

D.O.T. 66

DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA

INTERDEPARTMENT CORRESPONDENCE

**FILE** P. I. No. 0007140, Monroe County **OFFICE** Preconstruction  
CSSTP-0007-00(140)  
SR 74 at SR 42 – Intersection Improvements **DATE** July 26, 2007

**FROM** *Genetha Rice-Singleton*  
Genetha Rice-Singleton, Assistant Director of Preconstruction

**TO** *see* SEE DISTRIBUTION

**SUBJECT** APPROVED PROJECT CONCEPT REPORT

Attached for your files is the approval for subject project.

GRS/cj

Attachment

DISTRIBUTION:

Brian Summers

Ken Thompson

Jamie Simpson

Michael Henry

Keith Golden

Angela Alexander (file copy)

Babs Abubakari

Thomas Howell

BOARD MEMBER

**DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA**

**INTERDEPARTMENTAL CORRESPONDENCE**

**FILE:** P.I. No. 0007140, Monroe County  
CSSTP-0007-00(140)  
Intersection Improvement of SR 74 @ SR 42

**OFFICE:** Preconstruction

**DATE:** July 17, 2007

**FROM:**  Genetha Rice-Singleton, Assistant Director of Preconstruction

**TO:**  David E. Studstill, Jr., P.E., Chief Engineer

**SUBJECT: PROJECT CONCEPT REPORT**

This project is the intersection improvements at SR 74 and SR 42, located 1.0 mile south of Russellville, Georgia. State Route 74 and SR 42 are rural roadways, each consisting of two, 12' lanes and rural shoulders with a posted speed of 55 MPH. The intersection of SR 74 and SR 42 is poorly aligned on all four approaches. Currently, the existing skew at the intersection contributes to safety and operational challenges. In the project area, fourteen (14) accidents occurred between 2003 and 2005, four (4) in 2003, four (4) in 2004, and six (6) in 2005. Projected traffic volumes for this section of SR 74 are 1000 VPD in the year 2011 and 2400 VPD for the design year 2031. The projected volumes along SR 42 are 1500 VPD for the year 2011 and 3600 VPD in the design year 2031.

The proposed project will realign both roadways to correct substandard vertical curves and horizontal curves that inhibit sight distance. Improvements on SR 74 will extend from MP 8.73 to MP 9.79 utilizing a combination of new location and short portions of existing roadway. Improvements on SR 42 will consist of realigning the existing roadway to the west to provide better sight distance and correct the substandard vertical and horizontal alignment. The proposed typical section will consist of two, 12' lanes with 8' rural shoulders (2' paved). Left and right turn lanes will be provided at all approaches and provisions for radius returns to allow for efficient turning movements. All improvements will meet the design speed of 55 MPH for SR 74 and 45 MPH for SR 42. Traffic will be maintained via staging during construction.

Environmental concerns include requiring a Categorical Exclusion will be prepared; a Public hearing is not required; Time saving procedures is appropriate.

P.I. No. 0007140, Monroe County  
July 17, 2007

The estimated costs for this project are:

	<u>PROPOSED</u>	<u>APPROVED</u>	<u>FUNDING</u>	<u>PROG DATE</u>
Construction (includes E&C)	\$ 3,338,000	\$ 3,338,000	LS30	LUMP
Right-of-way & Utilities*	\$ 1,104,000 \$ 70,000	\$ 1,100,000	LS30	LUMP

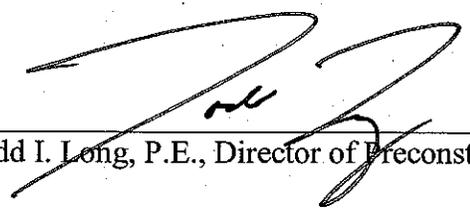
\*Notification letter sent 4-11-2005.

I recommend this project concept be approved.

GRS: JDQ

Attachment

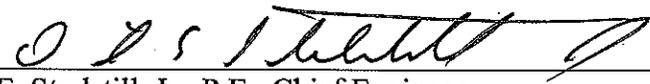
CONCUR



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Todd I. Long, P.E., Director of Preconstruction

APPROVED



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David E. Studstill, Jr., P.E., Chief Engineer

## SCORING RESULTS AS PER MOG 2440-2

<b>Project Number:</b> CSSTP-0007-00(140)		<b>County:</b> Monroe		<b>PI No.:</b> 0007140		
<b>Report Date:</b> June 12,2007		<b>Concept By:</b> DOT Office: District 3				
<input checked="" type="checkbox"/> Concept Stage						
<b>Project Type:</b> Choose One From Each Column		<input type="checkbox"/> Major <input checked="" type="checkbox"/> Minor	<input type="checkbox"/> Urban <input checked="" type="checkbox"/> Rural	<input type="checkbox"/> ATMS <input type="checkbox"/> Bridge Replacement <input type="checkbox"/> Building <input type="checkbox"/> Interchange Reconstruction <input checked="" type="checkbox"/> Intersection Improvement <input type="checkbox"/> Interstate <input type="checkbox"/> New Location <input type="checkbox"/> Widening & Reconstruction <input type="checkbox"/> Miscellaneous		
<b>FOCUS AREAS</b>	<b>SCORE</b>	<b>RESULTS</b>				
<b>Presentation</b>	100					
<b>Judgment</b>	100					
<b>Environmental</b>	100					
<b>Right of Way</b>	100					
<b>Utility</b>	100					
<b>Constructability</b>	100					
<b>Schedule</b>	100					

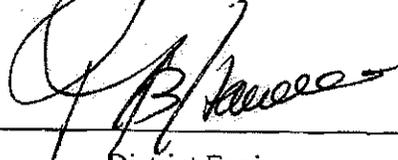
Project Concept Report Page 2  
Project Number: CSSTP-0007-00(140)  
P.I. Number: 0007140  
County: Monroe

Recommendation for approval:

DATE 6/12/07

  
Project Manager

DATE 6/12/07

  
District Engineer

The concept as presented herein and submitted for approval is consistent with that which is included in the Regional Transportation Improvement Program (RTP) and the State Transportation Improvement Program (STIP).

DATE \_\_\_\_\_

\_\_\_\_\_  
State Transportation Planning Administrator

DATE \_\_\_\_\_

\_\_\_\_\_  
State Transportation Financial Management  
Administrator

DATE \_\_\_\_\_

\_\_\_\_\_  
State Environmental/Location Engineer

DATE \_\_\_\_\_

\_\_\_\_\_  
State Traffic Safety & Design Engineer

DATE 7/13/07

  
Project Review Engineer

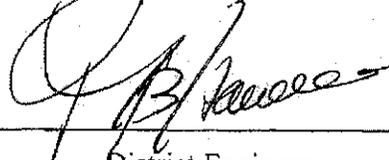
Project Concept Report Page 2  
Project Number: CSSTP-0007-00(140)  
P.I. Number: 0007140  
County: Monroe

Recommendation for approval:

DATE 6/12/07

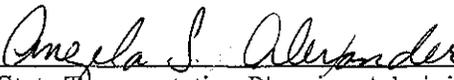
  
Project Manager

DATE 6/12/07

  
District Engineer

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DATE 6/18/07

  
State Transportation Planning Administrator

DATE \_\_\_\_\_

\_\_\_\_\_  
State Transportation Financial Management  
Administrator

DATE \_\_\_\_\_

\_\_\_\_\_  
State Environmental/Location Engineer

DATE \_\_\_\_\_

\_\_\_\_\_  
State Traffic Safety & Design Engineer

DATE \_\_\_\_\_

\_\_\_\_\_  
Project Review Engineer

Project Concept Report Page 2  
Project Number: CSSTP-0007-00(140)  
P.I. Number: 0007140  
County: Monroe

Recommendation for approval:

DATE 6/12/07

Project Manager

DATE 6/12/07

District Engineer

The concept as presented herein and submitted for approval is consistent with that which is included in the Regional Transportation Improvement Program (RTP) and the State Transportation Improvement Program (STIP).

DATE \_\_\_\_\_

\_\_\_\_\_  
State Transportation Planning Administrator

DATE \_\_\_\_\_

\_\_\_\_\_  
State Transportation Financial Management  
Administrator

DATE 6/10/07

State Environmental/Location Engineer

DATE \_\_\_\_\_

\_\_\_\_\_  
State Traffic Safety & Design Engineer

DATE \_\_\_\_\_

\_\_\_\_\_  
Project Review Engineer

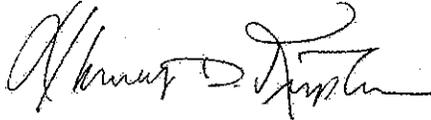
**DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA**

JUN 18 2007

**INTERDEPARTMENT CORRESPONDENCE**

**FILE: P.I. No. 0007140**

**OFFICE: Environment/Location**



**DATE: June 18, 2007**

**FROM: Harvey D. Keeper, State Environmental/Location Engineer**

**TO: Genetha Rice-Singleton, Assistant Director of Preconstruction**

**SUBJECT: PROJECT CONCEPT REPORT  
CSSTP-0007-00(140) / Monroe County  
Intersection Improvement of SR 74 @ SR 42**

The above subject concept report has been reviewed. There are a couple of potentially eligible historic structures on SR 42 south of the intersection. There are at least six (6) archaeological sites including a cemetery within the APE. Need to exercise care to avoid or minimize impacts to these resources.

If you have any questions, please contact me at (404) 699-4401.

HDK/lc

Attachment

cc: Brian Summers  
Keith Golden  
Angela Alexander  
Jamie Simpson  
Thomas Howell

## NOTICE OF LOCATION AND DESIGN APPROVAL

CSSTP-0007-00(140) Monroe County  
P.I. Number: 0007140

Notice is hereby given in compliance with Georgia Code 22-2-109 that the Georgia Department of Transportation has approved the Location and Design of the above project.

The date of location approval is July 26, 2007.

This project will improve the intersection of S.R 74 and S.R. 42 in southwest Monroe County. This intersection is located 18.22 miles west of the city limits of Thomaston, and 10.5 miles south of the city limits of Forsyth. This project includes areas in Georgia Militia District 599, Land District 12, and Land Lots 167, 168, 172, 173, 174, 175, 199, 200, 201, 202, 203, 208, and 209.

The project includes correcting a substandard intersection on SR74 at SR 42. This intersection will be relocated, and both roadways will be realigned. The proposed project includes the addition of an eastbound and westbound left turn lane, an eastbound and westbound right turn lane, and a northbound and southbound right turn lane. The length of the project is 1.06 miles (5597 feet), beginning at milepost 8.73, and ending at milepost 9.79 on SR 74. The project tie in begins at milepost 1.86, and ends at milepost 2.47 on SR 42. This project will improve the immediate and future safety needs at this intersection.

Drawings, maps or plats of the proposed project, as approved, are on file and are available for inspection at the Georgia Department of Transportation:

Tim Reeves  
Tim.reeves@dot.state.ga.us  
101 Transportation Blvd., Thomaston, Georgia 30286  
(706)-646-6100 / (706)-646-6609

Any interested party may obtain a copy of the drawings, maps or plats, or portions thereof, by paying a nominal fee and requesting in writing to:

William J. Rountree, P.E., District Design Engineer  
Department Of Transportation  
bill.rountree@dot.state.ga.us  
115 Transportation Blvd.  
Thomaston, Georgia 30286  
(706) 646-6604

Any written request or communication in reference to this project or notice SHOULD include the Project and P.I. Numbers as noted at the top of this notice.

**DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA**

*Office of District 3 Design*

**PROJECT CONCEPT REPORT**

Project Number: CSSTP-0007-00(140)

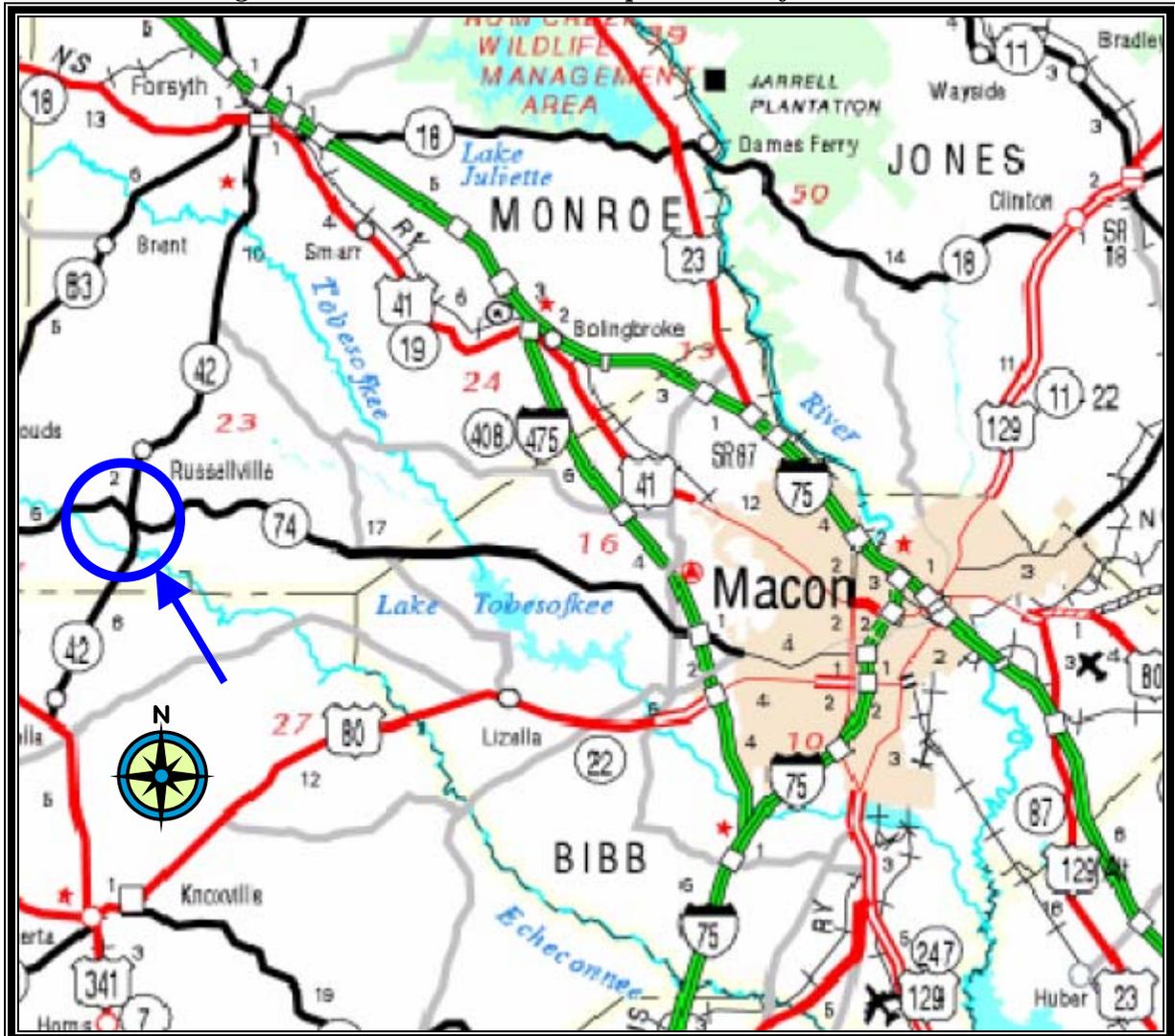
County: Monroe

P. I. Number: 0007140

Federal Route Number: N/A

State Route Number: 74 & 42

***Regional Sketch: Intersection Improvement of SR74 @ SR42***

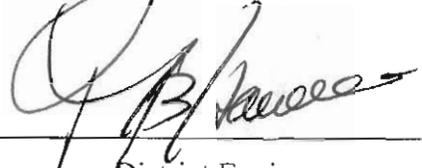


Recommendation for approval:

DATE 6/12/07

  
\_\_\_\_\_  
Project Manager

DATE 6/12/07

  
\_\_\_\_\_  
District Engineer

The concept as presented herein and submitted for approval is consistent with that which is included in the Regional Transportation Improvement Program (RTP) and the State Transportation Improvement Program (STIP).

DATE \_\_\_\_\_

\_\_\_\_\_  
State Transportation Planning Administrator

DATE \_\_\_\_\_

\_\_\_\_\_  
State Transportation Financial Management  
Administrator

DATE \_\_\_\_\_

\_\_\_\_\_  
State Environmental/Location Engineer

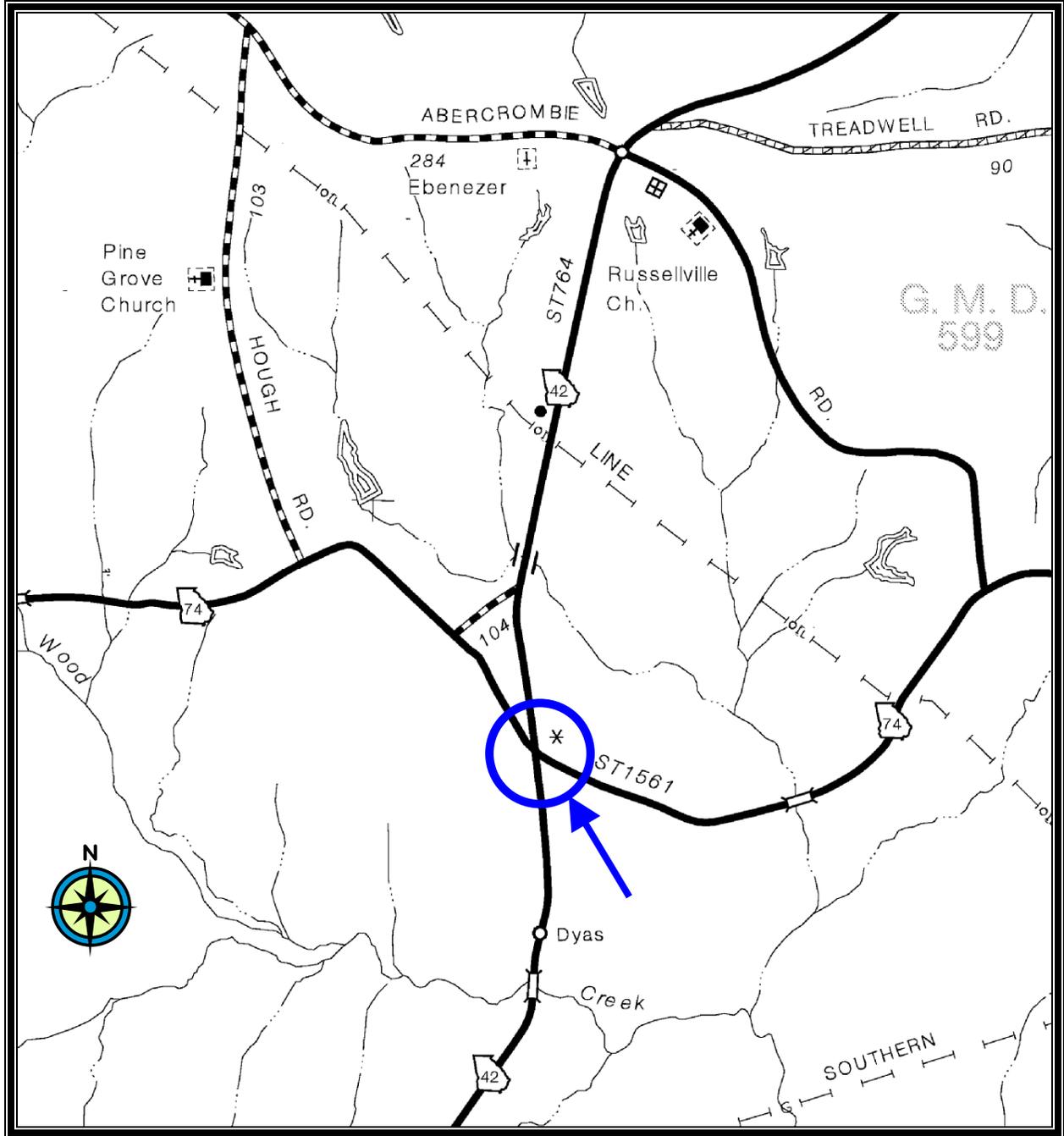
DATE \_\_\_\_\_

\_\_\_\_\_  
State Traffic Safety & Design Engineer

DATE \_\_\_\_\_

\_\_\_\_\_  
Project Review Engineer

**Project Location Map: Intersection Improvement of SR74 @ SR42**



**Need and Purpose:**

**Background**

Project CSSTP-0007-00(140) is a Lump Sum project that will relocate and realign the intersection of SR 42 at SR74. The existing roadway approaching the intersection of SR 42 at SR 74 is a two-lane roadway facility with 12-foot wide travel lanes. SR 42 is a north-south connector route through Monroe County and is functionally classified as a rural major collector. SR 74 is an east-west route through the county and is functionally classified as a rural minor arterial.

**Safety**

Currently, the existing skew at the intersection of SR 42 and SR 74 contributes to safety and operational challenges.

The injury and fatality rates for SR74 exceeds the statewide averages rate in 2004 with three (3) accidents and three (3) injuries. However, the accident rate exceeded the statewide average rate during 2003, 2004, and 2005. The statewide averages are based on a formula that is unique to each functional classification route statewide. The Statewide averages are updated each year by GDOT’s Office of Traffic Safety & Design as new data becomes available. In the project area, fourteen (14) accidents occurred between 2003 and 2005, four (4) in 2003, four (4) in 2004, and six (6) in 2005. The breakdown of crash types over the 3-year period is as follows: three (3) were angle intersect, one (1) was a head-on collision, one (1) was a side-swipe, and the other nine (9) resulted from the vehicles leaving the road and striking a fixed object.

Tables #1 and #2 below provide a comparison of the accident rates with the state average for a similarly functionally classified road for the years 2003 through 2005.

**Table #1  
 From MP 2.02 to 2.61**

SR 42	2003		2004		2005	
	SR 42	STATE	SR 42	STATE	SR 42	STATE
Accident Rate	774	212	469	243	860	181
Injury Rate	0	113	0	134	0	103
Fatality Rate	0	2.56	0	2.77	0	2.48

**Table #2**  
**From MP 9.02 to 11.02**

SR 74	2003		2004		2005	
	SR 74	STATE	SR 74	STATE	SR 74	STATE
Accident Rate	264	211	300	273	413	197
Injury Rate	0	110	225	145	69	111
Fatality Rate	0	2.95	0	2.93	0	3.00

### **Projects in the Area**

Currently there is a roundabout project (P.I. No. 0007647) in the preconstruction phase at the intersection of S.R. 74 and S.R.7/U.S. 341. This project is approximately 6.24 miles to the west of the existing intersection of S.R. 74 and S.R. 42.

### **Need and Purpose**

The need exists to improve the operational and safety conditions of the intersection at SR 42/SR 74. Roadway improvements should reduce the accident rate at the intersection, while improving safety.

**Description of the Proposed Project:**

The project includes correcting a substandard intersection on SR 74 at SR 42. This intersection will be relocated, and both roadways will be realigned. The proposed project includes the addition of an eastbound and westbound left turn lane, an eastbound and westbound right turn lane, and a northbound and southbound right turn lane.

The project's length is 1.06 miles (5597 feet), beginning at milepost 8.73 and ending at milepost 9.79 on existing SR 74. The project tie in begins at milepost 1.86, and ends at milepost 2.47 on SR 42. The intersection of SR 42 and SR 74 is 18.22 miles west of the city limits of Thomaston, and 10.5 miles south of the city limits of Forsyth. This project will improve the immediate and future safety needs at the intersection.

**Is the project located in a Non-attainment area?** Yes  No .

**PDP Classification:** Major  Minor

**Federal Oversight:** Full Oversight , Exempt , State Funded , or Other

**Functional Classification:** Rural Major Collector (SR 42) and Rural Minor Arterial (SR 74)

**U. S. Route Number(s):** None

**State Route Number(s):** 42 and 74

**Traffic (AADT):**

Traffic(AADT) Two Way Traffic	SR 42	SR 74
<b>Current Year: (2011)</b>	1000	2400
<b>Design Year: (2031)</b>	1500	3600
<b>K =</b>	10%	9.6%
<b>D =</b>	57%	55%
<b>T =</b>	17%	17%
<b>24 HR T =</b>	15%	15%

**Existing Design Features:**

- Typical Section: Two 12-ft lanes in each direction with 8 ft shoulders, two (2) feet of pavement, and six (6) feet of grass. (SR 42 & SR 74)
- Posted Speed: 55 mph (SR 42 & SR 74)
- Minimum Radius: 725 feet
- Maximum Grade: 6.00%
- Total Width of Right of Way: 100 feet (SR 42 & SR 74)
- Major Structures: None
- Major Interchanges or Intersections Along the Project: SR 42 at SR 74
- Existing Length of Roadway Segment and the Beginning Mile Logs for Each County Segment:
  - SR 42: Begin mile post at 1.86 and ending mile post at 2.47 (0.61 miles)
  - SR 74: Begin mile post at 8.78 and ending milepost at 9.79 (1.01 miles)

**Proposed Design Features:**

- Proposed Typical Section(s): Two 12-ft lanes in each direction with ten (10) foot shoulders, two (2) feet of pavement and eight (8) feet of grass. (SR 42 & SR 74)
- Proposed Design Speed: 45 mph (SR 42) 55 mph (SR 74)
- Proposed Maximum Grade Mainline: 5.00 %      Maximum Grade Allowable: 5.00%
- Proposed Maximum Grade Side Street: 5.00 %      Maximum Grade Allowable: 7.00%
- Proposed Maximum Grade Driveway: 10 %
- Proposed Minimum Radius of Curve: 660 feet (SR42)    2500 feet (SR74)
- Minimum Radius Allowable: 660 feet (SR42)    1065 feet (SR74)
- Right of Way:
  - Width: 140-230 feet
  - Easements: Temporary , Permanent , Utility , Other .
  - Type of access control: Full , Partial , By Permit , Other .
  - Number of parcels: 31      Number of displacements: 0
- Structures: None
- Major Intersections and Interchanges: SR 42 at SR 74
- Traffic Control During Construction: Traffic will be maintained on the existing roadway.

- Design Exceptions to Controlling Criteria Anticipated:
 

	<u>UNDETERMINED</u>	<u>YES</u>	<u>NO</u>
○ HORIZONTAL ALIGNMENT:	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
○ ROADWAY WIDTH:	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
○ SHOULDER WIDTH:	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
○ VERTICAL GRADES:	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
○ CROSS SLOPES:	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
○ STOPPING SIGHT DISTANCE:	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
○ SUPERELEVATION RATES:	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
○ HORIZONTAL CLEARANCE:	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
○ SPEED DESIGN:	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
○ VERTICAL CLEARANCE:	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
○ BRIDGE WIDTH:	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
○ BRIDGE STRUCTURAL CAPACITY:	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

- Design Variances: None
- Environmental Concerns:
  - There are three possible Historic Resources in the project area.
- Level of Environmental Analysis:
  - Are Time Savings Procedures appropriate? Yes , No ,
  - Categorical Exclusion
  - Environmental Assessment/Finding of No Significant Impact (FONSI)
  - Environmental Impact Statement (EIS)
- Utility Involvements:
  - Power – Southern Rivers Energies
  - Telecommunications – Bellsouth, Windstream, Public Service Telephone

**Project Responsibilities:**

- Design: GDOT
- Right of Way Acquisition: GDOT
- Relocation of Utilities: GDOT
- Letting to contract: GDOT
- Supervision of construction: GDOT
- Providing material pits: Contractor

**Coordination:**

- Concept Meeting Date and Brief Summary. (See attached minutes)
- Other projects in the Area: STP-0007-00(647) Monroe P.I. 0007647
- Other Coordination to Date: Public Information Open House – October 17, 2006 (See attached summary)

### **Scheduling – Responsible Parties’ Estimate**

- Time to Complete the Environmental Process: 12 Months
- Time to Complete the Preliminary Construction Plans: 8 Months
- Time to Complete Right of Way Plans: 3 Months
- Time to Complete the Section 404 Permit: 6 Months
- Time to Complete Final Construction Plans: 6 Months
- Time to Complete the Purchase of Right of Way: 12 Months
- Time to Complete the Utilities Relocation: 6 Months

### **Alternates Considered:**

- **Alternate 1:** This alternate realigned SR42 through Dyas School Road to intersect SR74 at a 90° angle. Both roadways were designed for 55mph. This alternate was presented at the Public Information Open House (PIOH). The overall public opinion was that the proposed design was excessive and would have detrimental effects to many environmentally sensitive areas. This alternate would be too expensive considering construction and right of way costs.
- **Alternate 2:** This alternate was suggested by some of the citizens in response to the PIOH. The intersection would be located very near the existing intersection. SR42 would be realigned using 45mph design, reducing project limits and required right of way. This alternate was rejected due to poor intersection sight distance. The intersection would be located in a sag vertical curve.
- **Alternate 3:** This alternate combined aspects of Alternates 1 and 2. SR42 would be realigned using 45mph design. It would be located in a crest allowing better intersection sight distance than Alternate 2. This was the chosen alternate, as it reduced the costs and impacts of Alternate 1 and provided better sight distance than Alternate 2.
- **Alternate 4:** No Build – This alternative was rejected because it did not improve the operational and safety conditions as required by the need and purpose.

### **Attachments:**

1. Cost Estimates:
  - a. Construction including E&C
  - b. Right of Way
  - c. Utilities
2. Layout
3. Typical Sections
4. Capacity Analysis
5. Concept Team Meeting Minutes
6. Public Information Open House Summary
7. Notice of Location and Design Approval

# PRELIMINARY COST ESTIMATE

PROJECT NUMBER: STP-0007-00(140) COUNTY: MONROE

PI #: 0007140 DESCRIPTION: INTERSECTION IMPROVEMENT

DATE: 6/11/2007 ESTIMATED LETTING DATE: July 2011

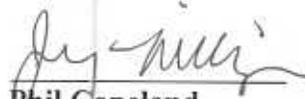
PREPARED BY: JEFF SWIDERSKI PROJECT LENGTH: 1.06 MILE

PROGRAMMING PROCESS  CONCEPT DEVELOPMENT  DURING PROJECT DEVELOPMENT

<b>A. RIGHT-OF-WAY:</b>			
1. PROPERTY (LAND & EASEMENT)	15 AC	\$15,000.00 PER AC	\$225,000.00
2. DISPLACEMENTS	RES: 0	BUS: 0	M.H.: 0
3. OTHER COST (ADM./COST, INFLATION)			\$783,430.00
<b>SUBTOTAL: A</b>			<b>\$1,103,430.00</b>
<b>B. REIMBURSABLE UTILITIES:</b>			
1. RAILROAD			\$0.00
2. TRANSMISSION LINES			\$40,000.00
3. SERVICES			\$30,000.00
<b>SUBTOTAL: B</b>			<b>\$70,000.00</b>
<b>C. CONSTRUCTION:</b>			
<b>1. MAJOR STRUCTURES</b>			
a. BRIDGES	0 LF	\$0.00 PER LF	\$0.00
b. CONC APPROACH SLAB	0 EA	\$0.00 PER EA	\$0.00
<b>c. CULVERTS</b>			
1) CLASS A CONCRETE	53 CY	\$500.00 PER CY	\$26,500.00
2) BAR REINF STEEL	5,300 LB	\$0.80 PER LB	\$4,240.00
d. RETAINING WALLS	0 SF	\$45.00 PER SF	\$0.00
<b>SUBTOTAL: C-1</b>			<b>\$30,740.00</b>
<b>2. GRADING AND DRAINAGE</b>			
a. EARTHWORK	100,000 CY	\$10.00 PER CY	\$1,000,000.00
<b>b. DRAINAGE</b>			
1) CROSS DRAIN PIPE	4 EA	\$11,300.00 PER EA	\$45,200.00
2) SIDE DRAIN PIPE	10 EA	\$6,200.00 PER EA	\$62,000.00
3) CURB AND GUTTER, 8" X 30"	0 LF	\$20.00 PER LF	\$0.00
4) LONGITUDINAL SYSTEM	0 LF	\$79.00 PER LF	\$0.00
<b>SUBTOTAL: C-2</b>			<b>\$1,107,200.00</b>
<b>3. BASE AND PAVING</b>			
a. AGGREGATE BASE	12,100 TN	\$25.00 PER TN	\$302,500.00
<b>b. ASPHALT PAVING</b>			
1) SURFACE	2,100 TN	\$100.00 PER TN	\$210,000.00
2) BINDER	2,500 TN	\$100.00 PER TN	\$250,000.00
3) BASE	4,900 TN	\$100.00 PER TN	\$490,000.00
4) LEVELING	500 TN	\$100.00 PER TN	\$50,000.00
<b>SUBTOTAL: C-3.b</b>			<b>\$1,000,000.00</b>
c. BITUM TACK COAT	1,700 GAL	\$2.00 PER GAL	\$3,400.00
d. MILLING	1,300 SY	\$5.00 PER SY	\$6,500.00
e. CONCRETE PAVING	0 SY	\$75.00 PER SY	\$0.00
f. CONCRETE MEDIAN, 8 IN	0 SY	\$35.00 PER SY	\$0.00
<b>SUBTOTAL: C-3</b>			<b>\$1,312,400.00</b>

<b>4. LUMP ITEMS:</b>			
a. GRASSING	8 AC	\$2,000.00 PER AC	\$16,000.00
b. CLEARING AND GRUBBING	13 AC	\$5,000.00 PER AC	\$62,500.00
c. LANDSCAPING			\$0.00
d. EROSION CONTROL			\$344,000.00
e. TRAFFIC CONTROL			\$100,000.00
f. SIGNAL			\$0.00
g. LIGHTING			\$0.00
h. ATMS			\$0.00
SUBTOTAL: C-4			<b>\$522,500.00</b>
<b>5. MISCELLANEOUS:</b>			
a. SIGNING	14 EA	\$560.00 PER EA	\$7,840.00
b. ASPH PAVEMENT MARKING	8,000 LF	\$1.20 PER LF	\$9,600.00
c. CONC PAVEMENT MARKING	0 LF	\$0.00 PER LF	\$0.00
d. GUARDRAIL	1,000 LF	\$30.00 PER LF	\$30,000.00
e. SIDEWALK, 6 IN	0 LF	\$25.00 PER LF	\$0.00
f. RIP RAP, TY 1	500 SY	\$28.00 PER SY	\$14,000.00
SUBTOTAL: C-5			<b>\$61,440.00</b>
<b>6. SPECIAL FEATURES:</b>			
a.	0 UNIT	\$0.00 COST PER UNIT	\$0.00
SUBTOTAL: C-6			<b>\$0.00</b>
<b>ESTIMATE SUMMARY</b>			
A. RIGHT-OF-WAY			<b>\$1,103,430.00</b>
B. REIMBURSABLE UTILITIES			<b>\$70,000.00</b>
C. CONSTRUCTION			
1. MAJOR STRUCTURES		\$30,740.00	
2. GRADING AND DRAINAGE		\$1,107,200.00	
3. BASE AND PAVING		\$1,312,400.00	
4. LUMP ITEMS		\$522,500.00	
5. MISCELLANEOUS		\$61,440.00	
6. SPECIAL FEATURES		\$0.00	
SUBTOTAL CONSTRUCTION COST			\$3,034,280.00
E. & C. (10%)			\$303,428.00
TOTAL CONSTRUCTION COST			<b>\$3,337,708.00</b>
<b>GRAND TOTAL PROJECT COST</b>			<b>\$4,511,138.00</b>
This project is 100 % in Congressional District 8			

# Preliminary Right of Way Cost Estimate



**Phil Copeland**  
 Right of Way Administrator  
 By: Jerry Milligan

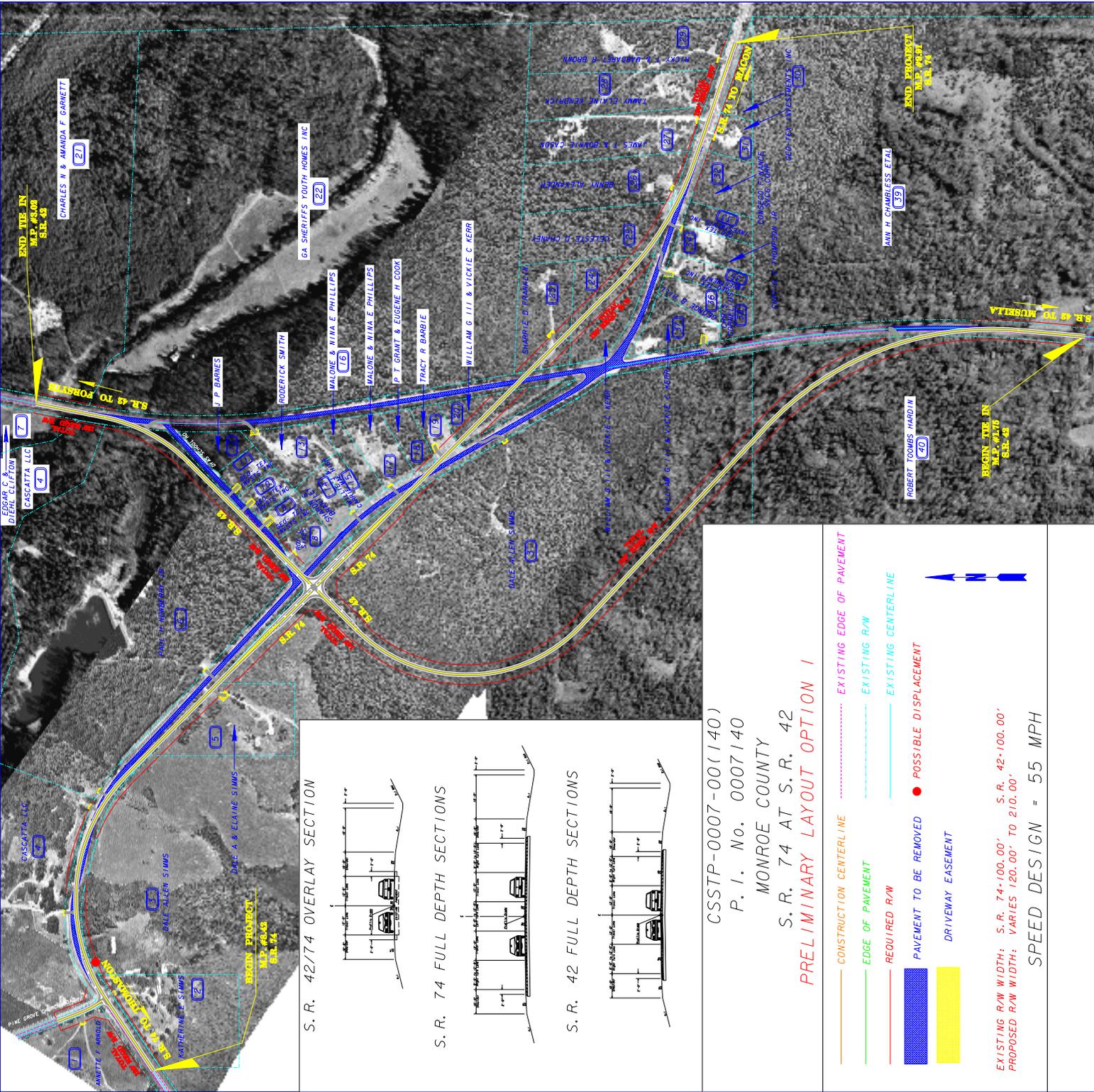
Date: February 7, 2007  
 Project: STP-0007-00(140)Monroe **UPDATED**  
 Existing/Required R/W: Varies/Varies  
 Project Termini: Intersection Improvement SR 74 @ SR 42  
 Project Description: SR 74 @ SR 42 Intersection Improvement

P.I. Number: 0007140  
 No. Parcels: 23

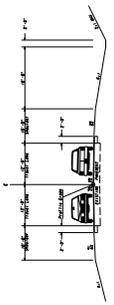
Land Agricultural / Residential	14.7 acres @ \$ 15,000	\$ 221,922
Improvements : fencing, gate, misc. site improvement		20,000
Relocation: Residential ( 0 ) Commercial ( 0 )		0
Damage : Cost to Cure (0) parcel Proximity ( 0)parcel Uneconomic Remnant(1)		
	Net Cost	\$ 316,922
	Net Cost	\$ 316,922
	Scheduling Contingency 55 %	174,307
	Adm/Court Cost 60 %	294,737
	Market Appreciation 40 %	314,386
		<u>\$ 1,100,352</u>

**Total Cost \$ 1,100,400**

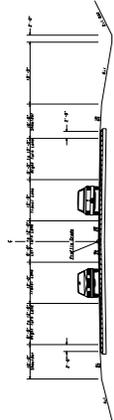




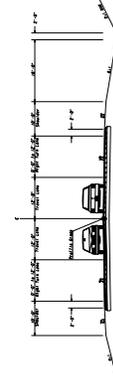
S. R. 42/74 OVERLAY SECTION



S. R. 74 FULL DEPTH SECTIONS



S. R. 42 FULL DEPTH SECTIONS

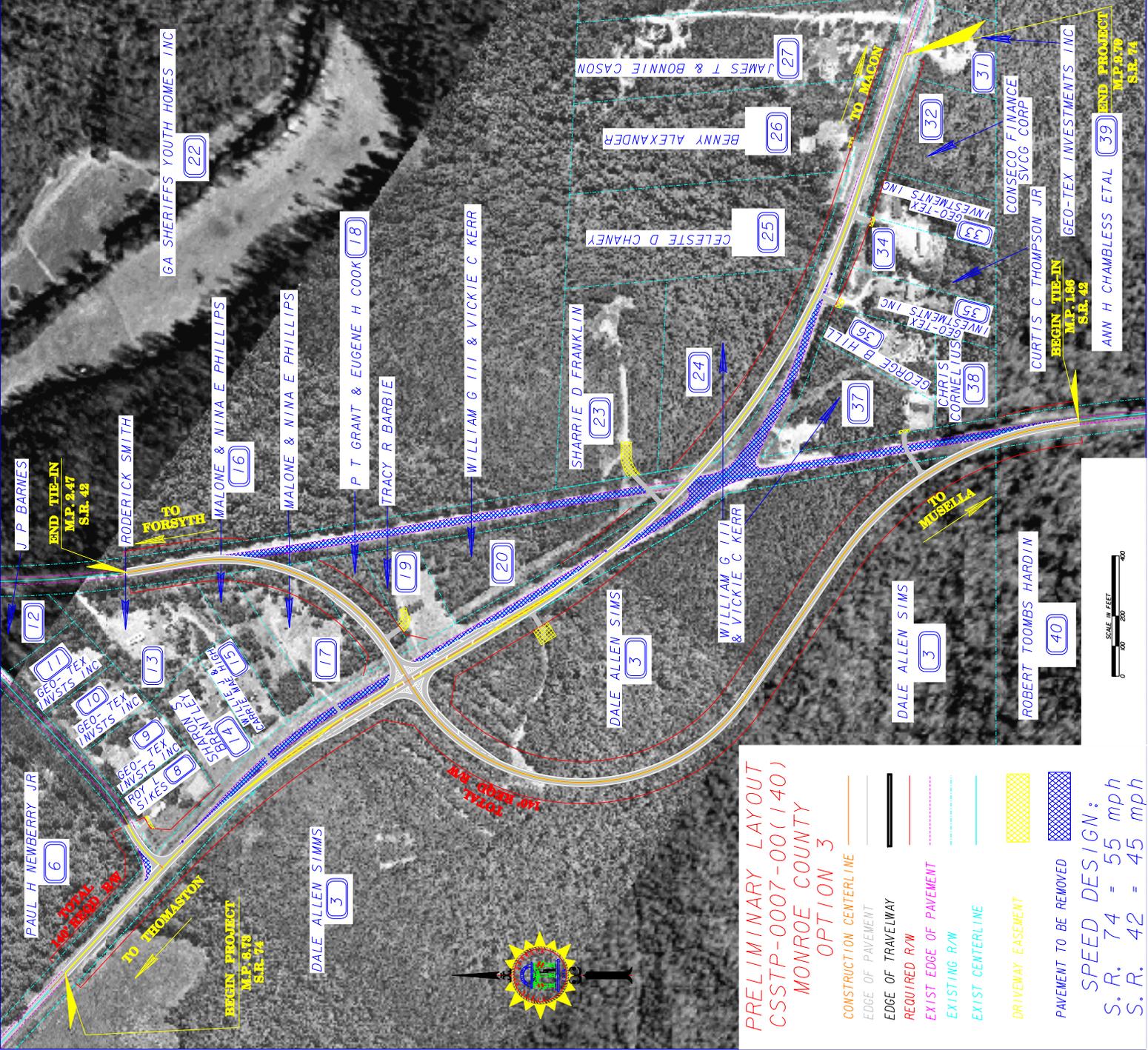


CSSTP-0007-00(140)  
 P. I. No. 0007140  
 MONROE COUNTY  
 S. R. 74 AT S. R. 42  
 PRELIMINARY LAYOUT OPTION 1

- CONSTRUCTION CENTERLINE
- EXISTING EDGE OF PAVEMENT
- EDGE OF PAVEMENT
- REQUIRED R/W
- PAVEMENT TO BE REMOVED
- DRIVEWAY EASEMENT
- EXISTING CENTERLINE
- POSSIBLE DISPLACEMENT

EXISTING R/W WIDTH: S. R. 74=100.00' S. R. 42=100.00'  
 PROPOSED R/W WIDTH: VARIES 120.00' TO 210.00'  
 SPEED DESIGN = 55 MPH



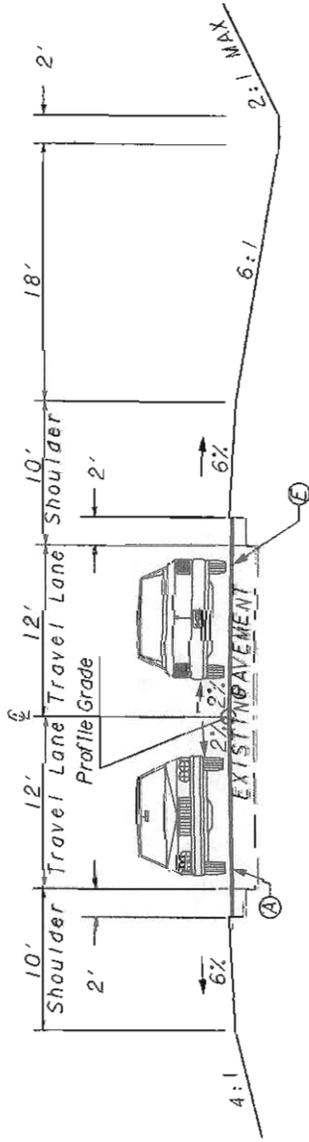


**PRELIMINARY LAYOUT**  
**CSSTP-0007-00(140)**  
**MONROE COUNTY**  
**OPTION 3**

CONSTRUCTION CENTERLINE  
 EDGE OF PAVEMENT  
 EDGE OF TRAVELWAY  
 REQUIRED R/W  
 EXIST EDGE OF PAVEMENT  
 EXISTING R/W  
 EXIST CENTERLINE  
 DRIVEWAY EASEMENT  
 PAVEMENT TO BE REMOVED

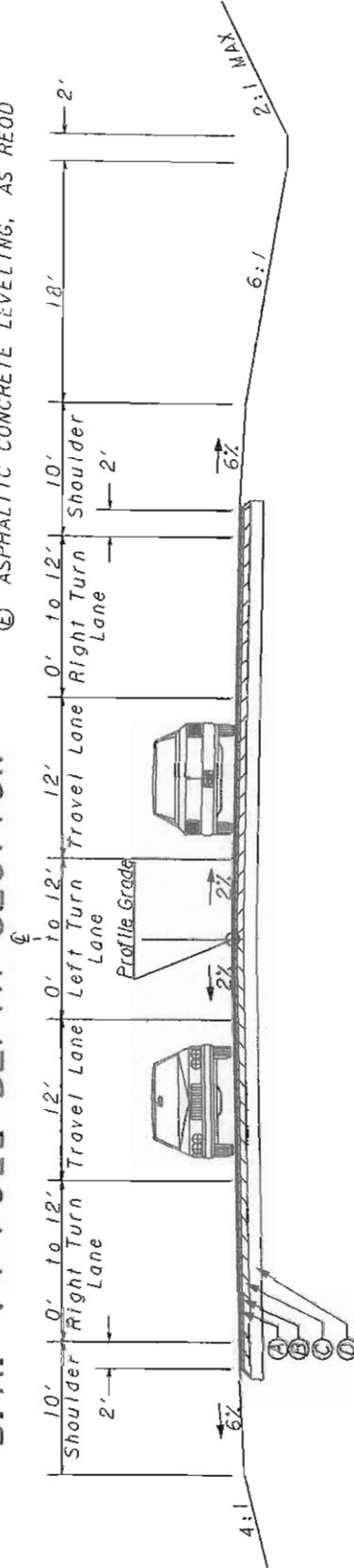
**SPEED DESIGN:**  
 S. R. 74 = 55 mph  
 S. R. 42 = 45 mph

# S. R. 42/74 OVERLAY SECTION

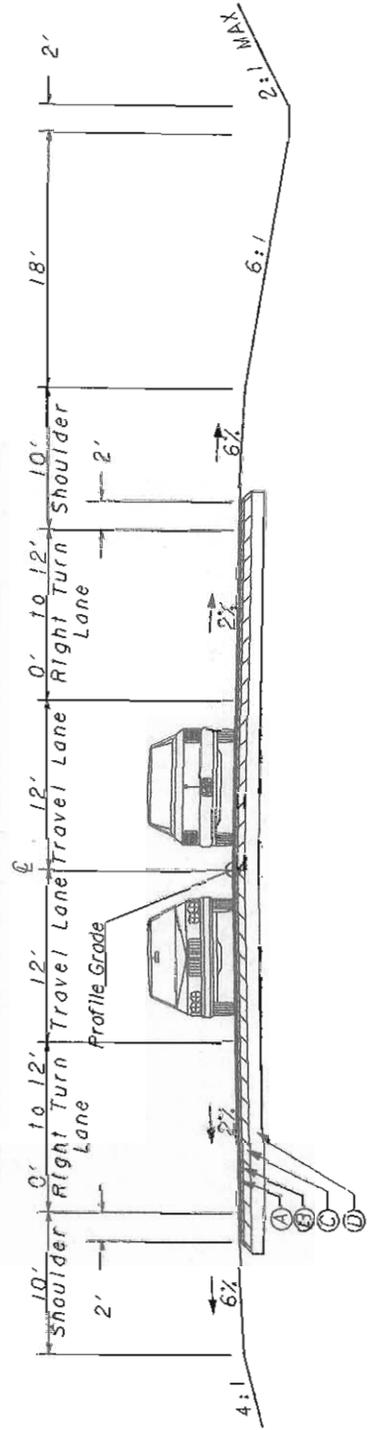


- (A) ASPHALTIC CONCRETE 9.5 mm. TYPE 11, 137.5 LBS/SY
- (B) ASPHALTIC CONCRETE 19 mm, 220 LBS/SY
- (C) ASPHALTIC CONCRETE 25 mm, 440 LBS/SY
- (D) GRADED AGGREGATE BASE, 10 in
- (E) ASPHALTIC CONCRETE LEVELING, AS RECD

# S. R. 74 FULL DEPTH SECTION



# S. R. 42 FULL DEPTH SECTION



## TWO-WAY STOP CONTROL SUMMARY

General Information			Site Information				
Analyst	JEFF SWIDERSKI		Intersection	SR74 @ SR42			
Agency/Co.	GDOT		Jurisdiction	District 3			
Date Performed	1/17/2007		Analysis Year	2031			
Analysis Time Period	AM						
Project Description CSSTP-0007-00(140), Monroe County, P.I. No. 0007140							
East/West Street: SR74			North/South Street: SR42				
Intersection Orientation: East-West			Study Period (hrs): 0.25				
Vehicle Volumes and Adjustments							
Major Street	Eastbound			Westbound			
Movement	1	2	3	4	5	6	
	L	T	R	L	T	R	
Volume	0	145	5	5	110	10	
Peak-Hour Factor, PHF	1.00	1.00	1.00	1.00	1.00	1.00	
Hourly Flow Rate, HFR	0	145	5	5	110	10	
Percent Heavy Vehicles	17	--	--	17	--	--	
Median Type	Undivided						
RT Channelized			0			0	
Lanes	1	1	1	1	1	1	
Configuration	L	T	R	L	T	R	
Upstream Signal		0			0		
Minor Street	Northbound			Southbound			
Movement	7	8	9	10	11	12	
	L	T	R	L	T	R	
Volume	5	75	5	10	55	0	
Peak-Hour Factor, PHF	1.00	1.00	1.00	1.00	1.00	1.00	
Hourly Flow Rate, HFR	5	75	5	10	55	0	
Percent Heavy Vehicles	17	0	0	17	0	0	
Percent Grade (%)		0			0		
Flared Approach		N			N		
Storage		0			0		
RT Channelized			0			0	
Lanes	0	1	1	0	1	1	
Configuration	LT		R	LT		R	
Delay, Queue Length, and Level of Service							
Approach	EB	WB	Northbound			Southbound	
Movement	1	4	7	8	9	10	11
Lane Configuration	L	L	LT		R	LT	R
v (vph)	0	5	80		5	65	0
C (m) (vph)	1380	1344	631		908	624	949
v/c	0.00	0.00	0.13		0.01	0.10	0.00
95% queue length	0.00	0.01	0.43		0.02	0.35	0.00
Control Delay	7.6	7.7	11.5		9.0	11.4	8.8
LOS	A	A	B		A	B	A
Approach Delay	--	--	11.4			11.4	
Approach LOS	--	--	B			B	

## TWO-WAY STOP CONTROL SUMMARY

General Information			Site Information					
Analyst	Jeffrey M. Swiderski		Intersection	SR 74 @SR 42				
Agency/Co.	GDOT		Jurisdiction	DISTRICT 3				
Date Performed	1/17/2007		Analysis Year	2031				
Analysis Time Period	PM							
Project Description CSSTP-0007-00(140), MONROE COUNTY, P.I. NO. 0007140								
East/West Street: SR 74			North/South Street: SR 42					
Intersection Orientation: East-West			Study Period (hrs): 0.25					
Vehicle Volumes and Adjustments								
Major Street	Eastbound			Westbound				
Movement	1	2	3	4	5	6		
	L	T	R	L	T	R		
Volume	0	150	5	5	180	5		
Peak-Hour Factor, PHF	1.00	1.00	1.00	1.00	1.00	1.00		
Hourly Flow Rate, HFR	0	150	5	5	180	5		
Percent Heavy Vehicles	17	--	--	17	--	--		
Median Type	Undivided							
RT Channelized			0			0		
Lanes	1	1	1	1	1	1		
Configuration	L	T	R	L	T	R		
Upstream Signal		0			0			
Minor Street	Northbound			Southbound				
Movement	7	8	9	10	11	12		
	L	T	R	L	T	R		
Volume	0	25	0	10	65	0		
Peak-Hour Factor, PHF	1.00	1.00	1.00	1.00	1.00	1.00		
Hourly Flow Rate, HFR	0	25	0	10	65	0		
Percent Heavy Vehicles	17	17	17	17	17	17		
Percent Grade (%)		0			0			
Flared Approach		N			N			
Storage		0			0			
RT Channelized			0			0		
Lanes	0	1	1	0	1	1		
Configuration	LT		R	LT		R		
Delay, Queue Length, and Level of Service								
Approach	EB	WB	Northbound		Southbound			
Movement	1	4	7	8	9	10	11	12
Lane Configuration	L	L	LT		R	LT		R
v (vph)	0	5	25		0	75		0
C (m) (vph)	1304	1339	552		858	552		826
v/c	0.00	0.00	0.05		0.00	0.14		0.00
95% queue length	0.00	0.01	0.14		0.00	0.47		0.00
Control Delay	7.8	7.7	11.8		9.2	12.5		9.4
LOS	A	A	B		A	B		A
Approach Delay	--	--	11.8		12.5			
Approach LOS	--	--	B		B			

**CONCEPT TEAM MEETING MINUTES  
CSSTP-0007-00(140) MONROE COUNTY  
P.I. NO. 0007140**

SR 74 from Pine Grove Church Road to Russellville Road

**LOCATION:** Thomaston District Three assembly room

**DATE/TIME:** March 1, 2007 at 10:00 a.m.

The meeting was called to order by Bill Rountree, and he asked attendees introduce themselves.

**LIST OF PARTICIPANTS:**

Harvey Booker-Booker Real Estate Services  
Tommy Cleveland-GDOT Location  
Ken Crabtree-GDOT Construction  
Clinton B. Ford-GDOT Area 4 Engineer  
Cheryl Griffin-GDOT Preconstruction  
Rob Hall-Southern Rivers Energy  
Thomas Howell-District Engineer  
David Millen-GDOT District Preconstruction Engineer  
Jason Mobley-GDOT Design  
Scott Parker-GDOT Traffic Operations  
Debra Pruitt-GDOT Environmental  
Lamar Pruitt-GDOT District Construction Engineer  
Tom Queen-GDOT District Planning & Programming Engineer  
W. Tim Reeves-GDOT Area 1 Engineer  
Scott Roberts-GDOT Right of Way  
Bill Rountree-GDOT District Design Engineer  
Jeff Swiderski-GDOT-GDOT Design  
Dr. Jeffery P. Turner-Monroe Co. Schools Director of Student Services  
Glenn A Williams-GDOT Utilities

**Alternates:**

- 1) This alternate realigned SR 42 through Dyas School Road to intersect SR 74 at a 90 degree angle. Both roadways were designed for 55mph. This alternate was presented at the Public Information Open House. The overall public opinion was that the proposed design was excessive and would have detrimental effects to many environmentally sensitive areas.

- 2) This alternate was suggested by some of the citizens in response to the Public Information Open House. The intersection would be located very near the existing intersection. SR 42 would be realigned using 45 mph design, reducing project limits and required right of way. This alternate was rejected due to poor intersection sight distance. The intersection would be located in a sag vertical curve.
- 3) This alternate combined aspects of alternates 1 and 2. SR 42 would be realigned using 45 mph design. It would be located in a crest allowing better intersection sight distance than Alternate 2. This was the chosen alternate, as it reduced the costs and impacts of Alternate 1 and provided better sight distance than alternate 2.
- 4) No Build – This alternative was rejected because it did not improve the operational and safety conditions required by the need and purpose.

### **Report Review:**

#### Page 4

- To Tommy Cleveland suggested we check the accident rates and to correct the text “0.00” on this page.
- 

#### Page 5

- TomTom Queen suggested we add the roundabout to the projects in the area and also that we show distance between the projects.

#### Page 6

- David Millen asked that the traffic data for 2011 be verified because it looked low.

#### Page 8

- Debra Pruitt indicated that a Categorical Exclusion would be the appropriate level of Environmental Analysis.
- Glenn Williams asked that Flint EMC be changed to Southern Rivers Energies.
- Bill Rountree stated that no detours would be required and to change the concept report accordingly.
- Tom Queen suggested adding the roundabout project at SR74 and US341/SR7 to the projects in the area. He also suggested removing the SR42 bridge replacement project because it was complete. He mentioned that the Need and Purpose statement would need to reflect these changes as well.
- Bill Rountree asked that the number of parcels be verified and updated in the Concept Report and in Tpro.

#### Cost Estimate

- Tommy Cleveland suggested that the project length be verified.

- David Millen suggested that the Benefit Cost Analysis be updated to include the updated costs.
- Lamar Pruitt suggested making the Erosion Control estimate 12% of the construction cost.

#### Typical Sections

- Bill Rountree suggested a pavement design request be sent to the lab because the truck percentages were higher than 10%.

#### Capacity Analysis

- Scott Parker indicated that the major and minor traffic may have been mislabeled.
- David Millen suggested the capacity analysis be recomputed after updated traffic is received.

#### Location and Design Notice

- Tim Reeves will be the Area Engineer in charge of this project.
- Tom Queen recommended the Code # be checked to see if it was 22 or 32.

#### **Additional Comments:**

**A. Planning** – It would be helpful if we could get some current accident data.

**B. Office of Financial Management** – No Attendees

**C. Environmental** – Debra Pruitt stated that there is one cemetery on the project. No reports are complete yet. There are three historic resources that are in the project area; there should be no adverse effect on any of them. All three resources have been deemed not eligible by our internal historians, but SHPO has not confirmed these. Environmental has not received anything back on Ecology.

**C. Utilities** – No Comment

**D. Right of Way** – Had a reminder that the relocation process takes time.

**E. Traffic Operations** – It was asked if we had considered a split intersection. Bill Rountree stated a split intersection would not be desirable. It was suggested that the curves be flattened. Curves are at 45 mph. The project should work well after construction is complete.

**F. Construction** – No Additional Comments

**G. Maintenance** – Tim Reeves asked if we had considered using Dyas School Road. Design stated that the public did not desire the State Route traffic on that road.

**H. Location** – Tommy Cleveland stated that survey would need the new alignment in CAiCE when available.

**J. Others -**

Thomas Howell asked if a roundabout was considered. Scott Parker indicated that there were twice as many vehicles on State Route 74 than 42 and that normally roundabouts are used where the traffic is balanced.

David Millen asked if the staging of SR74 had been reviewed. Design indicated that the staging had not been reviewed thoroughly. Survey data would be required before an accurate design and review could be completed. He also asked that the report indicate that all traffic will be maintained on state routes.

Bill Rountree requested that we post the new display on the website.

**Conclusion:** Alternate #3 will be used for this project.

**DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA**

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**INTERDEPARTMENT CORRESPONDENCE**

FILE: P. I. No. 0007140

OFFICE: Environment/Location

DATE: December 19, 2006

FROM: *HDK/cw*  
Harvey D. Keeper, State Environmental/Location Engineer

TO: Distribution Below

SUBJECT: Project CSSTP-0007-00(140), Monroe County, Summary of Comments Received During the Public Comment Period – Intersection Improvement of SR 42 at SR 74

COMMENT TOTALS:

A total of 35 people attended the public information open house held for the subject project. From those attending, 9 comment forms and 3 verbal statements were received. An additional 7 comments were received during the ten-day comment period following the public information open house, for a total of 13 comments. They are summarized as follows:

No. Opposed	No. In Support	Uncommitted	Conditional
<u>9</u>	<u>2</u>	—	<u>8</u>

MAJOR CONCERNS:

- Would like to see a Round-about at this intersection
- Proposed design is too excessive and would be too expensive to construct
- Proposed design would increase speeds along roadway
- Intersection is very dangerous; lots of accidents

OFFICIALS:

Officials attending included the following:

Mayor of Culloden