidated Government of Columbus-Muscogee County
PI# 0007563 CSSHP-0007-00 (563) Trolley Trail Jogging and Bike Path in Uptown Columbus	Multi-Use Trail	100% Local	Consolidated Government of Columbus-Muscogee County	100% Local	Consolidated Government of Columbus-Muscogee County	80% DOT/Fed (\$ 400,000) 20% City (\$ 100,000) >100% Sponsor	GDOT FY 2009	100% Consolidated Government of Columbus-Muscogee County
PI# 0007633 CSSHP-0007-00 (633) South Lumpkin Road Trail in Columbus	Multi-Use Trail	100% Local	Consolidated Government of Columbus-Muscogee County	100% Local	Consolidated Government of Columbus-Muscogee County	80% DOT/Fed (\$ 400,000) 20% City (\$ 100,000) >100% City	GDOT FY 2009	100% Consolidated Government of Columbus-Muscogee County

Note: 1. Maximum allowable GDOT reimbursible amount may be shown above in lieu of percentages when applicable. Local Government will only be reimbursed the percentage of the accrued invoiced amounts up to but not to exceed the maximum amount indicated.
 2. Cash participation limits may be shown above in lieu of percentages when applicable.

ATTACHMENT "B-1" CSHP-0007-00 (559) – Muscogee County

Proposed Project Schedule



Annual Reporting Requirements

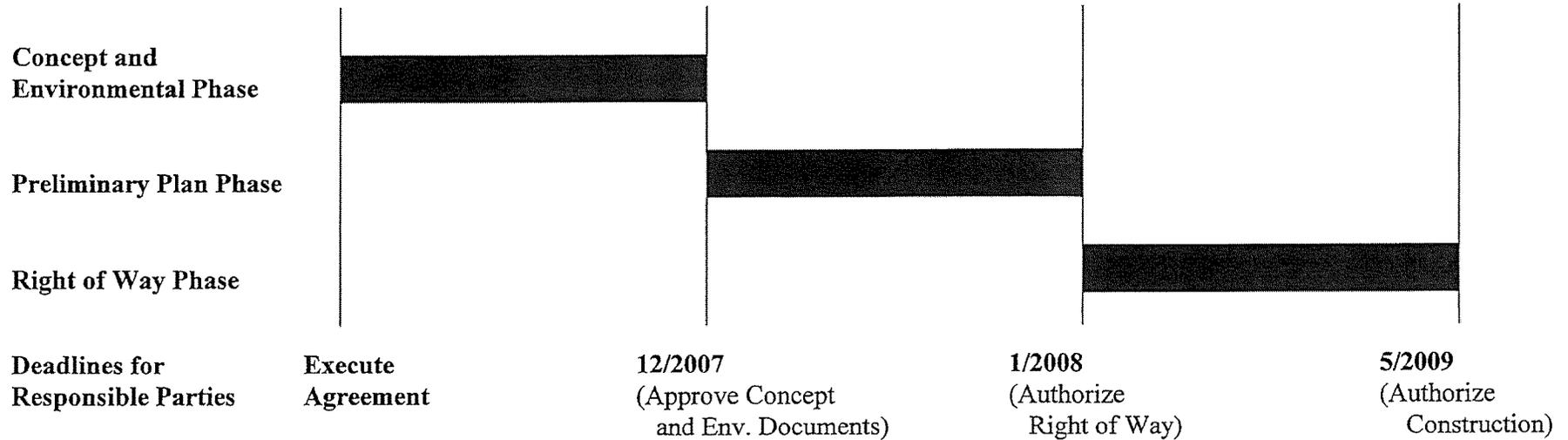
The Local Government shall provide a written status report to the Department's Project Manager with the actual phase completion date(s) and the percent complete/proposed completion date of incomplete phases. The written status report shall be received by the Department no later than the first day of February of every calendar year until all phases have been completed.

Training Certification Requirement

The Local Government shall provide a written certification that all appropriate staff (employees and consultants) involved in the Project have attended or are scheduled to attend the Department's Plan Development Process Training Course. The written certification shall be received by the Department no later than the first day of February of every calendar year until all phases have been completed.

ATTACHMENT "B-2" CSHPP-0007-00 (563) & (633) – Muscogee County

Proposed Project Schedule



Annual Reporting Requirements

The Local Government shall provide a written status report to the Department's Project Manager with the actual phase completion date(s) and the percent complete/proposed completion date of incomplete phases. The written status report shall be received by the Department no later than the first day of February of every calendar year until all phases have been completed.

Training Certification Requirement

The Local Government shall provide a written certification that all appropriate staff (employees and consultants) involved in the Project have attended or are scheduled to attend the Department's Plan Development Process Training Course. The written certification shall be received by the Department no later than the first day of February of every calendar year until all phases have been completed.