

ORIGINAL TO GENERAL FILES

D.O.T. 66

DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA

INTERDEPARTMENT CORRESPONDENCE

FILE STP-000-00(811) Forsyth County **OFFICE** Preconstruction
P. I. No. 0000811
DATE November 16, 2000

FROM 
C. Wayne Hutto, Assistant Director of Preconstruction

TO SEE DISTRIBUTION

SUBJECT PROJECT CONCEPT REPORT APPROVAL

Attached for your files is the approval for subject project.

CWH/cj

Attachment

DISTRIBUTION:

Tom Turner
David Mulling
Harvey Keepler
Jerry Hobbs
Herman Griffin
Michael Henry
Marion Waters
Marta Rosen
Paul Liles
Jimmy Chambers (ATTN: Ted Cashin)
Larry Dent

DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA

INTERDEPARTMENT CORRESPONDENCE

FILE STP-0000-00(811) Forsyth County
P.I. No. 0000811

OFFICE Preconstruction

DATE November 7, 2000

FROM Thomas L. Turner, P.E., Director of Preconstruction 

TO J. Tom Coleman, Jr., Commissioner

SUBJECT PROJECT CONCEPT REPORT

This project is the intersection improvements at six locations along SR 369/Browns Bridge Road in Forsyth County. State Route 369 is a rural two lane roadway with posted speed varying between 45 and 55 MPH. The proposed improvements will dramatically improve the safety and Level-of-Service (LOS) of these roadways, providing adequate storage and turning radii for all vehicle types and conditions.

Construction is proposed as follows:

Intersection 2A - SR 369/Browns Bridge Road at Jot-em Down Road/Bethel Road/CR 171

SR 369 will be widened to provide eastbound and westbound right turn lanes. The northbound and southbound approaches of CR 171 will be realigned 90'± east add 140' west respectively to intersect SR 369 at 90 degrees. Left turn lanes will be striped on both approaches and a channeled right turn lane with a raised turning island will also be added on each approach.

Intersection 2B - SR 369/Browns Bridge Road at Waldrip Road

The existing acute angled four way intersection will be reconfigured into a staggered intersection. State Route 369 will be widened and resurfaced with a center two way left turn lane and a right turn lane onto Waldrip Road. The existing southbound approach on Waldrip Road will be relocated 500'± west on new location to intersect SR 369 at 90 degrees, and will include a channelized right turn lane with a striped turning island. The remaining segment will be made into a cul-de-sac, tying into Waldrip Road north of SR 369. The existing northbound approach on Waldrip Circle will be relocated 300'± east of its existing location to intersect SR 369 at 90 degrees. The existing passing lane on SR 369 in this area will be striped as a center two way left turn lane.

STP-0000-00(811) Forsyth
November 7, 2000

Intersection 2C - SR 369/Matt Highway at Doc Bramblett Road

This project consists of widening SR 369 to provide a westbound left turn lane. The Doc Bramblett Road approach will be relocated to intersect SR 369, 157' east of its existing location and modified to provide a channelized right turn lane. The intersection of SR 369 at Doc Bramblett Road is located approximately 5.5 miles north of the city limits of Cumming and 2.5 miles directly south of the Dawson County line.

Intersection 2D - SR 369/Matt Highway at Hendrix Road

This project consists of widening SR 369 to provide a separate eastbound right turn lane and westbound left turn lane. Modification of the Hendrix Road approach to provide a channeled right turn lane. The intersection of SR 369 at Hendrix Road is located in Forsyth County approximately 2.6 miles west of Georgia 400 along SR 369, 3.5 miles directly south of the Dawson County line.

Intersection 2E - SR 369/Browns Bridge Road at Holtzclaw Road

This project consists of resurfacing and restriping SR 369 to provide a center turn lane that becomes a designated left turn lane at its intersection with Holtzclaw Road. The Holtzclaw Road approach to SR 369 will be widened to provide a separate left turn lane and a channeled right turn lane. The intersection of SR 369 at Holtzclaw Road is located in Forsyth County approximately 1.2 miles east of Georgia 400 along SR 369, 3.3 miles northeast of the city limits of Cumming.

Intersection 2F - SR 369/Browns Bridge Road at Shady Grove Road/Elrod Circle

This project consists of relocating Shady Grove Road to intersect SR 369 across from CR 147/Elrod Circle, creating a four way signalized intersection. State Route 369 will be restriped to provide separate eastbound and westbound left turn lanes. Elrod Circle will be widened to provide a separate left turn lane. Shady Grove Road will be relocated and will include a separate left turn lane and one shared lane for thru or right turning vehicles. The project is located approximately 1.7 miles east of Georgia 400 along SR 369 and 5.3 miles south of the Dawson County line.

Additional right-of-way will be required at all six intersections. Traffic will be maintained during construction.

Environmental concerns include requiring a Categorical Exclusion will be prepared; a public hearing is not required; time saving procedures are appropriate.

The estimated costs for this project are:

	<u>PROPOSED</u>	<u>APPROVED</u>	<u>PROG DATE</u>	<u>LET DATE</u>
Construction (includes E&C and inflation)	\$2,109,000	\$1,519,000	2006	05-07
Right-of-Way	\$1,472,000	\$1,193,000		
Utilities*	----	----		

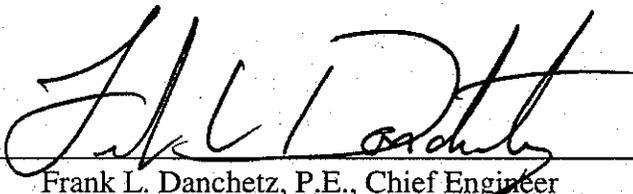
*Contract sent 4-4-00 requesting Forsyth County be responsible for PE and 20% of construction costs.

I recommend this project concept be approved.

TLT:JDQ/cj

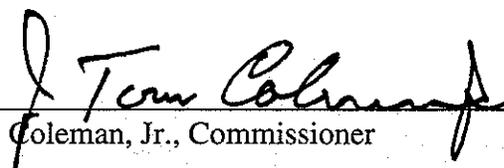
Attachment

CONCUR



Frank L. Danchetz, P.E., Chief Engineer

APPROVE



J. Tom Coleman, Jr., Commissioner

**DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA**

INTERDEPARTMENTAL CORRESPONDENCE

FILE: STP-0000-00(811) Forsyth
P.I. Number 0000811

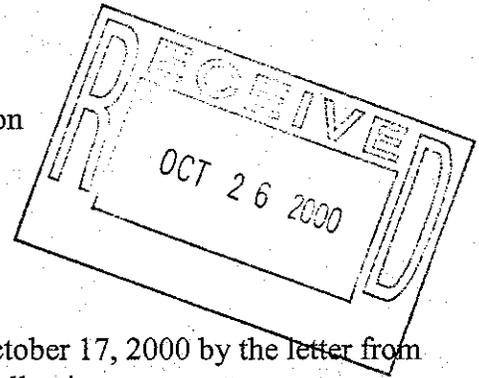
OFFICE: Atlanta, Georgia

DATE: October 25, 2000

FROM: David Mulling, Project Review Engineer *DM*

TO: Wayne Hutto, Assistant Director of Pre-construction

SUBJECT: CONCEPT REPORT



We have reviewed the concept report submitted October 17, 2000 by the letter from Larry Dent dated October 13, 2000, and have the following comment:

1. Estimated costs for reimbursable utilities were not provided.

The costs for the project are:

Construction	\$1,743,000
Inflation	\$ 174,000
E&C	\$ 192,000
Reimbursable Utilities	\$?
Right of Way	\$1,472,000

DTM

c: Todd Long – District 1 Design

**DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA**

PROJECT CONCEPT REPORT

FORSYTH COUNTY

<u>LOCATION</u>	<u>PROJECT NO.</u>	<u>P.I. #</u>
2A. S.R. 369/Browns Bridge Rd at Jot'em Down Rd	STP-0000-00(811)	0000811
2B. S.R. 369/Browns Bridge Rd at Waldrip Road	STP-0000-00(811)	0000811
2C. S.R. 369/Matt Hwy. at Doc Bramblett Rd	STP-0000-00(811)	0000811
2D. S.R. 369/Matt Hwy. at Hendrix Rd.	STP-0000-00(811)	0000811
2E. S.R. 369/Browns Bridge Rd at Holtzclaw Rd	STP-0000-00(811)	0000811
2F. S.R. 369/Browns Bridge Rd at Shady Grove Rd	STP-0000-00(811)	0000811

Date of Report: Oct. 13, 2000

RECOMMENDATION FOR APPROVAL

This project is contained in the Transportation Improvement Program (TIP) and/or in the State Transportation Improvement Plan (STIP). The concept as presented herein and submitted for approval is consistent with that which is included in the TIP and/or the STIP.

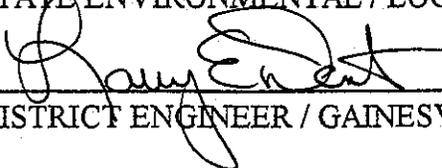
DATE STATE TRANSPORTATION PLANNING ADMINISTRATOR

DATE STATE TRANSPORTATION PROGRAMMING ENGINEER

DATE STATE ROAD DESIGN ENGINEER

DATE STATE ENVIRONMENTAL / LOCATION ENGINEER

10-13-00

DATE  DISTRICT ENGINEER / GAINESVILLE

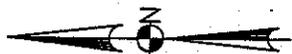
DATE PROJECT REVIEW ENGINEER

DATE STATE TRAFFIC OPERATIONS ENGINEER

DAWSON COUNTY

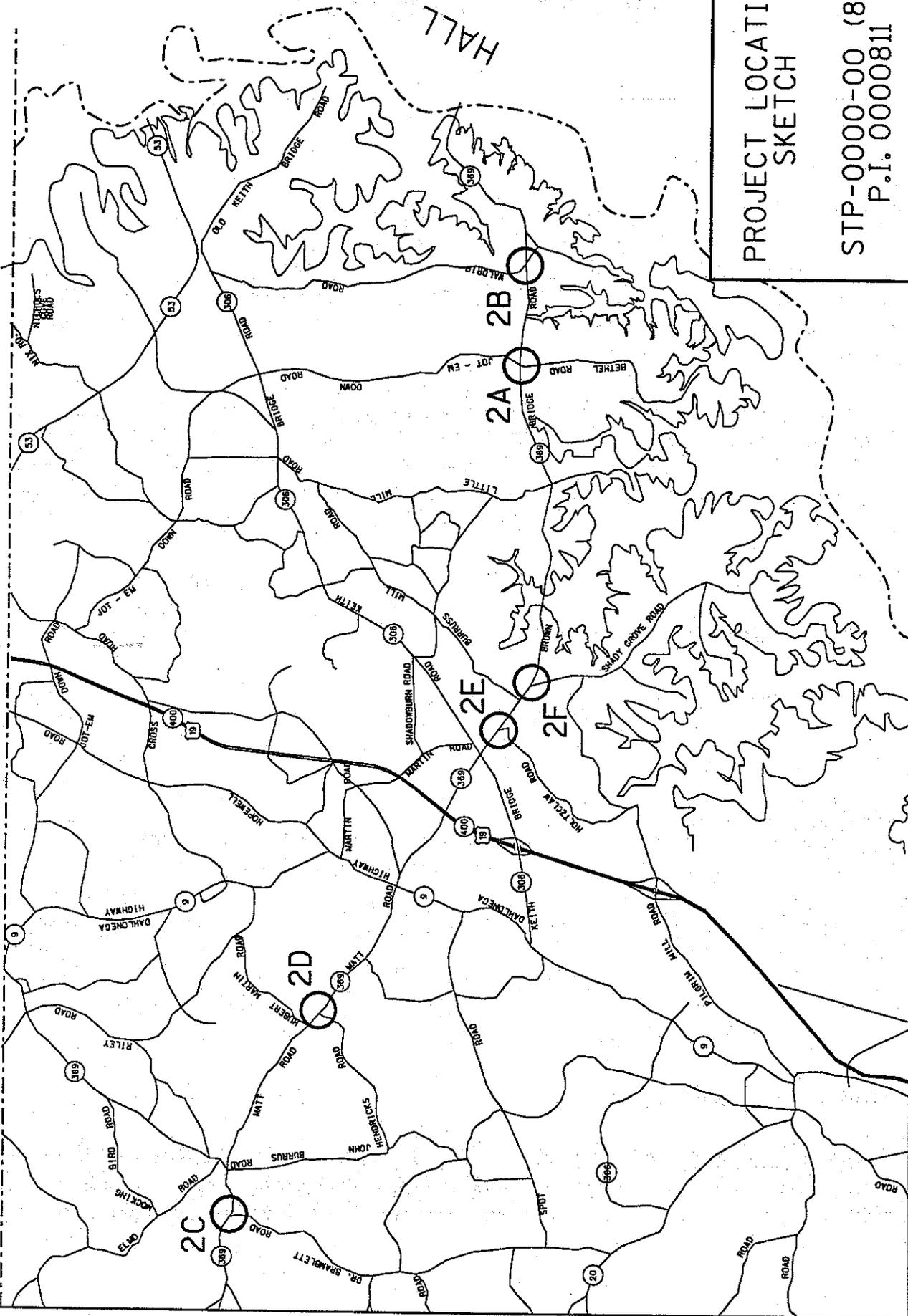
HALL COUNTY

HALL



PROJECT LOCATION
SKETCH

STP-0000-00 (811)
P.I. 0000811



PROJECT CONCEPT REPORT

P.I. No.: 0000811

Project No.: STP-0000-00 (811) Forsyth County

Route No.: S.R. 369 (Browns Bridge Road)
C.R. 171 (Jot-em Down Road)/C.R. 172 (Bethel Road)

Location: The intersection of S.R. 369 @ C.R. 171 is located approximately 5.53 miles east along Browns Bridge Road from its intersection with GA 400, northeast of the city of Cumming in Forsyth County.

Description: S.R. 369 would be widened to provide eastbound and westbound right turn lanes. The northbound and southbound approaches of C.R. 171 would be realigned approximately 90 feet east and 140 feet west respectively so as to intersect S.R. 369 at 90 degrees. Left-turn lanes would be striped on both approaches, and a channeled right-turn lane with a raised turning island would also be added on each approach. Additional right-of-way is required for the horizontal re-alignment.

<u>Traffic Volumes</u>	<u>1999 ADT</u>	<u>2003 ADT</u>
S.R. 369	14,000	16,300
C.R. 171/C.R. 172	1,750	2,050

<u>Existing Posted Speed Limit:</u>	S.R. 369	55 mph.
	C.R. 171	45 mph.
	C.R. 172	35 mph.

Existing Typical Section:

S.R. 369: One 12-foot rural travel lane in each direction with a center turn lane; grass shoulder.

C.R. 171: One 12-foot rural travel lane in each direction; grass shoulder.

<u>Existing Right-of-way:</u>	S.R. 369	100 ft.
	C.R. 171	60 ft.
	C.R. 172	60 ft.

Existing Traffic Control: Stop-sign control on C.R. 171.

Existing Major Structures: None

Need and Purpose: Currently, Jot-em Down Road and Bethel Road meet S.R. 369 at 50° and 60° angles, well below preferred alignments. These acute angles require extensive pavement turning areas increasing the exposure time for vehicles crossing or entering the main traffic flow, increasing the potential

for accidents. An accident history at this intersection from 1995 to 1997 indicate that the majority of accidents are "rear end" or "sideswipe" type collisions.

The re-alignment of the north and southbound approaches, along with additional left and right turn-lanes, with paved outside shoulders and a traffic signal will improve the safety and operational efficiency of the intersection by providing adequate storage capacity and turning radii for all vehicle types and conditions.

State Route Mile Post MP 16.63

PDP Class: Minor Existing

Functional Class: S.R. 369 Rural Minor Arterial
C.R. 171/ C.R. 172. Rural Local Road

Max. Degree of Curve:	Existing Max Grade:	Angle of Intersection:
S.R. 369: 6.0°	+/- 2 %	90°
C.R. 171: 6.0°	+/- 2 %	90°

Design Speed: S.R. 369 55 mph
C.R. 171 (Jot-em Down Road) 45 mph
C.R. 172 (Bethel Road) 40 mph

Proposed Typical Section:

S.R. 369: One 12-foot rural travel lane in each direction with left turn lanes and right turn lanes at its intersection with C.R. 171/ C.R. 172; grass 6' shoulders with open ditch section.

C.R. 171/C.R. 172: One 12-foot rural travel lane in each direction with left turn and right turn lanes at its intersection with S.R. 369 and 6' grass shoulders with open ditch section.

Proposed Right-of-way Width: S.R. 369 100 ft.
C.R. 171 (Relocated) 80 ft.
C.R. 172 (Relocated) 80 ft.

Number of Parcels Impacted: 5 **Displacements:** RES: 1 BUS: 0 MH: 0

Proposed Major Structures: None.

Type Access: State and County Permit.

Traffic Control During Construction: Traffic will be maintained during construction.

Alternatives Considered:

1. No Build Condition
2. Proposed Project

Design Exceptions Required:

CONTROLLING CRITERIA	UNDETERMINED	YES	NO
HORIZONTAL ALIGNMENT	()	()	(X)
ROADWAY WIDTH	()	()	(X)
SHOULDER WIDTH	()	()	(X)
VERTICAL GRADES	()	()	(X)
CROSS SLOPES	()	()	(X)
STOPPING SIGHT DISTANCE	()	()	(X)
SUPERELEVATION RATES	()	()	(X)
HORIZONTAL CLEARANCE	()	()	(X)
SPEED DESIGN	()	()	(X)
VERTICAL CLEARANCE	()	()	(X)

Utilities: Forsyth County shall be responsible for all utility adjustments. Potential utilities impacted are listed below:

- Telephone - BellSouth
- Power - GA Power or Sawnee EMC
- Water - Forsyth County
- Cable - Prestige Cable

Permits Required: None required.

Level of Environmental Analysis: Categorical Exclusion.

Level of Public Involvement: None required.

Time Saving Procedures Appropriate: Yes (X) No ()

The Estimated Project Cost: \$1,156,440.00 Detailed cost estimate is shown on the next page.

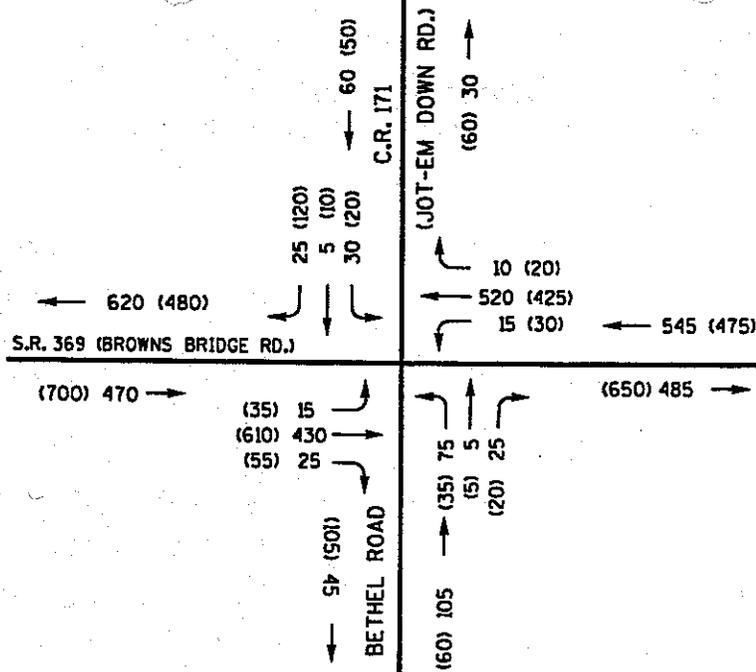
Attachments:

Traffic Flow Diagrams - 1999/2003 Peak Hour Traffic, Typical Section Diagrams, Concept Drawing

ESTIMATED COST

PROJECT NAME: BROWNS BRIDGE ROAD (S.R.369) AT JOT'EM DOWN ROAD

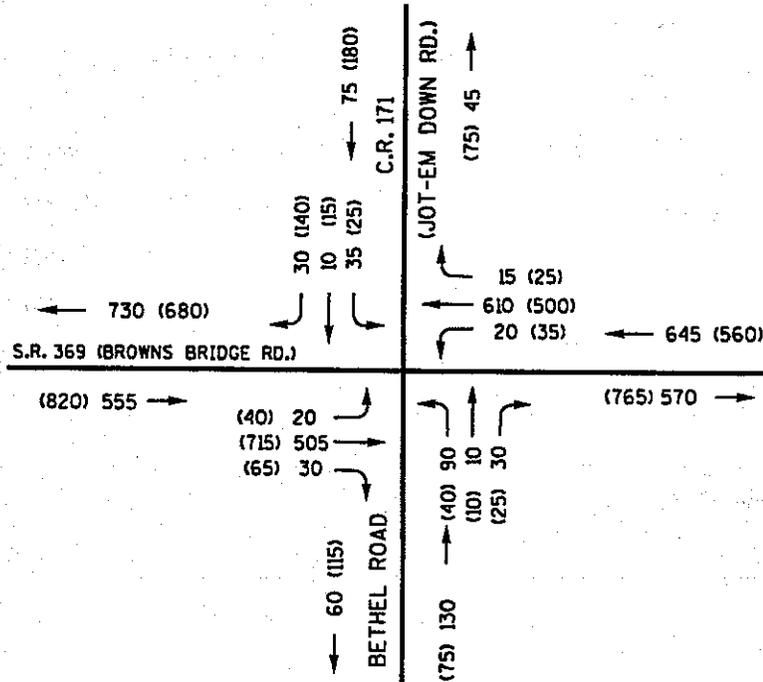
ITEM NO.	DESCRIPTION	UNIT	QUANTITY	UNIT PRICE	AMOUNT	TOTALS
1	Right-Of-Way				\$690,000.00	
2	Utilities				LGPA	
	Total Non-Construction					\$690,000.00
1	Mobilization	LS	LS	\$10,000.00	\$10,000.00	
2	Traffic Control	LS	LS	\$50,000.00	\$50,000.00	
3	Sediment and Erosion Control	LS	LS	\$20,000.00	\$20,000.00	
4	Clearing	LS	LS	\$50,000.00	\$50,000.00	
5	Graded Agg. Base 10"	TN	952	\$15.00	\$14,280.00	
6	Asphalt Concrete 12.5 mm SMA 165# /S.Y. (1 1/2")	TN	604	\$50.00	\$30,200.00	
7	Asphalt Concrete 19 mm Superpave 220# /s.y. (2")	TN	806	\$50.00	\$40,300.00	
8	Asphalt Concrete 25 mm Superpave 440# /s.y. (4")	TN	1,612	\$50.00	\$80,600.00	
9	Bitum Tack Coat	GL	2,000	\$1.50	\$3,000.00	
10	Signalization	LS	LS	\$60,000.00	\$60,000.00	
11	Signs & Pvm't Markings	LS	LS	\$4,500.00	\$3,500.00	
12	Grassing	AC	6.0	\$1,500.00	\$9,000.00	
13	Remove Existing Pavement	SY	5,940	\$3.00	\$17,820.00	
	Total Construction					\$388,700.00
	Engineering & Construction (10%)					\$38,870.00
	Inflation (5% for 2 years)					\$38,870.00
	TOTAL PROJECT COST					\$1,156,440.00



LEGEND

000 AM PEAK HOUR
 (000) PM PEAK HOUR

EXISTING 1999 PEAK HOUR TRAFFIC



LEGEND

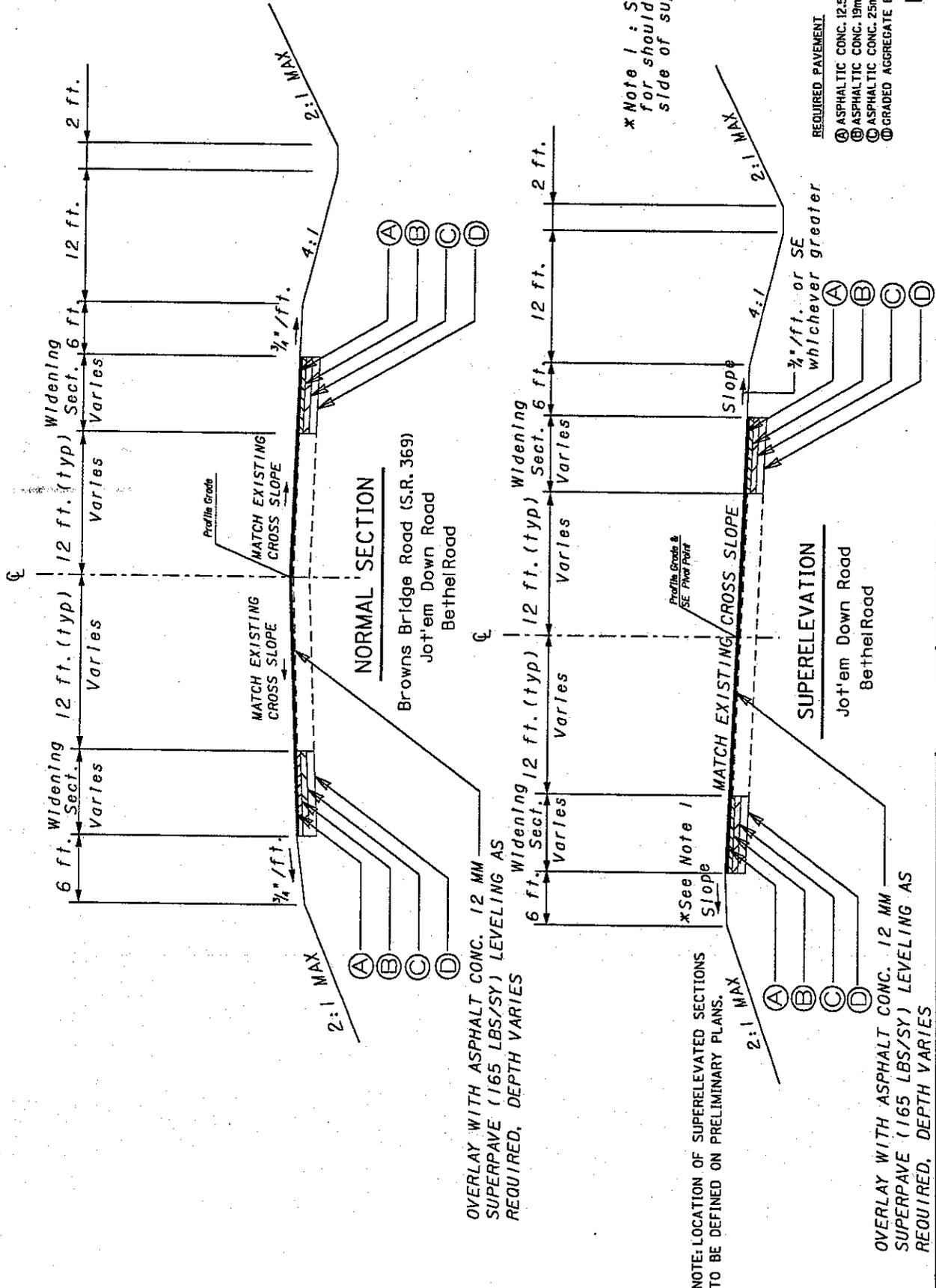
000 AM PEAK HOUR
 (000) PM PEAK HOUR

FUTURE 2003 PEAK HOUR TRAFFIC

MA Moreland Altobelli
 Associates, Inc.



TRAFFIC FLOW DIAGRAMS
 S.R. 369 (BROWNS BRIDGE RD.) @
 C.R. 171 (JOT-EM DOWN RD./BETHEL RD.)
 STP-0000-00 (811) P.I. 0000811
 FORSYTH COUNTY



OVERLAY WITH ASPHALT CONC. 12 MM SUPERPAVE (165 LBS/SY) LEVELING AS REQUIRED, DEPTH VARIES

NOTE: LOCATION OF SUPERELEVATED SECTIONS TO BE DEFINED ON PRELIMINARY PLANS.

OVERLAY WITH ASPHALT CONC. 12 MM SUPERPAVE (165 LBS/SY) LEVELING AS REQUIRED, DEPTH VARIES

* Note 1 : See GA STD 9028C for shoulder slope on high side of superelevation.

REQUIRED PAVEMENT

- Ⓐ ASPHALTIC CONC. 12.5mm SUPERPAVE (165 lbs/sy)
- Ⓑ ASPHALTIC CONC. 19mm SUPERPAVE (220 lbs/sy)
- Ⓒ ASPHALTIC CONC. 25mm SUPERPAVE (440 lbs/sy)
- Ⓓ GRADED AGGREGATE BASE (10')

NOT TO SCALE

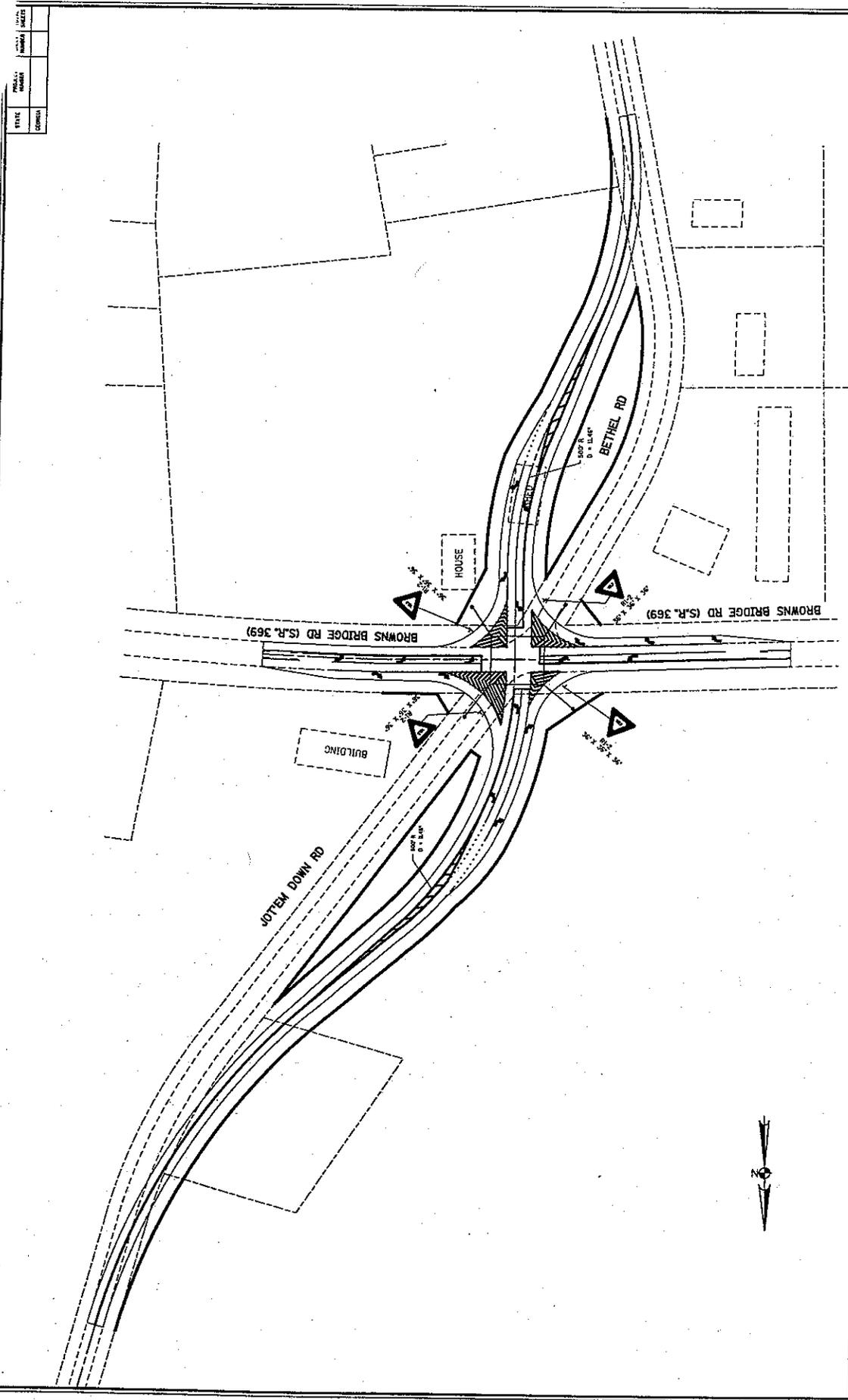


NORTH

TYPICAL SECTIONS

FORSYTH COUNTY INTERSECTION IMPROVEMENTS
 INTERSECTION 2A - STP-0000-00 (811) P.I. 0000811

MA Moreland Altabelli Associates, Inc.



STATE	COUNTY	PROJECT NUMBER	SHEET NUMBER	TOTAL SHEETS

MA Maryland A. Mahalli 222 Beaver Run Road, Suite 410 Norfolk, Georgia 30101 (404) 447-1111		DATE: _____ TIME: _____ DRAWN BY: _____ CHECKED BY: _____ APPROVED BY: _____	FORSYTH COUNTY, GEORGIA FORTSYTH COUNTY INTERSECTION CONCEPTS STP-0000-00010 P.I. 0000811 BROWNS BRIDGE RD (S.R. 369) AT JOYTEM DOWN RD/BETHEL RD	SHEET NUMBER 2A
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NO.	DATE	DESCRIPTION	BY

PROJECT CONCEPT REPORT

P.I. No.: 0000811

Project No.: STP-0000-00 (811) Forsyth County

Route No.: S.R. 369 (Browns Bridge Road)
C.R. 457 (Waldrip Road)
C.R. 206 (Waldrip Circle)

Location: The intersection of S.R. 369 @ C.R. 457/206 is located approximately 6.65 miles east along Browns Bridge Road from its intersection with GA 400, northeast of the city of Cumming in Forsyth County.

Description: The existing acute angled four-way intersection would be reconfigured into a staggered intersection. S.R. 369 would be widened and resurfaced with a center two-way left-turn lane and a right-turn lane onto Waldrip Road. The existing southbound approach on Waldrip Road would be relocated approximately 500 feet west onto new location to intersect S.R. 369 at 90 degrees, and would include a channelized right-turn lane with a striped turning island. The remaining segment would be made into a cul-de-sac, tying into Waldrip Road north of S.R. 369. The existing northbound approach on Waldrip Circle would be relocated approximately 300 feet east of its existing location to intersect S.R. 369 at 90 degrees. Additional right-of-way is required both north and south of the intersection for the new location and realignment of the roadways. The existing passing lane on S.R. 369 in this area would be restriped as a center two-way left turn lane.

<u>Traffic Volumes</u>	<u>1999 ADT</u>	<u>2003 ADT</u>
S.R. 369	12,500	14,600
C.R. 457	1,450	1,700
C.R. 206	550	750

<u>Existing Posted Speed Limit:</u>	S.R. 369	55 mph.
	C.R. 457	45 mph.
	C.R. 206	30 mph.

Existing Typical Section:

S.R. 369: Three 12-foot rural travel lanes - one westbound, two eastbound; 2-foot paved shoulder.

C.R. 457: One 12-foot rural travel lane in each direction; grass shoulder.

C.R. 206: One 12-foot rural travel lane in each direction; grass shoulder.

<u>Existing Right-of-way:</u>	S.R. 369	Varying; 75 - 105 ft.
	C.R. 457 / 206	60 - 70 ft.

Existing Traffic Control: Stop-sign control for Waldrip Road and Waldrip Circle.

Existing Major Structures: None

Need and Purpose: Waldrip Road and Waldrip Circle intersect S.R. 369 at substandard acute angles, creating a skewed intersection with poor stopping site distance and increased exposure time for vehicles crossing or entering from the side street. An accident history at this intersection from 1995 to 1997 indicate that the majority of accidents are "angle" type collisions between vehicles on the mainline conflicting oncoming turning vehicles making left turns, and with vehicles from the side street entering or crossing the mainline.

Due to the access continuity of the existing alignment of Waldrip Road and Waldrip Circle with respect to S.R. 369, re-configuration of the intersection into a staggered intersection would be beneficial because a crossing vehicle would first turn left onto the highway then turn right to reenter the minor road with minor interference to through traffic. These and the other improvements contained in this concept will improved the safety of the intersection by providing adequate storage capacity and turning radii for all vehicle types and conditions.

Intersection State Route Mile post: S.R. 369 MP 16.63

PDP Class: Minor Existing

Functional Class: S.R. 369 Rural Minor Arterial
C.R. 457 / 206. Rural Local Road

<u>Max. Degree of Curve:</u>	<u>Existing Max Grade:</u>	<u>Angle of Intersection:</u>
S.R. 369: 3.0°	+/- 2 %	90°
C.R. 457: 7.0°	+/- 2 %	90°
C.R. 206: 19.1°	+/- 2 %	90°

Design Speed: S.R. 369 55 mph
C.R. 457 45 mph
C.R. 206 30 mph

Proposed Typical Section:

S.R. 369: One 12-foot rural travel lane in each direction with a two-way center turn lane and right turn lanes at its intersection with C.R. 457 and C.R. 206; grass 6' shoulders with open ditch section.

C.R. 457: One 12-foot rural travel lane in each direction with left turn and right turn lanes at its intersection with S.R. 369 and 6' grass shoulders with open ditch section.

C.R. 206: One 12-foot rural travel lane in each direction and 6' grass shoulders with open ditch section.

Proposed Right-of-way Width: S.R. 369 100 ft.
 C.R. 457/ C.R. 206 80 ft.

Number of Parcels Impacted: 8 **Displacements:** RES: 0 BUS: 0 MH: 0

Proposed Major Structures: None.

Type Access: State and County Permit.

Traffic Control During Construction: Traffic will be maintained during construction.

Alternatives Considered:

1. No Build Condition
2. Proposed Project

Design Exceptions Required:

CONTROLLING CRITERIA	UNDETERMINED	YES	NO
HORIZONTAL ALIGNMENT	()	()	(X)
ROADWAY WIDTH	()	()	(X)
SHOULDER WIDTH	()	()	(X)
VERTICAL GRADES	()	()	(X)
CROSS SLOPES	()	()	(X)
STOPPING SIGHT DISTANCE	()	()	(X)
SUPERELEVATION RATES	()	()	(X)
HORIZONTAL CLEARANCE	()	()	(X)
SPEED DESIGN	()	()	(X)
VERTICAL CLEARANCE	()	()	(X)

Utilities: Forsyth County shall be responsible for all utility adjustments. Potential utilities impacted are listed below:

- Telephone - BellSouth
- Power - GA Power or Sawnee EMC
- Water - Forsyth County
- Cable - Prestige Cable

Permits Required: None required.

Level of Environmental Analysis: Categorical Exclusion.

Level of Public Involvement: None required.

Time Saving Procedures Appropriate: Yes (X) No ()

Total Estimated Project Cost: \$1,199,535.80 Detailed cost estimate is shown on the next page.

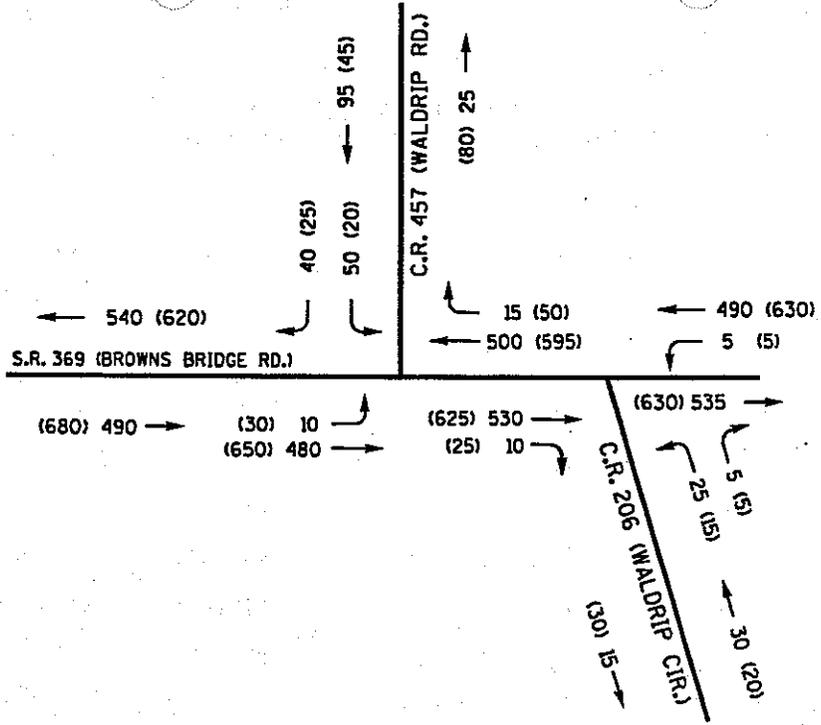
Attachments:

Traffic Flow Diagrams - 1999/2003 Peak Hour Traffic, Typical Section Diagrams, Concept Drawing

ESTIMATED COST

PROJECT NAME: BROWNS BRIDGE ROAD (S.R.369) AT WALDRIP ROAD

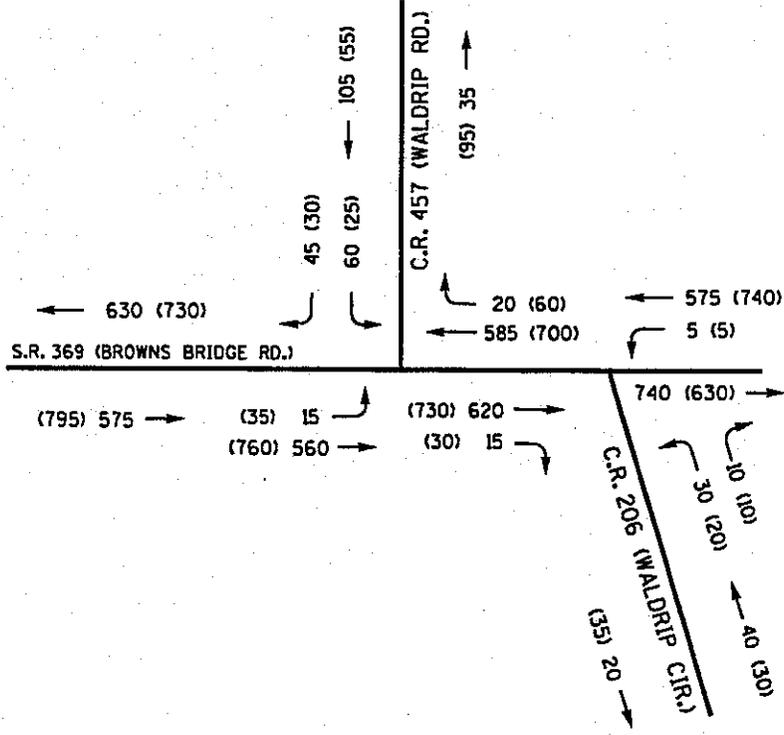
ITEM NO.	DESCRIPTION	UNIT	QUANTITY	UNIT PRICE	AMOUNT	TOTALS
1	Right-Of-Way				\$530,000.00	
2	Utilities				LGPA	
	Total Non-Construction					\$530,000.00
1	Mobilization	LS	LS	\$10,000.00	\$10,000.00	
2	Traffic Control	LS	LS	\$50,000.00	\$50,000.00	
3	Sediment and Erosion Control	LS	LS	\$20,000.00	\$20,000.00	
4	Clearing	LS	LS	\$75,000.00	\$75,000.00	
5	Graded Agg. Base 10"	TN	4,842	\$15.00	\$72,630.00	
6	Asphalt Concrete 12.5 mm SMA 165# /S.Y. (1 1/2")	TN	2,054	\$50.00	\$102,700.00	
7	Asphalt Concrete 19 mm Superpave 220# /s.y. (2")	TN	902	\$50.00	\$45,100.00	
8	Asphalt Concrete 25 mm Superpave 440# /s.y. (4")	TN	1,804	\$50.00	\$90,200.00	
9	Bitum Tack Coat	GL	2,025	\$1.50	\$3,037.50	
10	Retaining Wall - Class A Concrete	CY	110	\$500.00	\$55,000.00	
11	Signs & Pvm't Markings	LS	LS	\$10,000.00	\$10,000.00	
12	Grassing	AC	6.0	\$1,500.00	\$9,000.00	
13	Remove Existing Pavement	SY	5,093	\$3.00	\$15,279.00	
	Total Construction					\$557,946.50
	Engineering & Construction (10%)					\$55,794.65
	Inflation (5% for 2 years)					\$55,794.65
	TOTAL PROJECT COST					\$1,199,535.80



LEGEND

000 AM PEAK HOUR
 (000) PM PEAK HOUR

EXISTING 1999 PEAK HOUR TRAFFIC



LEGEND

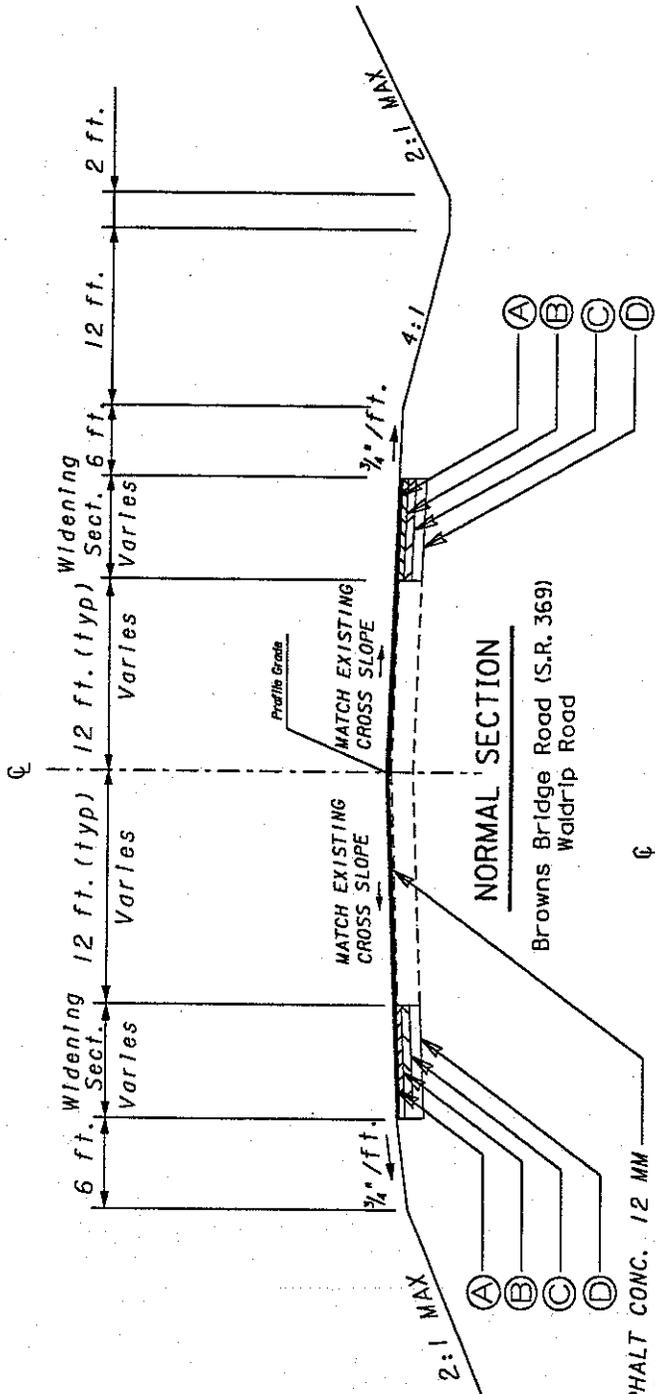
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 (000) PM PEAK HOUR

FUTURE 2003 PEAK HOUR TRAFFIC

MA Moreland Altobelli
 Associates, Inc.



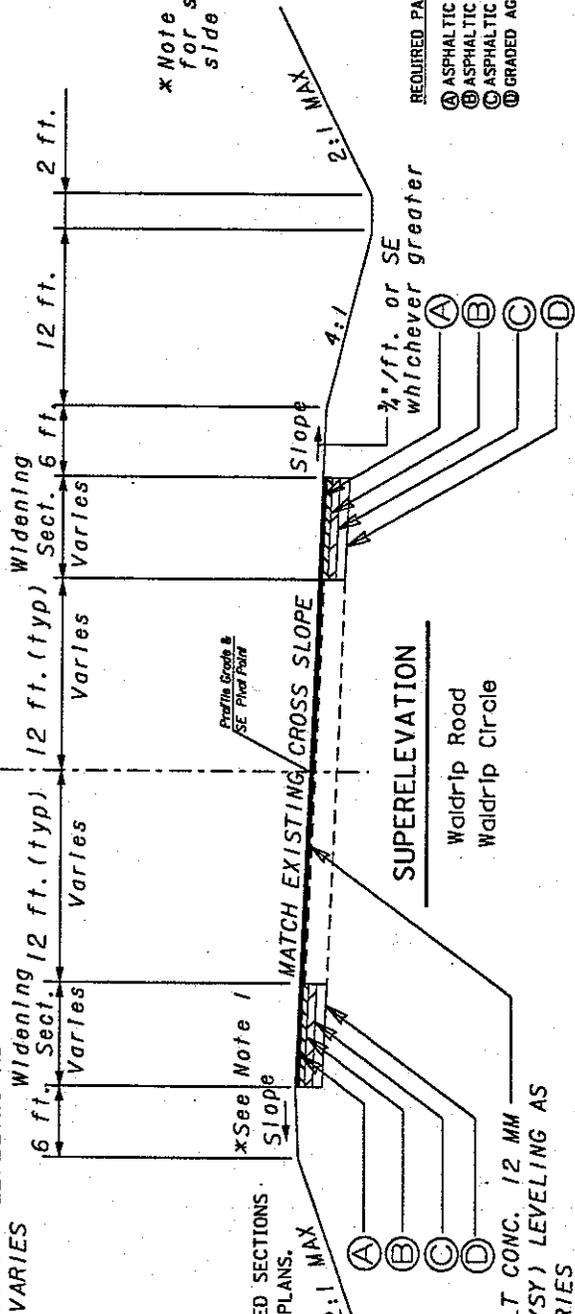
TRAFFIC FLOW DIAGRAMS
 S.R. 369 (BROWNS BRIDGE RD.) @
 C.R. 457 (WALDRIP ROAD)
 STP-0000-00 (811) P.I. 0000811
 FORSYTH COUNTY



OVERLAY WITH ASPHALT CONC. 12 MM SUPERPAVE (165 LBS/SY) LEVELING AS REQUIRED, DEPTH VARIES

NOTE: LOCATION OF SUPERELEVATED SECTIONS TO BE DEFINED ON PRELIMINARY PLANS.

OVERLAY WITH ASPHALT CONC. 12 MM SUPERPAVE (165 LBS/SY) LEVELING AS REQUIRED, DEPTH VARIES



* Note 1 : See GA STD 9028C for shoulder slope on high side of super-elevation.

REQUIRED PAVEMENT

- Ⓐ ASPHALTIC CONC. 12.5mm SUPERPAVE (165 lbs/sy)
- Ⓑ ASPHALTIC CONC. 19mm SUPERPAVE (220 lbs/sy)
- Ⓒ ASPHALTIC CONC. 25mm SUPERPAVE (140 lbs/sy)
- Ⓓ GRADED AGGREGATE BASE (10')

NOT TO SCALE



NORTH

TYPICAL SECTIONS

FORSYTH COUNTY INTERSECTION IMPROVEMENTS
INTERSECTION 2B - STP-0000-00 (811) P.I. 0000811

MA Moreland Altabelli Associates, Inc.

PROJECT CONCEPT REPORT

P. I. No.: 0000811

Project No.: STP-0000-00 (811) Forsyth County

Route No.: S.R. 369 (Matt Highway)
C.R. 452 (Doc Bramblett Road)

Location: The intersection of S.R. 369 @ Doc Bramblett Road is located approximately 5.5 miles north of the city limits of Cumming and 2.5 miles directly south of the Dawson county line in Forsyth county.

Description: S.R. 369 will be widened to provide a westbound left-turn lane and eastbound right turn lane. The Doc Bramblett Road approach will be relocated to intersect S.R. 369 157 feet east of its existing location, and modified to provide a channelized right-turn lane.

<u>Traffic Volumes</u>	<u>1999 ADT</u>	<u>2003 ADT</u>
S.R. 369	10,100	11,800
Doc Bramblett Road (C.R. 452)	2,900	3,500

Existing Posted Speed Limit: S.R. 369 45 mph.
C.R. 452 45 mph.

Existing Typical Section:

S.R. 369: One 12 ft. rural travel lane in each direction; variable grass shoulder.

Doc Bramblett Road (C.R. 452): One 12 ft. rural travel lane in each direction; variable grass shoulder.

Existing Right-of-way: S.R. 369----- 55 - 70 ft.
Doc Bramblett Road----- 60 ft.

Existing Traffic Control: S.R. 369 is a continuous through movement with curve warning signs along both approaches. Doc Bramblett Road is stop-sign controlled.

Existing Major Structures: None

Statement of Need and Purpose: Accident history at this intersection shows that of the 11 accidents reported between 1995 and 1997, the majority were of the "rear end" and "angle intersecting" type, with a slight increase occurring in 1997. There is also a relatively high percentage of heavy trucks that operate through the narrow intersection. Analysis of existing traffic volumes at the intersection shows a large percentage of right-turn movements from Doc Bramblett Road onto S.R. 369, as well as left-turn movements from S.R. 369 onto Doc Bramblett Road.

The recommended improvements shown in the concept plan will provide adequate storage and turning radii for all vehicle types and conditions, as well as increase the operational safety and efficiency of the intersection.

State Route Mile post: S.R. 369 MP 5.84

PDP Class: Minor Existing

Functional Class: S.R. 369 ----- Rural Major Collector
C.R. 452 ----- Rural Major Collector

<u>Max. Degree of Curve:</u>	<u>Existing Maximum Grade:</u>	<u>Angle of Intersection:</u>
S.R. 369: 6°	+/- 3%	85°
C.R. 452: 11.5°		

Design Speed: S.R. 369 ----- 45 mph
C.R. 452 ----- 45 mph

Proposed Typical Section:

S.R. 369: One 12-foot rural travel lane in each direction with a left turn lane and right turn lane at its intersection with C.R. 452; grass 6' shoulders with open ditch section.

C.R. 452: One 12-foot rural travel lane in each direction with left turn and right turn lanes at its intersection with S.R. 369 and 6' grass shoulders with open ditch section.

Proposed Right-of-way Width: S.R. 369 80 ft.
C.R. 452 80 ft.

Number of Parcels Impacted: 6 **Displacements:** RES: 0 BUS: 0 MH: 0

Proposed Major Structures: None.

Type Access: State and County Permit.

Traffic Control During Construction: Traffic will be maintained during construction.

Design Exceptions Required:

CONTROLLING CRITERIA	UNDETERMINED	YES	NO
HORIZONTAL ALIGNMENT	()	()	(X)
ROADWAY WIDTH	()	()	(X)
SHOULDER WIDTH	()	()	(X)
VERTICAL GRADES	()	()	(X)
CROSS SLOPES	()	()	(X)
STOPPING SIGHT DISTANCE	()	()	(X)
SUPERELEVATION RATES	()	()	(X)
HORIZONTAL CLEARANCE	()	()	(X)
SPEED DESIGN	()	()	(X)
VERTICAL CLEARANCE	()	()	(X)

Alternatives Considered:

1. No Build.
2. Proposed Project

Utilities: Forsyth County shall be responsible for all utility adjustments. Potential utilities impacted are listed below:

Telephone - BellSouth
Power - GA Power or Sawnee EMC
Water - Forsyth County
Cable - Prestige Cable

Permits Required: None required.

Level of Environmental Analysis: Categorical Exclusion.

Level of Public Involvement: None required.

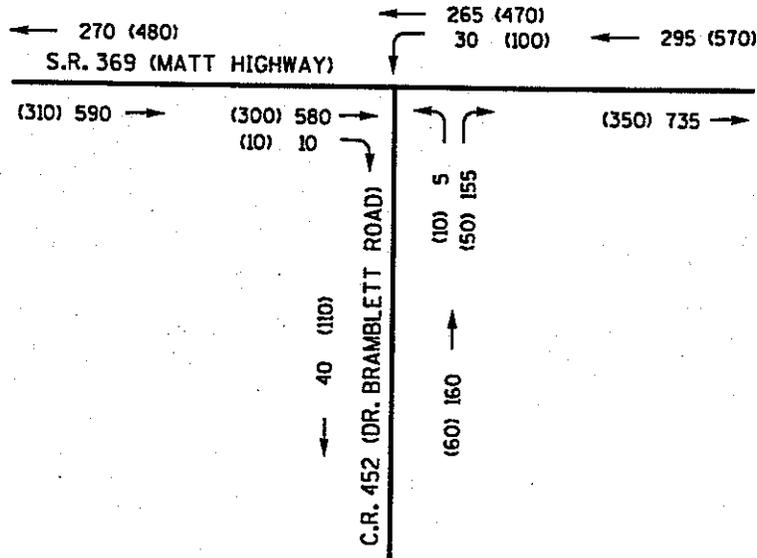
Time Saving Procedures Appropriate: Yes (X) No ()

Total Estimated Project Cost: \$419,764.00 Detailed cost estimate is shown on the next page.

Attachments: Traffic Flow Diagrams - 1999/2003 Peak Hour Traffic, Typical Section Diagrams, Concept Drawing

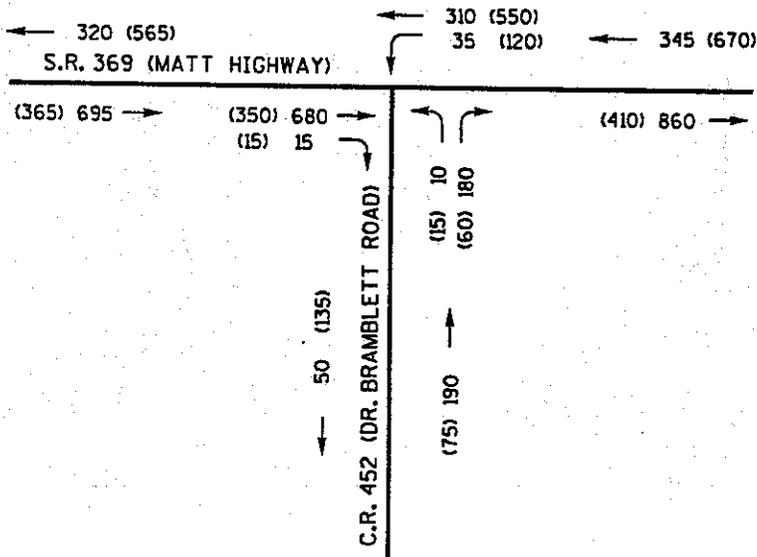
ESTIMATED COST
PROJECT NAME: MATT HIGHWAY (S.R.369) AT DOC BRAMBLETT ROAD

ITEM NO.	DESCRIPTION	UNIT	QUANTITY	UNIT PRICE	AMOUNT	TOTALS
1	Right-Of-Way				\$131,500.00	
2	Utilities				LGPA	
	Total Non-Construction					\$131,500.00
1	Mobilization	LS	LS	\$10,000.00	\$10,000.00	
2	Traffic Control	LS	LS	\$15,000.00	\$15,000.00	
3	Sediment and Erosion Control	LS	LS	\$20,000.00	\$20,000.00	
4	Clearing	LS	LS	\$15,000.00	\$15,000.00	
5	Graded Agg. Base 10"	TN	2,704	\$15.00	\$40,560.00	
6	Asphalt Concrete 12.5 mm SMA 165# /S.Y. (1 1/2")	TN	772	\$50.00	\$38,600.00	
7	Asphalt Concrete 19 mm Superpave 220# /s.y. (2")	TN	553	\$50.00	\$27,650.00	
8	Asphalt Concrete 25 mm Superpave 440# /s.y. (4")	TN	1,107	\$50.00	\$55,350.00	
9	Bitum Tack Coat	GL	918	\$1.50	\$1,377.00	
10	Signs & Pvm't Markings	LS	LS	\$4,500.00	\$4,500.00	
11	Grassing	AC	4.6	\$1,500.00	\$6,900.00	
12	Remove Existing Pavement	SY	1,761	\$3.00	\$5,283.00	
	Total Construction					\$240,220.00
	Engineering & Construction (10%)					\$24,022.00
	Inflation (5% for 2 years)					\$24,022.00
	TOTAL PROJECT COST					\$419,764.00



LEGEND
 () AM PEAK HOUR
 () PM PEAK HOUR

EXISTING 1999 PEAK HOUR TRAFFIC



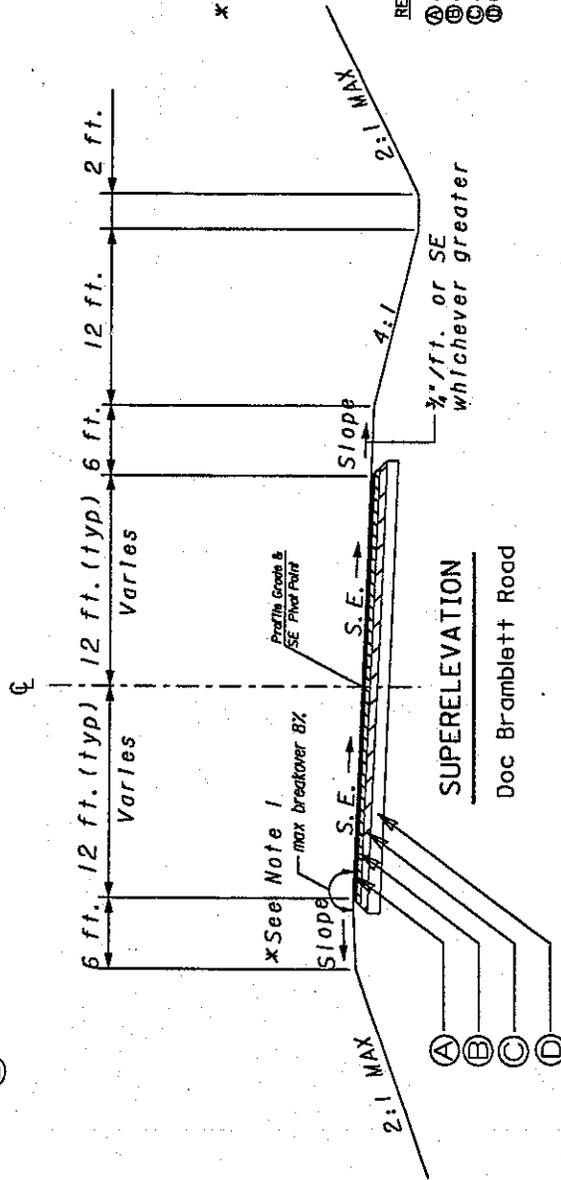
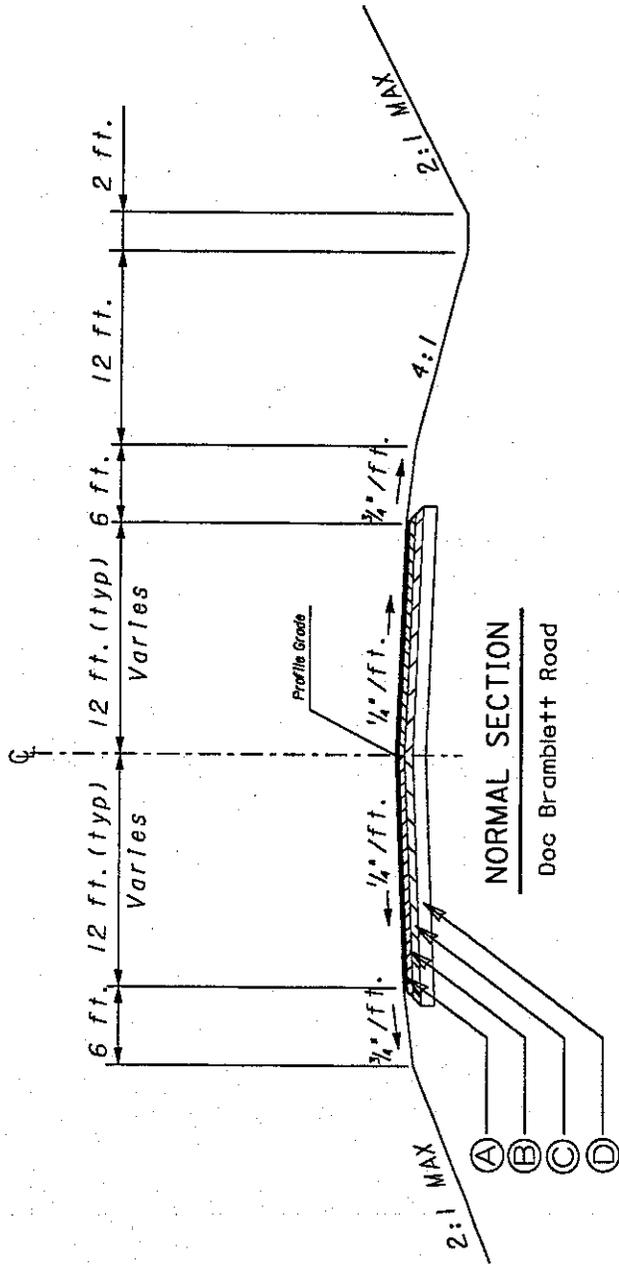
LEGEND
 () AM PEAK HOUR
 () PM PEAK HOUR

FUTURE 2003 PEAK HOUR TRAFFIC

MA Moreland Altobelli
 Associates, Inc.



TRAFFIC FLOW DIAGRAMS
 S.R. 369 (MATT HWY.) @ C.R. 452 (DR. BRAMBLETT ROAD)
 STP-0000-00 (811) P.I. 0000811
 FORSYTH COUNTY



* Note 1 : See GA STD 9028C for shoulder slope on high side of superelevation.

REQUIRED PAVEMENT

- Ⓐ ASPHALTIC CONC. 12.5mm SUPERPAVE (165 lbs/SY)
- Ⓑ ASPHALTIC CONC. 19mm SUPERPAVE (220 lbs/SY)
- Ⓒ ASPHALTIC CONC. 25mm SUPERPAVE (440 lbs/SY)
- Ⓓ GRADED AGGREGATE BASE (10')

NOTE: LOCATION OF SUPERELEVATED SECTIONS TO BE DEFINED ON PRELIMINARY PLANS.

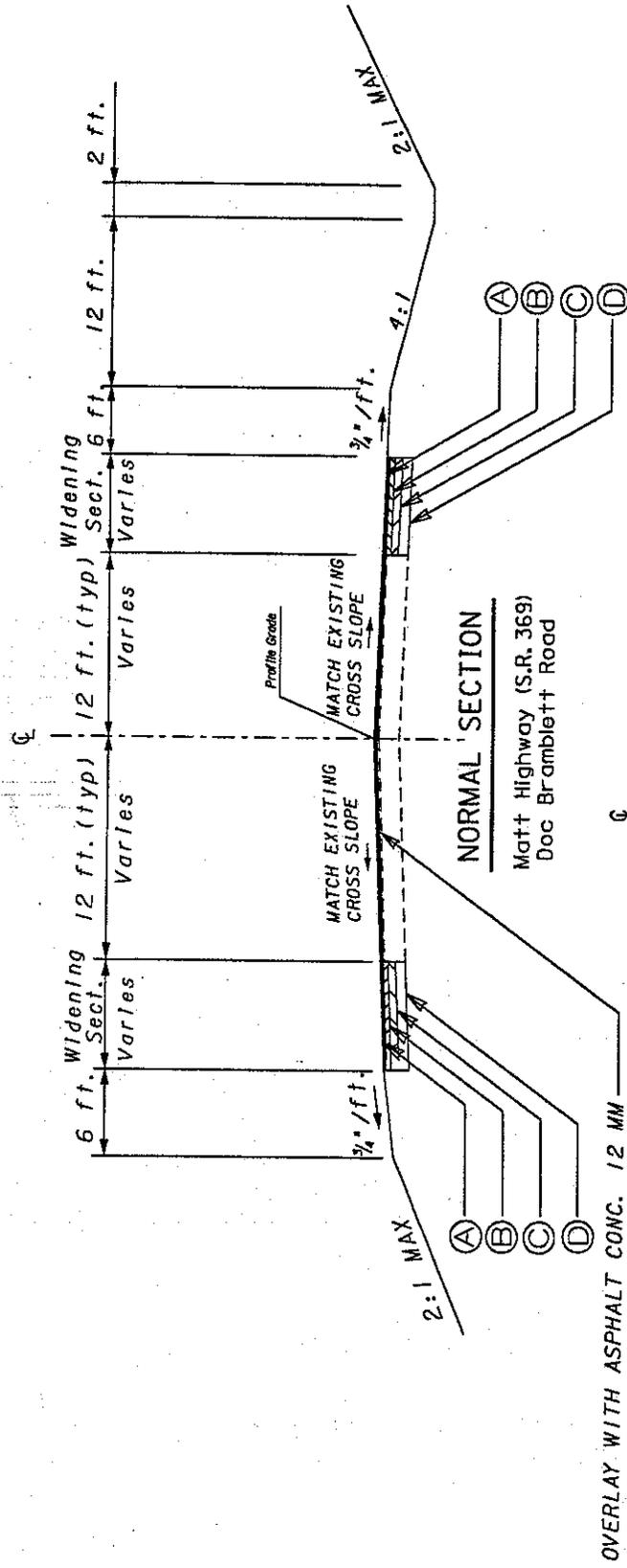
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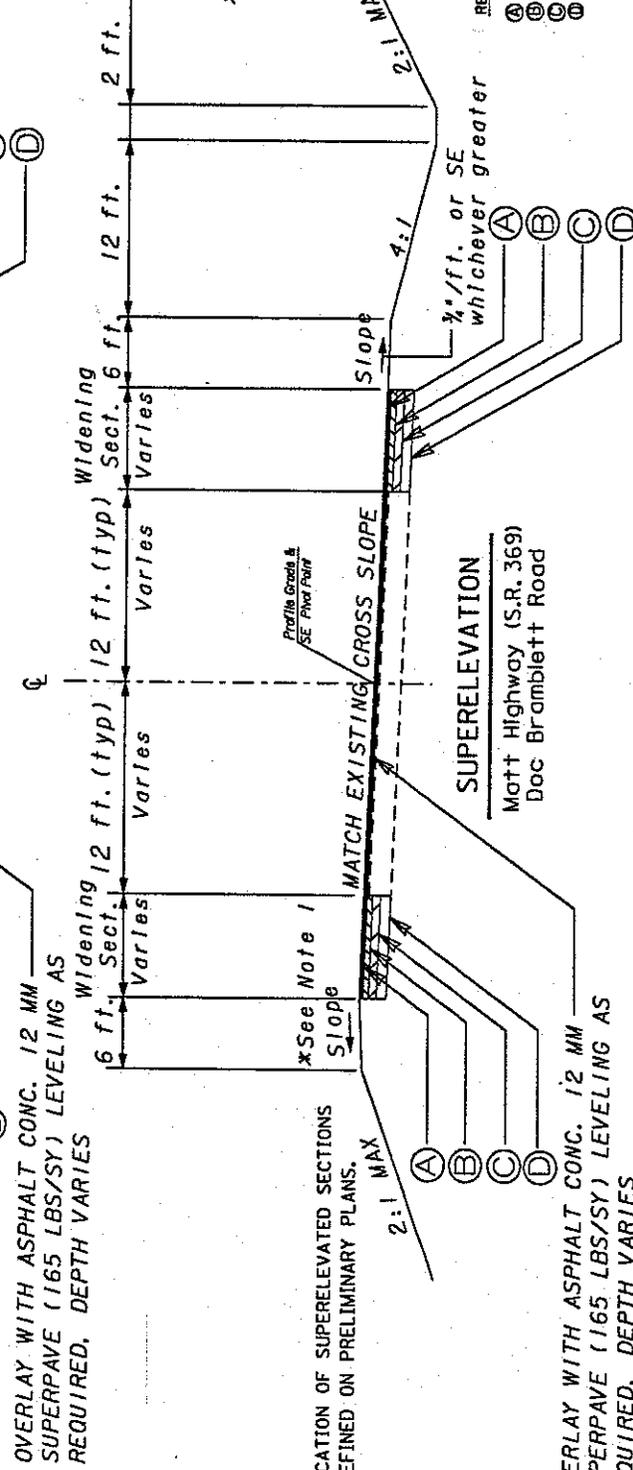
TYPICAL SECTIONS

FORSYTH COUNTY INTERSECTION IMPROVEMENTS
INTERSECTION 2C - STP-0000-00 (811) P.I. 0000811

MA Moreland Altabelli Associates, Inc.



OVERLAY WITH ASPHALT CONC. 12 MM SUPERPAVE (165 LBS/SY) LEVELING AS REQUIRED, DEPTH VARIES



NOTE: LOCATION OF SUPERELEVATED SECTIONS TO BE DEFINED ON PRELIMINARY PLANS.

OVERLAY WITH ASPHALT CONC. 12 MM SUPERPAVE (165 LBS/SY) LEVELING AS REQUIRED, DEPTH VARIES

* Note 1 : See GA STD 9028C for shoulder slope on high side of superelevation.

- REQUIRED PAVEMENT
- (A) ASPHALTIC CONC. 12.5mm SUPERPAVE (165 lbs/SY)
 - (B) ASPHALTIC CONC. 19mm SUPERPAVE (220 lbs/SY)
 - (C) ASPHALTIC CONC. 25mm SUPERPAVE (140 lbs/SY)
 - (D) GRADED AGGREGATE BASE (10')

NOT TO SCALE



TYPICAL SECTIONS
FORSYTH COUNTY INTERSECTION IMPROVEMENTS
 INTERSECTION 2C - STP-0000-00 (811) P.I. 0000811

MA Moreland Altabelli Associates, Inc.

PROJECT CONCEPT REPORT

P.I. No.: 0000811

Project No.: STP-0000-00 (811) Forsyth County

Route No.: S.R. 369 (Matt Highway)
C.R. 234 (Hendrix Road)

Location: The intersection of S.R. 369 @ Hendrix Road is located in Forsyth County approximately 2.6 miles west of Georgia 400 along S.R. 369, 3.5 miles directly south of the Dawson county line.

Description: S.R. 369 will be widened to provide a separate eastbound right-turn lane and westbound left-turn lane. Modification of the Hendrix Road approach to provide a channeled right-turn lane.

<u>Traffic Volumes</u>	<u>1999 ADT</u>	<u>2003 ADT</u>
S.R. 369	10,000	11,700
Hendrix Road (C.R. 234)	1,900	2,250

Existing Posted Speed Limit: S.R. 369 55 mph.
C.R. 234 40 mph.

Existing Typical Section:

S.R. 369: One 12 ft. rural travel lane in each direction; variable grass shoulder.

C.R. 234: One 12 ft. rural travel lane in each direction; variable grass shoulder.

Existing Right-of-way: S.R. 369 ----- 55 - 60 ft.
C.R. 234 ----- 60 ft.

Existing Traffic Control: S.R. 369 has a continuous through movement. Hendrix Road is stop-sign controlled.

Existing Major Structures: None

Statement of Need and Purpose: A total of four collision accidents were reported between 1995 and 1997. The intersection is located at a crest along S.R. 369, providing less than desirable sight distance from each approach.

The recommended improvements shown in the concept plan will provide adequate storage and turning radii for all vehicle types and conditions, and will increase the operational safety and efficiency of the intersection.

State Route Mile post: S.R. 369 MP 8.41

PDP Class: Minor Existing

Functional Class: S.R. 369 ----- Rural Major Collector
C.R. 234 ---- ----- Rural Local Road

<u>Max. Degree of Curve:</u>	<u>Existing Maximum Grade:</u>	<u>Angle of Intersection:</u>
S.R. 369: 3°	+/- 4%	85°
Hendrix Rd (C.R. 234): 11.5°	+/- 8%	

Design Speed: S.R. 369 ----- 55 mph
C.R. 234 ----- 40 mph

Proposed Typical Section:

S.R. 369: One 12-foot rural travel lane in each direction with left turn lane and right turn lanes at its intersection with C.R. 234; grass 6' shoulders with open ditch section.

C.R. 234: One 12-foot rural travel lane in each direction with left turn and right turn lanes at its intersection with S.R. 369 and 6' grass shoulders with open ditch section.

Proposed Right-of-way Width: S.R. 369 80 ft.
C.R. 234 80 ft.

Number of Parcels Impacted: 5 **Displacements:** RES: 0 BUS: 0 MH: 0

Proposed Major Structures: None.

Type Access: State and County Permit.

Traffic Control During Construction: Traffic will be maintained during construction.

Design Exceptions Required:

CONTROLLING CRITERIA	UNDETERMINED	YES	NO
HORIZONTAL ALIGNMENT	()	()	(X)
ROADWAY WIDTH	()	()	(X)
SHOULDER WIDTH	()	()	(X)
VERTICAL GRADES	()	()	(X)
CROSS SLOPES	()	()	(X)
STOPPING SIGHT DISTANCE	()	()	(X)
SUPERELEVATION RATES	()	()	(X)
HORIZONTAL CLEARANCE	()	()	(X)
SPEED DESIGN	()	()	(X)
VERTICAL CLEARANCE	()	()	(X)

Alternatives Considered:

1. No Build.
2. Proposed Project.

Utilities: Forsyth County shall be responsible for all utility adjustments. Potential utilities impacted are listed below:

Telephone - BellSouth
Power - GA Power or Sawnee EMC
Water - Forsyth County
Gas - Atlanta Gas Light
Cable - Prestige Cable

Permits Required: None required.

Level of Environmental Analysis: Categorical Exclusion.

Level of Public Involvement: None required.

Time Saving Procedures Appropriate: Yes (X) No ()

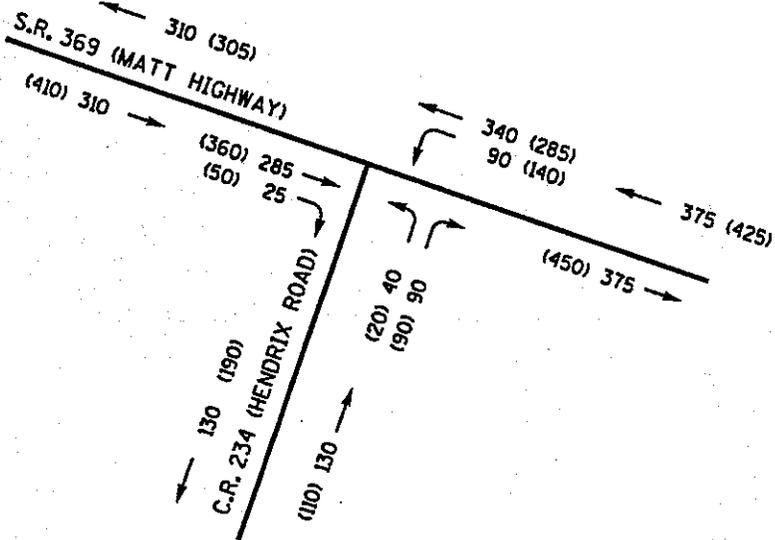
Total Estimated Project Cost: \$210,193.00 Detailed cost estimate is shown on the next page.

Attachments: Traffic Flow Diagrams - 1999/2003 Peak Hour Traffic, Typical Section Diagrams, Concept Drawing.

ESTIMATED COST

PROJECT NAME: MATT HIGHWAY (S.R.369) AT HENDRIX ROAD

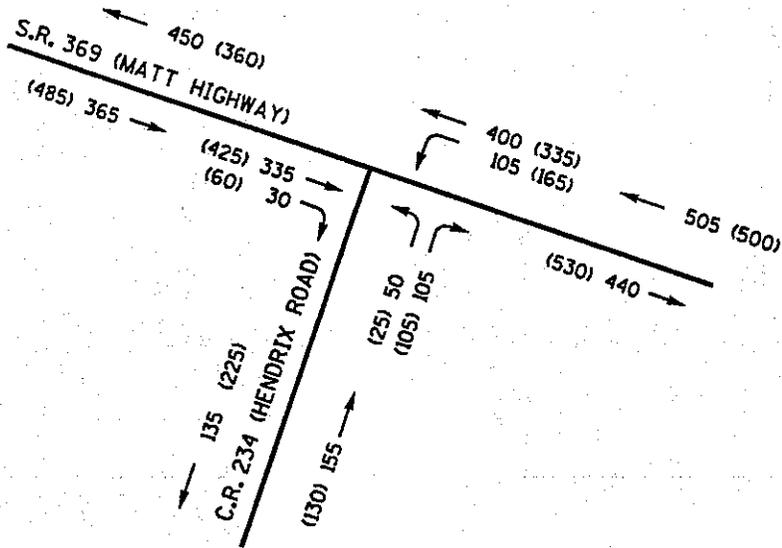
ITEM NO.	DESCRIPTION	UNIT	QUANTITY	UNIT PRICE	AMOUNT	TOTALS
1	Right-Of-Way				\$37,000.00	
2	Utilities				LGPA	
	Total Non-Construction					\$37,000.00
1	Mobilization	LS	LS	\$10,000.00	\$10,000.00	
2	Traffic Control	LS	LS	\$15,000.00	\$15,000.00	
3	Sediment and Erosion Control	LS	LS	\$20,000.00	\$20,000.00	
4	Clearing	LS	LS	\$5,000.00	\$5,000.00	
5	Graded Agg. Base 10"	TN	1,319	\$15.00	\$19,785.00	
6	Asphalt Concrete 12.5 mm SMA 165# /S.Y. (1 1/2")	TN	530	\$50.00	\$26,500.00	
7	Asphalt Concrete 19 mm Superpave 220# /s.y. (2")	TN	246	\$50.00	\$12,300.00	
8	Asphalt Concrete 25 mm Superpave 440# /s.y. (4")	TN	491	\$50.00	\$24,550.00	
9	Bitum Tack Coat	GL	545	\$1.50	\$817.50	
10	Concrete Driveway	SY	13	\$25.00	\$325.00	
11	Signs & Pvm't Markings	LS	LS	\$4,500.00	\$4,500.00	
12	Grassing	AC	3.7	\$1,500.00	\$5,550.00	
	Total Construction					\$144,327.50
	Engineering & Construction (10%)					\$14,432.75
	Inflation (5% for 2 years)					\$14,432.75
	TOTAL PROJECT COST					\$210,193.00



LEGEND

000 AM PEAK HOUR
 (000) PM PEAK HOUR

EXISTING 1999 PEAK HOUR TRAFFIC



LEGEND

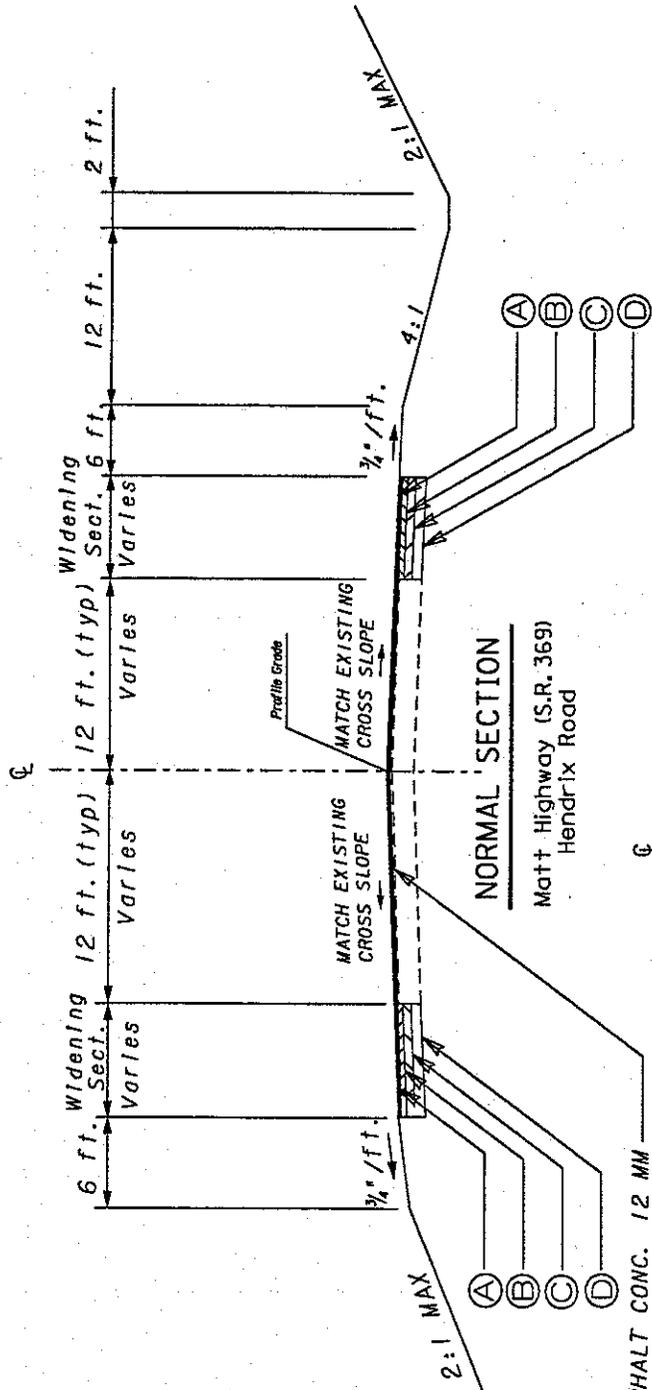
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FUTURE 2003 PEAK HOUR TRAFFIC

MA Moreland Altobelli
 Associates, Inc.

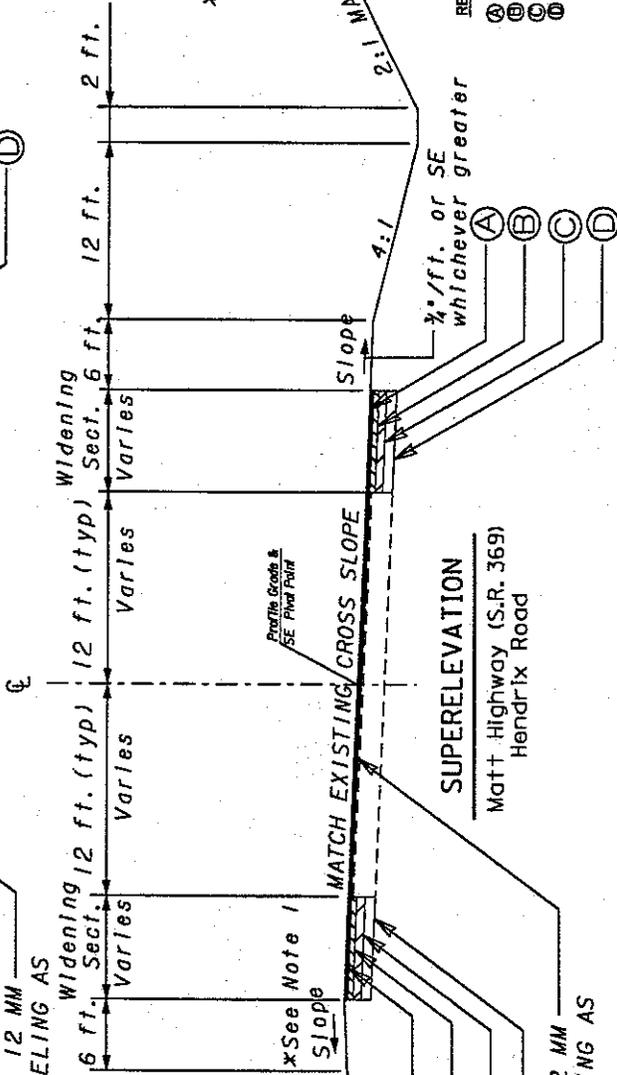


TRAFFIC FLOW DIAGRAMS
 S.R. 369 (MATT HWY.) @ C.R. 234 (HENDRIX ROAD)
 STP-0000-00 (811) P.I. 0000811
 FORSYTH COUNTY



NORMAL SECTION
Matt Highway (S.R. 369)
Hendrix Road

OVERLAY WITH ASPHALT CONC. 12 MM
SUPERPAVE (165 LBS/SY) LEVELING AS
REQUIRED, DEPTH VARIES



SUPERELEVATION
Matt Highway (S.R. 369)
Hendrix Road

OVERLAY WITH ASPHALT CONC. 12 MM
SUPERPAVE (165 LBS/SY) LEVELING AS
REQUIRED, DEPTH VARIES

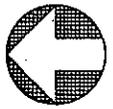
* Note 1 : See GA STD 902BC
for shoulder slope on high
side of superelevation.

NOTE: LOCATION OF SUPERELEVATED SECTIONS
TO BE DEFINED ON PRELIMINARY PLANS.

REQUIRED PAVEMENT

- Ⓐ ASPHALTIC CONC. 12.5mm SUPERPAVE (165 lbs/SY)
- Ⓑ ASPHALTIC CONC. 19mm SUPERPAVE (220 lbs/SY)
- Ⓒ ASPHALTIC CONC. 25mm SUPERPAVE (440 lbs/SY)
- Ⓓ GRADED AGGREGATE BASE (10')

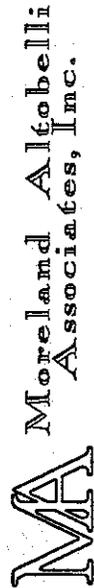
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NORTH

TYPICAL SECTIONS

FORSYTH COUNTY INTERSECTION IMPROVEMENTS
INTERSECTION 2D - STP-0000-00 (811) P.I. 0000811



PROJECT CONCEPT REPORT

P. I. No.: 0000811

Project No.: STP-0000-00 (811) Forsyth County

Route No.: S.R. 369 (Browns Bridge Road)
C.R. 104 (Holtzclaw Road)

Location: The intersection of S.R. 369 @ Holtzclaw Road is located in Forsyth County approximately 1.2 miles east of Georgia 400 along S.R. 369, 3.3 miles northeast of the city limits of Cumming.

Description: S.R. 369 will be widened to provide a right turn lane to Holtzclaw Road. The Holtzclaw Road approach to S.R. 369 will be widened to provide a separate left-turn lane and a right-turn lane.

<u>Traffic Volumes</u>	<u>1999 ADT</u>	<u>2003 ADT</u>
S.R. 369	20,200	23,600
Holtzclaw Road (C.R. 104)	2,000	2,300

Existing Posted Speed Limit: S.R. 369 55 mph.
C.R. 104 40 mph.

Existing Typical Section:

S.R. 369: One 12 ft. rural travel lanes in each direction with left turn lane; variable grass shoulder.
Holtzclaw Road (C.R. 104): One 12 ft. rural travel lanes in each direction; variable grass shoulder.

Existing Right-of-way: S.R. 369 ----- 100 ft.
C.R. 104 ----- 80 ft.

Existing Traffic Control: S.R. 369 has a continuous through movement; Holtzclaw Road is controlled by a stop-sign.

Existing Major Structures: None

Statement of Need and Purpose: A total of 13 accidents were reported between 1995 and 1997, with the majority being "rear end" and "angle intersecting" accidents. Analysis of existing traffic conditions at the intersection indicates a large through movement along S.R. 369, with a high percentage of right-turn movements from Holtzclaw Road onto eastbound S.R. 369.

The recommended improvements shown in the concept plan will provide adequate storage for left-turning vehicles without interrupting through traffic on S.R. 369. Additionally, increased turning radii and the addition of a separate left-turn and right-turn lane on the Holtzclaw Road approach will provide increased safety and operational efficiency to the intersection.

State Route Mile post: S.R. 369 MP 12.3

PDP Class: Minor Existing

Functional Class: S.R. 369 ----- Rural Minor Arterial
C.R. 104 ----- Rural Major Collector

Max. Degree of Curve: S.R. 369: Tangent Section
Holtzclaw Road (C.R. 104): 11.5°
Existing Maximum Grade: +/- 2%
Angle of Intersection: 85°

Design Speed: S.R. 369 ----- 55 mph
C.R. 104 ----- 40 mph

Proposed Typical Section:

S.R. 369: One 12-foot rural travel lane in each direction with left turn lane and right turn lanes at its intersection with C.R. 104; grass 6' shoulders with open ditch section.

C.R. 104: One 12-foot rural travel lane in each direction with left turn and right turn lanes at its intersection with S.R. 369 and 6' grass shoulders with open ditch section.

Proposed Right-of-way Width: S.R. 369 100 ft.
C.R. 104 80 ft.

Number of Parcels Impacted: 0 **Displacements:** RES: 0 BUS: 0 MH: 0

Proposed Major Structures: None.

Type Access: State and County Permit.

Traffic Control During Construction: Traffic will be maintained during construction.

Design Exceptions Required:

CONTROLLING CRITERIA	UNDETERMINED	YES	NO
HORIZONTAL ALIGNMENT	()	()	(X)
ROADWAY WIDTH	()	()	(X)
SHOULDER WIDTH	()	()	(X)
VERTICAL GRADES	()	()	(X)
CROSS SLOPES	()	()	(X)
STOPPING SIGHT DISTANCE	()	()	(X)
SUPERELEVATION RATES	()	()	(X)
HORIZONTAL CLEARANCE	()	()	(X)
SPEED DESIGN	()	()	(X)
VERTICAL CLEARANCE	()	()	(X)

Alternatives Considered:

1. No Build.
2. Proposed Project

Utilities: Forsyth County shall be responsible for all utility adjustments. Potential utilities impacted are listed below:

Telephone - BellSouth
Power - GA Power or Sawnee EMC
Water - Forsyth County
Cable - Prestige Cable

Permits Required: None required.

Level of Environmental Analysis: Categorical Exclusion.

Level of Public Involvement: None required.

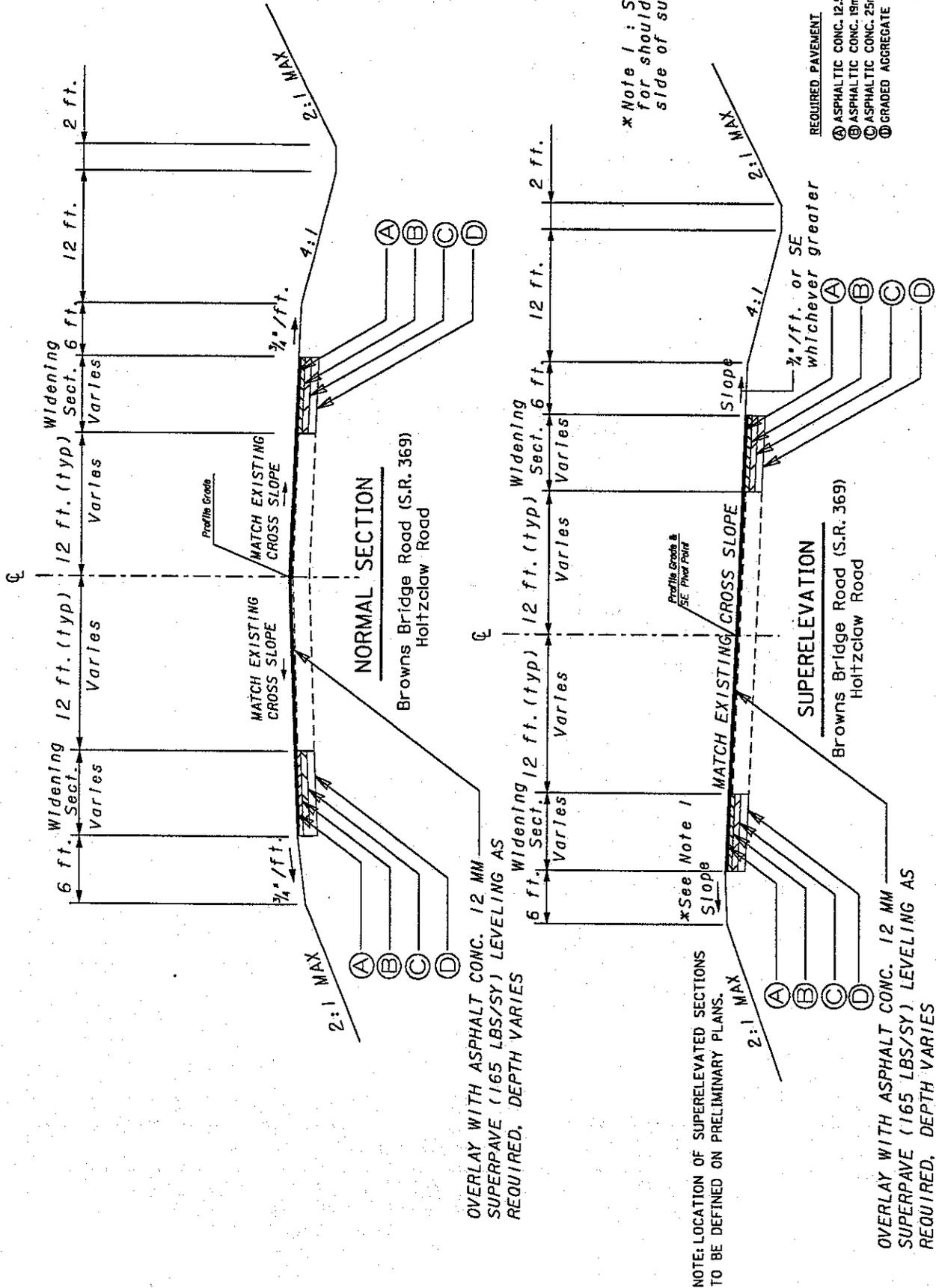
Time Saving Procedures Appropriate: Yes (X) No ()

Total Estimated Project Cost: \$137,263.20 Detailed cost estimate is shown on the next page.

Attachments: Traffic Flow Diagrams - 1999/2003 Peak Hour Traffic, Typical Section Diagrams, Concept Drawing.

ESTIMATED COST
PROJECT NAME: BROWN BRIDGE ROAD (S.R.369) AT HOLTZCLAW ROAD

ITEM NO.	DESCRIPTION	UNIT	QUANTITY	UNIT PRICE	AMOUNT	TOTALS
1	Right-Of-Way				\$0.00	
2	Utilities				LGPA	
	Total Non-Construction					\$0.00
1	Mobilization	LS	LS	\$10,000.00	\$10,000.00	
2	Traffic Control	LS	LS	\$15,000.00	\$15,000.00	
3	Sediment and Erosion Control	LS	LS	\$20,000.00	\$20,000.00	
4	Clearing	LS	LS	\$5,000.00	\$5,000.00	
5	Graded Agg. Base 10"	TN	469	\$15.00	\$7,035.00	
6	Asphalt Concrete 12.5 mm SMA 165# /S.Y. (1 1/2")	TN	669	\$50.00	\$33,450.00	
7	Asphalt Concrete 19 mm Superpave 220# /s.y. (2")	TN	87	\$50.00	\$4,350.00	
8	Asphalt Concrete 25 mm Superpave 440# /s.y. (4")	TN	174	\$50.00	\$8,700.00	
9	Bitum Tack Coat	GL	484	\$1.50	\$726.00	
10	Concrete Driveway	SY	15	\$25.00	\$375.00	
11	Signs & Pvm't Markings	LS	LS	\$4,500.00	\$4,500.00	
12	Grassing	AC	3.5	\$1,500.00	\$5,250.00	
13	Remove Existing Pavement	SY	738	\$3.00	\$2,214.00	
	Total Construction					\$114,386.00
	Engineering & Construction (10%)					\$11,438.60
	Inflation (5% for 2 years)					\$11,438.60
	TOTAL PROJECT COST					\$137,263.20



OVERLAY WITH ASPHALT CONC. 12 MM SUPERPAVE (165 LBS/SY) LEVELING AS REQUIRED. DEPTH VARIES

NOTE: LOCATION OF SUPERELEVATED SECTIONS TO BE DEFINED ON PRELIMINARY PLANS.

OVERLAY WITH ASPHALT CONC. 12 MM SUPERPAVE (165 LBS/SY) LEVELING AS REQUIRED. DEPTH VARIES

* Note 1 : See GA STD 9028C for shoulder slope on high side of superelevation.

REQUIRED PAVEMENT

- Ⓐ ASPHALTIC CONC. 12.5mm SUPERPAVE (165 lbs/sy)
- Ⓑ ASPHALTIC CONC. 19mm SUPERPAVE (220 lbs/sy)
- Ⓒ ASPHALTIC CONC. 25mm SUPERPAVE (440 lbs/sy)
- Ⓓ GRADED AGGREGATE BASE (10")

NOT TO SCALE



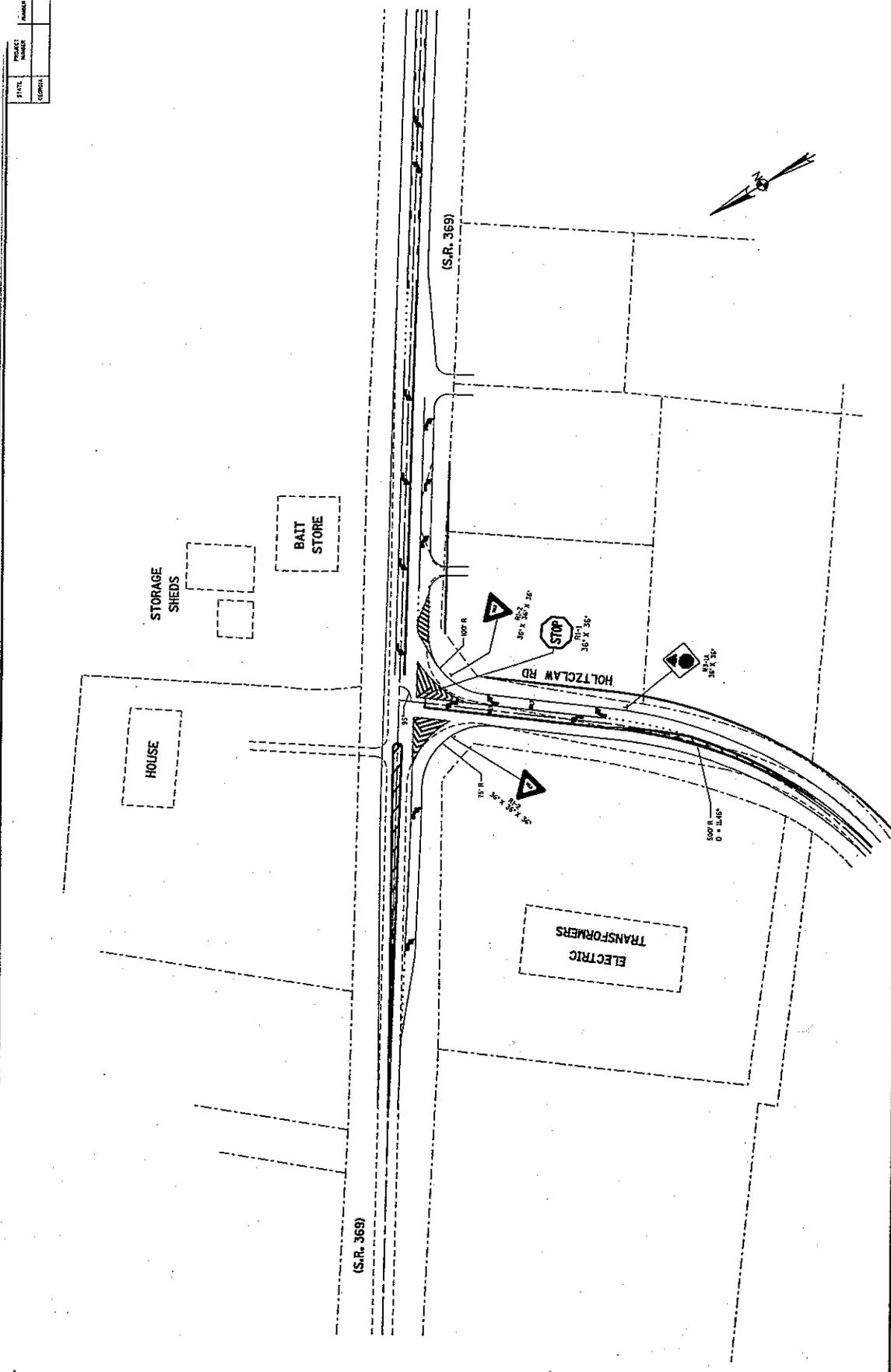
NORTH

TYPICAL SECTIONS

FORSYTH COUNTY INTERSECTION IMPROVEMENTS
INTERSECTION 2E - STP-0000-00 (811) P.I. 0000811

MA Moreland Altabelli Associates, Inc.

STATE	PROJECT NUMBER	DATE
CORNER		



DATE	BY	CHKD	DATE	BY	CHKD

MA Macleand Associates, Inc.
 2500 Macleand Blvd., Suite 100
 Jacksonville, FL 32217
 Telephone: 904-241-2500

FORSYTH COUNTY INTERSECTION CONCEPTS
 STP-0000-00181D P.L. 0000811
 BROWNS BRIDGE RD (S.R. 369) AT HOLTZCLAW RD

SHEET NUMBER
 2E

PROJECT CONCEPT REPORT

P.I. No.: 0000811

Project No.: STP-0000-00 (811) Forsyth County

Route No.: S.R. 369 (Browns Bridge Road)
C.R. 125 (Shady Grove Road)
C.R. 147 (Elrod Circle)

Location: The intersection of S.R. 369 @ Shady Grove Road is located approximately 1.7 miles east of GA 400 along S.R. 369 and 5.3 miles south of the Dawson County line on GA 400 in Forsyth County.

Description: The Shady Grove Road approach will be relocated to intersect S.R. 369 across from C.R. 147 (Elrod Circle), creating a four-way signalized intersection. S.R. 369 will be re-stripped to provide separate eastbound and westbound left-turn lanes. Elrod Circle will be widened to provide a separate left-turn lane. Shady Grove Road will be relocated and will include separate left-turn lane and one shared lane for thru or right turning vehicles..

<u>Traffic Volumes</u>	<u>1999 ADT</u>	<u>2003 ADT</u>
S.R. 369	20,200	23,600
Shady Grove Rd (C.R. 125)	3,000	3,300
Elrod Circle (C.R. 147)	200	300

<u>Existing Posted Speed Limit:</u>	S.R. 369	55 mph.
	C.R. 125	50 mph.
	C.R. 147	25 mph.

Existing Typical Section:

S.R. 369: Three 12 ft. undivided rural travel lanes; one eastbound, one westbound and one center turn lane; variable grass shoulders.

Shady Grove Road (C.R. 125): One 12 ft. rural travel lanes in each direction; variable grass shoulder.

Elrod Circle (C.R. 147): One 12 ft. rural travel lanes in each direction; variable grass shoulder.

<u>Existing Right-of-way:</u>	S.R. 369 -----	80 - 90 ft.
	C.R. 125 -----	65 ft.
	C.R. 147 -----	65 ft.

Existing Traffic Control: S.R. 369 has a continuous through movement. Shady Grove Road is controlled by a stop sign.

Existing Major Structures: None

Statement of Need and Purpose: Accident history at this intersection shows a doubling of the number of reported accidents per year from 1995 to 1997, with 18 alone reported in 1997. Of those accidents, 14 were equally split between "angle intersecting" and "rear end" type accidents, exceeding the minimum threshold of accidents to warrant the installation of a traffic signal. Analysis of existing traffic conditions at the intersection also satisfies warrants 2, 9, and 11 for the installation of a traffic signal.

The recommended improvements shown in the concept plan relocates Shady Grove Road to align with Elrod Circle, thus creating a standard four-way intersection with traffic signal control.

State Route Mile post: S.R. 369 MP 12.8

PDP Class: Minor Existing

Functional Class: S.R. 369 ----- Rural Minor Arterial
 C.R. 125 ----- Rural Major Collector
 C.R. 147 ----- Rural Local Road

<u>Max. Degree of Curve:</u>	<u>Existing Maximum Grade:</u>	<u>Angle of Intersection:</u>
S.R. 369: 4.75°	+/- 2%	90°
C,R, 125: 11.5°		
C.R. 147: 0°		

Design Speed: S.R. 369 ----- 55 mph
 C.R. 125 ----- 50 mph
 C.R. 147----- 25 mph

Proposed Typical Section:

S.R. 369: Three 12 ft. undivided rural travel lanes; one eastbound, one westbound and one two-way center left turn lane, and 6' grass shoulders with open ditch section.

C.R. 125: One 12-foot rural travel lane in each direction with left turn and right turn lanes at its intersection with S.R. 369 and 6' grass shoulders with open ditch section.

C.R. 147: One 12-foot rural travel lane in each direction with left turn and right turn lanes at its intersection with S.R. 369 and 6' grass shoulders with open ditch section.

Proposed Right-of-way Width: S.R. 369 80 ft.
 C.R. 125/ C.R. 147 80 ft.

Number of Parcels Impacted: 4 **Displacements:** RES: 1 BUS: 0 MH: 0

Design Exceptions Required:

CONTROLLING CRITERIA	UNDETERMINED	YES	NO
HORIZONTAL ALIGNMENT	()	()	(X)
ROADWAY WIDTH	()	()	(X)
SHOULDER WIDTH	()	()	(X)
VERTICAL GRADES	()	()	(X)
CROSS SLOPES	()	()	(X)
STOPPING SIGHT DISTANCE	()	()	(X)
SUPERELEVATION RATES	()	()	(X)
HORIZONTAL CLEARANCE	()	()	(X)
SPEED DESIGN	()	()	(X)
VERTICAL CLEARANCE	()	()	(X)

Proposed Major Structures:None.

Type Access: State and County Permit.

Traffic Control During Construction: Traffic will be maintained during construction.

Alternatives Considered:

1. No Build.
2. Proposed Project

Utilities: Forsyth County shall be responsible for all utility adjustments. Potential utilities impacted are listed below:

Telephone - BellSouth
Power - GA Power or Sawnee EMC
Water - Forsyth County
Cable - Prestige Cable

Permits Required: None required.

Level of Environmental Analysis: Categorical Exclusion.

Level of Public Involvement: None required.

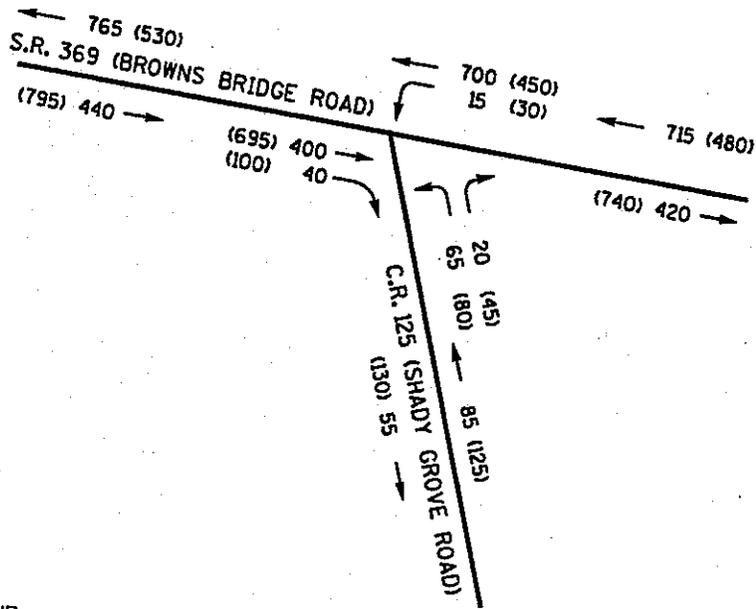
Time Saving Procedures Appropriate: Yes (X) No ()

Total Estimated Project Cost: \$439,849.40 Detailed cost estimate is shown on the next page.

Attachments: Traffic Flow Diagrams - 1999/2003 Peak Hour Traffic, Typical Section Diagrams, Concept Drawing.

ESTIMATED COST
PROJECT NAME: BROWN BRIDGE ROAD (S.R.369) AT SHADY GROVE ROAD

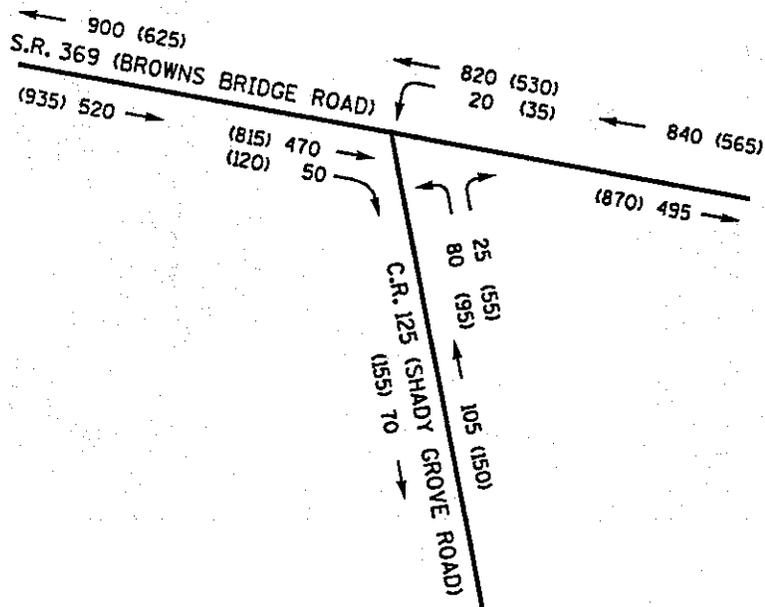
ITEM NO.	DESCRIPTION	UNIT	QUANTITY	UNIT PRICE	AMOUNT	TOTALS
1	Right-Of-Way				\$83,000.00	
2	Utilities				LGPA	
	Total Non-Construction					\$83,000.00
1	Mobilization	LS	LS	\$10,000.00	\$10,000.00	
2	Traffic Control	LS	LS	\$15,000.00	\$15,000.00	
3	Sediment and Erosion Control	LS	LS	\$20,000.00	\$20,000.00	
4	Clearing	LS	LS	\$50,000.00	\$50,000.00	
5	Graded Agg. Base 10"	TN	1,962	\$15.00	\$29,430.00	
6	Asphalt Concrete 12.5 mm SMA 165# /S.Y. (1 1/2")	TN	733	\$50.00	\$36,650.00	
7	Asphalt Concrete 19 mm Superpave 220# /s.y. (2")	TN	365	\$50.00	\$18,250.00	
8	Asphalt Concrete 25 mm Superpave 440# /s.y. (4")	TN	731	\$50.00	\$36,550.00	
9	Bitum Tack Coat	GL	777	\$1.50	\$1,165.50	
10	Signalization	LS	LS	\$60,000.00	\$60,000.00	
11	Signs & Pvm't Markings	LS	LS	\$10,000.00	\$10,000.00	
12	Grassing	AC	4.6	\$1,500.00	\$6,900.00	
13	Remove Existing Pavement	SY	1,143	\$3.00	\$3,429.00	
	Total Construction					\$297,374.50
	Engineering & Construction (10%)					\$29,737.45
	Inflation (5% for 2 years)					\$29,737.45
	TOTAL PROJECT COST					\$439,849.40



LEGEND

000 AM PEAK HOUR
 (000) PM PEAK HOUR

EXISTING 1999 PEAK HOUR TRAFFIC



LEGEND

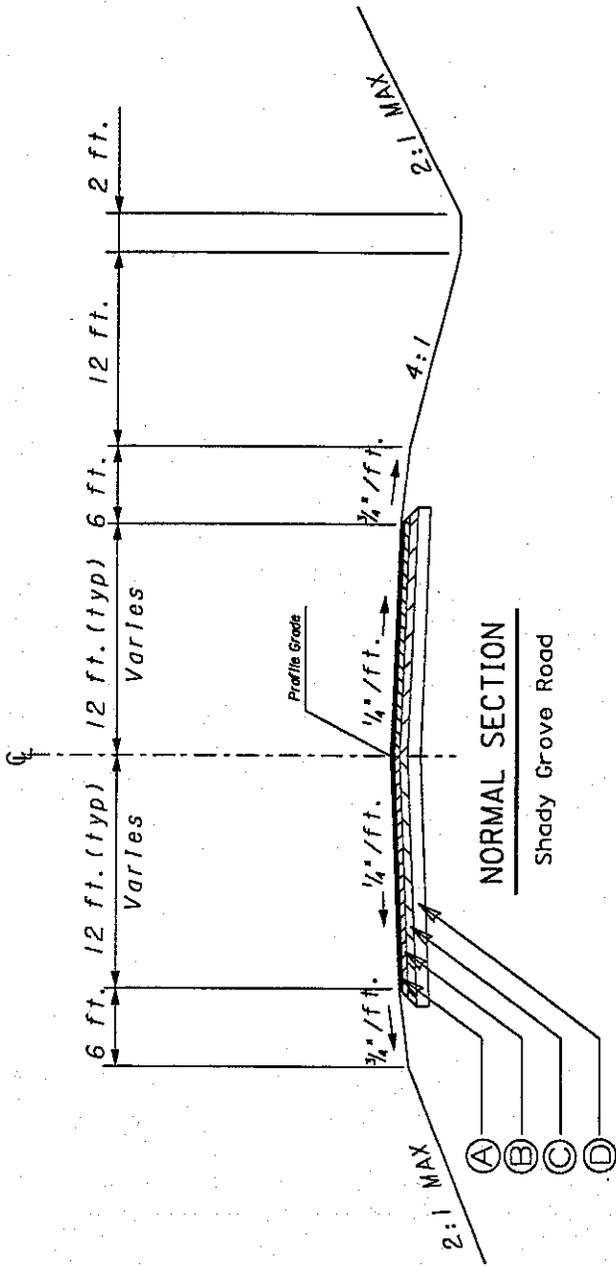
000 AM PEAK HOUR
 (000) PM PEAK HOUR

FUTURE 2003 PEAK HOUR TRAFFIC

MA Moreland Altobelli
 Associates, Inc.

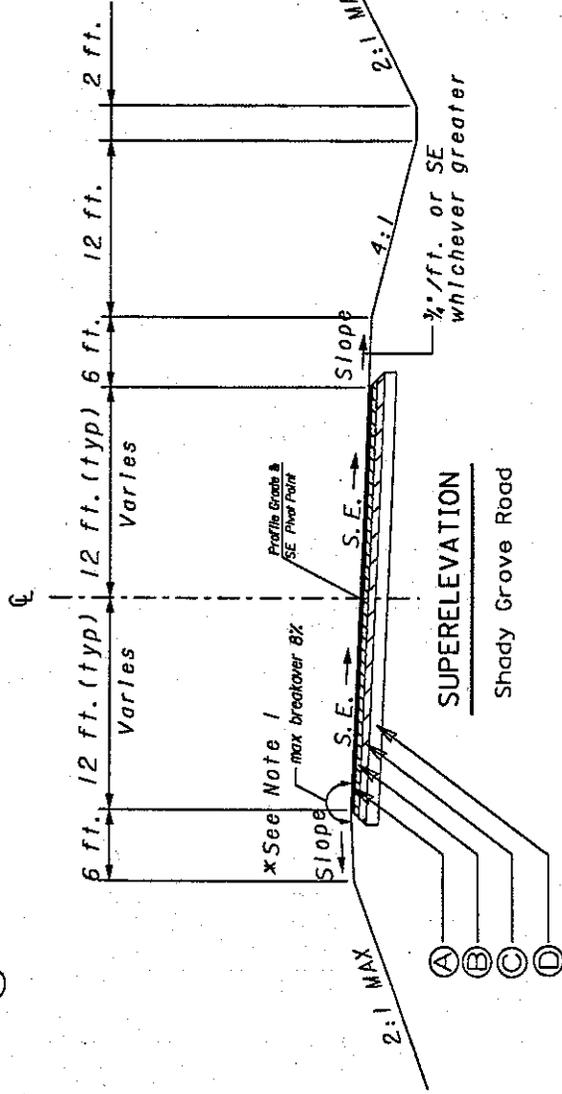


TRAFFIC FLOW DIAGRAMS
 S.R. 369 (BROWNS BRIDGE RD.) @
 C.R. 125 (SHADY GROVE RD.)
 STP-0000-00 (811) P.I. 0000811
 FORSYTH COUNTY



NORMAL SECTION

Shady Grove Road



SUPERELEVATION

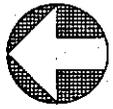
Shady Grove Road

* Note 1 : See GA STD 9028C for shoulder slope on high side of superelevation.

- REQUIRED PAVEMENT
- Ⓐ ASPHALTIC CONC. 12.5mm SUPERPAVE (165 lbs/SY)
 - Ⓑ ASPHALTIC CONC. 19mm SUPERPAVE (220 lbs/SY)
 - Ⓒ ASPHALTIC CONC. 25mm SUPERPAVE (440 lbs/SY)
 - Ⓓ GRADED AGGREGATE BASE (10')

NOTE: LOCATION OF SUPERELEVATED SECTIONS TO BE DEFINED ON PRELIMINARY PLANS.

NOT TO SCALE

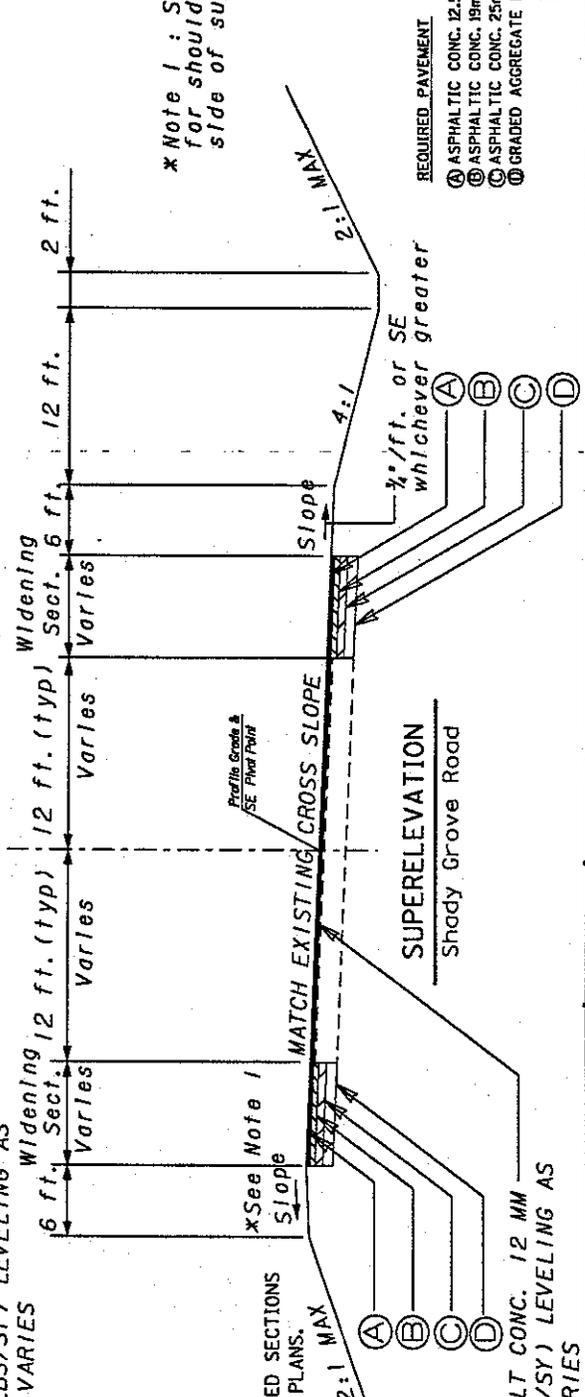
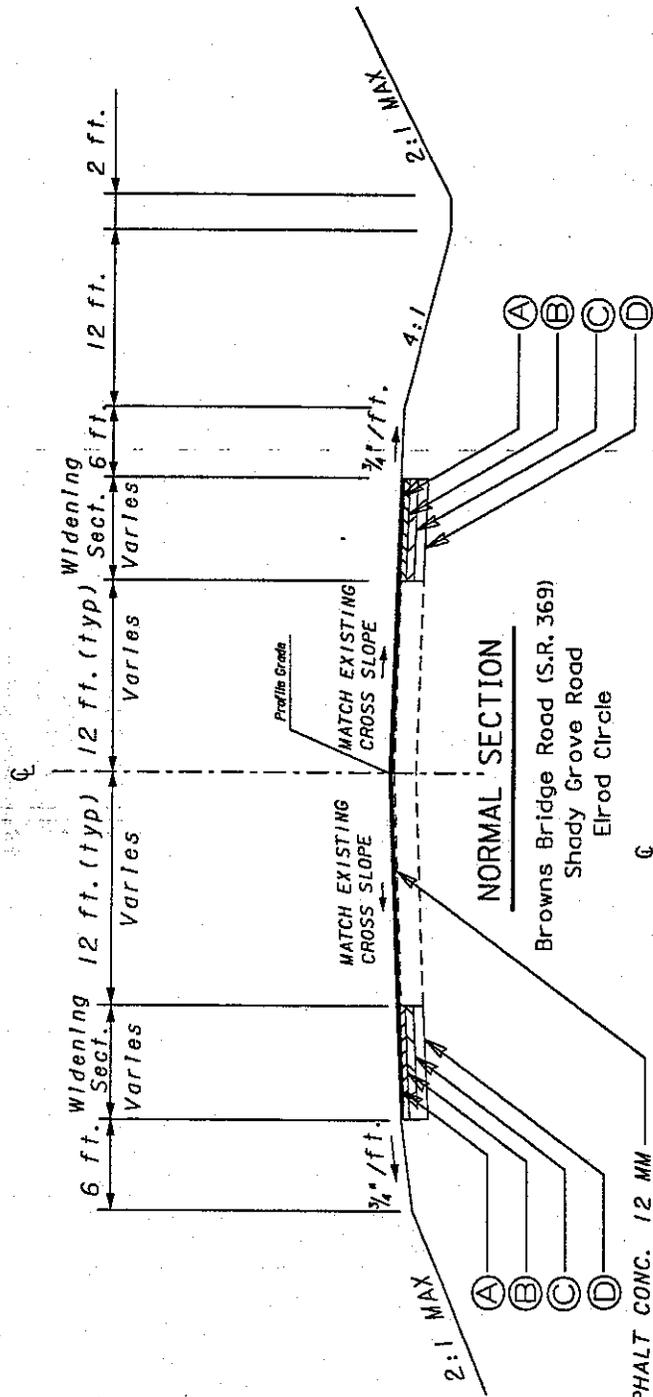


NORTH

TYPICAL SECTIONS

FORSYTH COUNTY INTERSECTION IMPROVEMENTS
 INTERSECTION 2F - STP-0000-00 (811) P.I. 0000811

MA Moreland Atobelli Associates, Inc.



* Note 1 : See GA STD 9028C for shoulder slope on high side of superelevation.

- REQUIRED PAVEMENT
- (A) ASPHALTIC CONC. 12.5mm SUPERPAVE (165 lbs/SY)
 - (B) ASPHALTIC CONC. 19mm SUPERPAVE (220 lbs/SY)
 - (C) ASPHALTIC CONC. 25mm SUPERPAVE (440 lbs/SY)
 - (D) GRADED AGGREGATE BASE (07)

NOT TO SCALE

OVERLAY WITH ASPHALT CONC. 12 MM SUPERPAVE (165 LBS/SY) LEVELING AS REQUIRED, DEPTH VARIES

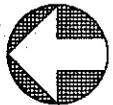
OVERLAY WITH ASPHALT CONC. 12 MM SUPERPAVE (165 LBS/SY) LEVELING AS REQUIRED, DEPTH VARIES

NOTE: LOCATION OF SUPERELEVATED SECTIONS TO BE DEFINED ON PRELIMINARY PLANS.

* See Note 1 Slope

TYPICAL SECTIONS

FORSYTH COUNTY INTERSECTION IMPROVEMENTS
INTERSECTION 2F - STP-0000-00 (811) P.I. 0000811



NORTH

MA Moreland Altabelli Associates, Inc.

**Minutes of Concept Team Meeting
August 25, 2000**

**Forsyth County Intersection Improvement Projects
Project Number: STP-0000-00-(810) P.I. No 00000810
Project Number: STP-0000-00-(811) P.I. No 00000811
Project Number: STP-0000-00-(812) P.I. No 00000812**

ATTENDEES	ORGANIZATION	PHONE NUMBER
Barry Lucas	Forsyth County Water	770-886-2793
Mike Wilson	Atlanta Gas Light Co.	404-584-4677
Andrew Aiello	GDOT Environment	404-699-4432
Dania Aponte	GDOT Environment	404-699-4438
Michael Seabolt	Utility Office	770-718-5033
Allen Ferguson	GDOT - Utilities	770-532-5510
Jimmy Vaughan	Moreland Altobelli Assoc., Inc.	770-886-9549
Karla Poshedly	Moreland Altobelli Assoc., Inc.	770-263-5945
Joe Garland	GDOT - Traffic Operations.	770-532-5563
Tim Allen	Forsyth County Engineer	770-781-2165
R. Keith Canup	GDOT - Traffic Operations	770-532-5563
Danny Seale	Alltel	706-279-7534
Ronnie Gallman	Alltel	770-479-3900
Jeannine Rispin	Georgia Transmission	770-270-7741
Don Frazier	City of Cumming	770-781-2010
Todd Long	GDOT Preconstruction Engineer	770-532-5520

Mr. Todd Long opened the meeting with introductions of everyone present. He then stated that he would give an overview description of each intersection project then Ms. Karla Poshedly will present each intersection improvement drawing so that everyone will have an opportunity to make their comments on each intersection.

Mr. Long stated that right-of-way acquisition will be set for the year 2002 with construction slated for the year 2003.

General Comments made:

- Show larger corner miters at all intersections including unsignalized intersections.
- There are no bike routes or bike lanes required at any of the intersections.
- Place overhead lane control signs at all intersections where two state routes cross.
- Place signal ahead signs approaching all existing and new traffic signal locations.
- Make all stop signs at unsignalized intersections 36" x 36".

Specify intersection comments:

- 1G. S.R. 20 @ Tribble Road /Post Road. Show double left turn lanes on westbound S.R. 20 to southbound Post Road (S.R. 371).
- 2B. S.R. 369 @ Waldrip Road/Waldrip Circle. Show connection of old Waldrip Circle to new Waldrip Circle.
- 2C. S.R. 369 @ Doc Bramblett Road. Show connection of old Doc Bramblett Road to new Doc Bramblett Road. Cul-de-sac the old section of Doc Bramblett Road south of the Church property.
- 2E. S.R. 369 @ Holtzclaw Road. Check traffic volumes during school. This intersection may need to be signalized.
- 2F. S.R. 369 @ Shady Grove Road. Show a dirt berm along S.R. 369 at the cul-de-sac of the old portion of Shady Gove Road.
- 3C S.R. 306 @ Waldrip Road. The percent grade of Waldrip Road is greater than what is shown in the concept report. MAAI will correct.

**DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA**

PROJECT CONCEPT REPORT

FORSYTH COUNTY

<u>LOCATION</u>	<u>PROJECT NO.</u>	<u>P.I. #</u>
2A. S.R. 369/Browns Bridge Rd at Jot'em Down Rd	STP-0000-00(811)	0000811
2B. S.R. 369/Browns Bridge Rd at Waldrip Road	STP-0000-00(811)	0000811
2C. S.R. 369/Matt Hwy. at Doc Bramblett Rd	STP-0000-00(811)	0000811
2D. S.R. 369/Matt Hwy. at Hendrix Rd.	STP-0000-00(811)	0000811
2E. S.R. 369/Browns Bridge Rd at Holtzclaw Rd	STP-0000-00(811)	0000811
2F. S.R. 369/Browns Bridge Rd at Shady Grove Rd	STP-0000-00(811)	0000811

Date of Report: Oct. 13, 2000

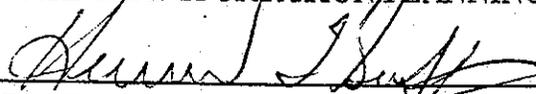
RECOMMENDATION FOR APPROVAL

This project is contained in the Transportation Improvement Program (TIP) and/or in the State Transportation Improvement Plan (STIP). The concept as presented herein and submitted for approval is consistent with that which is included in the TIP and/or the STIP.

DATE

10-18-00

STATE TRANSPORTATION PLANNING ADMINISTRATOR



DATE

STATE TRANSPORTATION PROGRAMMING ENGINEER

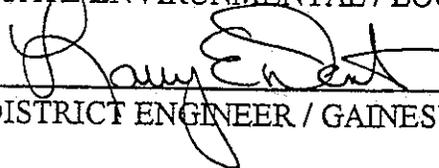
DATE

STATE ROAD DESIGN ENGINEER

DATE

10-13-00

STATE ENVIRONMENTAL / LOCATION ENGINEER



DATE

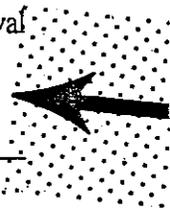
DISTRICT ENGINEER / GAINESVILLE

DATE

PROJECT REVIEW ENGINEER

DATE

STATE TRAFFIC OPERATIONS ENGINEER



**DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA**

PROJECT CONCEPT REPORT

FORSYTH COUNTY

<u>LOCATION</u>	<u>PROJECT NO.</u>	<u>P.I. #</u>
2A. S.R. 369/Browns Bridge Rd at Jot'em Down Rd	STP-0000-00(811)	0000811
2B. S.R. 369/Browns Bridge Rd at Waldrip Road	STP-0000-00(811)	0000811
2C. S.R. 369/Matt Hwy. at Doc Bramblett Rd	STP-0000-00(811)	0000811
2D. S.R. 369/Matt Hwy. at Hendrix Rd.	STP-0000-00(811)	0000811
2E. S.R. 369/Browns Bridge Rd at Holtzclaw Rd	STP-0000-00(811)	0000811
2F. S.R. 369/Browns Bridge Rd at Shady Grove Rd	STP-0000-00(811)	0000811

Date of Report: Oct. 13, 2000

RECOMMENDATION FOR APPROVAL

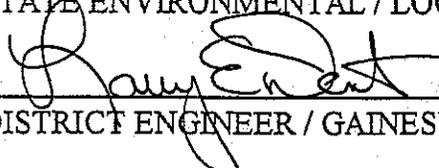
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DATE STATE TRANSPORTATION PLANNING ADMINISTRATOR

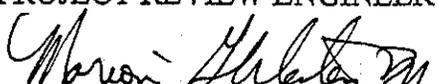
DATE STATE TRANSPORTATION PROGRAMMING ENGINEER

DATE STATE ROAD DESIGN ENGINEER

DATE STATE ENVIRONMENTAL / LOCATION ENGINEER

10-13-00
DATE  DISTRICT ENGINEER / GAINESVILLE

DATE PROJECT REVIEW ENGINEER

10/24/2000
DATE  STATE TRAFFIC OPERATIONS ENGINEER

NOTICE OF LOCATION AND DESIGN APPROVAL

GDOT Project STP-0000-00 (811); P.I. No. 0000811 FORSYTH COUNTY

Notice is hereby given *in compliance with Georgia code 22-2-109* that the Georgia Department of Transportation has approved the Location and Design of the above project.

Intersection 2A - S.R. 369/Brown Bridge Road at Jot-em Down Road/Bethel Road/C.R. 171.

This project consist of intersection re-alignment and configuration improvements, including the addition of turn lanes consistent with existing peak hour traffic volumes. This project is located within Land Lot 1035, District 14; approximately 5.53 miles east along Browns Bridge Road from its intersection with GA 400, northeast of the city of Cumming in Forsyth County.

Intersection 2B - S.R. 369/Brown Bridge Road at Waldrip Road

This project consist of intersection re-alignment and configuration improvements, including the addition of turn lanes consistent with existing peak hour traffic volumes. This project is located within Land Lots 1183 & 1182, District 14; approximately 6.65 miles east along Browns Bridge Road from its intersection with GA 400, northeast of the city of Cumming in Forsyth County.

Intersection 2C - S.R. 369/Matt Highway at Doc Bramblett Road.

This project consist of widening S.R. 369 to provide a westbound left-turn lane. The Doc Bramblett Road approach will be relocated to intersect S.R. 369 157 feet east of its existing location, and modified to provide a channelized right-turn lane. The intersection of S.R. 369 @ Doc Bramblett Road is located approximately 5.5 miles north of the city limits of Cumming and 2.5 miles directly south of the Dawson county line. This intersection improvement is within Land Lot 382, District 3 in Forsyth County.

Intersection 2D - S.R. 369/Matt Highway at Hendrix Road.

This project consist of widening S.R. 369 to provide a separate eastbound right-turn lane and westbound left-turn lane. Modification of the Hendrix Road approach to provide a channeled right-turn lane. The intersection of S.R. 369 @ Hendrix Road is located in Forsyth County approximately 2.6 miles west of Georgia 400 along S.R. 369, 3.5 miles directly south of the Dawson county line. This intersection improvement is within Land Lots 475, 534 and 535, District 3 in Forsyth County.

Intersection 2E- S.R. 369/Brown Bridge Road at Holtzclaw Road.

This project consist of resurfacing and restriping S.R. 369 to provide a center turn lane that becomes a designated left-turn lane at its intersection with Holtzclaw Road. The Holtzclaw Road approach to S.R. 369 will be widened to provide a separate left-turn lane and a channeled right-turn lane. The intersection of S.R. 369 @ Holtzclaw Road is located in Forsyth County approximately 1.2 miles east of Georgia 400 along S.R. 369, 3.3 miles northeast of the city limits of Cumming. This intersection improvement is within Land Lots 346, 347, 396 and 397, District 14 in Forsyth County.

Intersection 2F- S.R. 369/Brown Bridge Road at Shady Grove Road/Elrod Circle.

This project consist of relocating Shady Grove Road to intersect S.R. 369 across from C.R. 147 (Elrod Circle), creating a four-way signalized intersection. S.R. 369 will be re-stripped to provide separate eastbound and westbound left-turn lanes. Elrod Circle will be widened to provide a separate left-turn lane. Shady Grove Road will be relocated and will include separate left-turn lane and one shared lane for thru or right turning vehicles. The project is located approximately 1.7 miles east of GA 400 along S.R. 369 and 5.3 miles south of the Dawson County line. This intersection improvement is within Land Lot 442, District 14 in Forsyth County.

Date of Location Approval: _____

Drawings of maps, and/or plats of the proposed project *as approved* are on file and are available for inspection at the Georgia Department of Transportation, Gainesville District Office, P.O. Box 1057, Gainesville, Georgia 30503-1057. Or, any interested party may obtain a copy of the drawings of maps and/or plats by writing to the Georgia Department of Transportation, No. 2 Capital Square, Atlanta, Georgia 30334 and paying a nominal cost therefor.

Any written request in reference to this Notice *SHOULD include the PROJECT NUMBERS AS NOTED AT THE TOP OF THIS NOTICE AND may be referred to:*

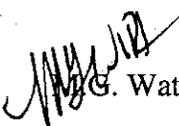
Todd Long, District Preconstruction Engineer
Department of Transportation
Gainesville District Office
P.O. Box 1057
Gainesville, Georgia 30503-1057
(770) 532-5520

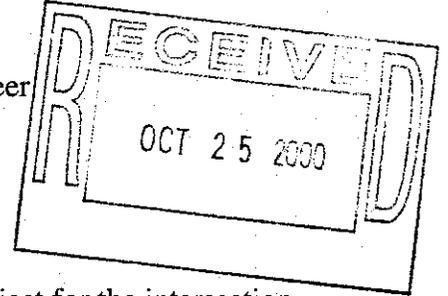
Department of Transportation
State of Georgia

INTERDEPARTMENTAL CORRESPONDENCE

File: STP-0000-00(811)/Forsyth County
P.I. No. 0000811

Office: Traffic Operations
Atlanta, Georgia
Date: October 18, 2000

From:  G. Waters, III, P.E., State Traffic Operations Engineer
To: Wayne Hutto, Assistant Director of Preconstruction



Subject: Project Concept Report Review

We have reviewed the concept report on the above project for the intersection improvements along SR 369(Browns Bridge Road) at several intersections.

SR 369 is a rural two lane roadway with posted speed limits varying between 45 and 55mph.

Improvements include, but are not limited to, adding turn lanes and possibly traffic signals. The intersections affected by this project are Jot'em Down Road, Waldrip Road, Doc Bramblett Road, Hendrix Road, Holtzclaw Road and Shady Grove Road.

We believe this concept will improve safety and traffic operations along this section of roadway.

We therefore find this report satisfactory for approval.

MGW:TWS

Attachment (signature page)

c: Harvey Keepler
Larry Dent, District Engineer, Gainesville
Attention: Todd I. Long, P.E.
David Mulling, w/ attachment
Marta Rosen
Chuck Hasty, TMC
General Files

**DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA**

PROJECT CONCEPT REPORT

FORSYTH COUNTY

<u>LOCATION</u>	<u>PROJECT NO.</u>	<u>P.I. #</u>
2A. S.R. 369/Browns Bridge Rd at Jot'em Down Rd	STP-0000-00(811)	0000811
2B. S.R. 369/Browns Bridge Rd at Waldrip Road	STP-0000-00(811)	0000811
2C. S.R. 369/Matt Hwy. at Doc Bramblett Rd	STP-0000-00(811)	0000811
2D. S.R. 369/Matt Hwy. at Hendrix Rd.	STP-0000-00(811)	0000811
2E. S.R. 369/Browns Bridge Rd at Holtzclaw Rd	STP-0000-00(811)	0000811
2F. S.R. 369/Browns Bridge Rd at Shady Grove Rd	STP-0000-00(811)	0000811

Date of Report: Oct. 13, 2000

RECOMMENDATION FOR APPROVAL

This project is contained in the Transportation Improvement Program (TIP) and/or in the State Transportation Improvement Plan (STIP). The concept as presented herein and submitted for approval is consistent with that which is included in the TIP and/or the STIP.

11/1/00
DATE *Marta Ruen*
STATE TRANSPORTATION PLANNING ADMINISTRATOR

DATE STATE TRANSPORTATION PROGRAMMING ENGINEER

DATE STATE ROAD DESIGN ENGINEER

DATE STATE ENVIRONMENTAL / LOCATION ENGINEER

10-13-00
DATE *Raymond*
DISTRICT ENGINEER / GAINESVILLE

10/25/00
DATE *C. J. Mullin*
PROJECT REVIEW ENGINEER

DATE STATE TRAFFIC OPERATIONS ENGINEER