

D.O.T. 66

DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA

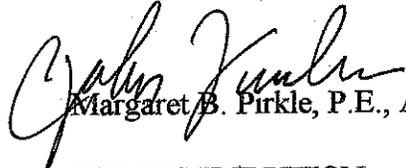
INTERDEPARTMENT CORRESPONDENCE

FILE STP-0005-00(08) Glynn County  
P. I. No. 0005008

OFFICE Preconstruction

DATE April 21, 2003

FROM

  
Margaret B. Pirkle, P.E., Assistant Director of Preconstruction

TO

SEE DISTRIBUTION

SUBJECT PROJECT CONCEPT REPORT APPROVAL

Attached for your files is the approval for subject project.

MBP/cj

Attachment

DISTRIBUTION:

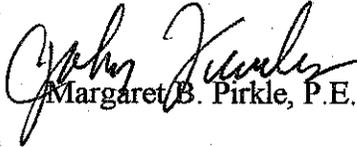
David Mulling  
Harvey Keeper  
Jerry Hobbs  
Percy Middlebrooks  
Michael Henry  
Phillip Allen  
Marta Rosen  
Paul Liles  
Ben Buchan  
Gary Priester  
BOARD MEMBER

**DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA**

**INTERDEPARTMENT CORRESPONDENCE**

**FILE** STP-0005-00(08) Glynn County **OFFICE** Preconstruction  
P.I. No. 0005008

**DATE** April 11, 2003

**FROM**  Margaret B. Pirkle, P.E., Assistant Director of Preconstruction

**TO** Frank L. Danchetz, P.E., Chief Engineer

**SUBJECT** PROJECT CONCEPT REPORT

This project is the Torras Causeway Extension from K Street to US 17/SR 25 for a total of 0.44 mile. The US 17 intersection with Torras Causeway/Stacy Street is the most heavily traveled intersection in Glynn County. Torras Causeway is an east/west route that serves as the primary connector between US 17 and Saint Simons Island. Stacy Street is a two lane city street. K Street is a two lane city street that runs from US 341 east to US 17. Morning and afternoon traffic on Stacy Street approaching its intersection with US 17 currently queues back into K Street. Projected 50% increase in traffic volumes for the design period will lead to longer queues in the future. Accident reports show that 22 accidents occurred at the intersection of US 17 and Torras Causeway/Stacy Street in the 3 year period from 1997-2000. Eleven of the 22 accidents occurred on the westbound approach of the intersection, 9 of which were rear-end accidents. The proposed improvements will provide safer, more efficient travel between the intersection of US 17 and SR 582 (Torras Causeway) and K Street.

The proposed construction will extend Torras Causeway on new location to tie in with existing K Street, replacing the Stacy Street approach of the US 17 intersection. The east end of K Street will be realigned with Putnam Street. Both roads will feature 12' lanes, curb and gutter and 5' sidewalks. The Torras Causeway Extension will have 4 through lanes with an alternating left turn lanes. The realigned east end of K Street will have 2 through lanes. Traffic will be maintained during construction

Environmental concerns include requiring a COE 404 Permit; an Environmental Assessment will be prepared; a public hearing will be held; time saving procedures are not appropriate.

The estimated costs for this project are:

	<u>PROPOSED</u>	<u>APPROVED</u>	<u>PROG DATE</u>	<u>LET DATE</u>
Construction (includes E&C and inflation)	\$619,000	\$618,000	LR	LR
Right-of-Way Utilities	Local	Local		

Frank L. Danchetz

Page 2

STP-0005-00(08) Glynn

April 11, 2003

I recommend this project concept be approved.

MBP:JDQ/cj

Attachment

CONCUR



Thomas L. Turner, P.E., Director of Preconstruction

APPROVE



Frank L. Danchetz, P.E., Chief Engineer

Georgia Department of Transportation  
State of Georgia

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Jesup, Georgia  
Interdepartmental Correspondence

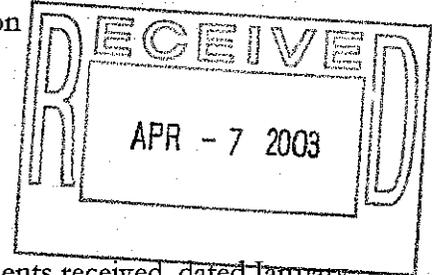
**FILE:** STP-0005-00 (08), Glynn County  
PI 000508

**DATE:** April 4, 2003

**TO:** Meg Pirkle, P.E., Assistant Director of Preconstruction  
Attn: Johnny Quarles

**FROM:** Gary D. Priester, District Engineer *AJC TB*

**RE:** Project Concept Report



Attached is the updated Concept Report, which includes comments received, dated January 8, 2003, for your further handling for approval in accordance with the Plan Development Process (PDP).

If you should have any questions concerning the attached concept report, please contact Teresa Scott, District Planning & Programming Engineer at (912) 427-5788.

Cc: File

DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA

Office of District Five Design

PROJECT CONCEPT REPORT

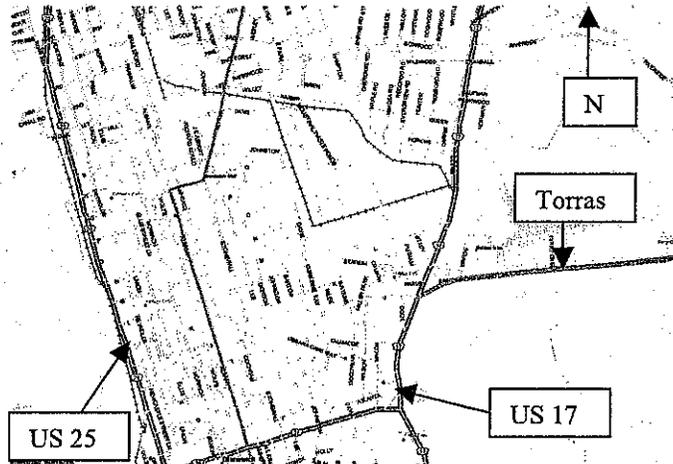
Project Number: STP 0005-00(08)

County: Glynn

P. I. Number: 0005008

Federal Route Number: N/A

State Route Number: SR 582 (F.J. Torras Causeway)



Recommendation for approval:

DATE 4/3/03

*Anthony J. Collins*  
Project Manager

DATE \_\_\_\_\_

Office Head/District Engineer

The concept as presented herein and submitted for approval is consistent with that which is included in the Regional Transportation Improvement Program (RTP) and the State Transportation Improvement Program (STIP).

DATE \_\_\_\_\_

State Transportation Planning Administrator

DATE \_\_\_\_\_

State Transportation Programming Engineer

DATE \_\_\_\_\_

State Environmental/Location Engineer

DATE \_\_\_\_\_

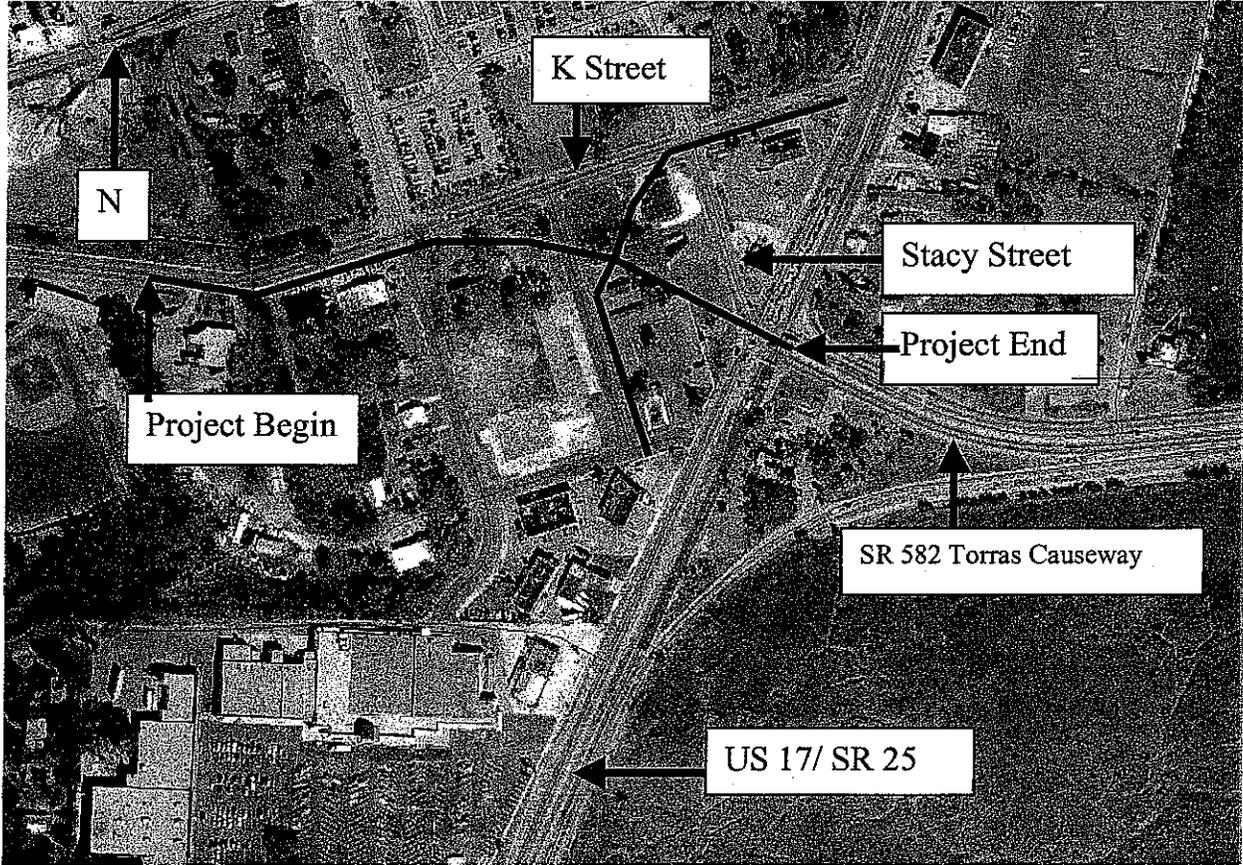
State Traffic Safety & Design Engineer

DATE 3/31/03

*Mary O. Prickett*  
District Engineer

DATE \_\_\_\_\_

Project Review Engineer



**Need and Purpose:**

The realignment of Stacy Street to meet with K Street was at one time a programmed project. This project also included widening K Street, which is also called Stadium Street and L Street further east. The project number was STP-0627 (1) and the PI number was 550510. The Brunswick Area Transportation Study (BATS) dropped this project from further consideration of funding on October 10, 1997. The project was revised as shown in the attached concept drawing and reintroduced into the program under the name "Torras Causeway Extension at US 17/SR 25." The project is listed in BATS TIP fiscal year 2001-2003 as a Tier 1 project.

The purpose of this project is to provide safer, more efficient travel between the intersection of US 17 and SR 582 (Torras Causeway) and K Street. Of particular interest on K Street is the Hercules Plant, one of the largest employers in Glynn County. The US 17 intersection with Torras Causeway/Stacy Street is the most heavily traveled intersection in Glynn County. Torras Causeway is a typical east/west route that serves as the primary connector between US 17 and Saint Simons Island. Stacy Street is a two-lane city street. The section of Stacy Street to be replaced is mostly used as a connector between US 17 and K Street. K Street is a two-lane city street that runs from US 341 east to US 17.

Morning and afternoon traffic on Stacy Street approaching its intersection with US 17 currently queues back onto K Street. Projected 50% increase in traffic volumes for the design period will lead to considerably longer queues in the future. HCS analysis of the K Street/Stacy Street intersection show these movements to currently operate at LOS "B" during the morning and afternoon peak hours. However, the analysis does not take into account the nearby signalized intersection of US 17 and Torras Causeway/Stacy Street. Observed travel delays at the K Street/Stacy Street intersection are more indicative of LOS "C" or worse.

Brunswick police accident reports show that 22 accidents occurred at the intersection of US 17 and Torras Causeway/Stacy Street in the three-year period from August 1, 1997 to July 31, 2000. Eleven of the 22 accidents occurred on the westbound approach of the intersection, nine of which were rear end accidents. This approach is currently under construction as part of the US 17 widening project. During the three-year period from August 1, 1998 to July 31, 2001, 8 accidents occurred at the intersection of US 17 and K Street. Only one of these accidents involved the K Street approach. During the same period, 2 accidents occurred at the Stacy Street/K Street intersection, 3 accidents occurred at the K Street/Putnam Street intersection and 6 accidents occurred at the K Street/Cook Street intersection. These accident rates are relatively low. Collision diagrams are enclosed.

**Description of the proposed project:** The proposed project will extend the Causeway to tie in with existing K Street, effectively replacing the Stacy Street approach of the US 17 intersection as shown in the concept drawing. The east end of K Street, which could be renamed as part of Putnam Street, will be realigned with Putnam street. A choker will be installed to physically enforce the existing one way section. Total project length is 0.446 miles.

Proposed improvements will remove stop control for vehicles travelling between US 17 and K Street and provide greater capacity for the west approach of the Torras Causeway/Stacy Street intersection with US 17. Stop delay for vehicles travelling between US 17 and K Street will essentially be removed.

**Is the project located in a Non-attainment area?**        Yes   X   No.

**PDP Classification:** Major X        Minor         
**Federal Oversight:** Full Oversight ( ), Exempt(X), State Funded( ), or Other ( )

**Functional Classification:** Urban Collector (Stacy St.)

**U. S. Route Number(s):** N.A. **State Route Number(s):** 582 (Torras)

**Traffic (AADT):**

Current Year: (2001) 37,807 (SR 582)      Design Year: (2021) 56,710 (SR 582)  
7,312 (Stacy St.)      11,022 (SR 582 Ext.)

**Existing design features:**

- Typical Section: Stacy St. – 22' roadway, K St. – 22' roadway
- Posted speed 25 mph      Maximum degree of curvature: 19
- Maximum grade: 3% %
- Width of right of way: Stacy St. – 60 ft., K St. - 80 ft.
- Major structures: None      List all bridge structures including length, width, and sufficient rating).
- Major interchanges or intersections along the project: US 17/SR 25 at SR 582
- Existing length along K Street and Stacy Street is 0.350 mile.

**Proposed Design Features:**

- Proposed typical section(s): Both roads will feature 12' lanes, curb and gutter and 5-foot wide sidewalks. The Torras Causeway Extension will have 4 through lanes and an alternating left turn lane. The realigned east end of K Street will have 2 through lanes. Typical section diagrams are attached.
- Proposed Design Speed Mainline 25 mph
- Proposed Maximum grade Mainline 4 %      Maximum grade allowable 7 %.
- Proposed Maximum grade Side Street 4 %      Maximum grade allowable 7 %.
- Proposed Maximum grade driveway 15 %
- Proposed Maximum degree of curve 27.3      Maximum degree allowable 27.9.
- Right of way
  - Width: Causeway – varies 60 – 100 ft., Putnam – 60 ft.
  - Easements: Temporary (X), Permanent ( ), Utility ( ), Other ( ).
  - Type of access control: Full ( ), Partial ( ), By Permit (X), Other ( ).
  - Number of parcels: Approx 9      Number of displacements:
    - Business: 2
    - Residences: 1
    - Mobile homes: \_\_\_\_\_
    - Other: \_\_\_\_\_
- Structures:
  - Bridges - None
  - Retaining walls - None
- Major intersections and interchanges – US 17 / SR 25 at SR 582
- Traffic control during construction: Traffic will be maintained on either Stacy Street, Putnam Street or Torras Extension at all times during construction.

- Design Exceptions to controlling criteria anticipated:

	<u>UNDETERMINED</u>	<u>YES</u>	<u>NO</u>
HORIZONTAL ALIGNMENT:	( )	(X)	( )
ROADWAY WIDTH:	( )	( )	(X)
SHOULDER WIDTH:	( )	( )	(X)
VERTICAL GRADES:	( )	( )	(X)
CROSS SLOPES:	( )	( )	(X)
STOPPING SIGHT DISTANCE:	( )	( )	(X)
SUPERELEVATION RATES:	( )	( )	(X)
HORIZONTAL CLEARANCE:	( )	( )	(X)
SPEED DESIGN:	( )	( )	(X)
VERTICAL CLEARANCE:	( )	( )	(X)
BRIDGE WIDTH:	( )	( )	(X)
BRIDGE STRUCTURAL CAPACITY:	( )	( )	(X)

A Design Exception is requested for realigned Putnam Street on the north approach of its intersection with the Causeway Extension. While the minimum radius required at 4% maximum superelevation and 25 mph is 205 feet, a 150-foot radius is intended for this approach. However, the radius is located near the stop line where speeds will be minimized. The 150-foot radius exceeds 20 mph design requirements.

A Design Exception is requested for the eastbound approach of the Torras Causeway Extension at the intersection with US 17. Without a taper, the eastbound through lanes would be offset 32 feet from their receiving lanes. The taper designed allows for alignment with receiving lanes. However, a 270' taper is used instead of the nominal 333' taper. The shortened taper length is desired in order to allow eastbound drivers adequate decision making time to maneuver into the left turn lane after passing the Putnam Street intersection.

- Design Variances; None
- Environmental concerns: Environmental Assessment is required. No underground storage tanks are anticipated. One potential hazardous site exists – environmental review will provide its determination. Nationwide 404 permit prepared by Stantec.
- Level of environmental analysis:
  - Are Time Savings Procedures appropriate? Yes ( ), No (X),
  - Categorical exclusion (X),
  - Environmental Assessment/Finding of No Significant Impact (FONSI) ( ), or
  - Environmental Impact Statement (EIS) ( ).
- Utility involvements: Power, phone, gas

**Project responsibilities:**

- Design – city of Brunswick
- Right of Way Acquisition – city of Brunswick
- Relocation of Utilities – city of Brunswick
- Letting to contract - GDOT
- Supervision of construction - GDOT
- Providing detours – city of Brunswick

### **Coordination**

- Concept Meeting was held on October 23, 2002. The concept report and drawings were presented by Stantec. Four alternatives were discussed and Alternate 3 was selected as the preferred alternate. Meeting minutes are attached.
- P. A. R. – Not anticipated
- FEMA, USCG, and/or TVA - None
- Public involvement. Opportunity for Public Hearing is required.
- Local government comments. City of Brunswick agreed Alternate 3 should be the preferred alternate.
- Other projects in the area – US 17/SR 25 widening, NH-009-2(60), PI No. 520890  
Norman Street and Warde Street, PR 8530-44(127)  
Carrie Street, PR 8530-45(127)
- Other coordination to date. None
- Railroads - None

### **Scheduling – Responsible Parties' Estimate**

- Time to complete the environmental process: 16 Months.
- Time to complete preliminary construction plans: 6 Months.
- Time to complete right of way plans: 3 Months.
- Time to complete the Section 404 Permit: 2 Months.
- Time to complete final construction plans: 4 Months.
- Time to complete to purchase right of way: 6 Months.
- List other major items that will affect the project schedule: None

### **Other alternates considered:**

1. No build.

2. Extend Torras Causeway at an 80 degree angle with US 17. This alignment would improve the taper used in Alternate-3 for the eastbound approach of the intersection with US 17. However, the business property at the intersection of Stacy Street and K Street would have to be acquired.

3 (a). Extend Torras Causeway as a five lane section of roadway to the intersection with Cook Street. The Extension will be signed for shared lane usage with bicycles. This alternate is selected as the Preferred Alternate since it will allow for improved travel between US 17 and the Hercules main entrance without affecting the aforementioned property. This alternate is shown as Concept Drawing – Alternate 3. This alternate may later be revised at the request of local government to close the section of K Street between Stacy Street and US 17.

3 (b). This alternate is the same as Alternate 3 (a) with a separate bicycle facility added. This alternate attempts to include a bicycle path, which is the current practice for Brunswick / Glynn County. Due to right-of-way constraints on the north side of Torras Causeway Extension, a two-way path is used on the south side. This alternate is not chosen because it lacks a feasible tie-in

with the existing bike path on Torras Causeway as shown in the Concept Drawing – Alternate 3 with Bicycle Path. The widening of US 17 / SR 25 currently under construction does not include separate bicycle facilities. It will be signed for shared lane usage. Another disadvantage of this alternate is the lack of a bike path from Cook Street to the recreation complex due to right-of-way constraints. Also, bicyclists going against traffic will tend to continue to travel on the wrong side of the extension when the path ends at Cook Street.

4. Extend Torras Causeway at an 88 degree angle with US 17. This alternate is an attempt to avoid displacing the business at the southwest corner of the K Street/Putnam Street intersection. Even with the additional eastbound lane removed from design, the attached Concept Drawing – Alternate 4 shows this displacement to be unavoidable. This alternate may also require displacement of the business at the southwest corner of K Street at Stacy Street.

**Comments:** In order to fit the horizontal curve on Torras Causeway Extension, a 25-mph design speed is used. In order to satisfy 10-mph speed limit decreases at 660 feet, the west end of Torras Causeway currently under construction will need to be posted at 35 mph. Speed ordinance for the Causeway is currently under revision and should include this 35-mph section.

**Attachments:**

1. Cost Estimates:
  - a. Construction including E&C,
  - b. Right of Way, and
  - c. Utilities.
2. Typical sections,
3. Accident summaries,
4. Capacity analysis,
5. Minutes of Initial Concept and Concept meetings,
6. Concept Drawings

## CONCEPT COST ESTIMATE

( )PROGRAMMING PROCESS (X)CONCEPT DEVELOPMENT ( )DURING PROJECT DEV.

<b>PROJECT COST</b>	
<b>A. RIGHT-OF-WAY:</b>	
1. PROPERTY (LAND – 28,200 S.F. @ \$1.80 / S.F.)	\$ 91,829.00
2. DISPLACEMENTS; RES: 1, BUS: 2 , M.H.: 0	\$161,130.00
3. OTHER COST (ADM./COST, INFLATION)	
SUBTOTAL:A	\$252,959.00 By Local Gov.
<b>B. REIMBURSABLE UTILITIES:</b>	
1. RAILROAD	
2. TRANSMISSION LINES (power, telephone)	\$ 236,400.00
3. SERVICES	
Gas, Sanitary Sewer, Water	\$ 813,350.00
SUBTOTAL:B	\$1,049,750.00 By Local Gov.
<b>C. CONSTRUCTION:</b>	
1. MAJOR STRUCTURES	
a. OVERPASSES	\$ 0.00
b. OTHER	\$ 0.00
SUBTOTAL:C-1	\$ 0.00
2. GRADING AND DRAINAGE:	
a. EARTHWORK (5,000 cy @ \$5.00/cy )	\$ 25,000.00
b. DRAINAGE:	
1) Cross Drain Pipe (0 ft @ \$25.00/ft)	\$ 0.00
2) Curb and Gutter (4,236 ft @ \$10.00/ft)	\$ 42,360.00
3) Longitudinal System(include catch basins)	\$ 95,350.00
SUBTOTAL:C-2	\$ 162,660.00
3. BASE AND PAVING:	

<b>PROJECT COST</b>		
a. AGGREGATE BASE (3,957 Tons @ \$15.00/Ton)		\$ 59,355.00
b. ASPHALT PAVING: Surface (884 Ton @ \$36.00/ Ton)	\$ 31,824.00	
Binder (1,179 Ton @ \$35.00/ Ton)	\$ 41,265.00	
Base (1,540 Ton \$35.00/Ton)	\$ 53,900.00	
	SUBTOTAL:C-3.b	\$ 126,989.00
c. CONCRETE PAVING		\$ 0.00
d. OTHER (Bituminous Tack: 1,287 Gal @ \$0.25/Gal)		\$ 322.00
	SUBTOTAL:C-3	\$ 186,666.00
<b>4. LUMP ITEMS:</b>		
a. GRASSING (2 ac @ \$1,600.00/ac)		\$ 3,200.00
b. CLEARING AND GRUBBING (2 ac @ \$4,000.00/ac)		\$ 8,000.00
c. LANDSCAPING		\$ 0.00
d. EROSION CONTROL		\$ 10,000.00
e. TRAFFIC CONTROL		\$ 10,000.00
	SUBTOTAL:C-4	\$ 31,200.00
<b>5. MISCELLANEOUS:</b>		
a. LIGHTING ( )		\$ 0.00
b. SIGNING - MARKING		\$ 20,000.00
c. GUARDRAIL - ( 0.0ft @ \$10.00/ft)		\$ 0.00
d. SIDEWALK ( 1690 yd <sup>2</sup> @ \$20.00/yd <sup>2</sup> )		\$ 33,800.00
	SUBTOTAL:C-5	\$ 53,800.00
<b>6. SPECIAL FEATURES</b>		
a. TRAFFIC SIGNALS		\$ 60,000.00
b. STRAIN POLES (6 @ \$3,300 each)		\$ 19,800.00
	SUBTOTAL:C-6	\$ 79,800.00

<b>ESTIMATE SUMMARY</b>		
<b>A. RIGHT-OF-WAY ( )</b>		\$ 252,959.00 By Local Government
<b>B. REIMBURSABLE UTILITIES</b>		\$ 1,049,750.00 By Local Government
<b>C. CONSTRUCTION</b>		
1. MAJOR STRUCTURES		\$ 0.00
2. GRADING AND DRAINAGE		\$ 162,660.00
3. BASE AND PAVING		\$ 186,666.00
4. LUMP ITEMS		\$ 53,800.00
5. MISCELLANEOUS		\$ 33,020.00
6. SPECIAL FEATURES		\$ 79,800.00
SUBTOTAL CONSTRUCTION COST		\$ 514,126.00
PRELIMINARY ENGINEERING (3%)		\$ 15,424.00
CONTINGENCIES (7%)		\$ 35,989.00
INFLATION (5% PER YEAR)		
NUMBER OF YEARS	2	\$ 52,698.00
<b>TOTAL CONSTRUCTION COST</b>		<b>\$ 618,237.00</b>
<b>GRAND TOTAL PROJECT COST</b>		<b>\$ 618,237.00</b> <b>+ R/W and Utilities</b>

Project construction will be funded 80% federal and 20% state. GDOT District 5 will contact the General Office and initiate an LPGA to be sent to local governments.

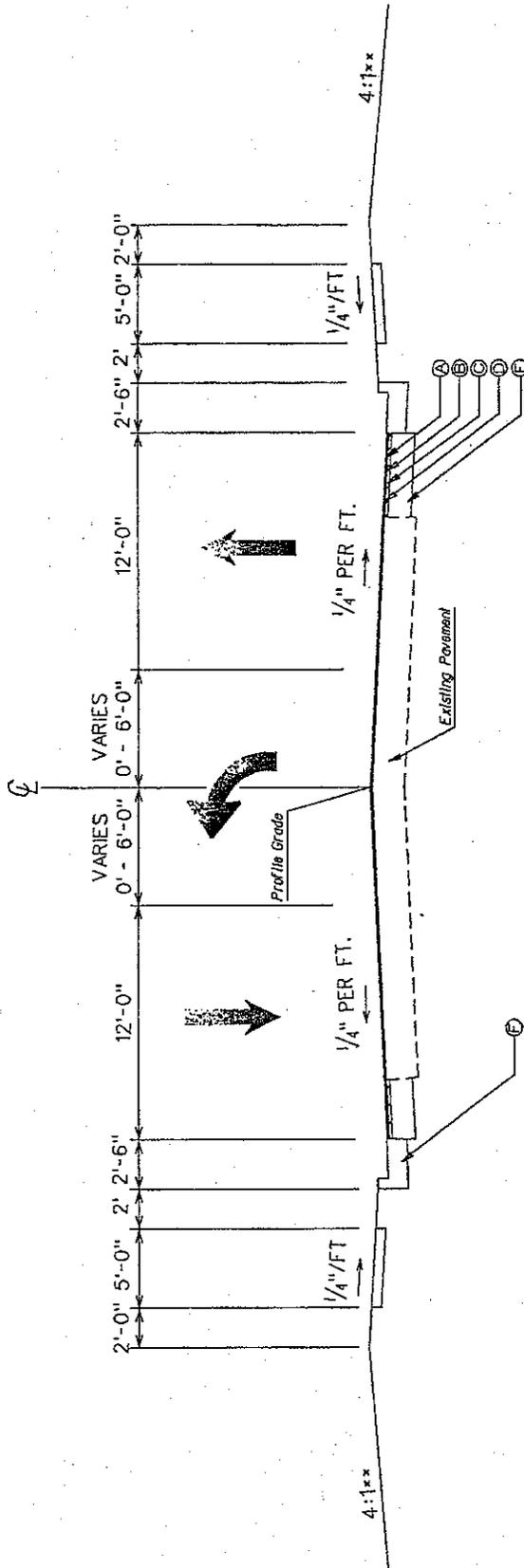
This project is 100 percent in congressional district 1.

TYPICAL SECTION

(NOT TO SCALE)

TORRAS CAUSEWAY EXT. - WEST OF COOK ST.

LOOKING EASTBOUND



DESIGN LEVEL "B"

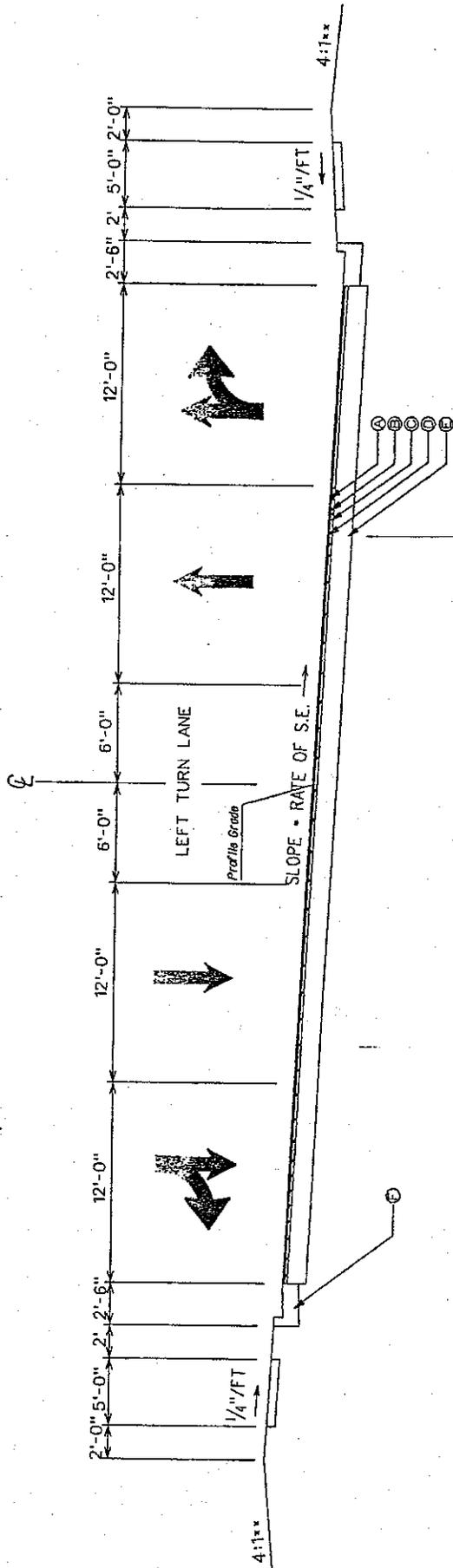
- Ⓐ 165 LBS/SY ASPH CONC 12.5 mm SUPERPAVE
- Ⓑ 220 LBS/SY ASPH CONC 19.0 mm SUPERPAVE
- Ⓒ 440 LBS/SY ASPH CONC 25.0 mm SUPERPAVE
- Ⓓ 0.06 GALLON/SQ.YD. BITUMINOUS TACK COAT
- Ⓔ 8" GRADED AGGREGATE BASE CRS.
- Ⓕ 8" X 30" TP 2 CURB & GUTTER

TYPICAL SECTION

(NOT TO SCALE)

TORRAS CAUSEWAY EXT. - EAST OF COOK ST.

LOOKING EASTBOUND

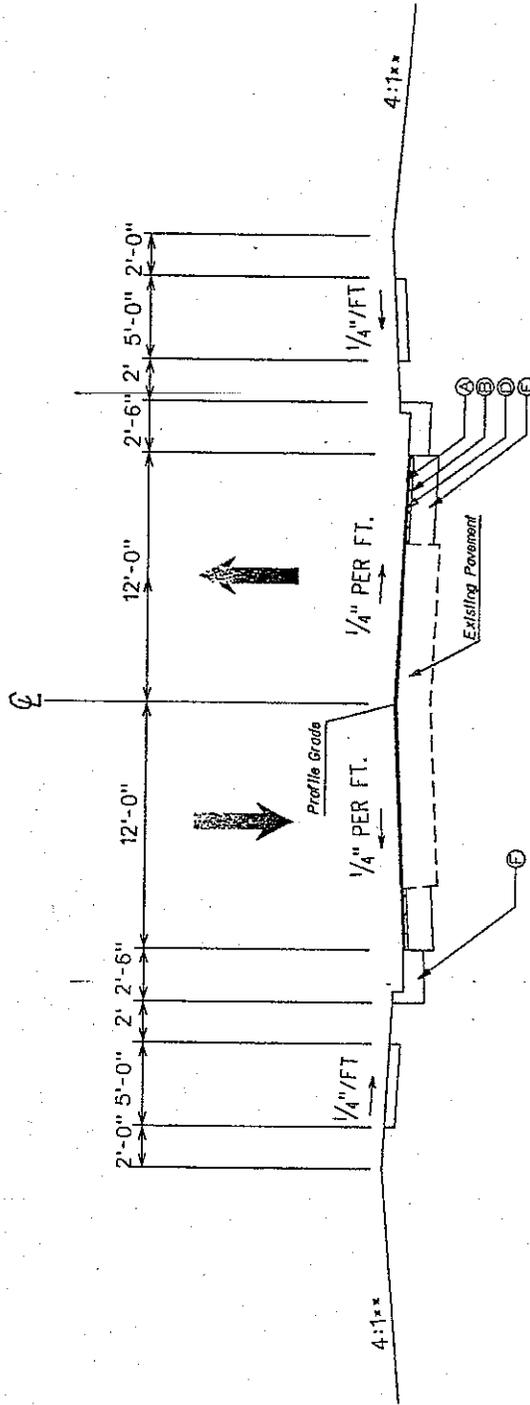


DESIGN LEVEL "B"

- Ⓐ 165 LBS/SY ASPH CONC 12.5 mm SUPERPAVE
- Ⓑ 220 LBS/SY ASPH CONC 19.0 mm SUPERPAVE
- Ⓒ 440 LBS/SY ASPH CONC 25.0 mm SUPERPAVE
- Ⓓ 0.06 GALLON/SQ.YD. BITUMINOUS TACK COAT
- Ⓔ 8" GRADED AGGREGATE BASE CRS.
- Ⓕ 8" X 30" TP 2 CURB & GUTTER

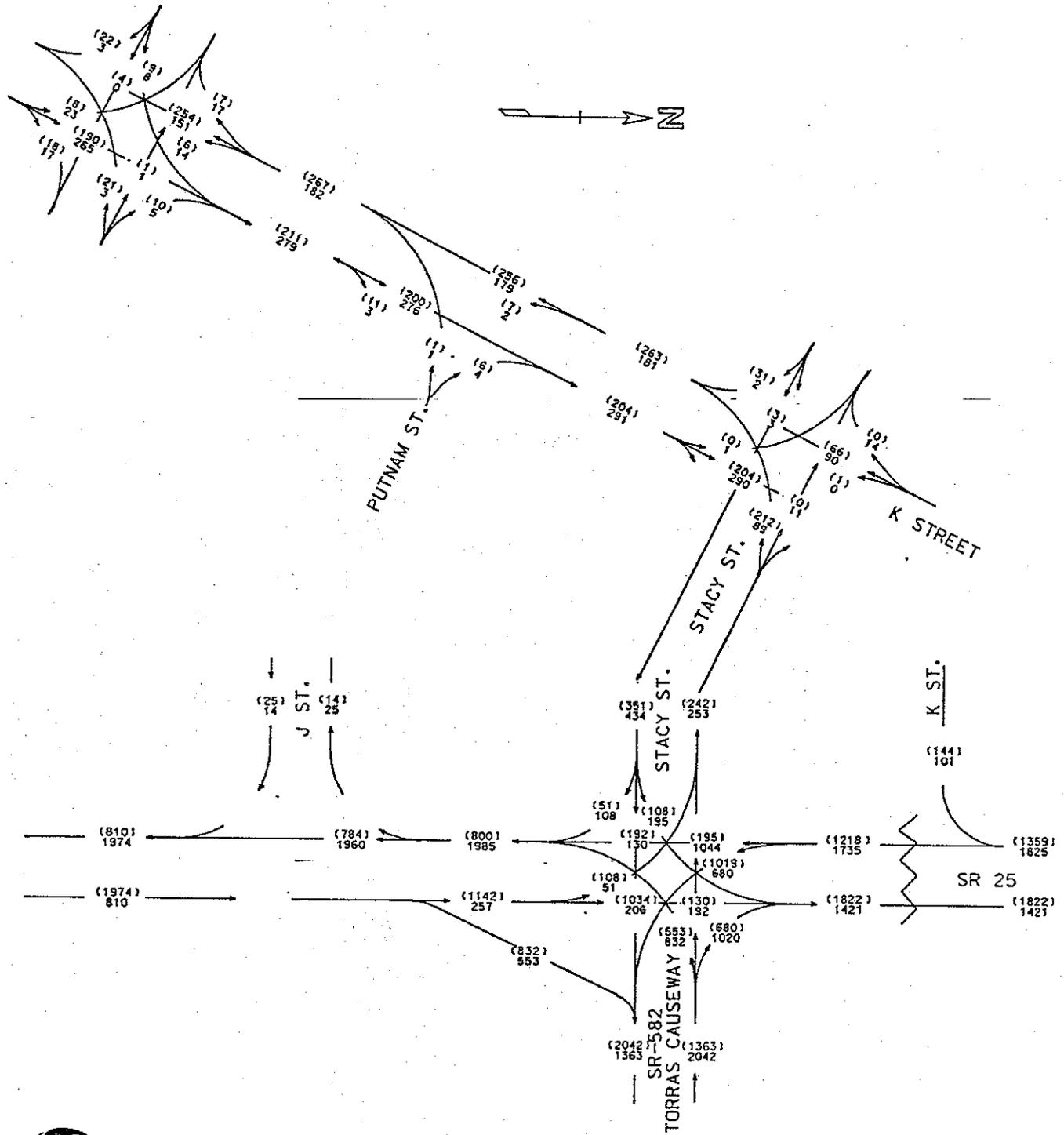
TYPICAL SECTION  
(NOT TO SCALE)

PUTNAM STREET / K STREET  
LOOKING NORTHBOUND



DESIGN LEVEL "B"

- Ⓐ 165 LBS/SY ASPH CONC 12.5 mm SUPERPAVE
- Ⓑ 330 LBS/SY ASPH CONC 19.0 mm SUPERPAVE
- Ⓒ 0.06 GALLON/SQ. YD. BITUMINOUS TACK COAT
- Ⓓ 8" GRADED AGGREGATE BASE CRS.
- Ⓔ 8" X 30" TP 2 CURB & GUTTER

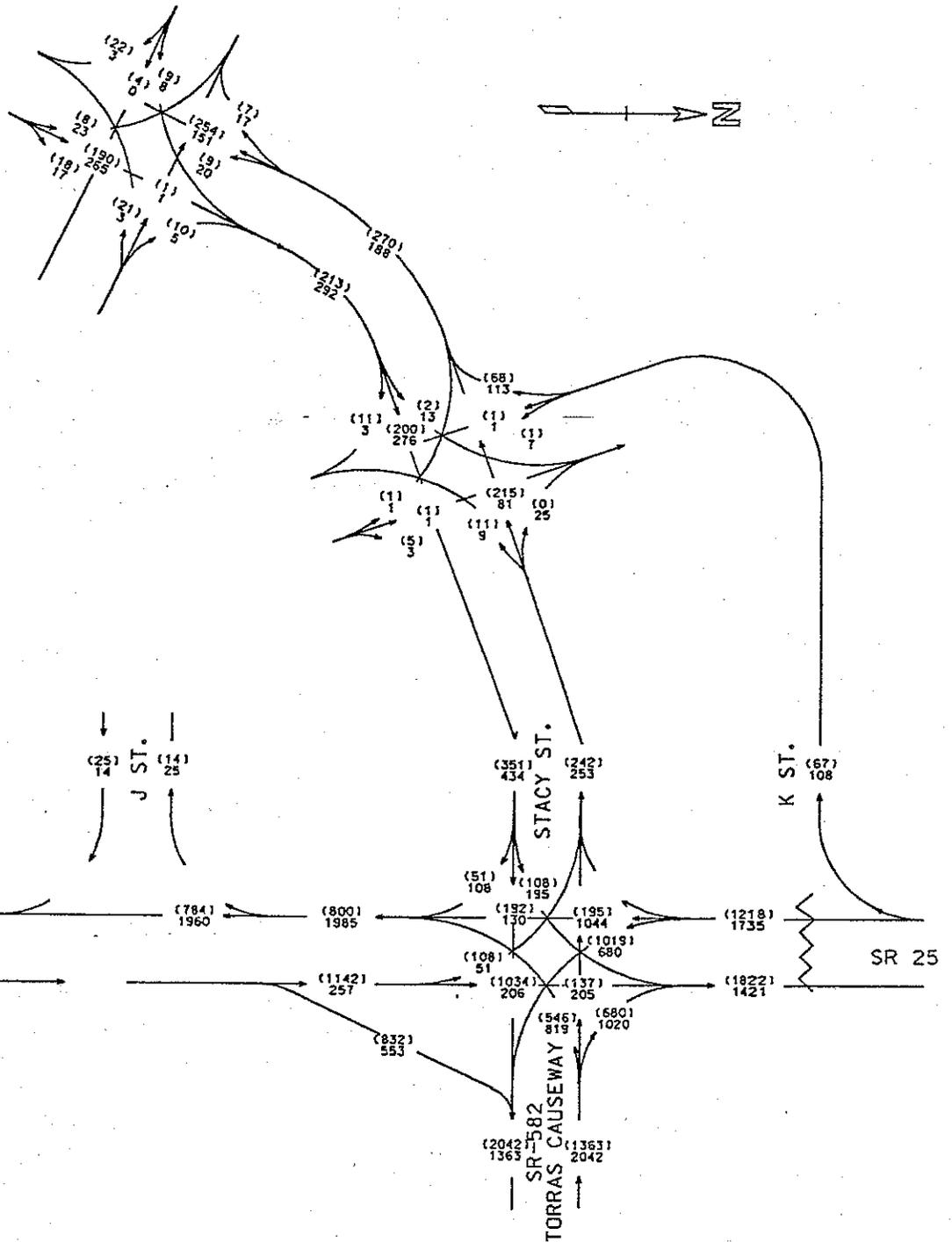


**Stantec**

Stantec  
 4875 Riverside Drive  
 Macon, Georgia U.S.A.  
 31210-1117  
 Tel. 478.474.6100  
 Fax. 478.474.8933  
 www.stantec.com

**2001 CURRENT ALIGNMENT**

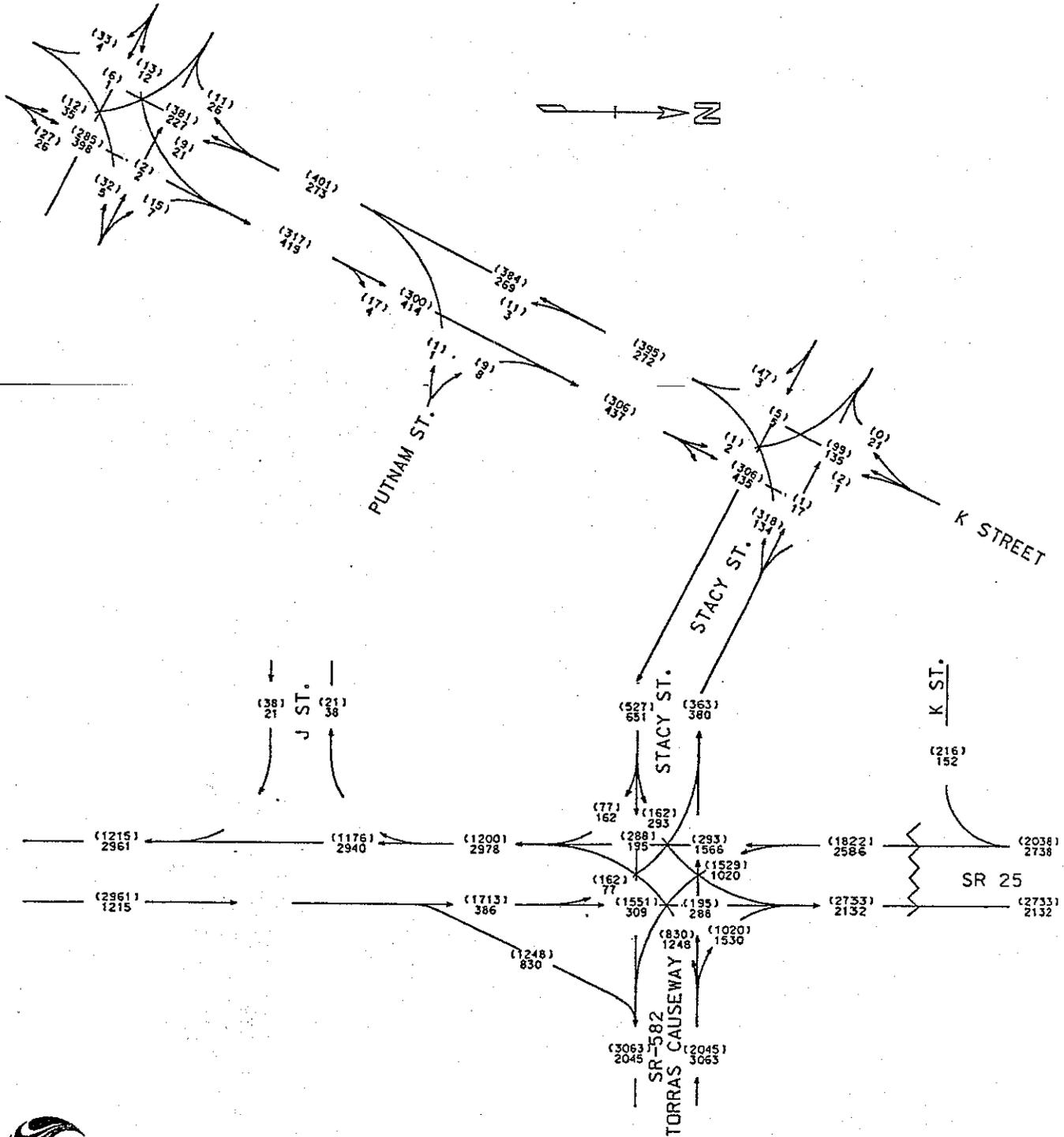
GLYNN COUNTY  
 2001 PM DHV = (000)  
 2001 AM DHV = 000  
 24 HR T = 4%  
 S.U. = 2%  
 COMB. = 2%  
 T = 2%



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2001 PROPOSED ALIGNMENT

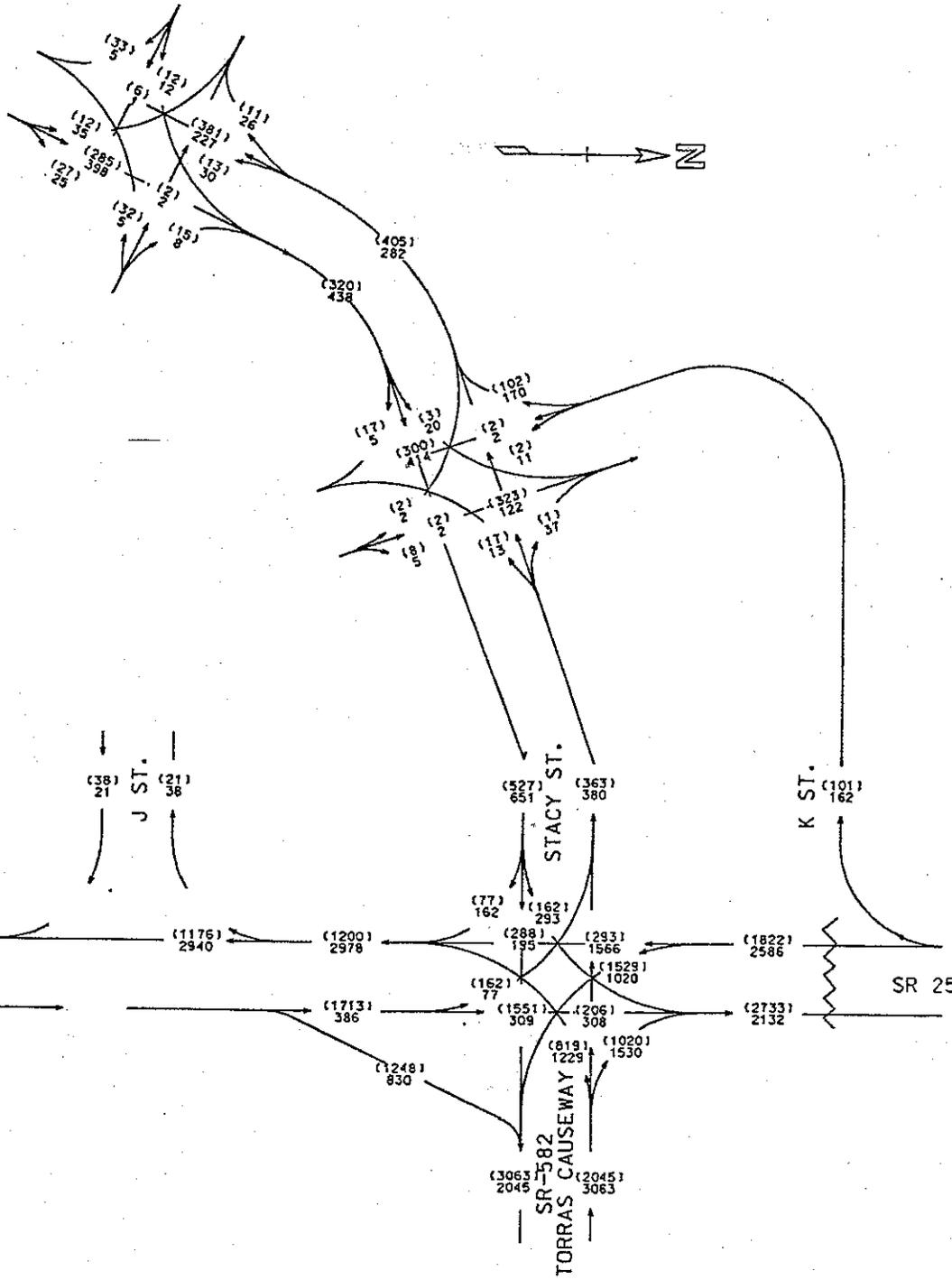
GLYNN COUNTY  
 2001 PM DHV = (000)  
 2001 AM DHV = 000  
 24 HR T = 4%  
 S.U. = 2%  
 COMB. = 2%  
 T = 2%



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2021 CURRENT ALIGNMENT

GLYNN COUNTY  
 2021 PM DHV = (000)  
 2021 AM DHV = 000  
 24 HR T = 4%  
 S.U. = 2%  
 COMB. = 2%  
 T = 2%



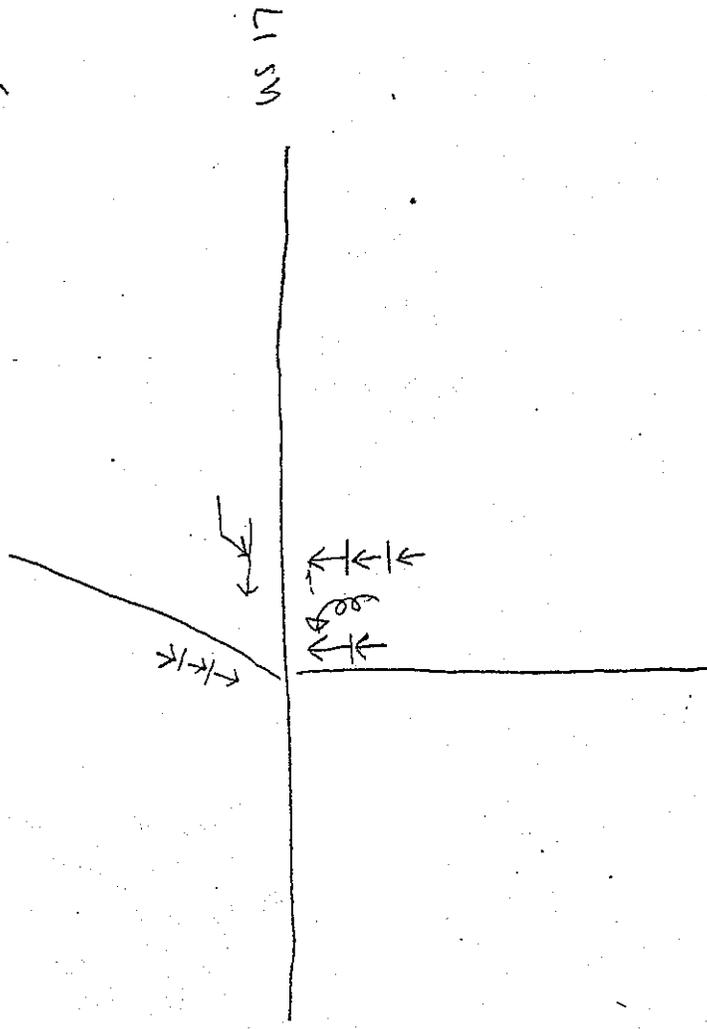
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2021 PROPOSED ALIGNMENT

GLYNN COUNTY  
 2021 PM DHV = (000)  
 2021 AM DHV = 000  
 24 HR T = 4%  
 S.U. = 2%  
 COMB. = 2%  
 T = 2%

COLLISION DIAGRAM

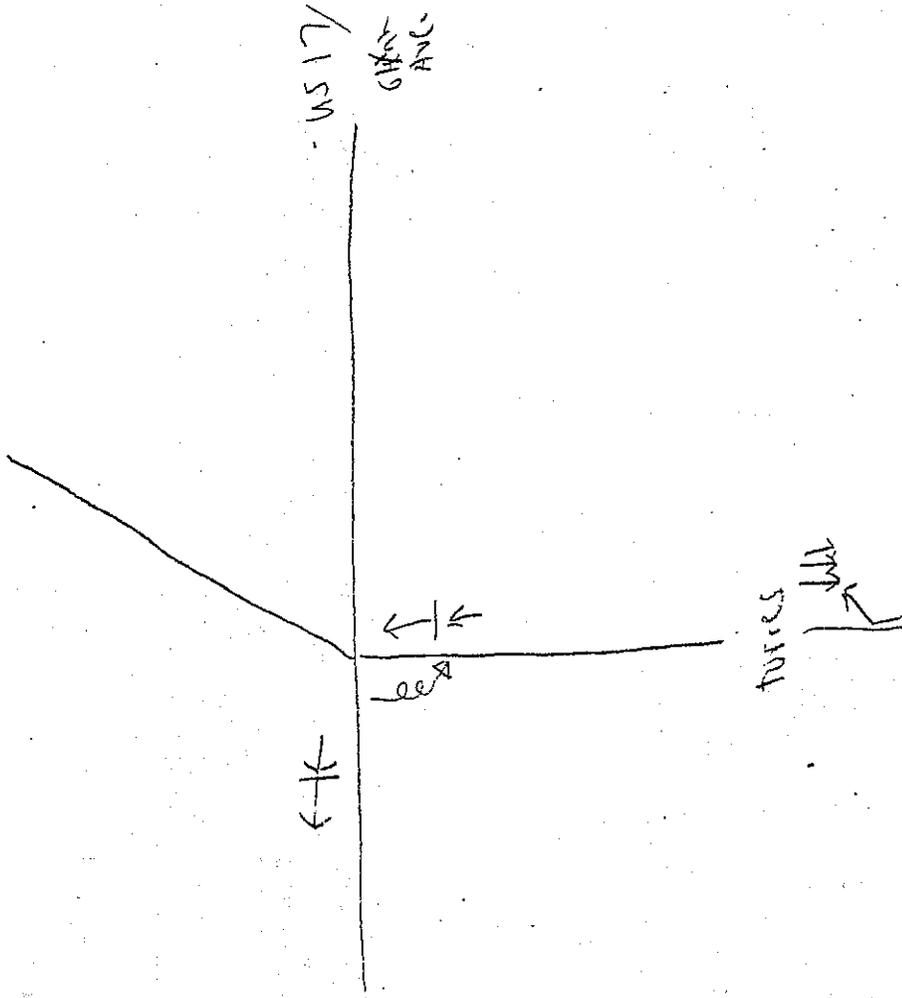


ACCIDENT SUMMARY

<p>TIME OF DAY</p> <p>6 AM - 10 AM</p> <p>10 AM - 4 PM</p> <p>4 PM - 7 PM</p> <p>7 PM - 12 MID</p> <p>12 MID - 6 AM</p> <p>TOTAL</p>	<p>NO. ACCS.</p> <p>1</p> <p>1</p> <p>1</p> <p>1</p> <p>1</p> <p>TOTAL</p>	<p>WEATHER</p> <p>CLEAR (Cl)</p> <p>CLOUDY (Cl)</p> <p>RAIN (R)</p> <p>FOG (F)</p> <p>SNOW (S)</p> <p>TOTAL</p>	<p>NO. ACCS.</p> <p>1</p> <p>1</p> <p>1</p> <p>1</p> <p>1</p> <p>TOTAL</p>
<p>TIME OF YR.</p> <p>DEC - FEB</p> <p>MAR - MAY</p> <p>JUNE - AUG</p> <p>SEPT - NOV</p> <p>TOTAL</p>	<p>NO. ACCS.</p> <p>1</p> <p>1</p> <p>1</p> <p>1</p> <p>TOTAL</p>	<p>DIRECTION</p> <p>NORTHBOUND</p> <p>SOUTHBOUND</p> <p>EASTBOUND</p> <p>WESTBOUND</p> <p>TOTAL</p>	<p>NO. ACCS.</p> <p>1</p> <p>1</p> <p>1</p> <p>1</p> <p>TOTAL</p>
<p>PAVEMENT</p> <p>DRY (D)</p> <p>WET (W)</p> <p>ICY (I)</p> <p>TOTAL</p>	<p>NO. ACCS.</p> <p>1</p> <p>1</p> <p>1</p> <p>TOTAL</p>	<p>ACC. SEVERITY</p> <p>PROP. DAM. ONLY</p> <p>NON-FATAL INJ.</p> <p>FATAL INJ.</p> <p>TOTAL</p>	<p>NO. ACCS.</p> <p>1</p> <p>1</p> <p>1</p> <p>TOTAL</p>
<p>TYPE ACCS.</p> <p>SIDESWIPE</p> <p>REAR END</p> <p>RT. ANGLE</p> <p>LEFT TURN</p> <p>OTHER</p> <p>TOTAL</p>	<p>NO. ACCS.</p> <p>1</p> <p>1</p> <p>1</p> <p>1</p> <p>TOTAL</p>	<p>TYPE VEH.</p> <p>AUTOMOBILE</p> <p>TRUCK</p> <p>MOTORCYCLE</p> <p>OTHER</p> <p>TOTAL</p>	<p>NO. ACCS.</p> <p>1</p> <p>1</p> <p>1</p> <p>1</p> <p>TOTAL</p>
<p>DATE _____ INTERSECTION _____</p>		<p>DAY OF THE WEEK</p>	
<p>COUNTY _____ CITY _____ DISTRICT - TWO</p>		<p>MON. TUE. WED. THUR. FRI. SAT. SUN.</p>	
<p>TIME PERIOD _____</p>		<p>TOTAL _____</p>	

August 1997 - July 1998

# COLLISION DIAGRAM



# ACCIDENT SUMMARY

TIME OF DAY	NO. ACCS	WEATHER	NO. ACCS.
6 AM - 10 AM		CLEAR (Cl)	
10 AM - 4 PM		CLOUDY (Cl)	
4 PM - 7 PM		RAIN (R)	
7 PM - 12 MID		FOG (F)	
12 MID - 6 AM		SNOW (S)	
TOTAL		TOTAL	

TIME OF YR.	NO. ACCS.	DIRECTION	NO. ACCS.
DEC - FEB		NORTHBOUND	
MAR - MAY		SOUTHBOUND	
JUNE - AUG		EASTBOUND	
SEPT - NOV		WESTBOUND	
TOTAL		TOTAL	

PAVEMENT	NO. ACCS.	ACC. SEVERITY	NO. ACCS.
DRY (D)		PROP. DAM. ONLY	
WET (W)		NON-FATAL INJ.	
ICY (I)		FATAL INJ.	
TOTAL		TOTAL	

TYPE ACCS.	NO. ACCS.	TYPE VEH.	NO. ACCS.
SIDESWIPE		AUTOMOBILE	
REAR END		TRUCK	
RT. ANGLE		MOTORCYCLE	
LEFT TURN		OTHER	
OTHER		TOTAL	
TOTAL			

DAY OF THE WEEK						
MON.	TUE.	WED.	THUR.	FRI.	SAT.	SUN.
TOTAL						

- RIGHT ANGLE COLL.
- HEAD ON COLLISION
- PARKING VEHICLE
- MOVING VEHICLE
- FATAL ACCIDENT
- LEFT TURN COLLISION
- REAR END COLLISION
- PARKED VEHICLE
- SIDE SWIPE
- PERSONAL INJURY
- BACKING REAR END
- FIXED OBJECT
- PEDESTRIAN
- OUT OF CONTROL
- PROPERTY DAMAGE

STATE \_\_\_\_\_ INTERSECTION \_\_\_\_\_

COUNTY \_\_\_\_\_ CITY \_\_\_\_\_ DISTRICT - TWO \_\_\_\_\_

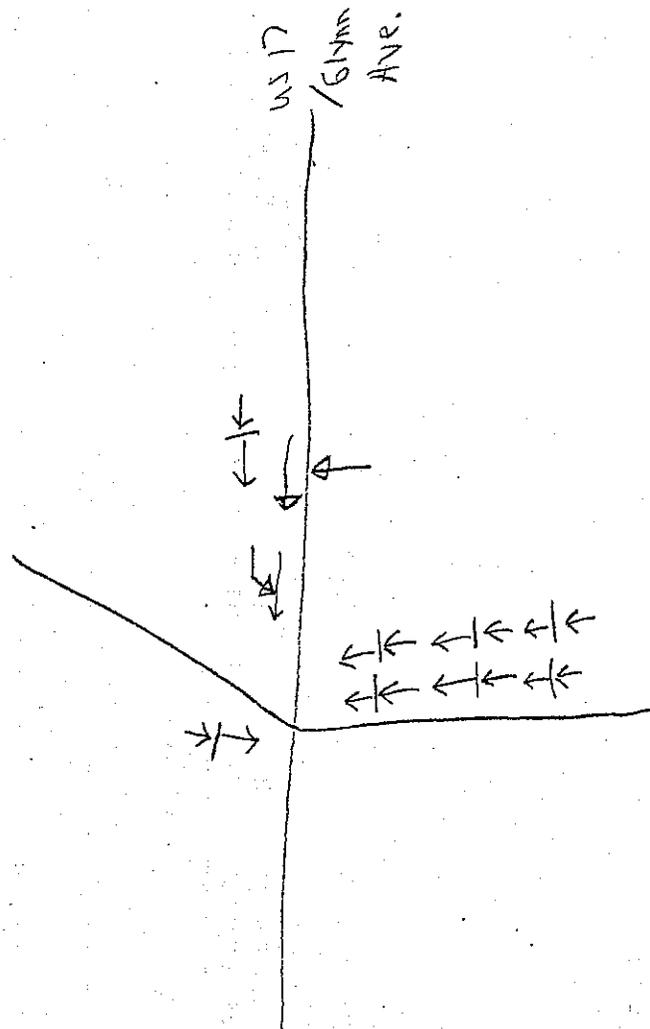
DATE \_\_\_\_\_ TIME PERIOD \_\_\_\_\_

August 1998 - July 30, 1999

# COLLISION DIAGRAM

# ACCIDENT SUMMARY

<b>TIME OF DAY</b> 6 AM - 10 AM 10 AM - 4 PM 4 PM - 7 PM 7 PM - 12 MID 12 MID - 6 AM TOTAL	<b>NO. ACCS</b>                  TOTAL	<b>WEATHER</b> CLEAR (Clr) CLOUDY (Cly) RAIN (R) FOG (F) SNOW (S) TOTAL	<b>NO. ACCS</b>                       TOTAL
<b>TIME OF YR.</b> DEC - FEB MAR - MAY JUNE - AUG SEPT - NOV TOTAL	<b>NO. ACCS</b>              TOTAL	<b>DIRECTION</b> NORTHBOUND SOUTHBOUND EASTBOUND WESTBOUND TOTAL	<b>NO. ACCS</b>                 TOTAL
<b>PAVEMENT</b> DRY (D) WET (W) ICY (I) TOTAL	<b>NO. ACCS</b>             TOTAL	<b>ACC. SEVERITY</b> PROP. DAM. ONLY NON-FATAL INJ. FATAL INJ. TOTAL	<b>NO. ACCS</b>             TOTAL
<b>TYPE ACCS.</b> SIDESWIPE REAR END RT. ANGLE LEFT TURN OTHER TOTAL	<b>NO. ACCS</b>                TOTAL	<b>TYPE VEIL.</b> AUTOMOBILE TRUCK MOTORCYCLE OTHER TOTAL	<b>NO. ACCS</b>                 TOTAL
<b>DAY OF THE WEEK</b> MON.     TUE.    WED.    THUR.     FRI.     SAT.     SUN.     TOTAL		TOTAL	

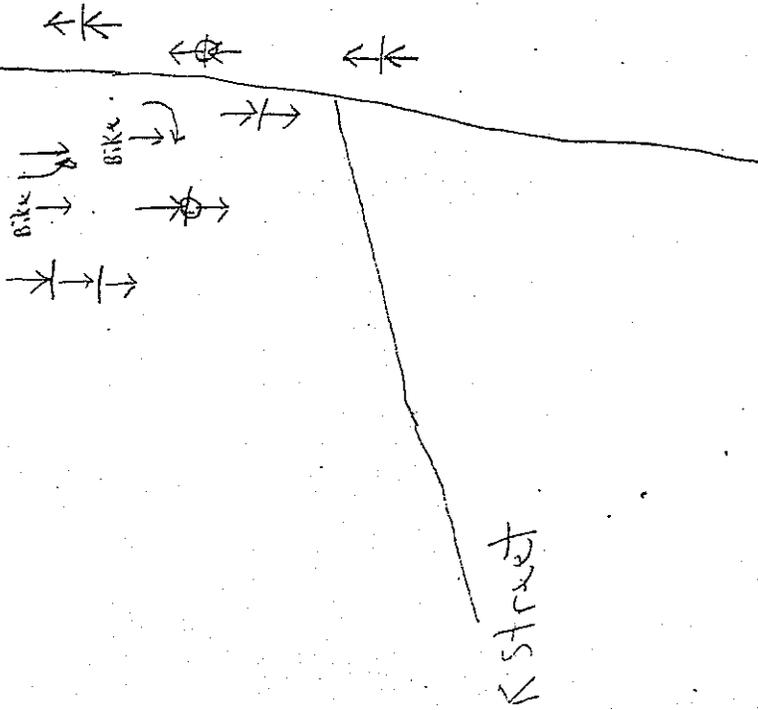


BACKING REAR END  
 FIXED OBJECT  
 PEDESTRIAN  
 OUT OF CONTROL  
 PROPERTY DAMAGE  
 LEFT TURN COLLISION  
 REAR END COLLISION  
 PARKED VEHICLE  
 SIDE SWIPE  
 PERSONAL INJURY  
 RIGHT ANGLE COLL.  
 HEAD ON COLLISION  
 PARKING VEHICLE  
 MOVING VEHICLE  
 FATAL ACCIDENT

SITE \_\_\_\_\_ INTERSECTION \_\_\_\_\_  
 COUNTY \_\_\_\_\_ CITY \_\_\_\_\_ DISTRICT - TWO  
 TIME PERIOD \_\_\_\_\_

August 1999 - July 30, 2000

# COLLISION DIAGRAM



K Street

WS 17

# ACCIDENT SUMMARY

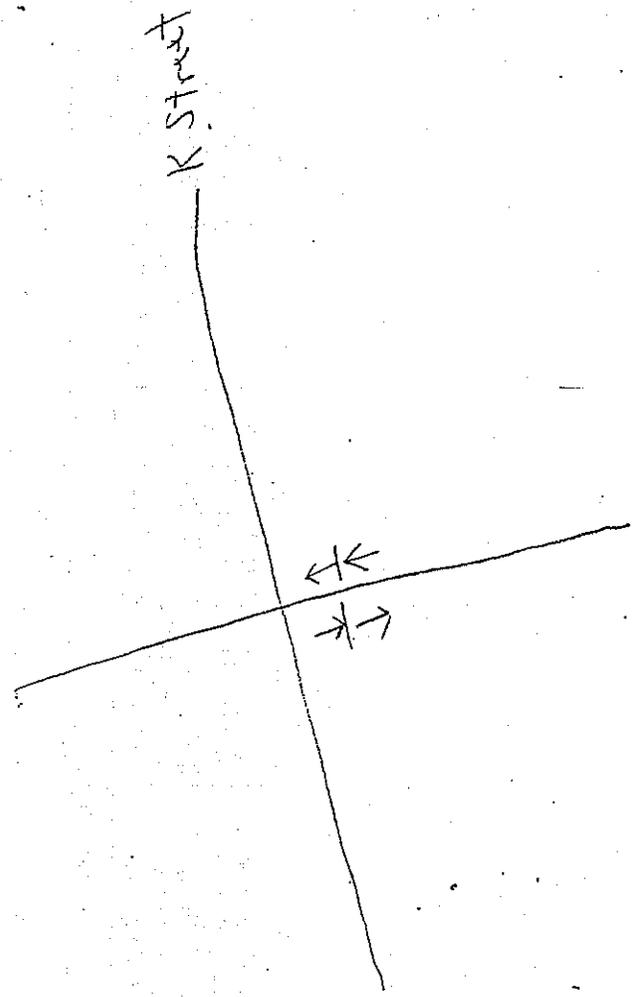
<b>TIME OF DAY</b> 6 AM - 10 AM 10 AM - 4 PM 4 PM - 7 PM 7 PM - 12 MID 12 MID - 6 AM TOTAL	<b>NO. ACCS</b>               _____ TOTAL	<b>WEATHER</b> CLEAR (Cl) CLOUDY (Clp) RAIN (R) FOG (F) SNOW (S) TOTAL	<b>NO. ACCS</b>               _____ TOTAL
<b>TIME OF YR.</b> DEC - FEB MAR - MAY JUNE - AUG SEPT - NOV TOTAL	<b>NO. ACCS</b>             _____ TOTAL	<b>DIRECTION</b> NORTHBOUND SOUTHBOUND EASTBOUND WESTBOUND TOTAL	<b>NO. ACCS</b>             _____ TOTAL
<b>PAVEMENT</b> DRY (D) WET (W) ICY (I) TOTAL	<b>NO. ACCS</b>           _____ TOTAL	<b>ACC. SEVERITY</b> PROP. DAM. ONLY NON-FATAL INJ. FATAL INJ. TOTAL	<b>NO. ACCS</b>          _____ TOTAL
<b>TYPE ACCS.</b> SIDESWIPE REAR END RT. ANGLE LEFT TURN OTHER TOTAL	<b>NO. ACCS.</b>             _____ TOTAL	<b>TYPE VEH.</b> AUTOMOBILE TRUCK MOTORCYCLE OTHER TOTAL	<b>NO. ACCS.</b>                _____ TOTAL
<b>DAY OF THE WEEK</b> MON.   TUE.   WED.   THUR.   FRI.   SAT.   SUN.                   TOTAL			

- RIGHT ANGLE COLL.
- HEAD ON COLLISION
- PARKING VEHICLE
- MOVING VEHICLE
- FATAL ACCIDENT
- LEFT TURN COLLISION
- REAR END COLLISION
- PARKED VEHICLE
- SIDE SWIPE
- PERSONAL INJURY
- BACKING REAR END
- FIXED OBJECT
- PEDESTRIAN
- OUT OF CONTROL
- PROPERTY DAMAGE

COUNTY \_\_\_\_\_ INTERSECTION \_\_\_\_\_ CITY \_\_\_\_\_ DISTRICT TWO  
 TIME PERIOD 8/98 - 7/01

COLLISION DIAGRAM

42



ACCIDENT SUMMARY

TIME OF DAY	NO. ACCS	WEATHER	NO. ACCS.
6 AM - 10 AM		CLEAR (Cl)	
10 AM - 4 PM		CLOUDY (Cl)	
4 PM - 7 PM		RAIN (R)	
7 PM - 12 MID		FOG (F)	
12 MID - 6 AM		SNOW (S)	
TOTAL		TOTAL	

TIME OF YR.	NO. ACCS.	DIRECTION	NO. ACCS.
DEC - FEB		NORTHBOUND	
MAR - MAY		SOUTHBOUND	
JUNE - AUG		EASTBOUND	
SEPT - NOV		WESTBOUND	
TOTAL		TOTAL	

PAVEMENT	NO. ACCS.	ACC. SEVERITY	NO. ACCS.
DRY (D)		PROP. DAM. ONLY	
WET (W)		NON-FATAL INJ.	
ICY (I)		FATAL INJ.	
TOTAL		TOTAL	

TYPE ACCS.	NO. ACCS.	TYPE VEH.	NO. ACCS.
SIDESWIPE		AUTOMOBILE	
REAR END		TRUCK	
RT. ANGLE		MOTORCYCLE	
LEFT TURN		OTHER	
OTHER		TOTAL	

DAY OF THE WEEK						
MON.	TUE.	WED.	THUR.	FRI.	SAT.	SUN.
						TOTAL

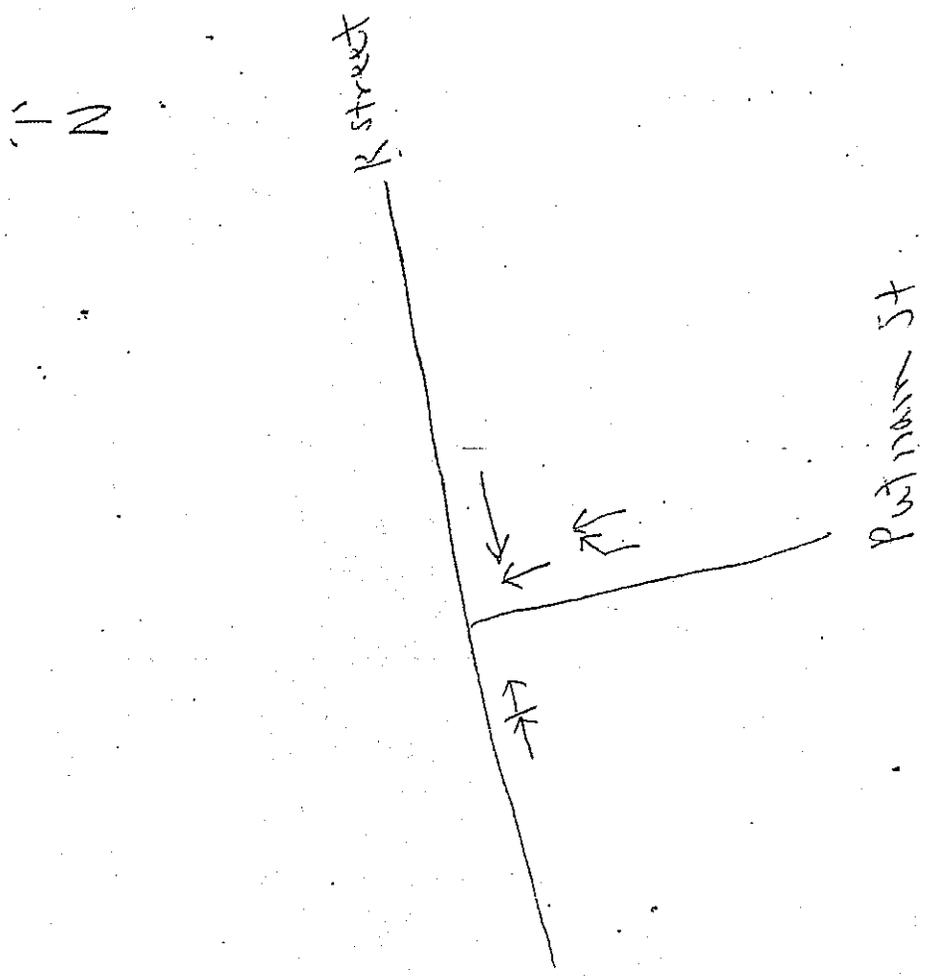
- RIGHT ANGLE COLL.
- HEAD ON COLLISION
- PARKING VEHICLE
- MOVING VEHICLE
- FATAL ACCIDENT
- LEFT TURN COLLISION
- REAR END COLLISION
- PARKED VEHICLE
- SIDE SWIPE
- PERSONAL INJURY
- BACKING REAR END
- FIXED OBJECT
- PEDESTRIAN
- OUT OF CONTROL
- PROPERTY DAMAGE

DATE \_\_\_\_\_ INTERSECTION \_\_\_\_\_

COUNTY \_\_\_\_\_ CITY \_\_\_\_\_ DISTRICT - TWO \_\_\_\_\_

TIME PERIOD 8/96 - 7/01

# COLLISION DIAGRAM



# ACCIDENT SUMMARY

TIME OF DAY	NO. ACCS	WEATHER	NO. ACCS.
6 AM - 10 AM	1	CLEAR (Cl)	11
10 AM - 4 PM	1	CLOUDY (Cl)	1
4 PM - 7 PM		RAIN (R)	
7 PM - 12 MID		FOG (F)	
12 MID - 6 AM		SNOW (S)	
TOTAL		TOTAL	

TIME OF YR.	NO. ACCS.	DIRECTION	NO. ACCS.
DEC - FEB	11	NORTHBOUND	11
MAR - MAY	1	SOUTHBOUND	
JUNE - AUG		EASTBOUND	11
SEPT - NOV		WESTBOUND	1
TOTAL		TOTAL	

PAYEMENT	NO. ACCS.	ACC. SEVERITY	NO. ACCS.
DRY (D)	11	PROP. DAM. ONLY	11
WET (W)	1	NON-FATAL INJ.	
ICY (I)		FATAL INJ.	
TOTAL		TOTAL	

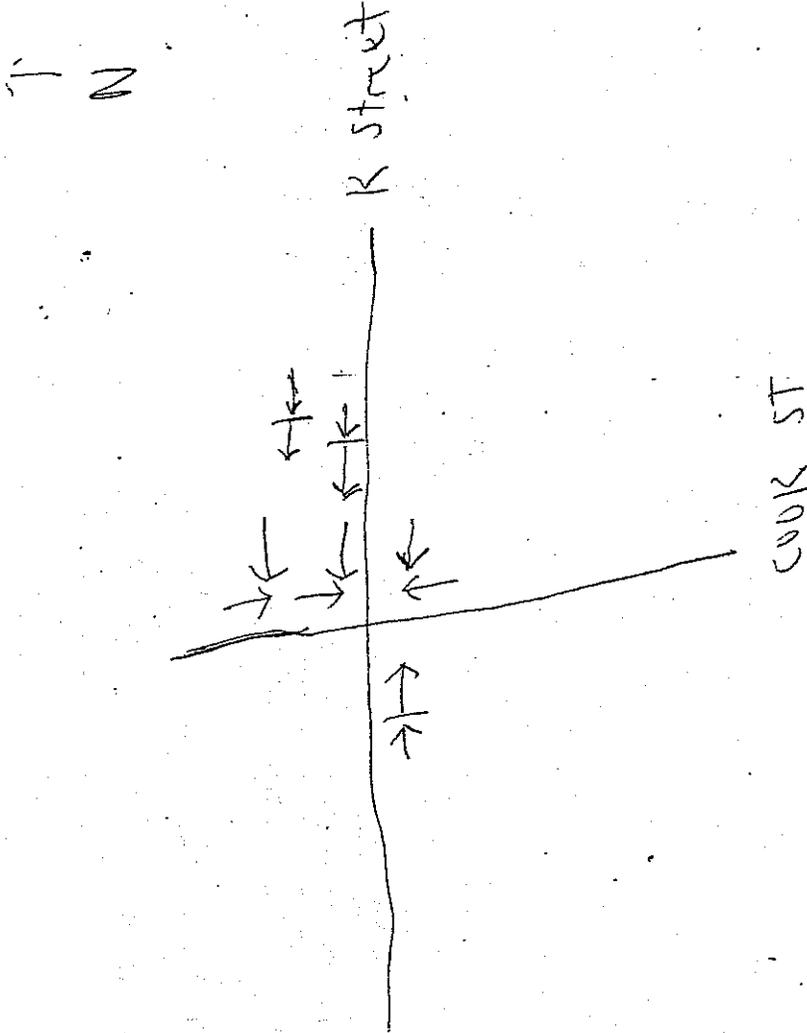
TYPE ACCS.	NO. ACCS.	TYPE VEH.	NO. ACCS.
SIDESWIPE	1	AUTOMOBILE	1111
REAR END	1	TRUCK	1
RT. ANGLE	1	MOTORCYCLE	
LEFT TURN		OTHER	
OTHER		TOTAL	

DAY OF THE WEEK						
MON.	TUE.	WED.	THUR.	FRI.	SAT.	SUN.
1						
TOTAL						

<input type="checkbox"/> RIGHT ANGLE COLL. <input type="checkbox"/> HEAD ON COLLISION <input type="checkbox"/> PARKING VEHICLE <input type="checkbox"/> MOVING VEHICLE <input type="checkbox"/> FATAL ACCIDENT	<input type="checkbox"/> LEFT TURN COLLISION <input type="checkbox"/> REAR END COLLISION <input type="checkbox"/> PARKED VEHICLE <input type="checkbox"/> SIDE SWIPE <input type="checkbox"/> PERSONAL INJURY	<input type="checkbox"/> BACKING REAR END <input type="checkbox"/> FIXED OBJECT <input type="checkbox"/> PEDESTRIAN <input type="checkbox"/> OUT OF CONTROL <input type="checkbox"/> PROPERTY DAMAGE
DATE _____ INTERSECTION _____		
COUNTY _____ CITY _____ DISTRICT - TWO _____		
TIME PERIOD 3/97-7/01		

# COLLISION DIAGRAM



# ACCIDENT SUMMARY

<b>TIME OF DAY</b> 6 AM - 10 AM 10 AM - 4 PM 4 PM - 7 PM 7 PM - 12 MID 12 MID - 6 AM TOTAL	<b>NO. ACCS</b>               _____ TOTAL	<b>WEATHER</b> CLEAR (Cl) CLOUDY (Cl) RAIN (R) FOG (F) SNOW (S) TOTAL	<b>NO. ACCS</b>               _____ TOTAL
<b>TIME OF YR.</b> DEC - FEB MAR - MAY JUNE - AUG SEPT - NOV TOTAL	<b>NO. ACCS</b>            _____ TOTAL	<b>DIRECTION</b> NORTHBOUND SOUTHBOUND EASTBOUND WESTBOUND TOTAL	<b>NO. ACCS</b>                _____ TOTAL
<b>PAVEMENT</b> DRY (D) WET (W) ICY (I) TOTAL	<b>NO. ACCS</b>           _____ TOTAL	<b>ACC. SEVERITY</b> PROP. DAM. ONLY NON-FATAL INJ. FATAL INJ. TOTAL	<b>NO. ACCS</b>           _____ TOTAL
<b>TYPE ACCS:</b> SIDESWIPE REAR END RT. ANGLE LEFT TURN OTHER TOTAL	<b>NO. ACCS</b>             _____ TOTAL	<b>TYPE VEH.</b> AUTOMOBILE TRUCK MOTORCYCLE OTHER TOTAL	<b>NO. ACCS</b>           _____ TOTAL
<b>DAY OF THE WEEK</b> MON.   TUE.   WED.   THUR.   FRI.   SAT.   SUN.		<b>TOTAL</b>	

DATE \_\_\_\_\_ INTERSECTION \_\_\_\_\_  
 COUNTY \_\_\_\_\_ CITY \_\_\_\_\_ DISTRICT - TWO \_\_\_\_\_  
 TIME PERIOD 8/98 - 7/01

- RIGHT ANGLE COLL.
- HEAD ON COLLISION
- PARKING VEHICLE
- MOVING VEHICLE
- FATAL ACCIDENT
- LEFT TURN COLLISION
- REAR END COLLISION
- PARKED VEHICLE
- SIDE SWIPE
- PERSONAL INJURY
- BACKING REAR END
- FIXED OBJECT
- PEDESTRIAN
- OUT OF CONTROL
- PROPERTY DAMAGE

DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA  
OFFICE OF DISTRICT FIVE DESIGN

# PROJECT CONCEPT REPORT

STP-0005-00(08)

P.I. NO. – 0005008

GLYNN COUNTY

FEDERAL ROUTE NO: N/A

STATE ROUTE NO: SR 582 (F. J. Torras Causeway)

Prepared By: Bret Gillis  
Bret Gillis, PE  
Stantec Consulting

Date: 11-15-02

## RECOMMENDATION FOR APPROVAL

This project concept is contained in the Regional Transportation Improvement Program (RTIP) and/or in the State Transportation Improvement Program (STIP). The concept as presented herein and submitted for approval is consistent with that which is included in the RTIP and/or the STIP.

12-5-02  
DATE

Marta Rosen  
State Transportation Planning Administrator

\_\_\_\_\_  
DATE

\_\_\_\_\_  
State Transportation Programming Engineer

\_\_\_\_\_  
DATE

\_\_\_\_\_  
State Environmental / Location Engineer

11/26/02  
DATE

Gary D. Priester ASCE  
District Engineer / Jesup

\_\_\_\_\_  
DATE

\_\_\_\_\_  
Project Review Engineer

\_\_\_\_\_  
DATE

\_\_\_\_\_  
State Traffic Operations Engineer

DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA  
OFFICE OF DISTRICT FIVE DESIGN

# PROJECT CONCEPT REPORT

STP-0005-00(08)  
P.I. NO. – 0005008  
GLYNN COUNTY  
FEDERAL ROUTE NO: N/A  
STATE ROUTE NO: SR 582 (E. J. Torras Causeway)

Prepared By: Bret Gillis Date: 11-15-02  
Bret Gillis, PE  
Stantec Consulting

## RECOMMENDATION FOR APPROVAL

This project concept is contained in the Regional Transportation Improvement Program (RTIP) and/or in the State Transportation Improvement Program (STIP). The concept as presented herein and submitted for approval is consistent with that which is included in the RTIP and/or the STIP.

_____ DATE	_____ State Transportation Planning Administrator
_____ DATE	_____ State Transportation Programming Engineer
_____ DATE	_____ State Environmental / Location Engineer
<u>11/26/02</u> DATE	<u>Gary D. Priester ASCE</u> District Engineer / Jesup
_____ DATE	_____ Project Review Engineer
_____ DATE	_____ State Traffic Operations Engineer

DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA  
OFFICE OF DISTRICT FIVE DESIGN

# PROJECT CONCEPT REPORT

STP-0005-00(08)

P.I. NO. – 0005008

GLYNN COUNTY

FEDERAL ROUTE NO: N/A

STATE ROUTE NO: SR 582 (F. J. Torras Causeway)

Prepared By: Bret Gillis Date: 11-15-02  
Bret Gillis, PE  
Stantec Consulting

## RECOMMENDATION FOR APPROVAL

This project concept is contained in the Regional Transportation Improvement Program (RTIP) and/or in the State Transportation Improvement Program (STIP). The concept as presented herein and submitted for approval is consistent with that which is included in the RTIP and/or the STIP.

DATE

State Transportation Planning Administrator

DATE

State Transportation Programming Engineer

DATE

State Environmental / Location Engineer

11/26/02  
DATE

Gary D. Prieta ASCE  
District Engineer / Jesup

DATE

Project Review Engineer

DATE

State Traffic Operations Engineer

DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA  
OFFICE OF DISTRICT FIVE DESIGN

# PROJECT CONCEPT REPORT

STP-0005-00(08)  
P.I. NO. – 0005008  
GLYNN COUNTY  
FEDERAL ROUTE NO: N/A  
STATE ROUTE NO: SR 582 (F. J. Torras Causeway)

Prepared By: Bret Gillis  
Bret Gillis, PE  
Stantec Consulting

Date: 11-15-02

## RECOMMENDATION FOR APPROVAL

This project concept is contained in the Regional Transportation Improvement Program (RTIP) and/or in the State Transportation Improvement Program (STIP). The concept as presented herein and submitted for approval is consistent with that which is included in the RTIP and/or the STIP.

DATE

State Transportation Planning Administrator

DATE

State Transportation Programming Engineer

DATE

State Environmental / Location Engineer

11/26/02

DATE

Gary D. Priester ASCE  
District Engineer / Jesup

DATE

Project Review Engineer

12/3/02

DATE

Phillip M. Allen  
State Traffic Operations Engineer

Safety & Design

Department of Transportation  
State of Georgia

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INTERDEPARTMENTAL CORRESPONDENCE

File: STP-0005-00(08), Glynn County  
P.I. No. 0005008

Office: Traffic Safety & Design  
Atlanta, Georgia

Date: December 3, 2002

From: <sup>PMA/KC</sup> Phillip M. Allen, State Traffic Safety and Design Engineer

To: Meg Pirkle, Assistant Director of Preconstruction

Subject: Project Concept Report Review

We have reviewed the above referenced concept report for the realignment of K Street and construction of the Torras Causeway Extension in Glynn County.

The Office of Traffic Safety & Design finds this report satisfactory for approval because it will improve safety and traffic operations within this area.

PMA/sz

Attachment (signature page)

Cc: Harvey Keeper, State Environment/Location Engineer  
Gary Priester, District Engineer - Jesup  
Attention: Tony Collins  
David Mulling, State Review Engineer, w/ attachment  
Marta Rosen, State Transportation Planning Administrator  
Paul Liles, State Bridge & Structural Design Engineer  
Chuck Hasty, TMC  
General Files  
Office Files